

Appendix A
Public and Stakeholder
Consultation Material

Appendix A.1

Public Consultation

Notice of Study Commencement

Environmental Assessment Study

NOTICE OF STUDY COMMENCEMENT FOR EXPANDED STUDY AREA

Background: In June 2009, we began a Schedule C Municipal Class Environmental Assessment (EA) for improvements to Mississauga Road from Olde Base Line Road to Bush Street and Bush Street from Mississauga Road to Winston Churchill Boulevard. We are now expanding the limits of our EA to include Olde Base Line Road from Mississauga Road to Winston Churchill Boulevard, and Winston Churchill Boulevard from Olde Base Line Road to Bush Street (see map).

Why did we expand the study area?

Based on the feedback received for the Mississauga Road / Bush Street EA, we expanded the study area to review road safety, sight lines, drainage, parking and pedestrian and cycling needs.

This EA will not consider increasing the number of lanes. We will build on the previous study to provide a solution for the study area that meets the needs of all road users and maintains the character of the community.

The Process

The aim of the Class EA process is to provide everyone who has an interest or stake in the study area with the opportunity to create the best solution for the corridor. The project team, with your input, will:

- define the problem statement;
- develop and evaluate planning alternatives; and,
- determine a preferred solution.

We will provide opportunities to be involved through planned public meetings at key milestones as the study progresses. A Community Working Group is also being established for participation in two workshop meetings. Information on participating as a representative will be available on the Region's website after July 25, 2012. When the study is finished, a Notice of Study Completion and an Environmental Study Report (ESR) that presents the study findings will be published. The results of the study will be available for review for 30 calendar days.

Comments

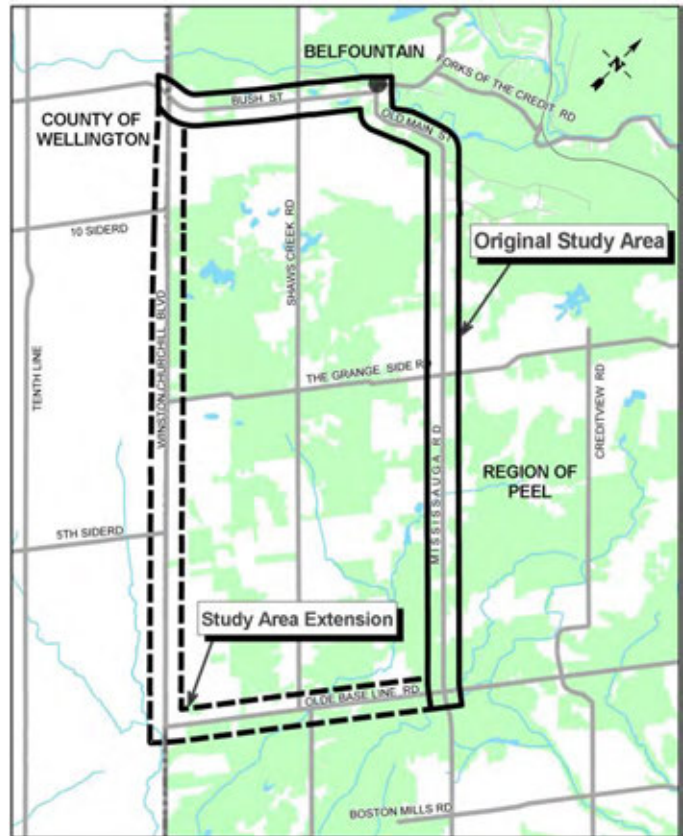
Your participation is important and your comments are valued. We invite you to provide input at future public meetings or by providing comment as the study develops. You can also review the study progress on our website at:

peelregion.ca/pw/roads/environ-assess

If you would like to comment on the study expansion or be notified about future public meetings, please contact either of the following team members.

Mr. Hitesh Topiwala, RPP, PMP
Project Manager, Transportation Program Planning
Public Works
Region of Peel
10 Peel Centre Dr., Suite B, 4th Floor
Brampton, ON L6T 4B9
Phone: 905-791-7800 ext 7866
Fax: 905-791-1442
E-mail: hitesh.topiwala@peelregion.ca

Mr. Stephen Keen, P.Eng.
Consultant Project Manager
HDR Corporation.
144 Front Street West, Suite 655
Toronto, ON M5H 2L7
Phone: 416-847-0005 ext 5557
Fax: 905-857-3127
E-mail: stephen.keen@hdrinc.com



Public Open House
October 30, 2012

Public Open House Tuesday, October 30, 2012

For Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road Municipal Class Environmental Assessment Study.

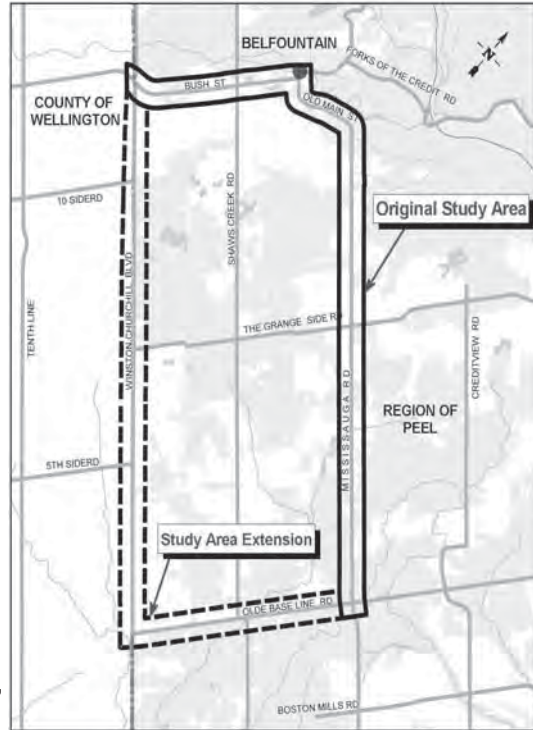
Open House begins at 6:30 p.m.
Presentation and Discussion:
7:00 p.m. - 9:00 p.m.

**at Belfountain Public School,
17247 Shaws Creek Road, Caledon**

The Open House will provide an overview of original study and boundaries, expanded study limits, relevant policies, technical studies to be undertaken and study schedule.

Please join us at this Public Open House to share your opinions on:

- Transportation issues and needs in the corridors;
- Future vision for the design of the corridors; and,
- Input to criteria for identifying preferred solutions.



If you require any accessibility needs to participate in this Public Open House, please contact Hitesh Topiwala, Region of Peel on 905-791-7800 ext. 7805

<http://www.peelregion.ca/pw/roads/environ-assess/mississauga-road-bush.htm>

For more information, please contact:

Hitesh Topiwala
Project Manager, Region of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, ON L6T 4B9
hitesh.topiwala@peelregion.ca
Tel: 905-791-7800 ext. 7805

Stephen Keen
Project Manager, HDR Consultants
231 Shearson Crescent, Suite 206
Cambridge, ON N1T 1J5
Stephen.Keen@hdrinc.com
Tel: 519-621-7886 ext. 5951

WELCOME

PUBLIC OPEN HOUSE

Mississauga Road, Olde Base Line Road, Winston Churchill Boulevard, Bush Street, and Old Main Street

Tuesday, October 30, 2012

Belfountain Public School



Meeting Purpose

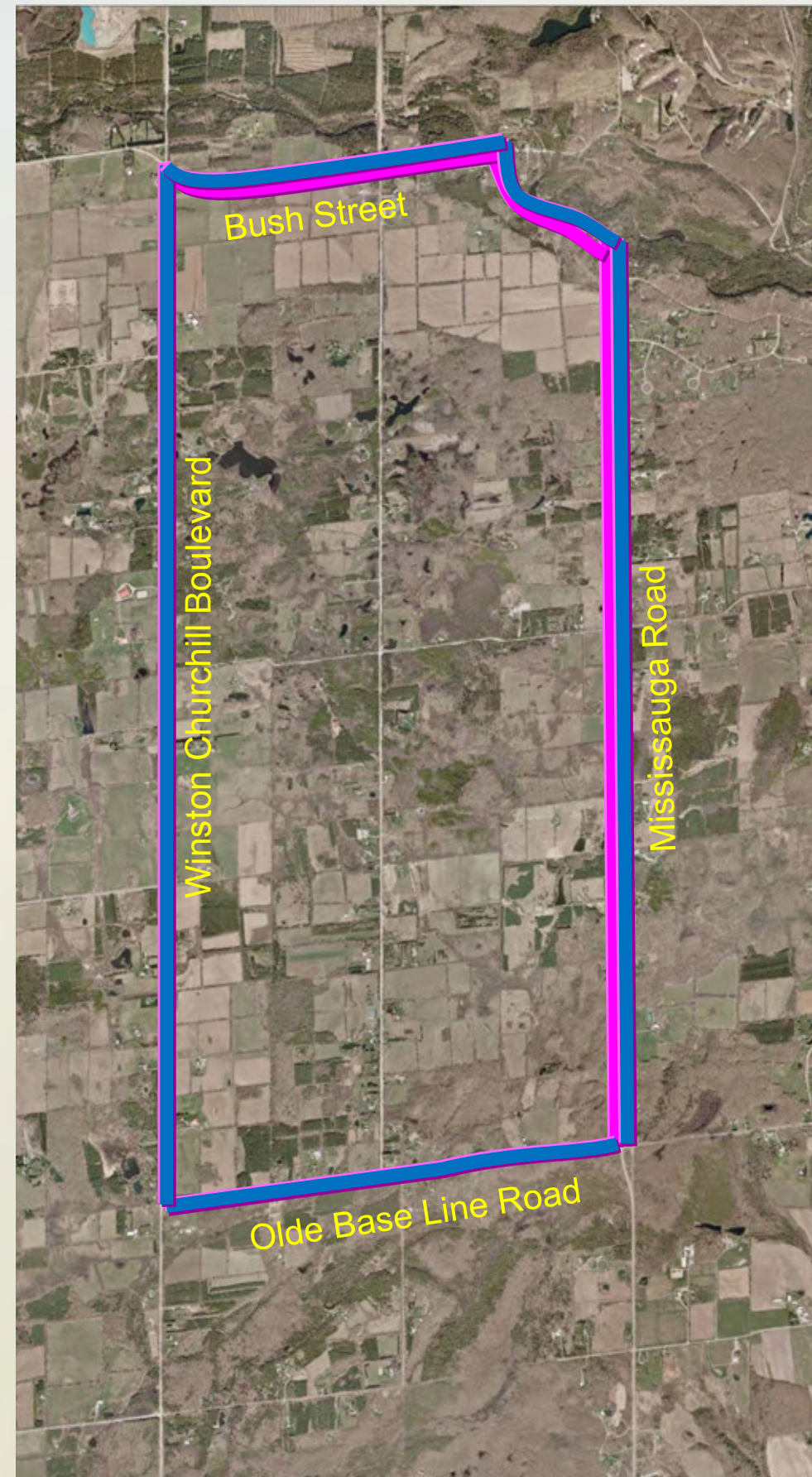
We are holding this open house to have a conversation with the residents about:

- transportation issues and needs,
- transportation vision for the corridors,
- context sensitive design, and
- Community Working Group participation.



History

- Study undertaken 1992-1998
- Study put on hold due to Rockfort Quarry application
- 2009 – new study commenced for Mississauga Road and Bush Street
- Schedule “C” Environmental Assessment
- 2010/2011 Ontario Municipal Board denied Quarry application
- Study area expanded to include Winston Churchill Boulevard and Old Base Line Road for a comprehensive transportation network review



Observations from the original EA study

Geotechnical Report

Deteriorated pavement

- Longitudinal and edge cracking
- Heaving and fatigued asphalt

Rehabilitation can be done:

- Full depth reconstruction (asphalt and base)
- Full depth asphalt replacement



Drainage and Stormwater Management

- Inadequate ditches and shoulders
- Overgrowth of vegetation
- Undersized culverts

...continued

Natural Environment

Vegetation Community Resources and Botanical Inventory

- 225 Flora species
- 7 butternut (endangered) trees
- 623 Trees (31 species)



Wildlife Resources

- 77 bird species
- 6 frog species
- Presence of Western Chorus Frog
- Jefferson Salamander (endangered)
- Large mammals and amphibian crossings

...continued

Archaeological Assessment

- Existing ROW - no archeological potential
- Stage 2 work required if proposed improvements go beyond existing ROW



Cultural and Built Heritage Report

- 48 Cultural and Built Heritage Resources identified
- Improvements need to avoid impacts

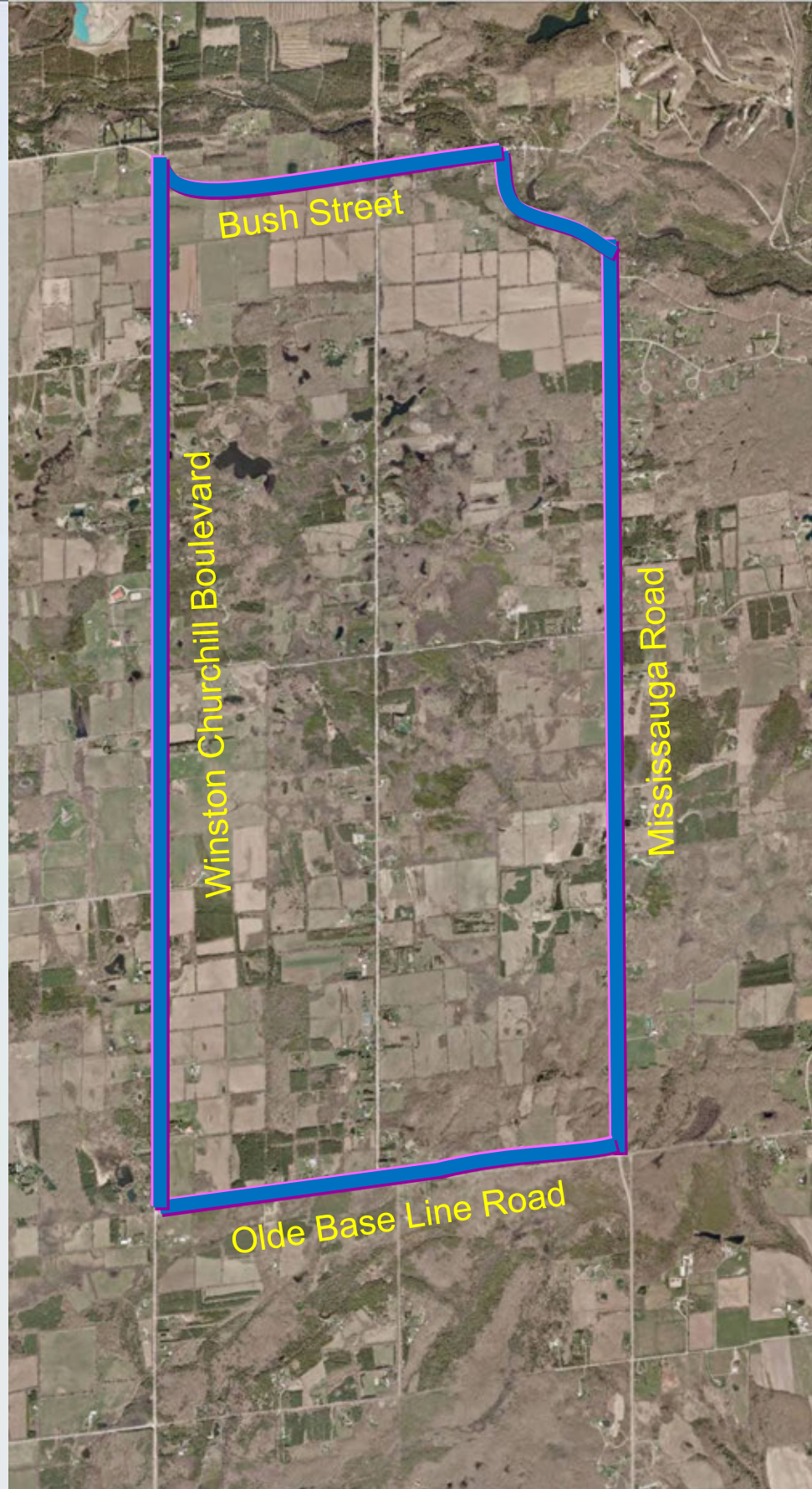
What We Heard

- Public Information Centre Held April 2010
- Focus Group Meeting Held June 2010



- If the roads were improved, it would attract more traffic through the village
- Study area should be expanded to include the broader network

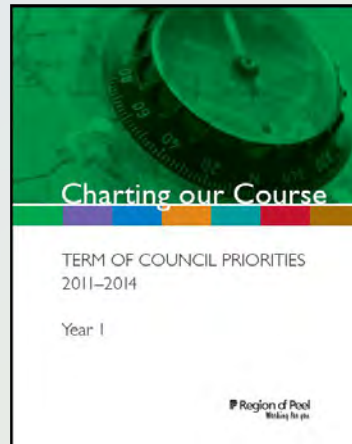
Where We Are Today



- Study area has been expanded to include Winston Churchill Boulevard and Olde Base Line Road
- Region retained HDR to undertake the study as a Schedule C Class Environmental Assessment
- Notice of Study Limits Expansion was mailed out and published in newspapers in July 2012
- Project Bulletin was circulated in July 2012
- Request for Community Working Group Membership Letters of Interest mailed out in July 2012 (additional outreach sent out in September 2012)

Vision

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community.



Goals

- Community and Stakeholder engagement
- Opportunities for Active Transportation
- Increase safety for all users
- Improve asset condition
- Improve natural environmental condition



What improvements are being considered?

This study will be considering improvements to address:

- Pavement and road base condition
- Drainage
- Safety (including sight lines)
- Active transportation (pedestrians and bikes)
- Intersection control



This project is not considering:

- Widening (no need for additional travel lanes)

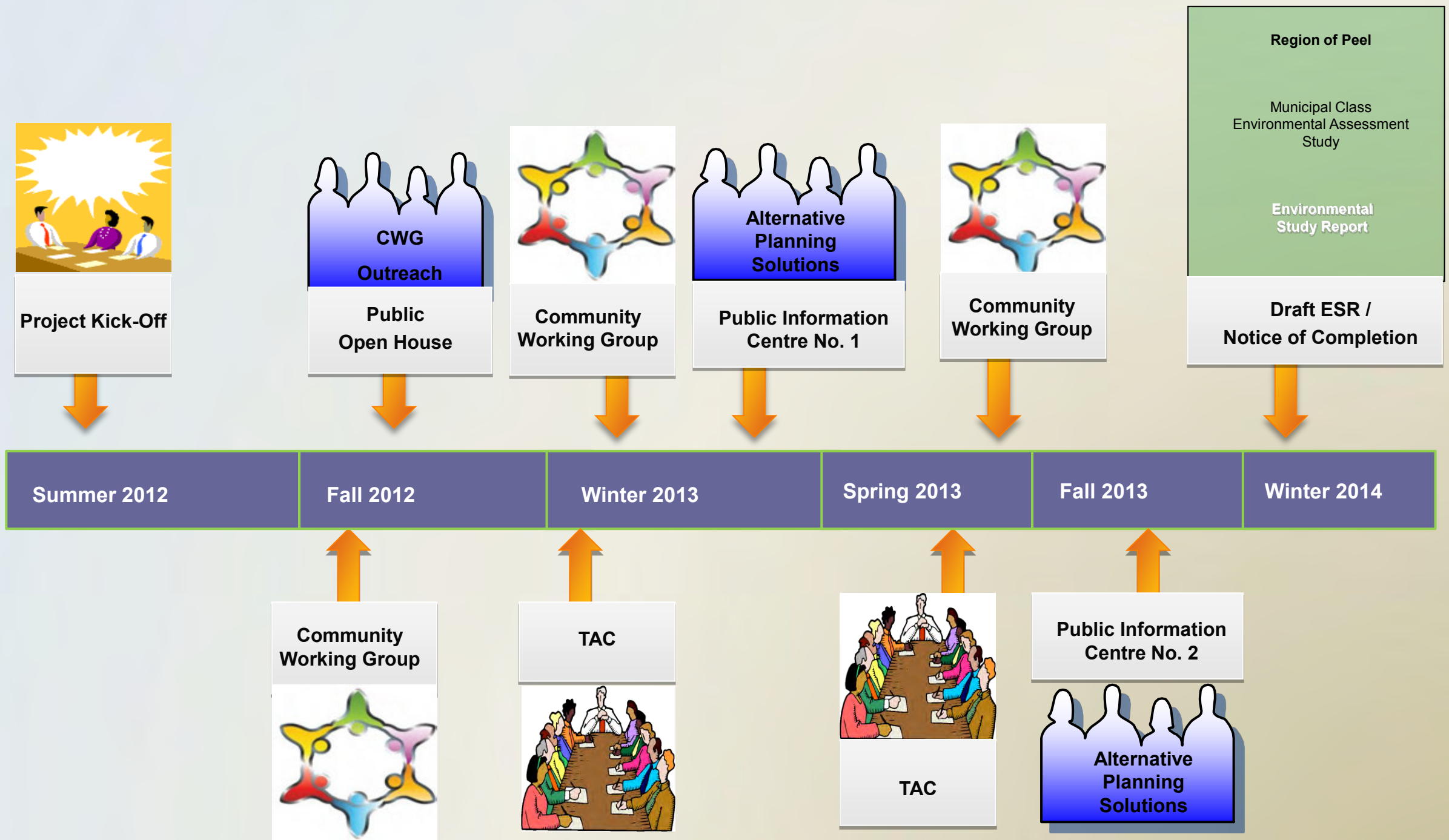
Context Sensitive Design

Context Sensitive Design (CSD) is the art of creating public works projects that meet the needs of the users, the neighboring communities and the environment.



1. Community members will play an important role in identifying problems and solutions that may better meet and balance the needs of all stakeholders.
2. The project will consider the wishes of the community on key design issues (e.g. rural versus urban cross-section, roundabouts versus stop-controlled intersections, paved sidewalk versus gravel shoulder, etc.).

Project Plan and Schedule

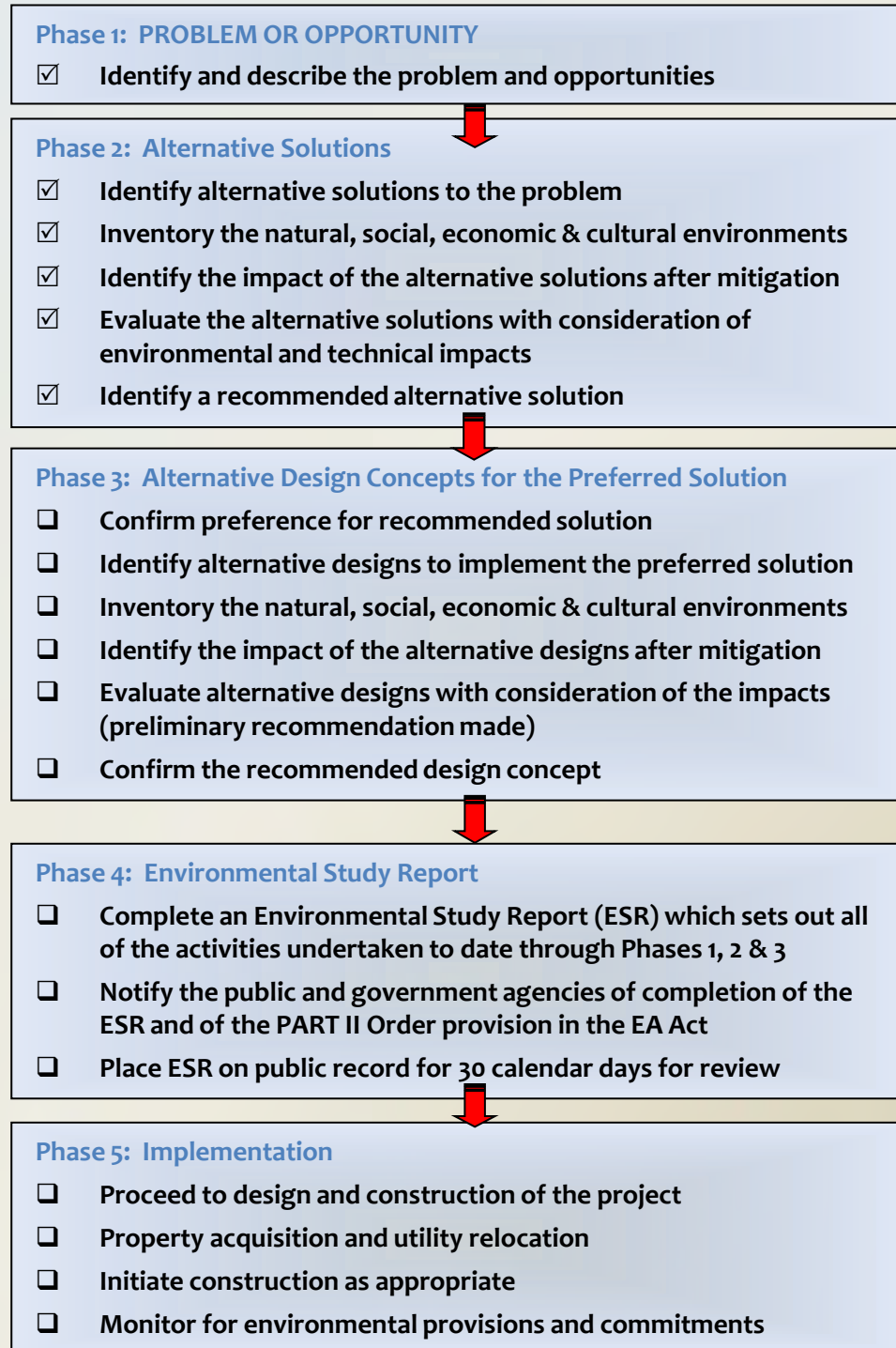


Beyond the Municipal Class EA Process

Additional Points of Public Contact



Phases

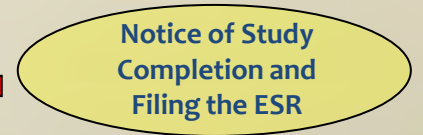


Mandated Points of Public Contact



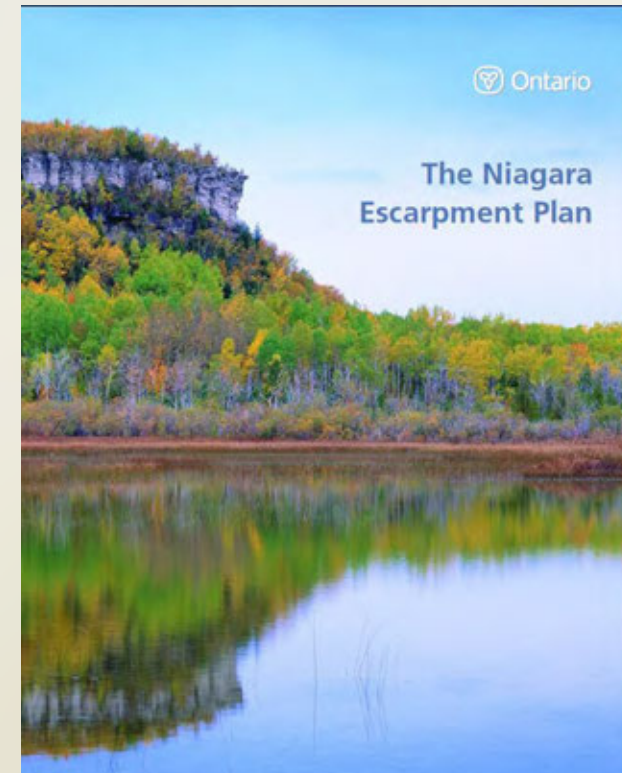
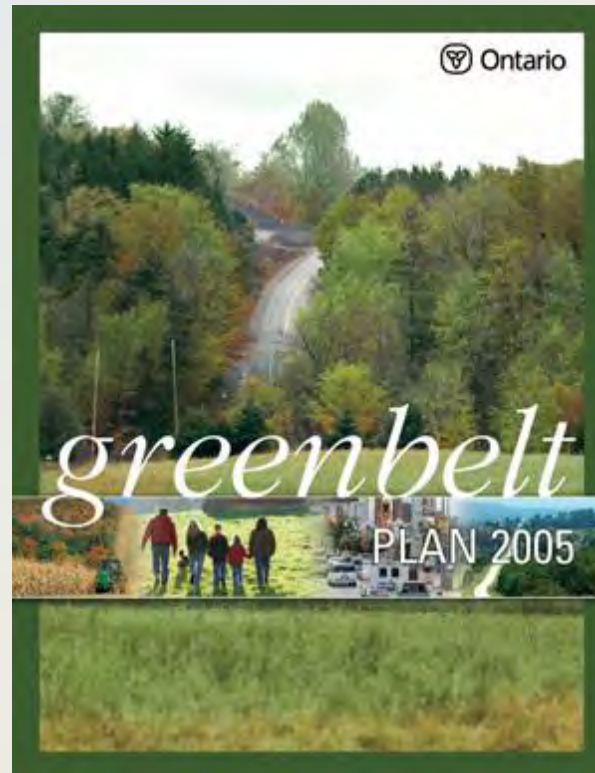
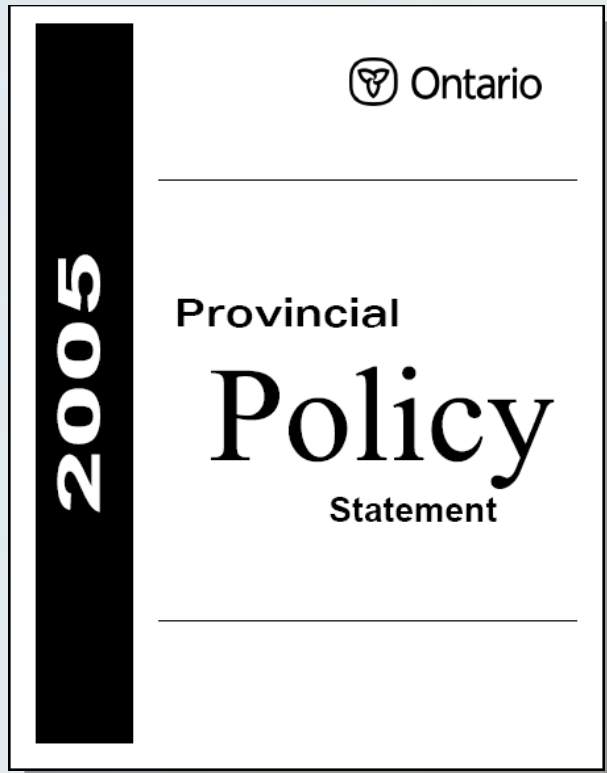
- PIC#1 Winter 2013**
- Needs and Justification
 - Planning Alternative Solutions
 - Evaluation of Planning Alternative Solutions
 - Preliminary Recommended Solution

- PIC#2 (Fall 2013)**
- Alternative designs for the preferred solution
 - Evaluation of alternative design concepts
 - Preliminary recommended design concept



In addition to the mandated points of public contact, the Region has chosen to organize a Community Working Group (CWG) for the study. The CWG members will play an important role in identifying problems and solutions that may better meet and balance the needs of all stakeholders.

Guiding Policies



■ Official Plan

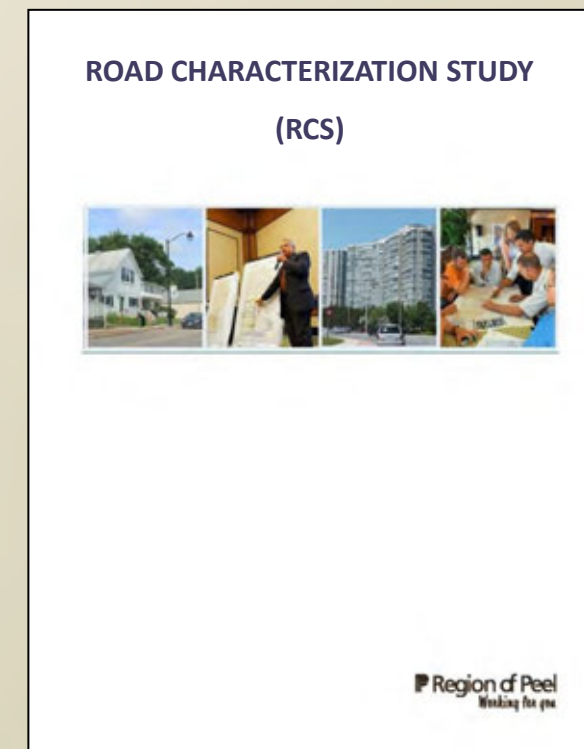
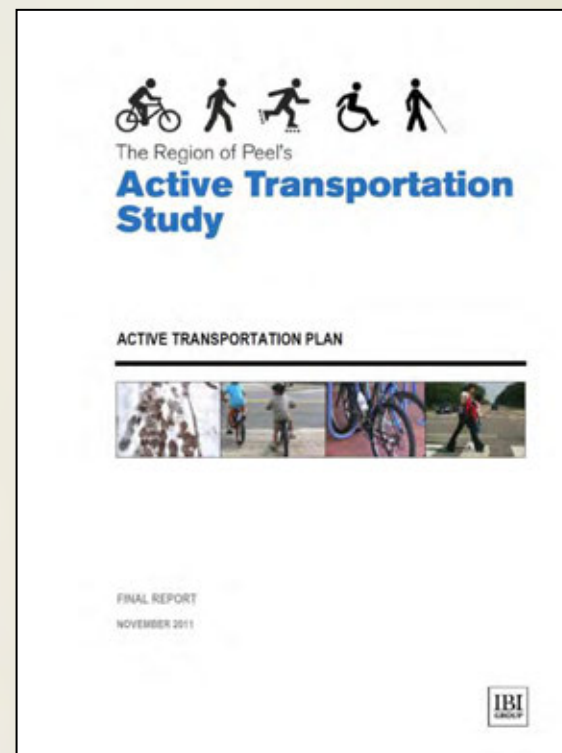
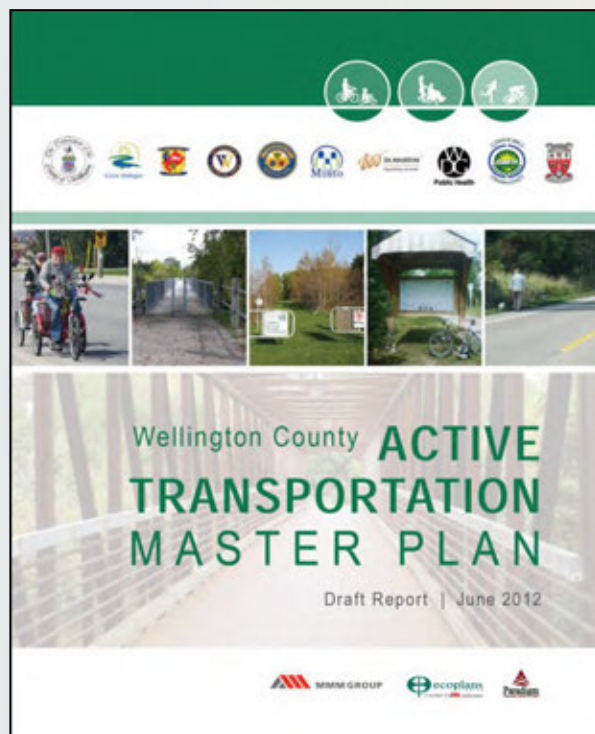
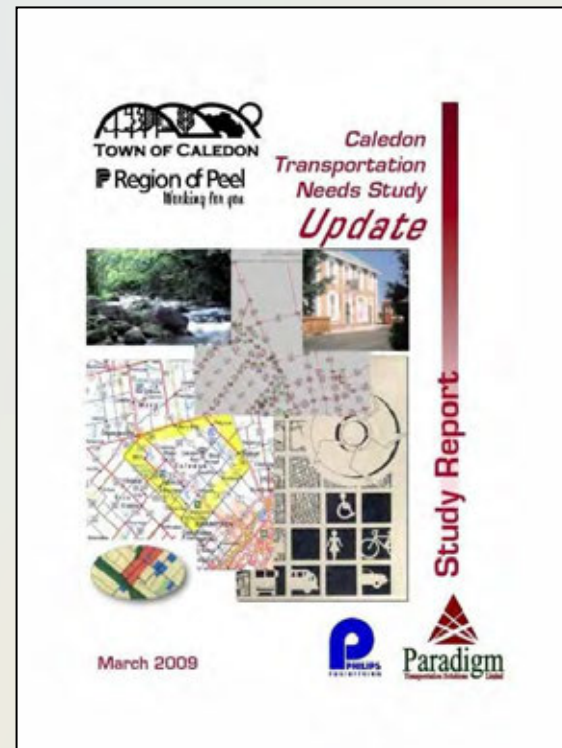
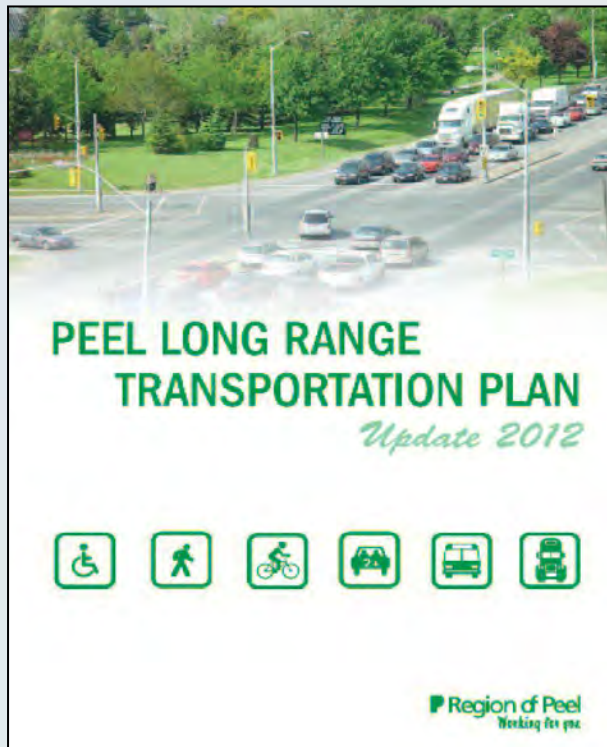


■ Official Plan



■ Official Plan

Transportation Studies and Initiatives



Technical Studies to be Undertaken

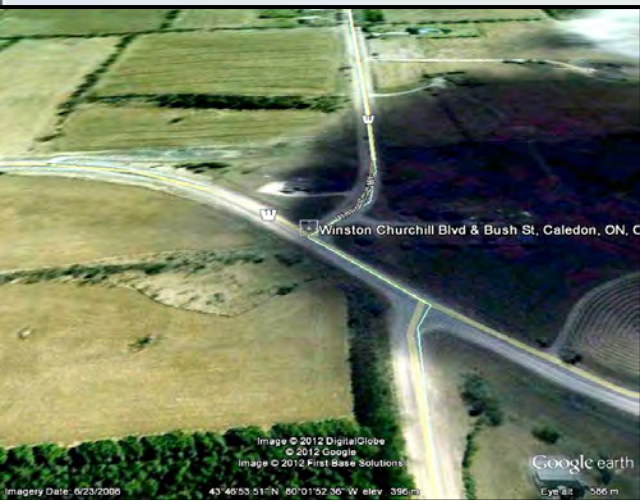
Traffic and Transportation Report

Traffic forecast:

- 2031 Horizon volumes

Operational Analysis

- Seasonal Traffic volumes
- Roundabout analysis



Safety:

- Pedestrians and bikes
- access locations
- sight-lines, geometrics
- collision data analysis

... continued

Geotechnical and Pavement Design

- Borehole locations and depth
- Potential use of Recycled aggregate

Utility Impacts

- Future Plans for upgrades
- Utility Relocation



Drainage and Stormwater Management Report

- Structure condition
- Hydrologic and Hydraulic analysis
- Floodplain mapping
- Stormwater Quality treatment

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Natural Environment and Wildlife Habitat

- Inventories
- Species at Risk



Archaeological and Cultural – Built Heritage Assessment

- Stage 1 work for the extended area

Public Consultation

A very important component for the success of the project will be the Community Working Group.

The Community Working Group (CWG) is:

- A forum for focused discussion
- Broad group of interested stakeholders representative of the community
- A non-voting liaison group that makes recommendations to the project team for their consideration.

• If you are interested in participating in the CWG, please submit a letter or an email summarizing:

- who you represent, and
- information about yourself.

• Please submit your one-page letter of interest to either of the following:

Hitesh Topiwala
Peel Region Project Manager
Transportation Division | Public Works
10 Peel Centre Dr, Suite B, 4th Floor
Brampton, ON L6T 4B9
Hitesh.Topiwala@peelregion.ca Ph. (519) 621-7886 ext. 5951
Ph. (905) 791-7800 ext. 7805

Stephen Keen
HDR Project Manager
231 Shearson Cres, Suite 206
Cambridge, ON N1T 1J5
Stephen.Keen@hdrinc.com

• Please note that CWG membership will be limited to 15 representatives.



Next Steps

Transportation and Traffic studies

Technical Studies

Define Problem/ Opportunity

Develop/Evaluate Alternative Solutions

Technical Advisory Committee meeting

Community Working Group meeting

PIC #1



Belfountain EA Study
Public Open House, October 30, 2012

Mississauga Road, Olde Base Line Road, Winston
Churchill Boulevard, Bush Street, and Old Main Street

Today's Agenda

6:30 PM

Arrival and Displays

7:00 PM

Presentation

7:45 to 8:45 PM

Discussion and Input

8:45 to 9:00 PM

Key Messages and Next Steps

PUBLIC OPEN HOUSE - PUBLIC WORKBOOK



Your Name

Address

Email

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle *this question artificially limits responses*
- Car pooling
- Cycling
- Motorcycle
- Truck

walk

What transportation issues are you experiencing in the study area?

none



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

none or a reduction in vehicular traffic



What existing design features would you like to see maintained?

all of them narrow hilly winding roads with steep ditches
no changes at all to sides no sidewalks no street lamps
no



What design features do you think would enhance the community character?

It's hard to think of what changes could be made that would not degrade the unique & scenic nature of this heritage hamlet.

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

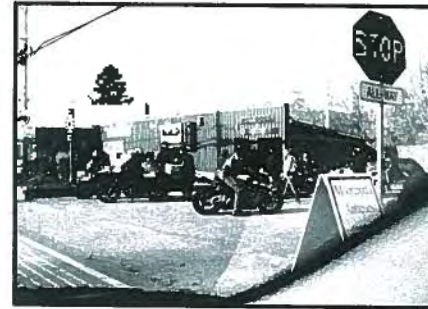
take a look around you - then leave it alone

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

look both ways



For accommodating parking?

not an issue

For improving conditions for cyclists?

not an issue cycle on a safe
roadway & don't hog the road

For accommodating motorcyclists?

fewer & less noisy

Please share other comments about this study here

~~strategy~~ a needed attempt to pursue an agenda of change where none is required through an "inclusive" ~~etc~~ search for a non-existent problem.

Why do I feel that we are being conned like some easement right of way is very telling?



Your input is
important.
Thank you!

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- Single occupant vehicle
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- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

I would like to be able to walk around more



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



What existing design features would you like to see maintained?

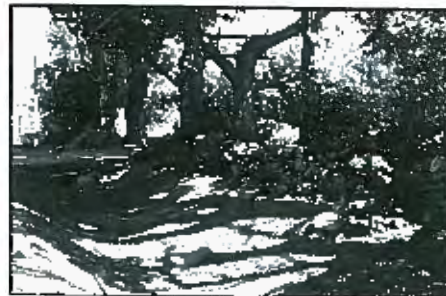
Rural like character of a beautiful country road.
1. Character and environmental + historical heritage
in the corner community garden in Balfountain



What design features do you think would enhance the community character?

a lookout on the bend/corner on the south end of
Balfountain on Mississauga road.

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

Wild life underpasses for Salamanders + Turtles .

Nature trees .

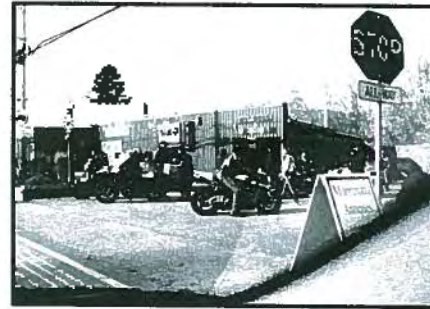
Split Rail Fences

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

Make more paths &
walking trails
Could use Orb/Thomas Lands



For accommodating parking?

Parking at south end of
Balfourland on Orb/Thomas lands

For improving conditions for cyclists?

Shoulders

For accommodating motorcyclists?

Please share other comments about this study here

- What are you planning for all of the tourists?
 - Use dark sky lighting
 - Spray for invasive species phragmites in newly constructed road ditches.
 - Clean construction equipment to stop spread of invasive species phragmites. (See invasive species at CVC)
 - Put signage in Belfountain for the environment
- * How are you going to collaborate with the new housing development on the south end of Belfountain? They are they putting in a road between Mississauga Road + Shaws Creek?



Your input is
important.
Thank you!

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- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

Speeding



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

None – these are country roads & quite passable for local residents. Improving the roads will lead to more people wanting to move into the area. Sprawl is unacceptable & constant growth unsustainable.



What existing design features would you like to see maintained?

leave the wonderful hills on Miss Rd.
No street lights



What design features do you think would enhance the community character?

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?

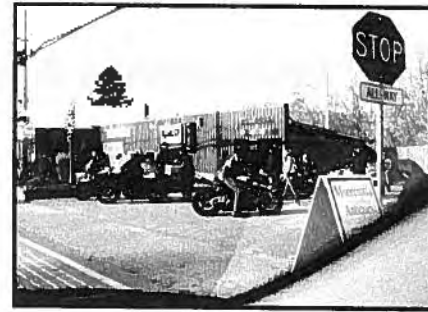


For improving conditions for pedestrians?

No Sidewalks
~~It's~~ It's not safe to walk on
country roads - people should
walk in parks or within
villages

For improving conditions for cyclists?

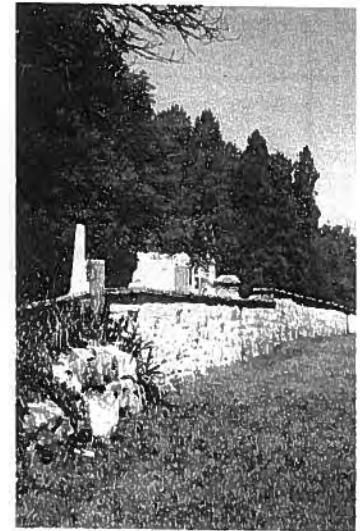
No bike lanes
Before accommodating cyclists,
we should ensure that they obey
the rules of the road. Many do
not pay any attention to stop signs



For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here



Your input is
important.
Thank you!

 **Region of Peel**
Working for you

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- Truck

What transportation issues are you experiencing in the study area?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

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What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

◦ need to slow traffic coming north in Mississauga around the bend into Belfountain – i.e. speed bumps

◦ path space between Caledon Main Rd and town for pedestrian traffic (not a typical sidewalk but perhaps some more natural stone, etc. to preserve rural country setting.)
(kids could bike to school; walk to school, etc.)
school to Conservation Area

What existing design features would you like to see maintained?

- roadside fences (split rail, rock, etc.)
- large old trees that line roadway.
- hills and curves.

What design features do you think would enhance the community character?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



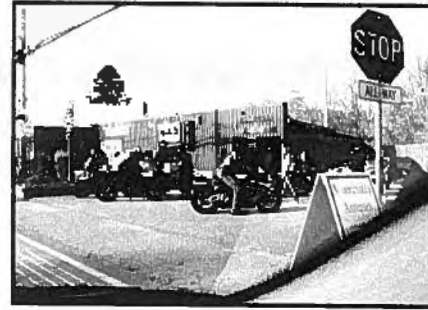
For improving conditions for pedestrians?

see prior page

• separation of vehicles from peds.

• speed bump onto Bellfontain

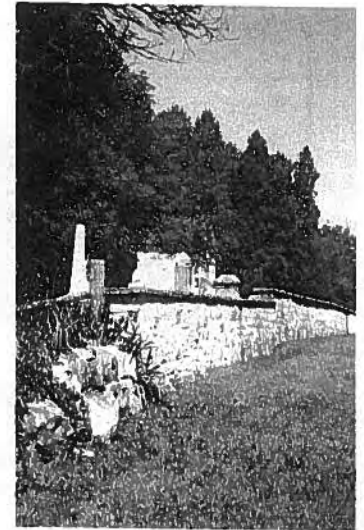
For improving conditions for cyclists?



For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here



**Your input is
important.
Thank you!**

Region of Peel
Working for you

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- Truck

What transportation issues are you experiencing in the study area?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

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What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



What existing design features would you like to see maintained?



What design features do you think would enhance the community character?

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



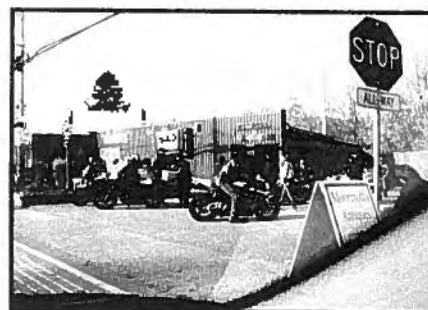
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PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

For improving conditions for cyclists?

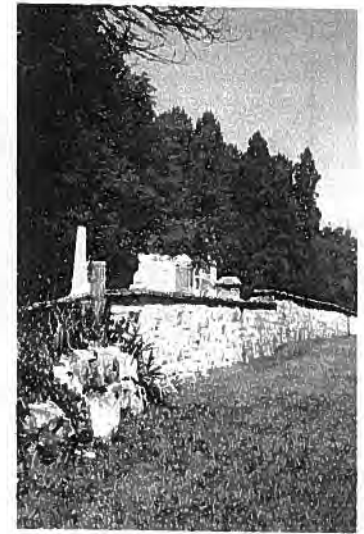


For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here

Why do meetings have such a delayed planning? If it was a business, things shouldn't take so long. I can understand why it is important to cover all issues but years seem ridiculous if needs are not met in good time.
Bicycle lanes seem to be an important safety issue also drains & ditches.



Your input is
important.
Thank you!

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- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

More trucks on Winston Churchill. which theoretically are not allowed - Speeding on WC Blvd.



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

Increased police patrol.



What existing design features would you like to see maintained?

Hills,



What design features do you think would enhance the community character?

The roads are just fine!

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

Just as they are now, rolling, curving, ~~and~~ rural!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

#



For accommodating parking?

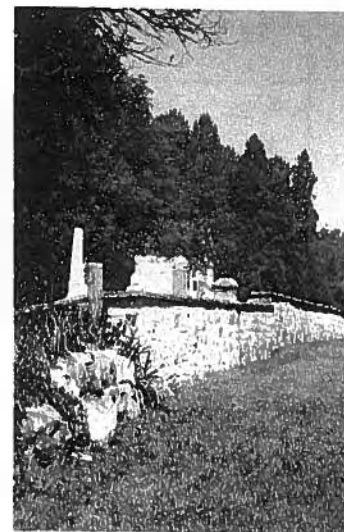
-

For improving conditions for cyclists?

- creating a bike path
but not widening the road

For accommodating motorcyclists?

Please share other comments about this study here



Your input is
important.
Thank you!

Region of Peel
Working for you

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- Truck

What transportation issues are you experiencing in the study area?

NONE



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

NOTHING WRONG WITH THE ROADS



What existing design features would you like to see maintained?

PARKETTE



What design features do you think would enhance the community character?

LEAVE IT ALONE - KEEP IT QUAIN

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

THEY ONLY WALK IN THE
IMMEDIATE VILLAGE - PUT SIDEWALKS
AROUND THE STORES



For accommodating parking?

ALLOW ON BOTH SIDES OF
THE ROAD =
DIRECT THEM TO PARK IN THE SCHOOL

For improving conditions for cyclists?

ALREADY HAVE BIKE PATHS
ALONG THE TRAIN TRACKS

AND WE HAVE A BIKE LANE THAT
WAS BUILT ABOUT 6 YEARS AGO.

For accommodating motorcyclists?

SAME AS CARS

Please share other comments about this study here

YOU ARE DANG THIS FOR OUTSIDERS
AND WE DON'T NEED THE HIGHER
TAXES THIS WILL BRING.

LEAVE US BE.

DO NOT CHANGE THE TOPOGRAPHY



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

all routes used

Check the modes of transportation that you

use:

- Family* Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

SPEED AND NOISE OF MANY VEHICLES (ie. mostly MOTORCYCLES BUT MANY CARS AND TRUCKS TOO).



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43° 46' 28" N 79° 52' 52" W plot: 1277 (1)

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

ADD BIKE LANES



What existing design features would you like to see maintained?



What design features do you think would enhance the community character?

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

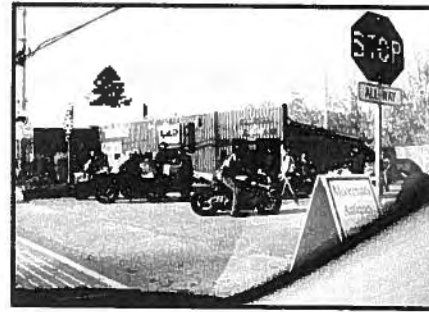
PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

For improving conditions for cyclists?

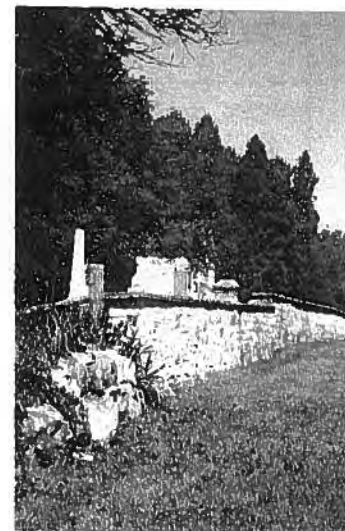
ADD BIKE LANES



For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here



Your input is
important.
Thank you!

P Region of Peel
Working for you

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

- speed control
- no trucks on all roads - enforce it.



What existing design features would you like to see maintained?

heritage fences
old trees



What design features do you think would enhance the community character?

I don't think any needed features are req'd to enhance the community.

The area has changed drastical over the years. and I would like it stay the same.

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



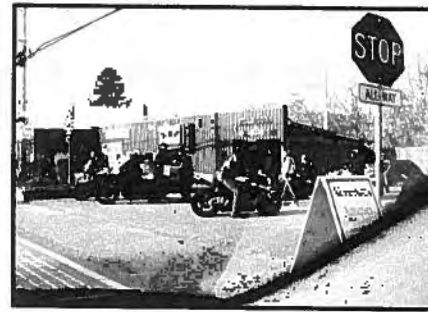
There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

-no cement sidewalk
but more like trail
walkway for students to
go from school to park



For accommodating parking?

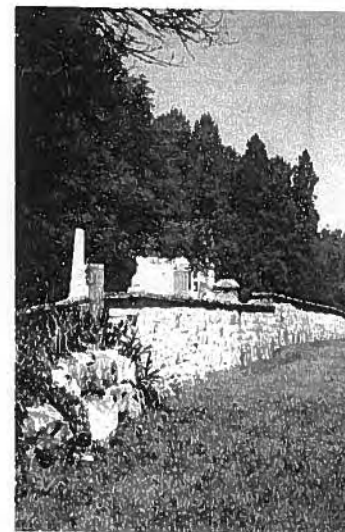
Weekend - invite tourists
to park at the school
and walk into the
village.

For improving conditions for cyclists?

signage stating
ride behind the other rider
as they usually ride in two's or
three beside each other.

For accommodating motorcyclists?

Please share other comments about this study here



Your input is
important.
Thank you!

 **Region of Peel**
Working for you

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



POLICE MONITORING SPEED LIMITS

VOLUME OF TRAFFIC DURING RUSH HOURS REQUIRES PATIENCE TO TURN ONTO MISSISSAUGA ROAD - FOR THIS REASON I WOULD BE CONCERNED ABOUT THE PLAN FOR 72 HOUSE SUBDIVISIONS WHICH WOULD ADD CONSIDERABLY TO NORMAL VOLUME INCREASE YEAR TO YEAR OF THROUGH TRAFFIC

What existing design features would you like to see maintained?

WHEN PURCHASING AND BUILDING A HOME ON MISSISSAUGA RD I WAS HAPPY TO COMPLY WITH ALL REQUESTS FROM CVC AND ESCARPMENT COMMISSION TO MAINTAIN AND SUPPORT THE HEALTH OF THE ENVIRONMENT i.e. FORESTS, WETLANDS & STREAMS BEFORE BUILDING

What design features do you think would enhance the community character?



SAVING THE ORIGINAL FENCE LINES STONE AND CEDAR RAIL THAT EXIST AND REPLACING THE ROW OF BEAUTIFUL MAPLES ALONG THE ROAD THAT I HAD TO REMOVE AS THEY HAD DETERIORATED OVER MORE THAN A HUNDRED YEARS - I AM CONCERNED THAT ENHANCING

 Region of Peel
Working for you

THE ROAD WOULD TAKE PRECEDENCE OVER THE ENVIRONMENTAL CONCERNS THAT OUR VERY IMPORTANT TO EVERYONE!

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



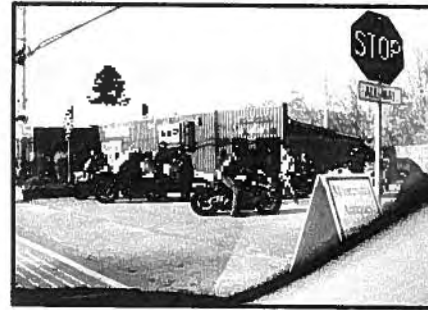
There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

For improving conditions for cyclists?



For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here

(This area contains faint, illegible text, likely representing a form or survey content.)



Your input is important.
Thank you!

Region of Peel
Working for you

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

WHOLE AREA

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



BIKE "PASSING LANES" (i.e. EXTENDED SHOULDER) ON BLIND HILLS. SO CYCLISTS CAN MOVE OVER AND AHEAD VEHICLES TO PASS. (THERE IS AN ENFORCEMENT COMPONENT TO THIS ISSUE AS WELL TO "ENCOURAGE" CYCLISTS TO TRAVEL SINGLE FILE AND TO THE RIGHT

What existing design features would you like to see maintained?

RURAL CHARACTER - MATURE TREES, DITCH PROFILE, NATURAL VEGETATION, CEDAR RAIL AND HERITAGE FENCES



What design features do you think would enhance the community character?

COULD YOU CONTINUE FUNDING HERITAGE STONE FENCES AS PART OF PROJECT 2

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



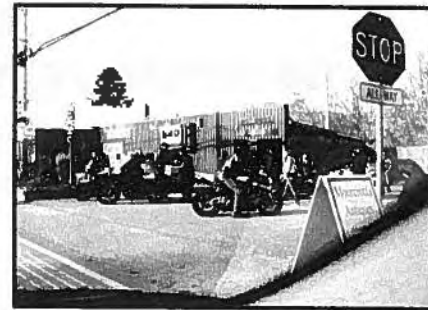
There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

SEE PREVIOUS COMMENTS RE. DESIGN FEATURES.

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?



For accommodating parking?

For improving conditions for cyclists?

PLEASE SEE "PART 2 - TRANSPORTATION
IMPROVEMENTS re. "BIKE PASSING
LANES."

For accommodating motorcyclists?

Please share other comments about this study here

ARE THERE OPPORTUNITIES TO HELP FACILITATE INTERNET SERVICE PROVIDERS (BELL, ROGERS, etc) TO EXTEND HIGH SPEED I/N SERVICE AS PART OF THIS PROJECT? THIS COULD BE A SIGNIFICANT COMMUNITY BENEFIT.



Your input is important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



*Improve roadway conditions
Cyclists pathway
Scenic designation - sensitive design
No Heavy Truck Traffic*

What existing design features would you like to see maintained?

*Hills, Valleys & two lanes - nature's Corridor
No sidewalks - low lighting*



What design features do you think would enhance the community character?

Different walkway options

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

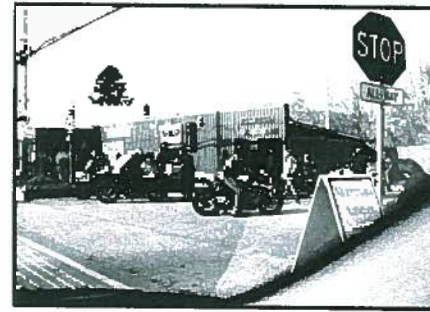


There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?



For accommodating parking?

For improving conditions for cyclists?

cycle path

For accommodating motorcyclists?

*Designated parking
Low Noise*

Please share other comments about this study here

*Place the south portion of Winston Churchill
to King Street*



**Your input is
important.
Thank you!**

Regional Council
Working for you

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?



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49° 16' 29.23" N, 79° 58' 58.14" W, elev: 127.740

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

Improve base of road



What existing design features would you like to see maintained?

same topography



What design features do you think would enhance the community character?

bicycle lanes

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

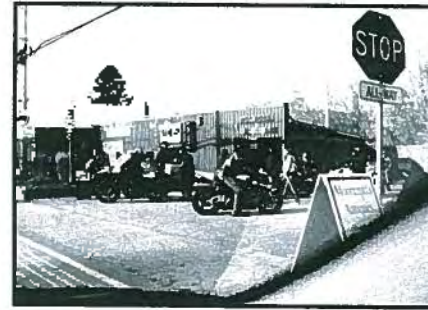
*maintain the current form & topography.
Do not shave off hill-tops,*

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

—



For accommodating parking?

—

For improving conditions for cyclists?

bicycle paths

For accommodating motorcyclists?

—

Please share other comments about this study here

When the Forks of Credit reno
happened, a thousand truck loads of
rubble were dumped (& a property owner
was paid a fee!) at the west end of
Belfountain, burying a valley.
Please, do not repeat this
atrocitiy.



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling *WHEELCHAIR ACCESSIBLE VAN.*
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

- PAVING OF WINSTON CHURCHILL SOUTH OF OLDE BASE LINE TO TERRA COTTA.
- ∴ LESS TRAFFIC ON OLDE BASE LINE.
- INCREASE POLICE TRAFFIC PRESENCE.



- LOWER SPEED LIMITS, 50-60 KPH. ON SUBJECT ROADS.

What existing design features would you like to see maintained?

- RE-BUILD OF STONE FENCES FOR A REASONABLE DISTANCE FROM PROPERTY ENTRANCES.



What design features do you think would enhance the community character?

- ABOVE -

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

MAINTAIN THE STATUS QUO.

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

DESIGNATED VEHICLE PARKING.

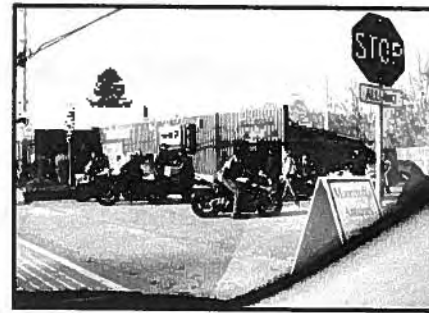
- TRAFFIC CALMING.

- INCREASED TRAFFIC POLICE PRESENCE ON WEEKENDS.

For improving conditions for cyclists?

CHARGE USER FEES.

OR LICENSE CYCLISTS.



For accommodating parking?

TOWN TO BUY PROPERTIES WITHIN WALKING DISTANCE OF BENEFOUNTAIN TO PROVIDE PARKING FOR DAY-TRIPPERS.

For accommodating motorcyclists?

TREATED AS VEHICLE TRAFFIC.

Please share other comments about this study here

DON'T CHANGE WHAT WE HAVE!
I^W MY FAMILY LIVES HERE BECAUSE
OF THE WAY OUR NEIGHBOURHOOD IS!!!



Your input is
important.
Thank you!

Region of Peel
Working for you.

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

At the moment in my case, just speeding vehicles, and bicyclists.



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

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What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



ROUNDBABOUTS ?

They work very well in other areas and are so easy to navigate

What existing design features would you like to see maintained?



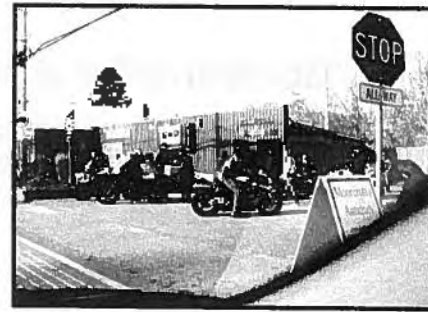
What design features do you think would enhance the community character?

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

Sorry, I cannot comment because I don't live in this town.



For accommodating parking?

For improving conditions for cyclists?

I understand that cyclists navigate to this area because of the hills and curves in our environment. However, each year I find that ^{they} tend to overtake the roads, having difficulties maintaining the rules of the roadway. There have been near hits when they cut cars off. I especially do not care for having to go out of my way to get home because they have a function or a ride. I'm not mean. Just concerned.

For accommodating motorcyclists?

Please share other comments about this study here

Will the phone companies be involved in any of the planning?

They are constantly doing work along Winston Churchill.

Are any of the above ground cables going underground?

A shame doing all this planning when technology is advancing so quickly and may need to disrupt the countryside, that has limits to the world web.



**Your input is important.
Thank you!**

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
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Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

MAKE SAFE THE EXTREME BARRERDUC CROSSING OF ISALLINARD RD WITH CONTINUATION WELLSBORO HALTON IS AND WINSTON CHURCHILL DUE EXTREME SHORT BUILT LINE SOUTH
1) REMOVE HILL SOUTH OF THE E/W RDS ON WINSTON CHURCHILL
2) FOUR WAY STOP SIGN OR TRAFFIC LIGHTS



What existing design features would you like to see maintained?

What design features do you think would enhance the community character?

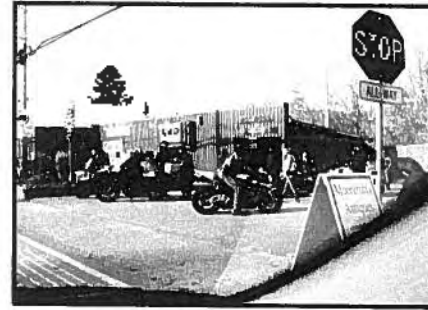


PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

For improving conditions for cyclists?



For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here



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important.
Thank you!

Region of Peel
Working for you

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

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Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

- the lack of bicycle lanes make it unsafe for bicyclists + slowdowns for motorists



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

bicycle lanes added.



What existing design features would you like to see maintained?

perhette @ Bush st/mississauga Rd.



What design features do you think would enhance the community character?

bicycle lanes. (is an important tourist component in village of Belfountain)

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

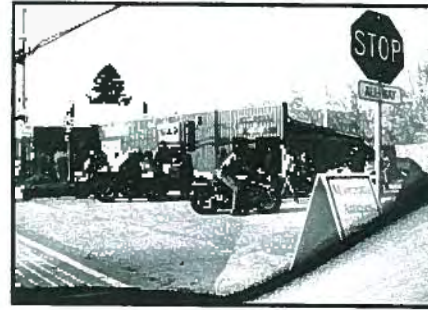
- native plantings where plantings are used

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

- provide parking in the village of Belfountain so there is room for the pedestrians to use existing wide shoulders.



For accommodating parking?

- vacant lot at corner of Mississauga Rd + River Rd (kitty corner to River Rd.)
Kia beside the shops.

For improving conditions for cyclists?

- bike lanes

For accommodating motorcyclists?

- they seem to be fine.

Please share other comments about this study here

- although part of the study, natural heritage is not mentioned in this booklet in specific questions.
- I would like to know what if anything is going to be done in the current road drainage that is funnelled into the creeks and wetlands in Belfontain w/o any treatment ~~can~~ (the quantity or quality). In some cases asphalt spillways into the creeks exist which contributes to the degradation of the West Credit and its tributaries in the study area.
- I also would like to see wildlife passage built into the roads as I often see roadkill (outside of the village).



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

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Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

There are a lot of cyclists who don't "share the road" - it's a wonderful concept that doesn't actually work on these roads. You can't ride a bike 5 abreast on Mississauga road, hold up traffic and be safe - for the cyclists or the drivers



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

traffic calming entering Belfountain.
use the road grips like they have on Winston Churchill
approaching Mayfield - instead of speed bumps.



What existing design features would you like to see maintained?

- all of them -
if we need safe walkways for pedestrians - build walking trails.
From the village to the school. No bike lanes.
- keep the heritage walls, split rail fences etc. We live in a heritage village - in a town of the 20th century home for a Reason

What design features do you think would enhance the community character?

leave it alone.



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

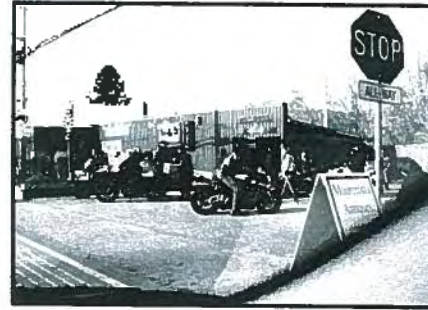
PRESERVATION

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

walking trails



For accommodating parking?

There is parking at the Conservation
that is adequate.

For improving conditions for cyclists?

It is not particularly safe
for cyclists, motorists to share
the roads around Belfountain.

For accommodating motorcyclists?

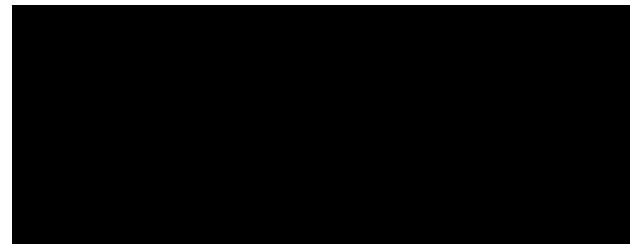
Please share other comments about this study here

This seems to be a way to spend lots of money trying to "fix" what should be left alone.

We live in a home built before the 20th century - with trees that are very old and very beautiful.

We moved here to live in a small village and raise our daughter in a beautiful part of Ontario.

If we wanted our tax money put towards developments that support, quarry's or brampton creation of traffic we would have stayed in Toronto.



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single ^{Multi} occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

Very rough, uneven areas, poor sides of pavement along Miss. Rd + W.C. Blvd. Bush. St area when paved was spread by a grader - not paving machine - The distance from center line to respective fences is so "disproportionate". Take a measure! Soon the road + all the drivers will be in our yard or hung up on our fences. It has already happened many times. You built up the road + don't cut the grass (as we used to but can't now) Then you fill the ditches full of gravel from the road. The new road should be many feet to the S. of present location



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

Improve site line @ Ballinafad Sldrd (I know this is South of Area)
Wider area (bike lanes or extended pavement past white/yellow lines) to allow safety for non-motorised users
Purchase land for parking so visitors may not park on or /2 on the road - (as if they are the "only" users). Our roads are used by many!



What existing design features would you like to see maintained?

Street lights with "character" for safety + appeal.
Large old trees, good fences - with character.
Hills + Curves - they have been here forever - this is why people come to & love our area.

What design features do you think would enhance the community character?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

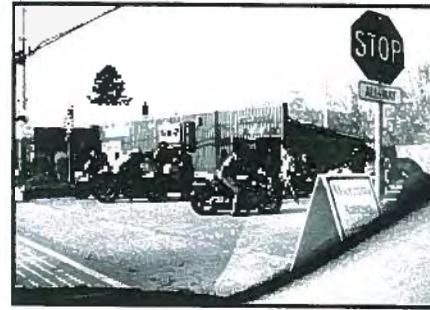
Maintain the beautiful stone fences, many well kept, wood, wire etc. & huge old trees.

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

Make area a bit away from actual road - whether dirt, gravel, or sidewalk
Reasonable speed limits - enforced



For accommodating parking?

Buy the farm with old silo (or portion of) for parking for sight seeing folk, bikers, walkers, hikers etc.
(Not In Study Area - Parking for Badland Viewing)

For improving conditions for cyclists?

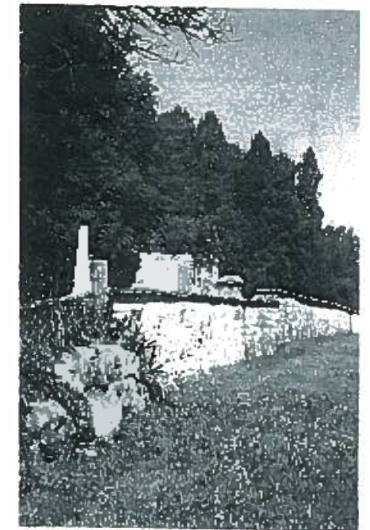
- No cyclists after dark - especially when wearing dark clothing, no lights, etc.
Reasonable speed limits - enforced

For accommodating motorcyclists?

Accept them - they spend @ the stores would not see without their support. Motorcycles have been here & on these roads for more years than most Belfountainites.
As with vehicles - reprimand those not obeying rules of road.
They make a shorter time of "noise" going past than lawn mowers or chain saws, etc.

Please share other comments about this study here

Thankfully people travel from everywhere to see our beautiful, unique area & spend \$ here.
Our roads must be maintained, upgraded but while trying to keep with community spirit + heritage.
Good Luck!



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

All of them.

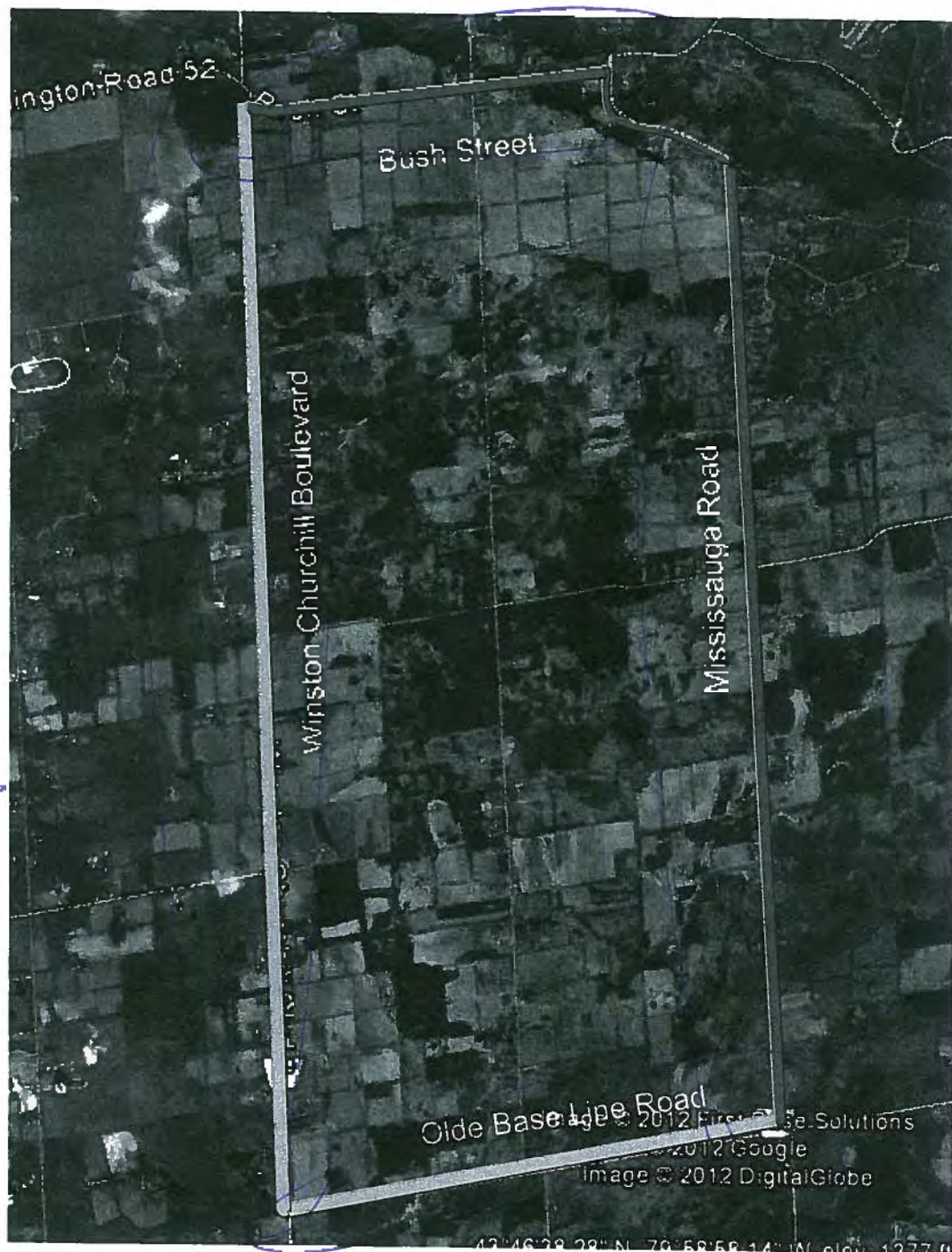
Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

Pedestrian

What transportation issues are you experiencing in the study area?

- speeding by cars and motorcycles*
- motorcycle racing after midnight*
- passing on curves*



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

- less speeding
- no expansion of streetlights



What existing design features would you like to see maintained?

- the hilly nature of the road - very scenic
- ditches - no underground sewers and curbs



What design features do you think would enhance the community character?

- roads which don't do seasonal buckling
- proper repaving (no bumps) when resurfacing

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

- ditches
- no curbs
- single lane in each direction
- one bicycle lane (not two) for ~~two~~ bi-directional bicycle travel
- no need to widen existing roads
- pave any dirt road sections

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

in Belfountain the concrete wall should have sidewalk for pedestrians



For accommodating parking?

it's OK now.

For improving conditions for cyclists?

- small bike lane (one side of road only) into Belfountain and out

For accommodating motorcyclists?

it's OK now.

But

- need speed enforcement*
- need nighttime racing reduction*

Please share other comments about this study here

- designate the entire study area as scenic

The end result is that I do not want these roads to be Bramptonized! There is no character to Brampton roads. These roads help define the character of Belfountain and area.



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

- trucks on roads
designated "no trucks"



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



- no trucks!

- "Share the Road" signs

- "scenic" designation for this area

What existing design features would you like to see maintained?

- hills, curves



What design features do you think would enhance the community character?

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

- please don't change these corridors, especially as they reflect the local "environment"/character of the community

- don't add street lights, sidewalks

NO SIDEWALKS OR
STREET LIGHTS

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?

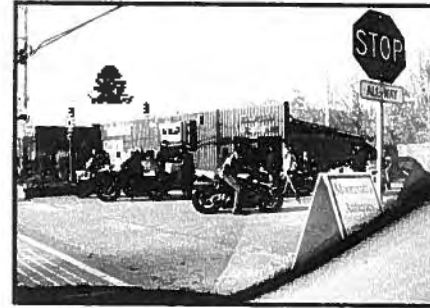


For improving conditions for pedestrians?

- please know that there are only a few times when there are enough pedestrians to pose a problem. Those pedestrians need to know that they must walk on the shoulders - they're in the country now.

For improving conditions for cyclists?

"Share the Road" signs



For accommodating parking?

For accommodating motorcyclists?

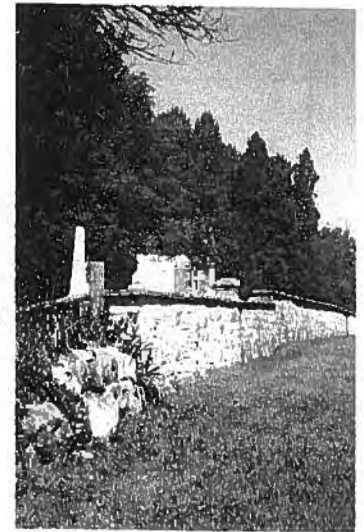
Please share other comments about this study here

Does the traffic study of these roads really reflect the "need" for replacing/redesigning them? (Especially in times of fiscal restraint)

Make stats on above available, easily!

There is no need for sidewalks or street lights.

Much greater publicity needed to give opportunity for more participation in the process. These ~~meetings~~ meetings have not been widely publicized.



Your input is important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

too many speeders



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



What existing design features would you like to see maintained?



What design features do you think would enhance the community character?

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

For improving conditions for cyclists?

Educate them

Re Rules of Road.



For accommodating parking?

◦ wide shoulders

◦ the Fall is a short season
do we really have to block
all the shoulders.

For accommodating motorcyclists?

Please share other comments about this study here

please no more speed bumps
like on Forks of Credit.



Your input is
important.
Thank you!

 **Region of Peel**
Working for you

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
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Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

*cycling on Winston Churchill
& Mississauga Road would
be safer & easier if the
shoulders were paved, especially
if gravel trucks are going by.*



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



What existing design features would you like to see maintained?



What design features do you think would enhance the community character?

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



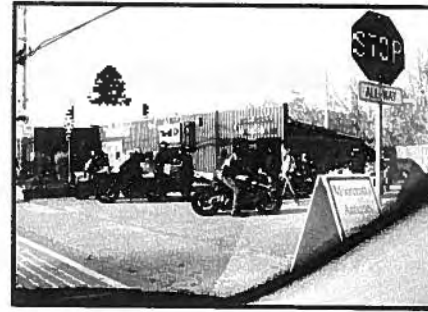
There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

For improving conditions for cyclists?

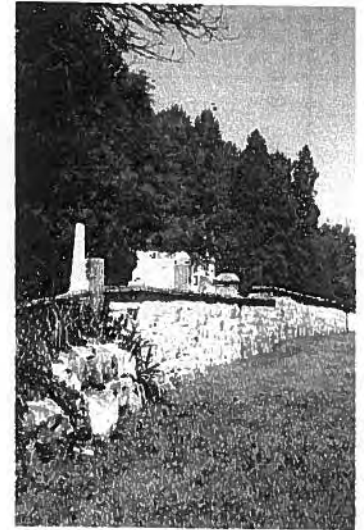


For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here

Keep heritage fences & trees, keep hills,
keep single streetlights marking intersections
at night.



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

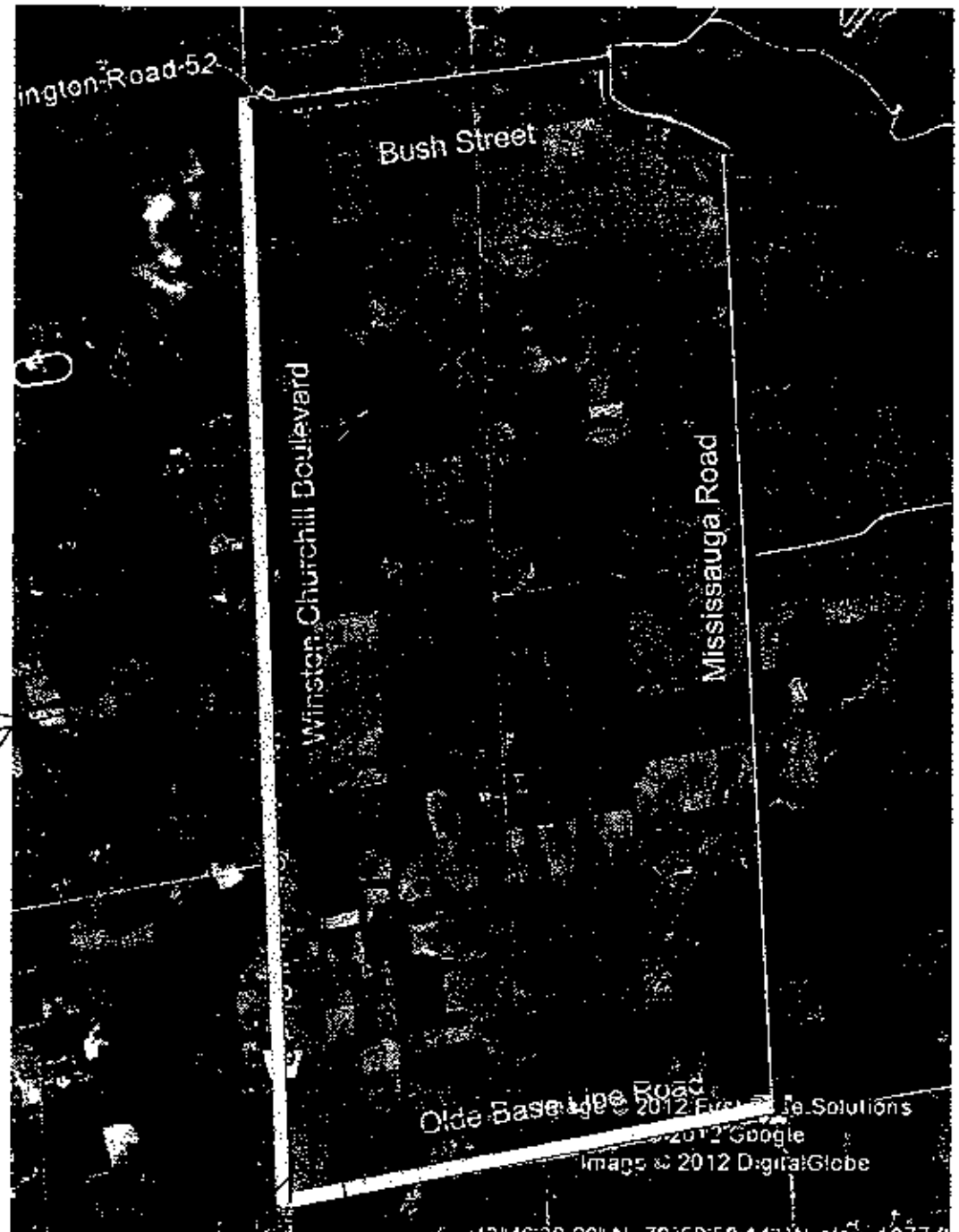
How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

Area that is paved (hard topped) is very uneven, cause by frost leaves excess water in base, poor base.

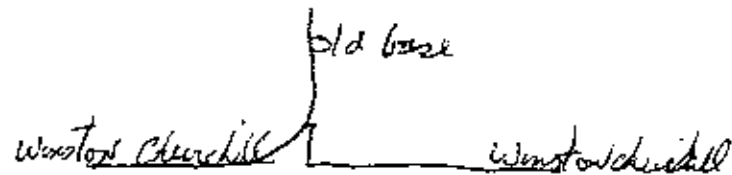


PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

At Winston Church ? Old base, purchase a portion of the North East corner and route traffic safely across Old base line.



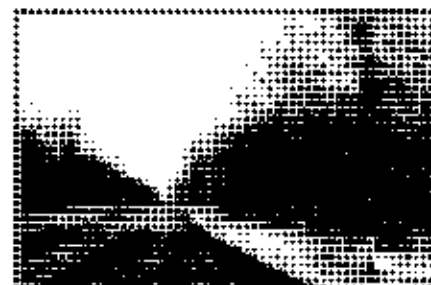
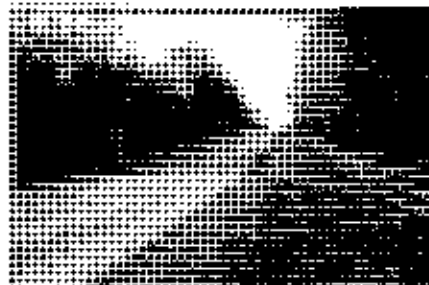
What existing design features would you like to see maintained?

existing rolling hills ? turns naturally help to keep speed down.



What design features do you think would enhance the community character?

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

Intersection Control.

*Baltesford S.R. + Winstonchurchill, 2 way stop E-W very dangerous.
if not changed to 4 way stop, it should be atleast 3 way, E-W + North*

Information for local owners should be sent by arial Mail, not just Email as many don't use Email.

Heritage Fence lines - Winstonchurchill.

*Both sides of Winstonchurchill have the same stone fences
yet 2yr ago only the east side was call heritage and plans were to take road
allowance from the West side (Wellington) This has since been changed, but,
is both sides now call Heritage fence lines. If not, why not, both
were built at the same time.*

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

Landowners etc.
More police control on Winston Churchill
below Old base drive, Gravel road, and
they pass our house doing 80-100 km
The police are welcome to use our land to
the barn as a sitting spot for speed
control.

For improving conditions for cyclists?

Cycle clubs from GTA use our roads
with little respect for others on the road
yet our local taxes pay for these roads,
not theirs. Why improve on cycle conditions
at my cost.



For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here

Below Old Base line on Winston Churchill (Gravel)
-It is my understanding that they re ditched this area to allow water to be pulled from the road, eliminating pot holes etc. When? why ditch I was told the ditches were always there and they were just re doing them. WRONG. I've lived here since 1952 and there were never ditches there. My property now get excessive water in front paddock. I can't cut the grass along the road (as I have for years) because the ditch blocks the area and the banks are now too steep. Cars continually are being pulled out of the ditches yet we never had this problem before. Old saying, if it aint broke, don't fix it.



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

There is too much speed & truck traffic ~~on~~ on Winst. Church & Olde Baseline. This is a very special area of Ontario - very scenic, historical & importance of the Niagara Escarpment must be our highest priority. Low impact roads on the green infrastructure.



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

- Slower speeds
- no truck traffic



What existing design features would you like to see maintained?

We like the hilly scenic character of the roads. Do not cut down the hills or widen the roads.



What design features do you think would enhance the community character?

I think the whole EA rectangle should be made a scenic area for roads so the character is maintained.

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

- Lots of hills & tree lined roads - historical fences - rural scenic countryside is very important
- Not wide
- lovely streams & springs
- historical markings.

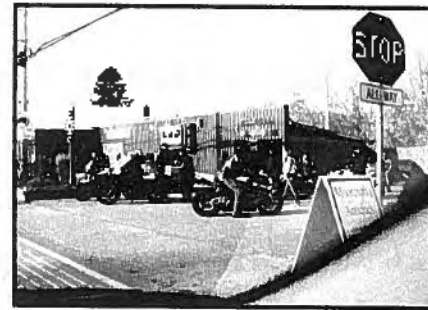
PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

Encourage pedestrians to use Bruce trail

For improving conditions for cyclists?



For accommodating parking?

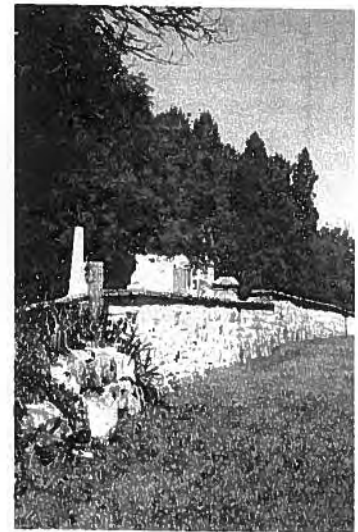
- At the Badlands

For accommodating motorcyclists?

Please share other comments about this study here

James Dick is applying for a new pit north of Bush on ~~West~~ Church St. How will you ensure his trucks ^{gravel} do not head south down West Church, & enter this EA area.

It's very important to protect the heritage fences and very old maple trees along the roadside within the Region's right of way — do not destroy these!



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

Horse back



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

Reduce speed.
Speed traps regularly on Winston Church
not just Old Base Line



What existing design features would you like to see maintained?



What design features do you think would enhance the community character?

- 3 way stop sign on Winston Church & Old Base
- 4 way stop at Winston Church & Bellin Road Rd

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



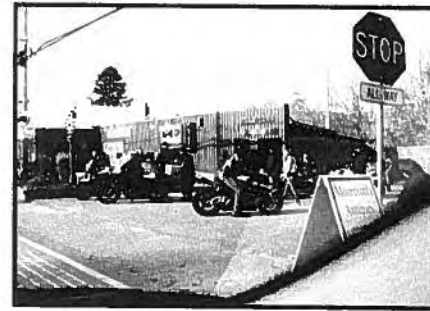
There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

Reduce speed



For accommodating parking?

Definite problem at the "Bad Lands" -- a bay property on north side for proper parking lot?

For improving conditions for cyclists?

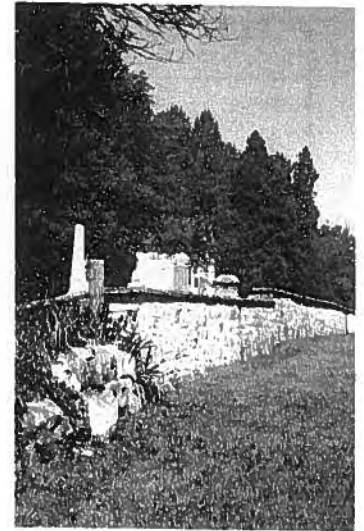
most of the group cycling are bike clubs from outside the area.

For accommodating motorcyclists?

no problem

Please share other comments about this study here

"Winston Churchill" is a series of other roads loosely commented. the southern end is actually 6 line ~~created~~
"Line BOM". It dead ends in Terra Cotta & basically ends at Bush St. ~~It would~~
went from a ~~low~~ back road to a major road.



Your input is important.
Thank you!

 Region of Peel
Working for you

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck
- WALK

What transportation issues are you experiencing in the study area?

PARKING CONGESTION
IN BELMONTAIN ON
WEEKENDS DURING
HIGH TOURIST PERIODS
MAKES WALKING IN THE
VILLAGE VERY UNSAFE
* NEED PROTECTED WALKING AREAS *
* NEED DESIGNATED PARKING AREAS *



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

1. IMPROVE ROAD STORM RUNOFF – KEEP IT CLEAN
2. DESIGNATED WALKING AREAS



What existing design features would you like to see maintained?

- * RURAL ROAD CROSS SECTION – NO CURBS + GUTTERS
- * MAINTAIN ROLLING HILLS
- * PROTECT STONE FENCES.



What design features do you think would enhance the community character?

- ~~STORM~~ KEEP IT SIMPLE – MODEL DESIGN BASED
ON CURRENT CHARACTER – VISTAS, AESTHETICS

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

➡ MORE TREES

➡ ~~LESS~~ KEEP OUT INVASIVE SPECIES = ROADSIDE DITCHES

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

ONE SIDE OF ROAD FOR
PEDESTRIANS, ONE SIDE FOR
PARKING



For accommodating parking?

CREATE PAY PARKING AREA
OUTSIDE OF VILLAGE

For improving conditions for cyclists?

DESIGNATED BIKE ROUTE
THROUGH VILLAGE

For accommodating motorcyclists?

MAINTAIN CURRENT CHARACTER
OF ROADS

Please share other comments about this study here

PROPERTY BOUNDARY OF 17263 OLD MAIN IS INCORRECTLY SURVEYED - ~~TITLE~~ TITLE BOUNDARY ~~TITLE~~ IS LARGER THAN WHAT THE IRON BARS CURRENTLY DEPICT. PLEASE RESURVEY BASED ON TITLE/DEED. (IN LAND REGISTRY OFFICE)



Your input is important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?



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43° 46' 29.28" N 79° 59' 59.14" W elev: 127.71 m

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

Enforce speed limits
" stop signs

Regular police presence on busy weekends.
Speed Bumps

What existing design features would you like to see maintained?

What design features do you think would enhance the community character?

No sidewalks
when



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

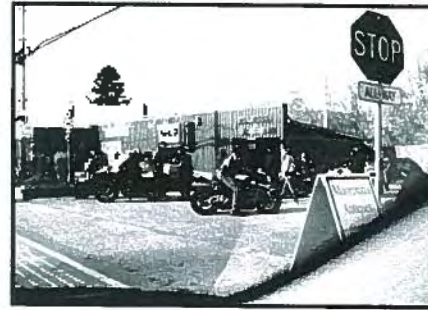
PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

For improving conditions for cyclists?

More share the road signage



For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here

This is the country not the city - leave it that way!



Your input is
important.
Thank you!

Region of Peel
Working for you

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

Very busy - difficulty
entering southbound on Miss Rd
from Caleton Mt. Dr.
More traffic (ie. Orb Dev's
& Erin Growth) will
seriously impact hamlet,
quality of life in Berfontein



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?



I fear that the "Build It + They Will Come" Agate may irreversibly destroy the character of the area.

What existing design features would you like to see maintained?

- rural quality - 2 lanes & perhaps designated bike lane -
- no sidewalks / suburban type infrastructure.



What design features do you think would enhance the community character?

~~the~~ No additional "adornment" to encourage more traffic / tourism.

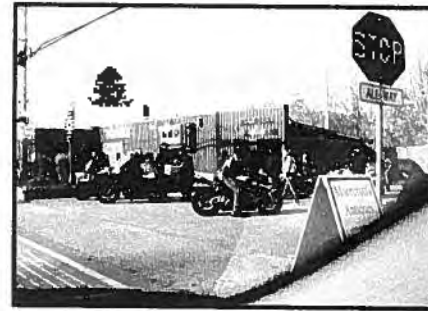
- Less is definitely more

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

at most - an unimproved
narrow walkway - no curbs.
etc.



For accommodating parking?

as to
- policing of violation
& excessive speed
& noise.

For improving conditions for cyclists?

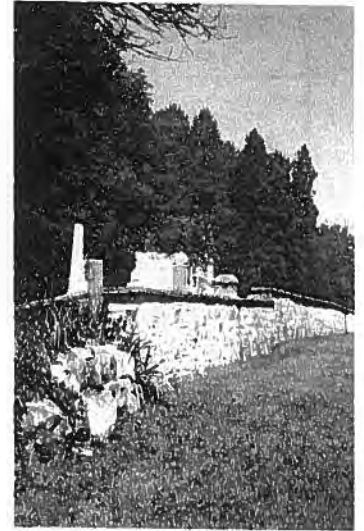
Do not have "add ons" to make
this more of a tourist mecca

We are on the cusp of mega development all
around + we need to preserve something!

Please share other comments about this study here

There will be many pressures
-housing, gravel, tourism.

Progress does not mean growth;
presumably we have become
more enlightened about the fragile
nature of this environment.



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

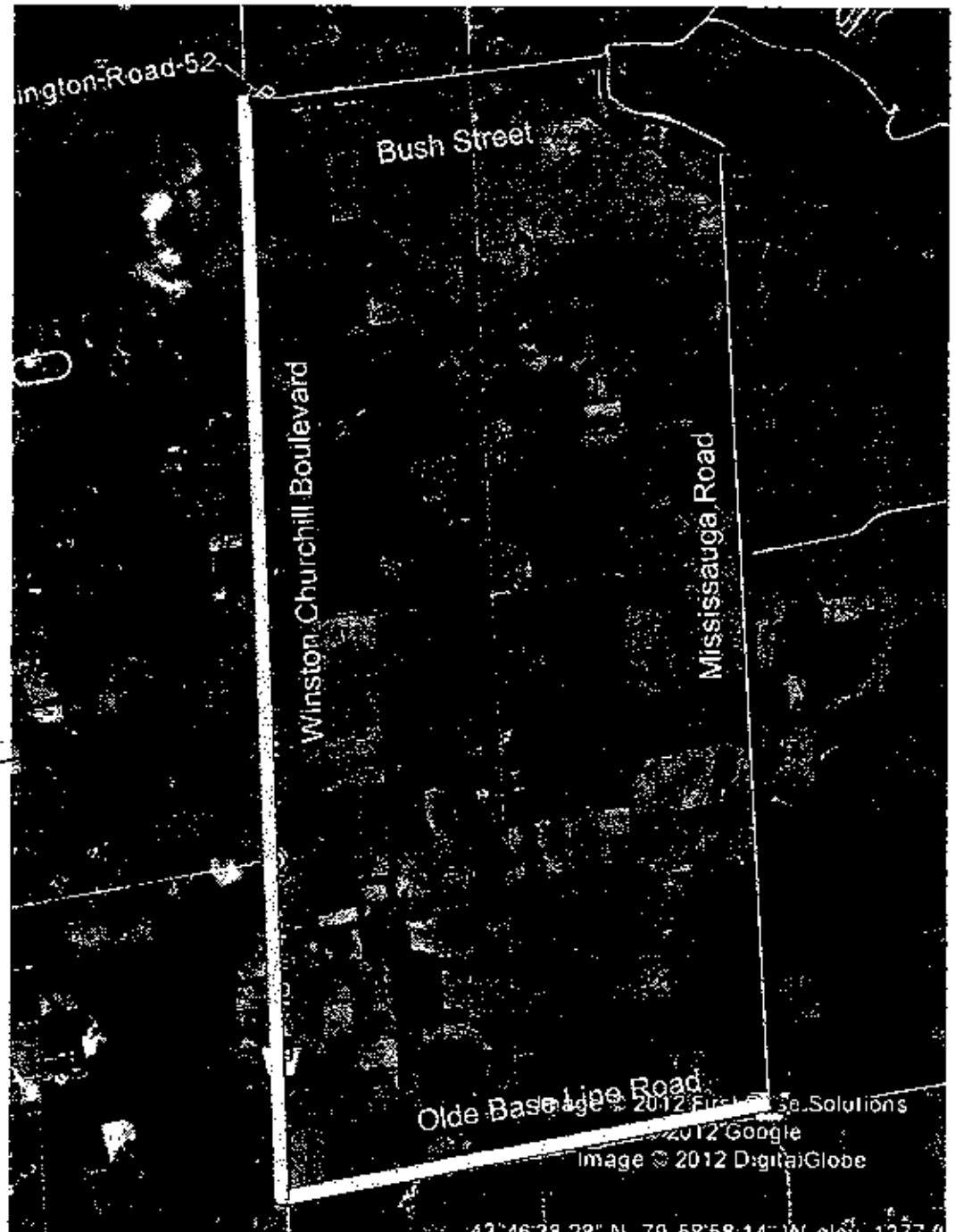
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Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

None, We can only comment on Olde Base Road Road, which we use everyday.



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

Road Shoulders should be improved



What existing design features would you like to see maintained?

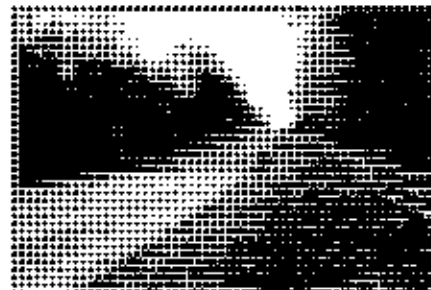
Hills, bends & trees. These are what define the area and make the roads special, which in turn calm traffic.



What design features do you think would enhance the community character?

?

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

the roadway corridors should remain pretty much as they are.
these are country roads with character popular with cyclists & weekend drivers because they offer a difference from most other roads in that they are hilly. It is wonderful to crest a hill and get an unexpected view.

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

?



For accommodating parking?

?

For improving conditions for cyclists?

improve road shoulders.

For accommodating motorcyclists?

?

Please share other comments about this study here

Our property is fronted at Old Baseline
with large majestic oak trees which
we would like protected.



Your input is
important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

Generally, traffic flows well on the relevant roads, with only minor crowding during commute periods. Traffic safety is a concern, with frequent speeding and periodic aggressive driving (tailgating, unsafe passing). The intersection at Olde Baseline and Mississauga Rd presents significant risk due to poor site lines and speeding.



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

Abstract curbing should be avoided. Subtle transition to vegetated roadside areas is preferred. Natural and low-maintenance vegetation would be best. To the extent possible, do not obstruct existing fences, hedgerows, etc of bordering properties (most are well designed + maintained and reflect community preferences).
Maintain roadside trees as much as possible.

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

Road condition is not a concern. Any design features to reduce likelihood of speeding would be beneficial



What existing design features would you like to see maintained?

no comment



What design features do you think would enhance the community character?

no comment

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

The roads in question are not really suited to pedestrian use (except in village contexts). Wide shoulders at some demarcation of edge of auto travel would probably help -

For improving conditions for cyclists?

The cycling lanes that exist on Old Baseline (east of May 10) and King Rd are quite effective. Similar cycling lanes would be beneficial on all the relevant roads, all of which are well used by cyclists.



For accommodating parking?

no comment

For accommodating motorcyclists?

no special considerations

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

WALKING

What transportation issues are you experiencing in the study area?



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

DOUBLE LINES TO DISCOURAGE ~~DRIVING~~
PASSING ON HILLS.
'NO PASSING' SIGNS.



What existing design features would you like to see maintained?

What design features do you think would enhance the community character?

DO NOT ADD SIDE WALKS.
IGNORE LIGHTING @ INTERSECTIONS.



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

ENFORCE 40 KPH LIMIT

ENFORCE STOP SIGNS



For accommodating parking?

How CAN YOU STOP PEOPLE

GOING TO THE 'PARK' FROM

PARKING IN TOWN FOR 6 TO 8 HOURS?

For improving conditions for cyclists?

STOP CYCLISTS FROM

TRAVELLING 2-3 + 4

AT BREASt, ENFORCE

STOP SIGNS

For accommodating motorcyclists?

Don't + ENCOURAGE

MORE BIKERS

Please share other comments about this study here

At a time when budgets are
strained, think of who is going
to pay for unwanted changes.
We ARE!!!



Your input is
important.
Thank you!

 **Region of Peel**
Working for you.

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

- ① poor sight lines on Winston Churchill just south of Ballinacah Rd presents serious safety hazard - the site of numerous accidents
- ② cycling on Miss Rd a challenge bike lanes would help
- ③ speeding vehicles on Winston Churchill a problem for pedestrians and cyclists



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

pull Winton Chubbell and remove hill just south of Ballinapad Rd



What existing design features would you like to see maintained?



What design features do you think would enhance the community character?

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

For improving conditions for cyclists?

*bicycling lanes on
Mississauga Rd, as it
is well used by cyclists*



For accommodating parking?

For accommodating motorcyclists?

Please share other comments about this study here

*1.2 transportation
2.2 below Old Baiters*

since prev study's recommendations not implemented, what is the probability of that recommendations from this study will be implemented? i.e. lots of studies no action - no money

due to lack of finances

also left out part of W.C. that is not paved and is a safety risk when it rains, snow as it is well used by foot moving through traffic

I like walking and recently have avoided W.C. and Old Baiters because of the significant increase in foot traffic - trails or designated pedestrian walkways (paths) would be appreciated in some areas

I am in favour of bicycle lanes on Mississauga Rd it is already used by cyclists and should be made safer



**Your input is important.
Thank you!**

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

*excessive traffic + speed beyond limits
most traffic heads down WINSTON CHURCHILL
approx. 5 of every 7 heading on BUSH ST.
turn down*



© 2012 Eartheye Solutions
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Image © 2012 DigitalGlobe

42°46'39.28"N 79°58'58.14"W elev: 1277 ft

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

again an issue of quantity + speed



What existing design features would you like to see maintained?

road is generally designed and maintained for area



What design features do you think would enhance the community character?

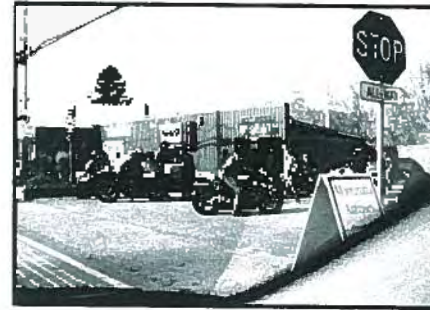
- leave it for community to enjoy with no more to increase traffic.*
- time to share with other arterials north + south?*

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

-sides are wide enough for our runners
and walkers



For accommodating parking?

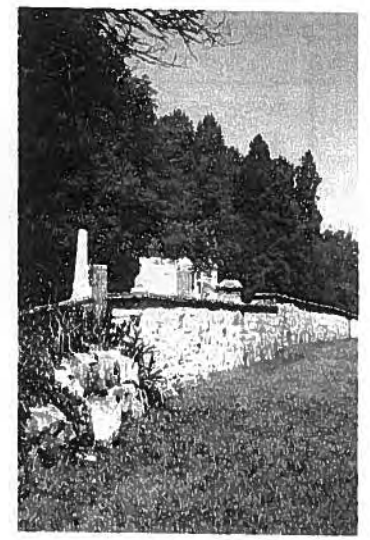


For improving conditions for cyclists?

For accommodating motorcyclists?

weekends they enjoy the area.

Please share other comments about this study here



Your input is important.
Thank you!

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

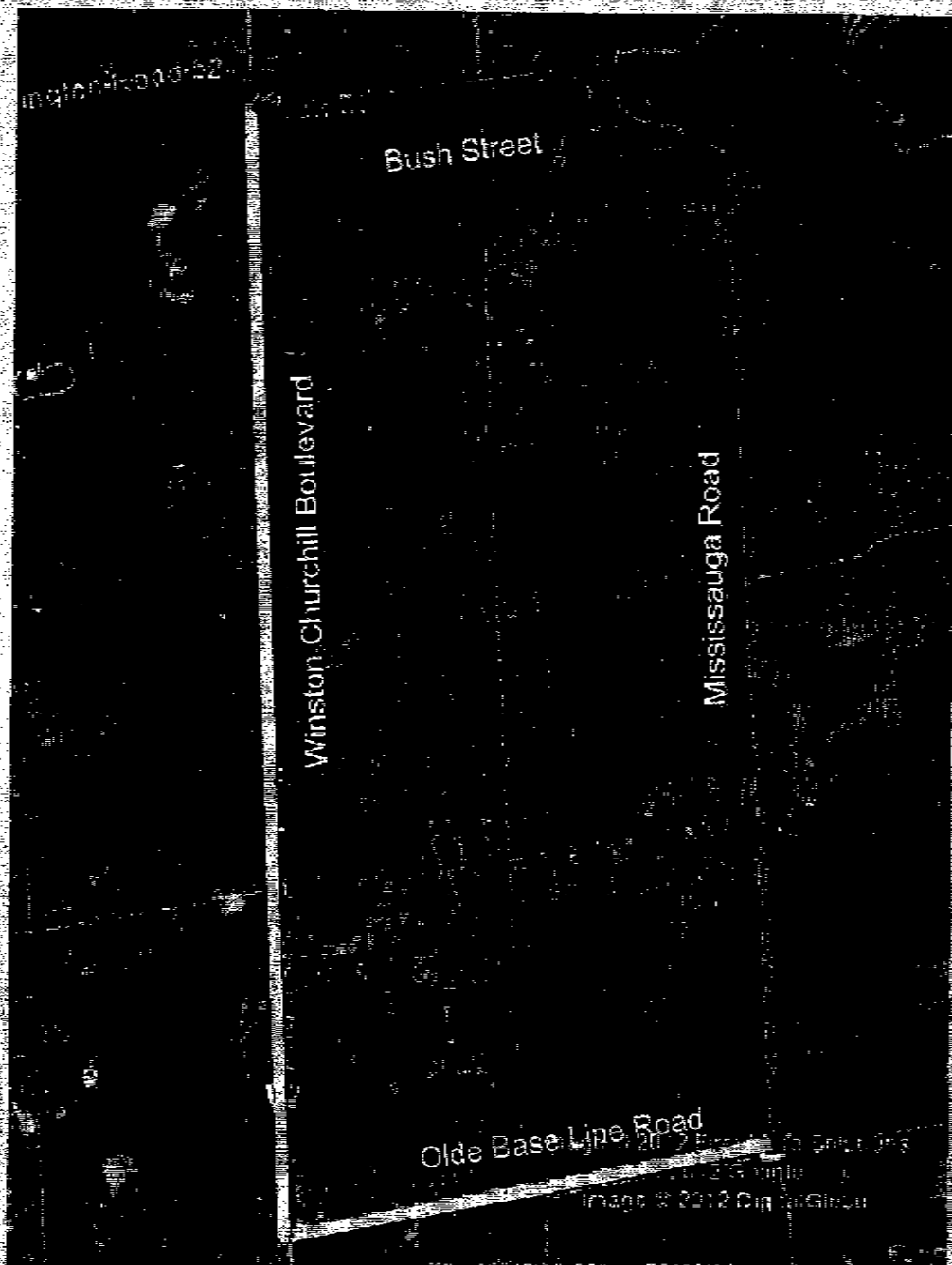
How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

as residents of Mississauga Rd,
our main concern is with
excessive traffic noise and
speeding (cars and
motorcycles) on Mississauga
Road.



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

the road conditions seem generally fine - speed limits are too high for safe use though



What existing design features would you like to see maintained?

maintain rural / farm feel with fences, walls, trees etc.



What design features do you think would enhance the community character?

more of the above

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?

~~We~~ don't typically travel these roads on foot but wider shoulders and reduced speed limits should help here

For improving conditions for cyclists?

significant reduction ~~in~~ allowable speed for cars will improve conditions for cyclists and reduce traffic noise too. Win-win situation.



For accommodating parking?

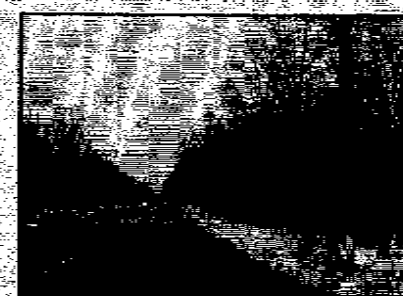
We feel there is sufficient parking already in the area to accommodate visitors. This is really only a concern 2-3 weekends a year in the autumn when people come to view the trees

For accommodating motorcyclists?

We do not wish to accommodate motorcyclists. The loud noise they create for our Mississauga Road property is very unpleasant. We have added water features + noise barriers to try to reduce the impact and it is still too loud and takes away from enjoyment of our property into summer.

Region of Peel
Working for you

PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

a focus on anything that can enhance the rural charm of the area - stone wall restoration, farm fence restoration, tree plantings, reducing speed limits etc. etc

Please share other comments about this study here

Our family has owned this property since the 1960s. When I was a child we used to run back and forth the road to visit our neighbours since there was very little traffic to be concerned with. Now the traffic is so heavy we have had to put in a gate to ensure our children do not go near the road. The properties on Mississauga road tend to be close to the road and the road should be treated more as a residential road than a highway. If the speed limit was reduced to 50 km, most safety concerns regarding sightlines etc. + cyclist safety could be eliminated.



Your input is
important.
Thank you!

Region of Peel
Working for you

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

- Traffic Load way too high
- Speeds are wildly excessive
- Truck ban is not enforced
-



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

- Enforce speed limits + Truck bans

- Put more stop signs at intersections to control speeds
- Put speed bumps in posted 60 km areas
- ~~Maintain~~



What existing design features would you like to see maintained?

- Maintain rural nature of road (no curbs, sidewalks)
- Preserve wet lands, water courses and other ecologically sensitive areas

What design features do you think would enhance the community character?

- see above



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



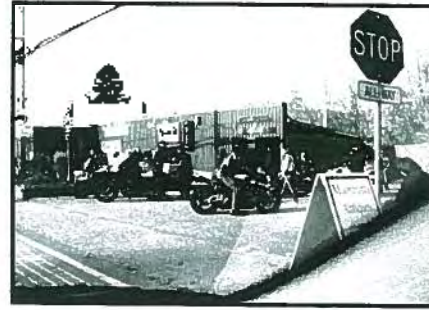
There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

- Maintain existing rural character + nature.

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?



For accommodating parking?

For improving conditions for cyclists?

- Bike lanes if it does
not require road
widening

For accommodating motorcyclists?

Please share other comments about this study here



**Your input is
important.
Thank you!**

Region of Peel
Working for you.

PART 1: TELL US ABOUT YOUR EXPERIENCES IN THE STUDY AREA

How do you travel within the Study Area?
Please identify by circling on the map the travelling routes that you and your family use in the study area.

Bush St to ERM, Miss. Rd to Toronto

Check the modes of transportation that you use:

- Single occupant vehicle
- Car pooling
- Cycling
- Motorcycle
- Truck

What transportation issues are you experiencing in the study area?

*Cyclists traveling
2 abreast on
Mississauga Rd*



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

What transportation improvements would you like to see considered – i.e. to increase safety for all users, to improve roadway conditions etc?

- Ban cyclists on Mississauga Rd.
- Put a roundabout at Winston Churchill and Bush, at Miss. Rd and Old Base Line and perhaps one at Old Base Line + Winston Churchill.

What existing design features would you like to see maintained?

Keep all hills and grades → do not straighten or widen or make any cuts into the sides of the right-of-way for Mississauga Rd.

What design features do you think would enhance the community character?

"Quaint" Street lighting in the village



PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?



There is an aesthetic component to the roadway corridors since they provide a public space separating the properties. What would you like to see within the corridors? Please describe your ideas!

Plant new maples along Miss. Rd
to replace dying + dead maple trees

PART 2: WHAT IDEAS DO YOU HAVE FOR THE FOLLOWING?



For improving conditions for pedestrians?



For accommodating parking?

Buy property from ORB
at south end of Belfountain
to make a public parking
lot for Tourists

For improving conditions for cyclists?

Ban them from Miss.
Rd. Create new route
else where for them.
Miss. Rd cannot be made
safe for cyclists without
changing the width/topography
which most Belfountain
Residents do not want

For accommodating motorcyclists?

Enforce noise bylaw so
locals will feel welcoming
to motorcyclists

BELFOUNTAIN ENVIRONMENTAL ASSESSMENT STUDY

Mississauga Road, Olde Base line
Road, Winston Churchill Boulevard,
Bush Street and Old Main Street



10/30/2012

PUBLIC OPEN HOUSE FEEDBACK REPORT

This report was prepared by the Independent Facilitator and represents the feedback received at the Public Open House held on October 30, 2012. It includes the verbatim comments from the wall stations, themes and questions noted through the discussion and input received through the completed and returned workbooks.

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For further information contact Asha Saddi, Communications Lead, Transportation Program Planning
Transportation Division, Public Works, Region of Peel, Tel: 905-791-7800, extension 7794
Fax: 905-791-1442 asha.saddi@peelregion.ca

Comments about this report should be directed to Facilitator, Sue Cumming, MCIP RPP,
Cumming+Company at cumming1@total.net or Tel: 866 611-3715.

BELFOUNTAIN ENVIRONMENTAL ASSESSMENT STUDY

Mississauga Road, Olde Base line Road, Winston Churchill Boulevard, Bush Street and Old Main Street

PUBLIC OPEN HOUSE FEEDBACK REPORT

1. ABOUT THE ENVIRONMENTAL ASSESSMENT STUDY

Background: In June 2009 Peel Region began a Schedule “C” Municipal Class Environmental Assessment (EA) for improvements to Mississauga Road from Olde Base Line Road to Bush Street and Bush Street from Mississauga Road to Winston Churchill Boulevard. The Region of Peel is now expanding the limits of our EA to include Olde Base Line Road from Mississauga Road to Winston Churchill Boulevard, and Winston Churchill Boulevard from Olde Base Line Road to Bush Street (see map).

Why was the study area expanded?

Based on the feedback received for the Mississauga Road / Bush Street EA the Region of Peel expanded the study area to review road safety, sight lines, drainage, parking and pedestrian and cycling needs.

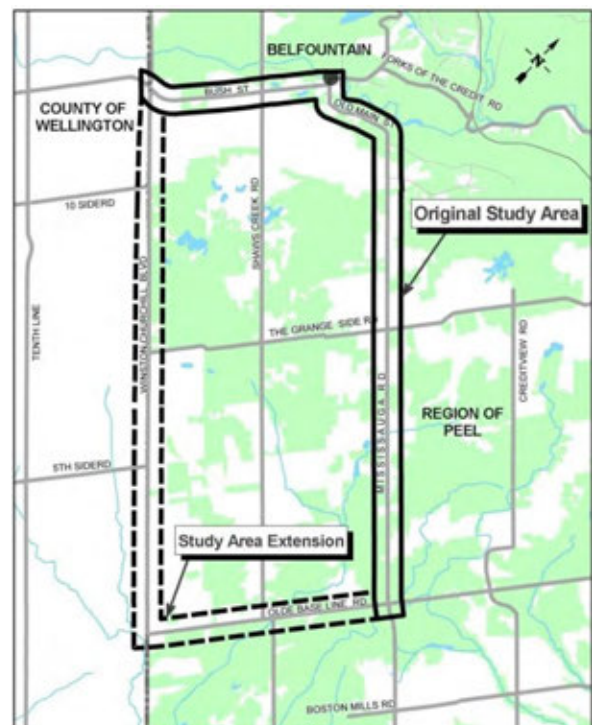
As described at the Public Open House held on October 30, 2012 this EA study will **not** consider increasing the numbers of lanes. The Project Team will build on the previous study information to develop a reconstruction plan for the study area roads that meets the needs of all road users and maintains the character of the community.

The Process

The aim of the Class EA process is to provide everyone who has an interest or stake in the study area with the opportunity to create the best solution. The project team, with your input, will:

- define the problem statement (Winter 2013)
- develop and evaluate planning alternatives; (Winter and Summer 2013) and
- determine a preferred solution (2014)

Opportunities will be provided for the public to be involved through planned public meetings at key milestones as the study progresses. A Community Working Group (CWG) has also been established. The CWG is a representative broadly based group of interested stakeholders who will participate in focused discussion on project issues through two workshops held over the course of the study. A CWG orientation meeting was held on October 24, 2012 and additional representation is being confirmed. The next CWG meeting will be held in February 2013. The meeting notes are available on the study web site.



Your participation is important and your comments are valued. Two public meetings will be held in 2013 to discuss the problem statement, review alternatives and discuss potential solutions. The Region of Peel encourages you to provide input at future public meetings or by providing comment as the study develops. Contact information is found at page 6 of this report.

Purpose of the October 30, 2012 Open House

The Region of Peel is undertaking this study through a Context Sensitive Planning and Design Approach for roads which will focus on improvements to enhance the experience for all of the users and reflect the character of the community. This is important as it will ensure that the solutions fit with the rural and scenic quality of the area and are sensitive to the needs of the community.

Enhanced consultation and public involvement is being undertaken through the establishment of the Community Working Group and the holding of a Public Open House at the outset of the study, prior to the formal public meetings. The Open House was held so the Project Team could meet with community residents and stakeholders to discuss the process, and to learn about transportation issues and valued community characteristics. Over 100 attendees were at the Open House held from 6:30 p.m. to 9:00 p.m. at the Belfountain Public School. Eighty-four individuals provided their contact information at the welcome table. A workshop booklet was provided and tables were set up to allow people to provide their written responses contained in the booklet. Over 40 booklets were returned.

Notice for the Open House was provided through the following:

- Mailing of notices to property owners fronting / backing along the study area corridors
- Web site
- Local newspaper advertisement:
 - Erin Advocate on October 17, 2012
 - Caledon Enterprise/Caledon Citizen on October 18, 2012
 - Georgetown Independent/Acton Free Press on October 18, 2012
 - Bolton Caledon on October 18, 2012
 - Wellington Advertiser on October 18, 2012
- Posting on the community board at the local Community Centre and copies of the notice made available at the Belfountain Village Store and the Higher Ground Coffee Shop.

The meeting was organized with a drop-in from 6:30 p.m. to 7:00 p.m. with the opportunity for viewing project background displays. During this time community residents and stakeholders provided written comments on paper posted on the walls around room. This was followed by a short presentation at 7:00 p.m. by Steve Keen of HDR Corporation lead transportation planning consultant for the project. A public discussion followed. The meeting was facilitated by an independent facilitator, Sue Cumming, Cumming+Company lead to the public engagement for the project.

This report, written by the Facilitator, is intended to provide a synthesis of the overall key messages heard and provided information on next steps. The Appendices contain the detailed public comments through the wall stations (Appendix A), the public meeting discussion (Appendix B) and the responses in the workbooks (Appendix C). The comments received through the Open House will be considered by the Project Team and will help in informing the project team as the project moves forward.

The public requested that the Open House Feedback Report be made available for viewing at the Belfountain Library. This report will be posted on the study web site and a letter will be forwarded to all of those that included their addresses on the sign-in registrar for the Open House advising of the availability of the report.

2. KEY MESSAGES HEARD

There is significant community interest in the Belfountain EA Study. The 100 residents and stakeholders who attended the October 30, 2012 Public Open House had many comments about the study scope and process, transportation issues, condition of roads, natural environment and community characteristics. Many indicated that they had attended previous meetings. Many sought clarification on the stated commitment that no lane widenings were being considered. The majority of those that attended provided comments (written and through the discussion) conveying the importance of ensuring that the study outcomes do not impact the historic and much valued countryside and scenic character of the Village of Belfountain and the surrounding community. Some questioned the need for the study citing that things are just fine the way that they are. Others identified concerns about uneven pavements, excessive speeding, increasing traffic from areas to the north, site line impairments, how trucks and cyclists use the roads and the need for improved pedestrian pathways. The following is a synthesis of key messages heard.

- a. **Maintain the rural character and countryside scenic quality** described as mature trees, natural vegetation, cedar rail, heritage fences, hilly nature of the topography, extensive natural areas and curvature of the existing roadways. There is strong support for maintaining the rural character of the area.
- b. **Preserve historic fences and features** throughout the area. Some identified the need for repair and enhancement to these structures some of which may be on or near regional property and close to existing roads.
- c. **Preserve / enhance natural environment.** The beauty and significance of the natural environment is enjoyed by the residents and is an attraction for visitors. The streams, trees, and forested areas were noted to be of significance. There are many ideas for how to preserve and enhance the natural environment contained in the workbook responses. The Credit Valley Conservation Authority representatives are involved in the study and attended the Open House.
- d. **Develop solutions that balance interests of all residents in the area.** Residents who live on Mississauga Road and Winston Churchill Boulevard each spoke of the increase in traffic along these corridors. Residents do not want to see improvements made in one part of the study area that results in traffic being shifted to another part and are looking for a balanced approach for developing solutions.
- e. **Address poor conditions of roadway pavement** along roads to improve rough and uneven pavement, due to frost heaves and water in the base. Much opposition was noted of flattening the roadways. Residents support creative solutions that would repair the roadways without changing the existing topography and character of the existing roadway.
- f. **Improve pedestrian safety.** While there is an interest in improving pedestrian safety, there are mixed opinions on how that should be done. Off-road paths are preferred by some for certain areas, others favour sidewalks in other areas. Better connections to and within Belfountain village were particularly noted, with some favouring sidewalks and others not.
- g. **Minimize impact of increase in traffic volumes.** Many residents indicated that there has been a noticeable increase in traffic volume resulting from the growth to the north of Belfountain. Specific concerns were noted about the traffic impact from new development in Erin (600 homes) and the potential Orb property development. Residents noted that future increases could impact the quality of life in the hamlet in Belfountain. Residents living on Winston Churchill noted that they have seen a spike in traffic including truck traffic with higher than permitted speeds, resulting in

noise, conflicts and concern about safety. Both Mississauga Road and Winston Churchill are increasing an increase in traffic volumes. Residents would like to see measures explored for slowing down traffic and for minimizing the impact from future growth.

- h. Address excessive speeds being experienced along Mississauga and Winston Churchill Roads.** Residents would like to see better, appropriately placed signage or other means to slow the traffic down. It was noted that there is too much speed on Mississauga Road, on Winston Churchill Boulevard and Olde Base Line Road.
- i. Deal with trucks that travel too fast, creating noise and unsafe conditions.** Many noted that they see trucks on roads that are not designated for trucks. Winston Churchill Boulevard was noted to be experiencing high truck volumes and speeding which is impacting safety and quality of life for the residents.
- j. Accommodate cyclists outside the travelling lane of traffic.** Many concerns were noted about cyclists who “don’t share the road”, in some cases travelling more than two abreast thereby putting themselves and others at risk. This together with the lack of safe areas for bicycles makes it unsafe for cyclists and for motorists. Cycling on Mississauga Road was noted to be a challenge with some preferring bike lanes. Speeding vehicles on Winston Churchill Boulevard were noted to be a problem for both pedestrians and cyclists. Some note that cycling on Winston Churchill Boulevard and Mississauga Road would be safer and easier if the shoulders were paved, especially if gravel trucks are going by. While there is a desire for improved cycling infrastructure, there are mixed opinions as to how best to accommodate cyclists with a preference for a rural solution as opposed to solutions that are more urban in nature. The underlying theme of maintaining the rural character and scenic quality of the area must be considered in determining future cycling infrastructure.
- k. Address issues with motorcycle traffic and speeding.** Belfountain is an attractive area for motorcycles. Concerns were noted about speeding (racing after midnight), passing on curves, safety and parking. There are mixed views with respect to motorcycle traffic with some preferring a prohibitive approach of restricting motorcycles in the area and others indicating that only a few are creating problems.
- l. Improve poor sight lines in some locations.** It was noted that there are a number of locations where poor sight lines are being experienced. Although Ballinafad is outside of the study area, poor sight lines on Winston Churchill Boulevard just south of Ballinafad Road were noted to be a safety hazard resulting in unsafe conditions and potential for accidents. Similarly there are issues when entering southbound on Mississauga Road from Caledon Mountain Drive. A number of other locations are identified in the workbook responses for consideration.
- m. Address parking congestion in Belfountain** on weekends during high tourist periods which is impacting the safety of walking in the Village. Parking at the Badlands was noted for improvement. There are mixed views on whether to create more designated parking or to continue with what is currently available.

3. NEXT STEPS

The comments received through Public Open House will be considered by the Project Team and will help in informing the project team as the project moves forward. A number of technical and traffic studies underway were referenced at the open house and these will also be incorporated. One of the next steps will be the formulation of the study's 'problem statement' as well as the preliminary development of range of road reconstruction concepts.

The formal problem statement and preliminary reconstruction concepts will be prepared in advance of the Winter/Spring 2013 and presented at the first public meeting which will be held in spring 2013. The second Community Working Group Meeting will be held in winter 2013 at which these materials will be reviewed. Notice for the future public meetings will be mailed to those on the project mailing list (will include those that included their addresses at the October 30, 2012 meeting) and those located on all roads within the study area.

Progress on the study can be viewed on the website at:

peelregion.ca/pw/roads/environ-assess

If you would like to comment on the study expansion or be notified about future public meetings, please contact either of the following team members.

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Project Manager,
Transportation Program Planning
Public Works
Region of Peel
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Fax: 905 857-3127
E-mail: stephen.keen@hdrinc.com

Appendices

Appendix A Public Comments recorded at wall stations at Open House

Appendix B Comments/ Questions noted through the discussion after presentation

Appendix C Public Comments noted through Workbooks

Appendix A

VERBATIM COMMENTS NOTED BY PARTICIPANTS AT WALL STATIONS

The following are the detailed notes that people wrote on the paper at the wall stations during the first half hour and last part of the October 30, 2012 Public Open House. These are organized by the question/topic posted on the wall.

What transportation improvements would you like to see considered?

- a. Reduce speed on gravel section of Winston Churchill.
- b. Control speed on all of Winston Churchill – 100 km is not a good idea – need stop signs and speed bumps.
- c. More “share the road” signs on Mississauga Road et al.
- d. Bush Street (near 332) moves road sign on South side back to where it used to be. The centre of the road to the fence on each side is so different in distance.
- e. Site line on Winston Churchill south of Ballinafad Road needs improving safety a serious problem – many accidents.
- f. No speed bumps – dangerous to drivers and increase maintenance of car. Increase vehicle emissions.
- g. How much money has been spent on Olde Baseline between Mississauga Road and Winston Churchill in last 20 years and how many accidents in the same period?
- h. How many accidents have happened in the EA area in that period?
- i. How much money has been spent in the EA area in the last 20 years?
- j. There has been a huge increase in large truck traffic on Winston Churchill and Olde Baseline with very little enforcement. How is the Region going to prevent this?

What existing design features would you like to see maintained?

- a. Leave the hilly character of Mississauga Road (2) all of them.
- b. Yes, leave all of the hills (3).
- c. Maintain the countryside scenic quality otherwise it will just look like Brampton with hills (horrors).
- d. Leave fence lines untouched – trees, stones and rails.

() denotes number of time same comment noted

What new design features could enhance the community character?

- a. The west side of Winston Churchill’s Wellington Township. They should be involved.
- b. Bike lanes (2)
- c. Move stop signs at rural intersections
- d. Speed control in 60 to 70 km sections
- e. No bike lane – this is the country. Our roads are too narrow.
- f. More “share the road signs”.
- g. No more signs.
- h. Use more speed slowing devices.

<p>Ideas for pedestrians</p> <ul style="list-style-type: none"> a. Winston Churchill is completely unsafe for pedestrians due to the extreme speed of cars. Trucks would make it worse. Instead link up more walking trails – not on the roads. (5) b. Enforce speed limits c. Four way stop sign Winston Churchill and Ballinafad and Halton 32 and Wellington 42. d. More off road walking trails. (3) 	<p>Ideas for parking</p> <ul style="list-style-type: none"> a. At the Badlands for safety reasons (2). b. At CVC area on Winston Churchill – off the road. c. Orb lands (south of Belfountain) combine with new development. <div style="border: 1px solid black; padding: 5px; margin-top: 10px;"> <p>() denotes number of time same comment noted</p> </div>
<p>Ideas for cyclists</p> <ul style="list-style-type: none"> a. No widening of roads for designated bike lane. b. Enforce no trucks. (3) c. Temporary police presence to force cyclists to obey the rules of the road (actually stopping at all stop signs). d. Why ride side by side – at night with limited lighting? (2) Try single file e. Make a shoulder for bikes somewhere. (2) 	<p>For motorcyclists</p> <ul style="list-style-type: none"> a. No noise, they're too loud. b. So is a lawn mower for longer time. c. Enforcement or elimination. d. This is the best area for motorcycles. Most are excellent drivers. Reprimand those that are not. (2) Same as vehicles and bikers.

What other ideas do you have?

- a. We do not want straight wide roads with no hills or hills cut way down. We live in this beautiful area because of its scenic value, its cultural values and natural heritage. These roads should have as little impact on the aforementioned. If you widen and lower the hills people will just speed more. Totally agree (3).
- b. Make Old Baseline Road a scenic road designation. No, don't do that!
- c. Four way stop sign at Winston Churchill and Ballinafad Road.
- d. Designate these roads as scenic.

Questions

1. What is the Region going to do to protect the natural heritage and the Niagara Escarpment from the impacts of proposed changes?
2. Why is this project being undertaken?
3. What is the problem being addressed? This seems to be a solution in search of a problem – a laundry list.
4. Why are we always trying to improve (urbanize) and existing utopian? (2)
5. What are you planning for the tourists?

Appendix B

COMMENTS/ QUESTIONS RAISED THROUGH DISCUSSION AFTER PRESENTATION

The following comments and questions were noted through the discussion:

Comments:

- We like Mississauga Road the way that it is now and do not agree that it needs to change. The roads are fine. There is no room for sidewalks. Bikes are on Mississauga Road now. McLaughlin Road would be a better place for bikes to be.
- We don't want to see the roads smoothed/flattened out. If you took out the hills it would change the character of the area dramatically.
- Would like to see speed limits reduced and what is there now enforced. Have to listen to cling on the brakes as people particularly truckers navigate the curves.
- Don't agree with assessment that there are drainage problems. Drive these roads all the time and do not see pooling of water or water staying on roads.
- The area is very quaint and we want it to stay that way. People come here on weekends to enjoy the Village. There is lots of traffic then but this shouldn't warrant widening or it will take away the uniqueness of the area.
- Don't want to see more cycles on these roads if it means that the roads are widened.
- Concern that this study will change the scenic designations for the roads in the area
- Many in the study area do not access to internet and mail is preferred as well as posting on the web site.
- Concerns about what may be coming with James Dick Property. Response from Steve Keen: not aware of a license for this property.
- Heritage of area is important.
- Infrared street lighting in Belfountain. Who put lights up – just done recently and it is very intrusive. Want these turned off.
- This is a special place and we like it the way that it is. This is not Brampton.
- Do not support urban looking roads.
- Do not support widening. Happy to learn that this is not going to occur here. Will there still need to be widening to accommodate land on either side for sidewalks or cycling.
- Growth is happening here and we need to ensure a balance on the roads so that all of the traffic doesn't end up on either one of Winston Churchill or Mississauga Road.
- Don't see the need for sidewalks. People can walk on the side of the road as they have been.

- We would like to see better pedestrian infrastructure – not an urban sidewalk but a better pathway for seniors and residents. Could have more walkability along the rail trail. Important to make it safe without changing the look of the area.

Questions:

- How will the entrance to the Orb property affect traffic patterns? Will this affect the commitment to not have lane widenings?
- Is the Niagara Escarpment involved?
- Has a problem statement been put together?
- Are the Community Working Groups closed meetings? Response: The public can observe the meetings.
- Is there a design standard that you are going to be looking at?
- Does the study include the town loads?
- On Winston Churchill on Peel side – are we looking at upgrading for trucks? Many large trucks on Winston Churchill today. North of Bush there is a shallow creek and Winston Churchill curves here. Some trees have been cleared on Winston Churchill.
- Will there be four lanes to accommodate truck traffic?
- Two years ago when there was a dig up the rubble was sold and dumped in areas that it shouldn't have been. Where will the rubble go? Concern noted that any rubble from roadworks must not be dumped in the natural and conservation areas and not within the village.
- Does the region own lands on both sides that are wider than the area that is paved today? Do you intend to collect more land? When anyone wants to do something, a strip of land is extracted from the owner. What do you intend to do with this land?
- Have traffic surveys been undertaken can we see the data? Response noted: Traffic surveys for all four roads will be available at the next public meeting next year.
- Many people are going southbound at peak times. Will the surveys take into account new homes being built in Erin and traffic from other parts going through this area? There are 600 more homes being built in Erin and 2/3 of the traffic goes along Winston Churchill with the other 1/3 through the Village of Belfountain. These roads are regional roads and they do carry regional thru traffic and local traffic. The surveys will address traffic coming from other areas.
- Will you have to widen if these other areas continue to grow and affect Belfountain? There is no desire to widen these roads.
- We have worked with the CVC on our property. What is their role in this study?

Appendix C

FEEDBACK FROM WORKBOOKS

Participant workbooks were provided to obtain written responses about experiences in the study areas and ideas for what could be improved. The following responses were noted. These are verbatim comments transcribed from the individual booklets.

PART 1: EXPERIENCES WITHIN THE STUDY AREA

1. How do you travel within the Study Area?

Table 1 – Mode of transportation that people identified

Single Occupant Vehicle	Car pooling	Cycling	Motorcycle	Truck	Other (added on workbooks)
30	3	10		2	Walk – 4 Horseback – 1

2. What transportation issues are you experiencing in the study area?

The following responses were provided through the workbook. Each number represents a different individual's response. Not everyone responded to all questions.

Table 2 - Transportation issues identified in the study area

1. As residents of Mississauga Road, our main concern is with excessive traffic noise and speeding (cars and motorcycles) on Mississauga Road.
2. Area that is paved on Winston Churchill Boulevard is very uneven because of frost heaves, excess water in base, poor base.
3. None, we can only comment on Olde Base Line Road, which we use every day.
4. I would like to be able to walk around more.
5. Very rough, uneven areas, poor sides of pavement along Mississauga Road and Winston Churchill Boulevard. Bush Street area when paved was spread by a grader – not paving machine. The distance from centerline to respective fences is so “disproportionate”. Take a measure! Soon the road and all the drivers will be in our yard or hung up on our fences. It has already happened many times. You built up the road and don't cut the grass (as we used to but can't now). Then you fill the ditches full of gravel from the road. The new road should be many feet to the south of the present location.
6. Excessive traffic and speed beyond limits. Most traffic heads down Winston Churchill Boulevard approximately 5 of every 7 heading on Bush Street turn south.
7. There are a lot of cyclists who don't “share the road”. It's a wonderful concept that doesn't actually work on these roads. You can't ride a bike five abreast on Mississauga Road, hold up traffic and be safe – for the cyclists or the drivers.
8. None.
9. Very busy – difficulty entering southbound on Mississauga Road from Caledon Mountain

Drive. More traffic (i.e. Orb Development and Erin growth) will seriously impact the quality of life in the hamlet in Belfountain.

10. Cyclists travelling 2 abreast on Mississauga Road.
11. The lack of bicycle lanes makes it unsafe for bicyclists and slows down for motorists.
12. Speeding.
13. Trucks on roads designated “no trucks”.
14. Poor sight lines on Winston Churchill Boulevard just south of Ballinafad Road prevents serious safety hazard – the site of numerous accidents.
15. Cycling on Mississauga Road a challenge, bike lanes could help. Speeding vehicles on Winston Church Boulevard a problem for pedestrians and cyclists.
16. Speeding by cars and motorcycles. Motorcycle racing after midnight. Passing on curves.
17. At the moment, in my case, just speeding vehicles and bicyclists.
18. There is too much speed and truck traffic on Winston Churchill Boulevard and Olde Base Line Road. This is a very special area of Ontario – very scenic, historical and importance of the Niagara Escarpment must be our highest priority. Low impact roads on green infrastructure.
19. Parking congestion in Belfountain on weekends during high tourist periods. Makes walking in the village very unsafe. Need protected walking areas. Need designated parking areas.
20. More trucks on Winston Churchill Boulevard which theoretically are not allowed.
21. Speeding on Winston Churchill Boulevard.
22. Cycling on Winston Churchill Boulevard and Mississauga Road would be safer and easier if the shoulders were paved, especially if gravel trucks are going by.
23. None.
24. Speed and noise of many vehicles (i.e. mostly motorcycles but many cars and trucks too).
25. Traffic load way too high. Speeds are wildly excessive.
26. Truck ban is not enforced. Too many speeders.

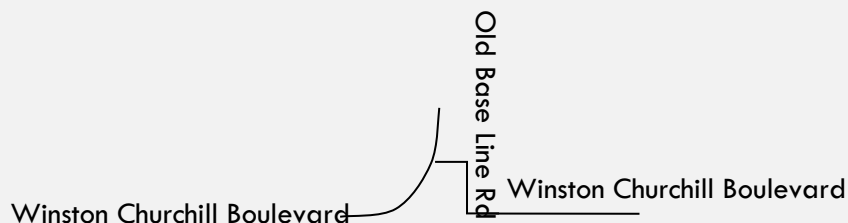
PART 2: HOW DO YOU ENVISION THESE ROADWAYS IN YOUR COMMUNITY?

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

1. What transportation improvements would you like to see considered?

Table 3 - Transportation Improvements people would like to see considered

1. The road conditions seem generally fine – speed limits are too high for safe use though.
2. At Winston Churchill Boulevard and Olde Base Line Road, purchase a portion of the northeast corner and route traffic safely across Old Base Line Road.



3. Road shoulder should be improved.
4. Improve sight line at Ballinafad Sideroad (I know this is south of area). Wider area (bike lanes or extended pavement past white / yellow lines) to allow safety for non-motorised users. Purchase land for parking so visitors may not park on or half on the road (as if they are the only users). Our roads are used by many!

5. Police monitoring speed limits. Volume of traffic during rush hours requires patience to turn onto Mississauga Road – for this reason I would be concerned about the plan for 72 house subdivision which would add considerable to normal volume increase year to year of through traffic.
6. Bike “passing” lanes (i.e. extended shoulder) on blind hills so cyclists can move over and allow vehicles to pass. (There is an enforcement component to this issue as well to “encourage” cyclists to travel single file and to the right.)
7. Again an issue of quantity and speed.
8. Traffic calming entering Belfountain. Use the road grips like they have on Winston Churchill Boulevard approaching Mayfield – instead of speed bumps.
9. None or a reduction in vehicular traffic.
10. I fear that the “Build it and they will come” agage may erreversibly destroy the character of the area.
11. Ban cyclists on Mississauga Road. Put a roundabout at Winston Churchill and Bush, at Mississauga Road and Olde Base Line Road and perhaps one at Olde Base Line Road and Winston Churchill Boulevard.
12. Improve base of road.
13. Bicycle lanes added.
14. Non – these are country roads and quite passable for local residents. Improving the roads will lead to more people wanting to move into the area. Sprawl is unacceptable and constant growth unsustainable.
15. Speed control. No trucks on all roads – enforce it.
16. No trucks! “Share the Road” signs. “Scenic” designation for this area.
17. Pave Winston Churchill Boulevard and remove hill just south of Ballinafad Road.
18. Reduce speed. Speed traps regularly on Winston Churchill Boulevard, not just Olde Base Line Road.
19. Improve roadway conditions.
20. Cyclist’s pathway. Scenic designation – sensitive design. No gravel truck traffic.
21. Need to slow traffic coming north on Mississauga Road around the bend into Belfountain – i.e. speed bumps. Path space between Caledon Mountain Road and town for pedestrian traffic (not a typical sidewalk but perhaps some more natural stone, etc. to preserve rural country setting). Kids could bike to school; walk to school, etc. – School to conservation area.
22. Less speeding. No expansion of streetlights.
23. Enforce speed limits. Enforce stop signs. Regular police presence on busy weekends. Speed bumps.
24. Double line to discourage passing on hills. “No passing” signs.
25. Roundabouts? They work very well in other areas and are so easy to navigate.
26. Paving of Winston Churchill Boulevard south of Olde Base Line Road to Terra Cotta, therefore less traffic on Olde Base Line Road. Increase police traffic presence. Lower speed limits. 50-60 kph on subject roads.
27. Increased police patrol.
28. Improve road storm runoff – keep it clean. Designated walking areas.
29. Slower speeds. No truck traffic.
30. Nothing wrong with the roads.
31. Add bike lanes.
32. Enforce speed limits and truck bans. Put more stop signs at intersections to control speeds. Put speed bumps in posted 60 km/h areas.

33. Make safe the extremely dangerous crossing of Ballinafad Road with continuation of Wellington-Halton 12 and Winston Churchill Boulevard due extremely short sight line south. Ideas 1) Remove hill south of the E/W roads on Winston Churchill Boulevard; 2) Four way stop sign.

2. What existing design features would you like to see maintained?

Table 4 – Existing design features that people would like to see maintained

1. Maintain rural / farm feel with fences, walls, trees, etc.
2. Existing rolling hills and trees naturally help to keep speed down.
3. Hills, bends and trees. These are what define the area and make the roads special, which in turn calms traffic.
4. Rural hilly character of a beautiful country road. Rural character and environmental and historical heritage in the corner community garden in Belfountain.
5. Street lights with “character” for safety and appeal. Large old trees, good fences – with character. Hills and curves – they have been here forever – this is why people come to and love our area.
6. When purchasing and building a home on Mississauga Road I was happy to comply with all requests from CVC and Escarpment Commission to maintain and support the health of the environment i.e. forests, wetlands, and streams before building.
7. Rural character: mature trees, ditch profile, natural vegetation, cedar rail, heritage fences.
8. Road is generally designed and maintained for area.
9. All of them – if we need safe walkways for pedestrians – build walking trails from the village to the school. No bike lanes. Keep the heritage walls, split rail fences, etc. We live in a heritage village in a turn of the 20th century home for a reason.
10. All of them, narrow hilly winding roads with steep ditches. No changes at all no sidewalks, no street lamps.
11. Rural quality – 2 lanes and perhaps designated bike lane. No sidewalks or urban type infrastructure.
12. Keep all hills and grades – do not straighten or widen or make any cuts into the sides of the right-of-way for Mississauga Road. Same topography.
13. Parkette at Bust Street and Mississauga Road.
14. Leave the wonderful hills on Mississauga Road. No street lights.
15. Heritage fences, old trees, hills, curves
16. Hills, valleys and two lanes – nature’s corridor. No sidewalks – low lighting
17. Roadside fences (split rail, rock, etc.). Large old trees that line roadway. Hills and curves.
18. The hilly nature of the road – very scenic. Ditches – no underground sewers or curbs.
19. Re-build of stone fences for a reasonable distance from property entrances.
20. Hills
21. Rural Road cross section – no curbs and gutters. Maintain rolling hills and stone fences.
22. We like the hilly scenic character of the roads. Don’t cut down the hills or widen roads.
23. Parkette. Maintain rural nature of road (no curbs, sidewalks). Preserve wetlands, water courses and other ecologically sensitive areas.

3. What existing design features would you like to see maintained?

Table 5 - Design features that people think would enhance the community character?

1. More fences, walls, trees, etc.
2. A lookout on the bend / corner on the south end of Belfountain on Mississauga Road.
3. Saving the original fence lines stone and cedar rail that exist and replacing the row of beautiful maples along the road that I had to remove as they had deteriorated over more than a hundred years – I am concerned that enhancing the road would take precedence over the environmental concerns that are very important to everyone!
4. Could you continue funding heritage stone fences as part of project?
5. Leave it for community to enjoy with no more to increase traffic. Time to share with other arteries north and south!
6. Leave it alone.
7. It's hard to think of what changes could be made that would not degrade the unique and scenic nature of this heritage hamlet.
8. No additional “adornment” to encourage more traffic / tourism. Less is definitely more.
9. “Quaint” street lighting in the village.
10. Bicycle lanes.
11. Bicycle lanes. (is an important tourist component in village of Belfountain).
12. I don't think any needed features are required to enhance the community. The area has changed drastically over the years and I would like it to stay the same.
13. 3-way stop sign at Winston Churchill Boulevard and Olde Base Line Road. 4-way stop sign at Winston Churchill Boulevard and Ballinafad Road.
14. Different walkway options
15. Road which don't do seasonal buckling. Proper repairing (no bumps) when resurfacing.
16. No sidewalks.
17. Do not add sidewalks. Improve lighting at intersections.
18. Re-build of stone fences for a reasonable distance from property entrances.
19. The roads are just fine!
20. Keep it simple – model design based on current character – vistas, aesthetics.
21. I think the whole EA rectangle should be made a scenic area for roads so the character is maintained.
22. Leave it alone – keep it quaint.
23. Maintain rural nature of road (no curbs, sidewalks). Preserve wetlands, water courses and other ecologically sensitive areas.

4. What would you like to see within the corridors?

Table 6 – What people would like to see within the corridors

1. A focus on anything that can enhance the rural charm of the area – stone wall restoration, farm fence restoration, tree plantings, reducing speed limits, etc., etc.
2. Intersection Control. Ballinafad Side Road and Winston Churchill Boulevard, 2-way stop control East-West is very dangerous. If not changed to 4-way stop, it should be at least 3-way, East-West and North.
3. Information for local owners should be sent by snail mail, not just email as many don't use email.
4. Heritage fence lines – Winston Churchill Boulevard. Both sides of Winston Churchill Boulevard have the same stone fences yet two years ago only the east side was called heritage and plans were to take road allowance from the West side (Wellington). This has since been changed, but are both sides now called Heritage fence lines? If not, why not, both were built at the same time.
5. The roadway corridors should remain pretty much as they are. These are country roads with character, popular with cyclists and weekend drivers because they offer a difference from most other roads in that they are hilly. It is wonderful to crest a hill and get an unexpected view.
6. Wildlife underpasses for salamanders and turtles. Mature trees. Split rail fences.
7. Maintain the beautiful stone fences, many well-kept, wood, wire, and huge old trees.
8. Rural character – mature trees, ditch profile, natural vegetation, cedar rail and heritage fences.
9. Preservation
10. Take a look around you – then leave it alone.
11. Plant new maples along Mississauga Road to replace dying and dead maple trees.
12. Maintain the current form and topography. Do not shave off hill tops.
13. Native plantings where plantings are used.
14. Please don't change these corridors, especially as they reflect the local environment / character of the community. Don't add street lights, sidewalks. No sidewalks or street lights.
15. Ditches, no curbs, single lane in each direction, one bicycle lane (not two) for bi-directional bicycle travel
16. no need to widen existing roads, pave any dirt road sections
17. Just as they are now, rolling, curving, rural!
18. Maintain the status quo.
19. More trees. Keep out invasive species – roadside ditches.
20. Lots of hills and tree lined roads – historical fences – rural scenic countryside is very important. Not wide
21. Lovely streams and springs. Historical markings.
22. Maintain existing rural character and nature.

5. What ideas do you have for improving conditions for pedestrians?

Table 7 - Ideas for improving conditions for pedestrians

1. Don't typically travel these roads on foot but wider shoulders and reduced speed limits should help here.
2. More police control on Winston Churchill Boulevard below Olde Base Line Road. Gravel road and they pass our house doing 80 to 100 km/h. The police are welcome to use our land as a sitting spot for speed control.
3. Make more parks and walking trails. Could use Orb Lands.
4. Make area a bit away from actual road – whether dirt, gravel or sidewalk. Reasonable speed limits – enforced. Sides are wide enough for our runners and walkers. Walking trails. At most – an improved narrow walkway – no curbs, etc.
5. Provide parking in the village of Belfountain so there is room for the pedestrians to use existing wide shoulders.
6. No sidewalks. It's not safe to walk on country roads –should walk in parks/within villages.
7. No cement sidewalk but more like trail walkway for students to go from school to park.
8. Please know that there are only a few times when there are enough pedestrians to pose a problem. Those pedestrians need to know that they must walk on the shoulders – they're in the country now.
9. Reduce speed. Look both ways.
10. Path space between Caledon Mountain Road and town for pedestrian traffic (not a typical sidewalk but perhaps some more natural stone, etc. to preserve rural country setting). Kids could bike to school; walk to school, etc. – School to conservation area. Separation of vehicles from pedestrians. Speed bump into Belfountain.
11. In Belfountain, the concrete wall should have sidewalk for pedestrians.
12. Enforce 40 kph speed limit. Enforce stop signs.
13. Sorry, I cannot comment because I don't live in this town.
14. Designated vehicle parking. Traffic calming. Increased traffic police presence on weekends. One side of road for pedestrians, one side for parking.
15. Encourage pedestrians to use Bruce Trail.
16. They only walk in the immediate village – put sidewalks around the stores.

6. What ideas do you have for accommodating parking?

Table 8 – Ideas for accommodating parking

1. We feel there is sufficient parking already in the area to accommodate visitors. This is really only a concern 2 to 3 weekends a year in the autumn when people come to view the leaves.
2. Parking at south end of Belfountain on Orb Lands.
3. Buy the farm with old silo (or a portion of) for parking for sight-seeing folk, bikers, walkers, hikers, etc. (not in study area – parking for Badland viewing)
4. There is parking at the conservation that is adequate.
5. Not an issue.
6. As is. Policing of violation and excessive speed and noise.
7. Buy property from Orb at south end of Belfountain to make a public parking lot for tourists.
8. Vacant lot at corner of Mississauga Road and River (kitty corner to River Rd) i.e. beside the shops.
9. Weekend – invite tourists to park at the school and walk into the village.
10. Definite problem at the “Bad Lands” – buy property on north side for proper parking lot?
11. It’s okay now.
12. How can you stop people going to the park from parking in town for 6 to 8 hours?
13. Town to buy property within walking distance of Belfountain to provide parking for day trippers.
14. Create pay parking area outside of village.
15. At the Badlands.
16. Allow on both sides of the road. Direct them to park in the school.
17. Wide shoulders. The fall is a short season; do we really have to block all the shoulders?

7. What ideas do you have for improving conditions for cyclists?

Table 9 - Ideas for improving conditions for cyclists

1. Significant reduction in allowable speed for cars will improve conditions for cyclists and reduce traffic noise too. Win-win situation.
2. Cycle clubs from GTA use our roads with little respect for others on the road yet our local taxes pay for these roads, not theirs. Why improve on cycle conditions, at my cost?
3. Improve road shoulders.
4. Shoulders.
5. No cyclists after dark – especially when wearing dark clothing, no lights, etc. Reasonable speed limits – enforced.
6. Bike “passing” lanes (i.e. extended shoulder) on blind hills so cyclists can move over and allow vehicles to pass. (There is an enforcement component to this issue as well to “encourage” cyclists to travel single file and to the right.)
7. It is not particularly safe for cyclists and motorists to share the roads around Belfountain.
8. Not an issue. Cycle on a safe roadway and don’t hog the road.
9. Do not have “add ons” to make this more of a tourist mecca. We are on the sup of mega development all around and we need to preserve something!
10. Ban them from Mississauga Road. Create new route elsewhere for them. Mississauga Road cannot be made safe for cyclists without changing the width / topography which most

Belfountain residents do not want.

11. Bicycle paths.
12. Bike lanes.
13. No bike lanes. Before accommodating cyclists, we should ensure that they obey the rules of the road – many do not pay any attention to stop signs.
14. Signage stating “Ride behind the other rider” as they usually ride in two’s or three’s beside each other.
15. “Share the Road” signs.
16. Bicycling lanes on Mississauga Road as it is well used by cyclists.
17. Most of the group cycling is bike clubs from outside the area.
18. Cycle path.
19. Small bike lane (one side of road only (into Belfountain and out.
20. More share the road signage.
21. Stop cyclists from traveling 2 to 3 and 4 abreast. Enforce stop signs.
22. I understand that cyclists navigate to this area because of the hills and curves in our environment. However, each year I find that they tend to overtake the roads, having difficulties maintaining the rules of the roadway. There have been near hits when they cut cars off. I especially do not care for having to go out of my way to get home because they have a function or a ride. I’m not mean. Just concerned.
23. Charge user fees or license cyclists.
24. Creating a bike path but NOT widening the road
25. Designated bike route through village.
26. Already have bike paths along the train tracks. And we have a bike lane that was built about 6 years ago.
27. Add bike lanes.
28. Bike lanes IF it does not require road widening.
29. Educate them re rules of road.

8. What ideas do you have for accommodating motorcyclists?

Table 10 – Ideas for accommodating motorcyclists

1. We do not wish to accommodate motorcyclists. The loud noise they create for our Mississauga Road property is very unpleasant. We have added water features and noise berms to try to reduce the impact and it is still too loud and takes away from our enjoyment of our property.
2. Accept them – they spend \$ the stores would not see without their support. Motorcycles have been here and on these roads for more years than most Belfountainites. As with vehicles – reprimand those not obeying rules of road. They make a shorter time of “noise” going past than lawn mowers or chain saws, etc.
3. Weekends they enjoy the area.
4. Fewer and less noisy.
5. Enforce noise bylaw so locals will feel welcoming to motorcyclists.
6. They seem to be fine.
7. No problem.
8. Designated parking. Low noise.
9. It’s okay now. But need speed enforcement, need nighttime racing reduction.
10. Don’t encourage more bikers.
11. Treated as vehicle traffic.

12. Maintain current character of roads.
13. Same as cars.

9. Others comments about the study

The following are additional comment noted in the workbooks. Each number represents a different individual's comment.

1. Our family has owned this property since the 1960s when I was a child we used to run back and forth the road to visit our neighbours since there was very little traffic to be concerned with. Now the traffic is so heavy we have had to put in a gate to ensure our children do not go near the road. The properties on Mississauga Road tend to be close to the road and the road should be treated more as a residential road than a highway. If the speed limit was reduced to 50 km/h, most safety concerns regarding sightlines and cyclist safety would be eliminated.
2. Below Olde Base Line Road on Winston Churchill (gravel) – It is my understanding that they re-ditched this area to allow water to be pulled from the road, eliminating potholes, etc. When? Why ditch? I was told the ditches were always there and they were just re-doing them. WRONG. I've lived here since 1952 and there were never ditches there. My property now gets excessive water in front paddock. I can't cut the grass along the road (as I have for years) because the ditch blocks the area and the banks are now too steep. Cars continually are being pulled out of the ditches yet we never had this problem before. Old saying, "if it isn't broke, don't fix it".
3. Our property is fronted at Olde Base Line Road with large majestic oak trees which we would like protected.
4. What are you planning for all the tourists? Use dark sky lighting. Spray for invasive species phragmites in newly constructed road ditches. Clean construction equipment to stop spread of invasive species phragmites (see invasive species at CVC). Put signage in Belfountain for the environment. How are you going to collaborate with the new housing development on the south end of Belfountain? Are they putting in a road between Mississauga Road and Shaws Creek Road?
5. Thankfully people travel from everywhere to see our beautiful, unique area and spend \$ here. Our roads must be maintained, upgraded but while trying to keep with community spirit and heritage.
6. Are there opportunities to help facilitate internet service providers (Bell, Rogers, etc.) to extend high speed internet service as part of this project? This could be a significant community benefit.
7. This seems to be a way to spend lots of money trying to "fix" what should be left alone. We live in a home built before the 20th century – with trees that are very old and very beautiful. We moved here to live in a small village and raise our daughter in a beautiful part of Ontario. If we wanted our tax money put towards developments that support quarries or Brampton overflow of traffic, we would have stayed in Toronto.
8. An attempt to pursue an agenda of change where none is required through and "inclusive" search for a non-existent problem. Why do I feel that we are being conned. The 30m easement right of way is very telling?
9. There will be many pressures – housing, gravel, tourism. Progress does not mean growth; presumably we have become more enlightened about the fragile nature of this environment.

10. When the Forks of Credit reno happened, a thousand truckloads of rubble were dumped (and a property owner was paid a fee) at the west end of Belfountain, burying a valley. Please do not repeat this atrocity.
11. Although part of the study, natural heritage is not mentioned in this booklet with specific questions. I would like to know what if anything is going to be done with the current road drainage that is funneled into the creeks and wetlands in Belfountain without any treatment (quantity or quality). In some cases asphalt spillways into the creeks exist which contributes to the degradation of the West Credit and its tributaries in the study area. I also would like to see wildlife passage built into the roads as I often see road kill (outside of the village).
12. Does the traffic study of these roads really reflect the “need” for replacing / redesigning them? (Especially in times of fiscal restraint) Make stats on above available, easily! There is NO need for sidewalks or street lights. Much greater publicity needed to give opportunity for more participation in the process. These meetings have not been widely publicized.
13. Since previous study’s recommendations, not improvements, what is the probability that recommendations from this study will be implemented? i.e. lots of studies, no action – no money.
14. Also left out part of Winston Churchill Boulevard that is not paved and is a safety risk when it rains, snows as it is well used by fast moving through traffic. I like walking and recently have avoided Winston Churchill Boulevard and Olde Base Line Road because of the significant increase in fast traffic – trails or designated pedestrian walkways (trails) would be appreciated in some areas. I am in favour of bicycle lanes on Mississauga Road – it is already used by cyclists and should be made safer.
15. Winston Churchill Boulevard is a series of other roads loosely commented. The southern end is actually 6 Line Caledon / 11 Line Erin. It dead ends in Terra Cotta and basically ends at Bush Street. It went from a back road to a major road.
16. Pave the south portion of Winston Churchill Boulevard to King Street.
17. Designate the entire study area as scenic. The end result is that I do not want these roads to be Bramptonized! There is no character to Brampton roads. These roads help define the character of Belfountain and area.
18. This is the country, not the city – leave it that way!
19. At a time when budgets are strained, think of who is going to pay for unwanted changes. WE ARE!!!
20. Why do meetings have such a delayed planning? If it was a business, things wouldn’t take so long. I can understand why it is important to cover all issues but years seem ridiculous if needs are not met in good time. Bicycle lanes seem to be an important safety issue also drains and ditches.
21. Will the phone companies be involved in any of the planning? They are constantly doing work along Winston Churchill Boulevard. Are any of the above ground cables going underground? A shame doing all this planning when technology is advancing so quickly and may need to disrupt the countryside that has limits to the world web.
22. Don’t change what we have! My family lives here because of the way our neighbourhood is!!!!
23. Property boundary of 17263 Old Main is incorrectly surveyed – title boundary is larger than what the iron bars currently depict. Please re-survey based on title / deed (in land registry office).

24. XX is applying for a new gravel pit north of Bush Street on Winston Churchill Boulevard. How will you ensure the trucks do not head south down Winston Churchill Boulevard and enter this EA area?
25. It's very important to protect the heritage fences and very old maple trees along the roadside within the Region's right-of-way – do not destroy these!
26. Keep heritage fences and trees, keep hills, and keep single streetlights marking intersections at night.
27. You are doing this for outsiders and we don't need the higher taxes this will bring. Leave us be; Do not change the topography.
28. Please no more speed bumps like Forks of Credit.

Mississauga Road, Old Main Street, Bush Street, Olde Base Line Road and Winston Churchill Boulevard Environmental Assessment

Frequently Asked Questions

What is the study area for this project?

The study area consists of Mississauga Road, Old Main Street, Bush Street, Olde Base Line Road and Winston Churchill Boulevard.

What is the purpose of this project?

The purpose of the Environmental Assessment (EA) study is to develop a context sensitive design solution for Region of Peel owned roadways in the study area that will improve pavement conditions, drainage and sightlines, meet the needs of pedestrians and cyclists, enhance safety for all road users and maintain the character of the community.

What is context sensitive solution?

Context sensitive solution involves designing local land use projects so that they meet the needs of the road users and the environment while respecting the local land use context.

"Context sensitive solutions (CSS) is a collaborative, interdisciplinary approach that involves all stakeholders to develop a transportation facility that fits its physical setting and preserves scenic, aesthetic, historic and environmental resources, while maintaining safety and mobility. CSS is an approach that considers the total context within which a transportation improvement project will exist."
Federal Highway Administration (FHWA)

Are the roads going to be widened in this study area?

No. The widening of the roads is not considered for this project. This is a road rehabilitation project which will include enhancing the safety of all road users while maintaining the character of the community. The Region of Peel's Long Range Transportation Plan, 2012 has not identified the need to widen these roads. Our transportation needs assessment has also concluded that there is no need for road widening along the roadways in the study area.

How did the current project come about?

An EA study undertaken between 1992 -1998 was put on hold due to the Rockfort Quarry application. In 2009, a new Schedule C EA study commenced for Mississauga Road, Bush Street, and Old Main Street; based on input from the public, the study area was expanded in 2012 to include Winston Churchill Boulevard and Olde Base Line Road to provide a more comprehensive review of the road network.

Are the new subdivisions of Erin and Belfountain being considered in the study?

Inquiries regarding the status of development applications should be addressed to the Town of Caledon and County of Wellington.

What are the criteria for becoming a CWG member? Do you have to live in the area?

Community Working Group (CWG) members were required to submit a letter or email of interest summarizing who they represent and information about themselves. The deadline for submissions was November 14, 2012. The project team reviewed the submissions and CWG members were selected to provide a broad cross-section of interests, various skills, and knowledge. All our CWG members currently either live or work within the study area.

How many times will the CWG meet during the course of the EA?

CWG meetings were held on October 23, 2012 and April 4, 2013. Another meeting will be scheduled prior to the Public Information Centre #2 anticipated for winter 2013. Additional meetings with the CWG will be added throughout the study, as required.

What perspectives are represented on the CWG?

The CWG includes new and long time residents, business owners and employees in the study area. With the varied interests represented on the CWG, the views of the community are represented and will help us in developing a solution for the study.

When will the EA be completed?

The EA is tentatively scheduled to be completed and filed with the Ministry of Environment for review in Spring 2014.

When will the project go to construction? How long will construction take?

The implementation plan for the roadways in the study area is proposed in two stages:

1. 2017 for Winston Churchill Boulevard and Olde Base Line Road; and,
2. 2019 for Mississauga Road, Bush Street, and Old Main Street.

These timelines are subject to change pending the Region of Peel's annual capital budget process.

Construction for each phase will take approximately 2-3 years.

What is the cost of this project?

The budget for this EA study is approximately \$980,000.

For further information please contact:

Gino Dela Cruz, P.Eng.
Project Manager, Region of Peel
Transportation Division
10 Peel Centre Drive, Suite B
Brampton, ON L6T 3B9
Tel: 905-791-7800 x 7805
Email: gino.delacruz@peelregion.ca

Asha Saddi
Technical Analyst, Region of Peel
Transportation Division
10 Peel Centre Drive, Suite B
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Public Information Centre #1
May 9, 2013

NOTICE OF PUBLIC INFORMATION CENTRE #1 MISSISSAUGA ROAD, OLD MAIN STREET, BUSH STREET, WINSTON CHURCHILL BOULEVARD AND OLDE BASE LINE ROAD

The Project

The Region of Peel is undertaking a Schedule 'C' Municipal Class Environmental Assessment (EA) to develop a road design that addresses safety and pavement condition deficiencies on Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road. The approximate limits of the project area are illustrated on the map.

The EA will review and recommend solutions to address:

- road safety deficiencies
- sight lines that do not meet standards
- pavement condition deficiencies
- road drainage problems
- parking
- pedestrian and cycling needs

This EA will not be considering road widening or increasing the number of lanes and will provide a solution that meets the needs of all road users and maintains the rural character of the community.

The Process

The Class EA process includes:

- public and agency consultation
- an evaluation of road improvement alternatives
- an assessment of the possible environmental effects of the improvements
- the identification of reasonable means to mitigate any adverse impacts

Public Information Centre

Two Public Information Centres (PICs) are planned for this study. The first PIC will consist of an Open House followed by a presentation and facilitated discussion. The PIC will provide an overview of what we have heard and the technical work completed to date. Please join us at this PIC to share your opinions on:

- Draft Problem Statement
- Preliminary Alternative Solutions
- Preliminary Evaluation Criteria

The Project Team members will be present to answer questions and discuss the next steps of the study. The first PIC is scheduled for:

Date: Thursday, May 9, 2013
Time: Open House begins at 6:30 p.m.
Presentation and Discussion 7:00 p.m. - 9:00 p.m.
Location: Belfountain Public School, 17247 Shaws Creek Road, Caledon

Comments and Information

Please visit our website: <http://www.peelregion.ca/pw/roads/enviro-assess/mississauga-road-bush.htm> for updates on this project. Comments and information regarding the study are being collected to assist the Region of Peel in meeting the requirements of the EA Act. This material will be maintained on file for use during the project and may be included in project documentation.

To provide comments or request additional information about this project, please contact any of the following team members:

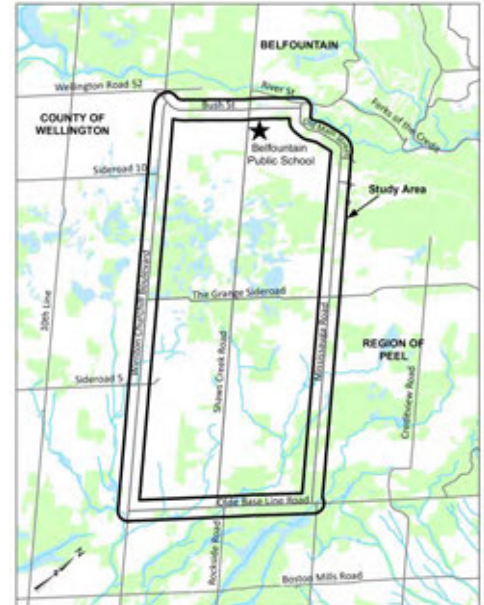
Gino Dela Cruz
Project Manager, Region of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, ON L6T 4B9
Tel: 905-791-7800 ext. 7805
Fax: 905-791-1442
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Asha Saddi
Technical Analyst, Region of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, ON L6T 4B9
Tel: 905-791-7800 ext. 7794
Fax: 905-791-1442
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Tyrone Gan
Project Manager, HDR
100 York Boulevard, Suite 300
Richmond Hill, ON L4B 1J8
Tel: 289-695-4622
Fax: 905-882-1557
Email: Tyrone.Gan@hdrinc.com

The Region of Peel is committed to ensure that all Regional services, programs and facilities are inclusive and accessible for persons with disabilities. Please contact the Project Manager if you need any disability accommodations to participate in the PIC.

This notice first issued April 24, 2013



**Mississauga Road, Olde Base Line Road, Bush
Street, Old Main Street, and Winston
Churchill Boulevard Class EA**

Public Information Centre #1

Thursday, May 9, 2013.
Belfountain Public School



Welcome

- Please sign in and take a feedback form
- If you have any questions our team is available to help you
- Place your completed feedback form in the Comment Box, or send it to:

Gino Dela Cruz

Project Manager

Gino.DelaCruz@peelregion.ca

Asha Saddi

Technical Analyst

asha.saddi@peelregion.ca

by Thursday, May 23, 2013.

Strategic Goods Movement Network (SGMN)



Approved by Council May 9th, 2013

The long term network map will be reviewed and updated every 5 years.

The SGMN identifies the following roads in the study area as a Primary Truck Route:

- Winston Churchill Blvd.
- Olde Base Line Rd.

From a network perspective,

- Winston Churchill Blvd. provides north-south connectivity and
- Olde Base Line Rd. avoids truck traffic through Belfountain & Terra Cotta and provides connections to Mississauga Rd. and King St.

The EA process will be used to further investigate the feasibility of these road sections within the recommended network.

Road Characterization Study (RCS)

Road Character Map



Approved by Council May 9th, 2013

The RCS identifies the following roads in the study area as a **Rural Road**:

- Mississauga Road
- Bush Street
- Winston Churchill Boulevard
- Olde Base Line Road

And Old Main Street through Belfountain as a **Rural Main Street**

The RCS supports the EA study's mandate to take balanced approach and address the deficiencies in the context of the rural character of the area.

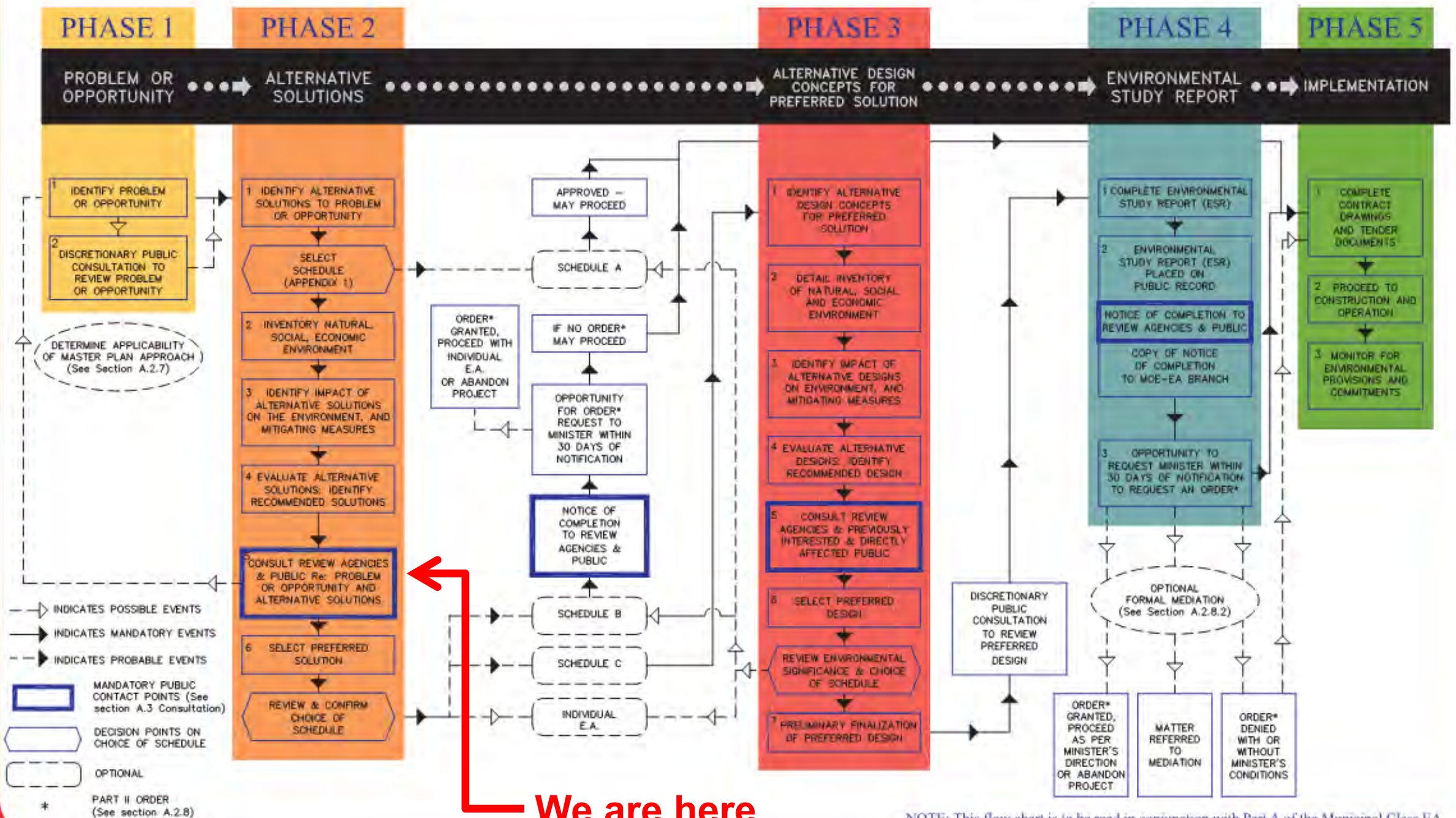
Purpose of PIC #1

The purpose of this Public Information Centre (PIC) is to:

- Provide a project update on
 - What has been done to date
 - What we have heard
- Present a summary of Technical Work
- Share with you the
 - Draft Problem Statement
 - Guiding Principles
 - Preliminary Alternative Solutions
 - Preliminary Evaluation Criteria
- Discuss Next Steps
- Ask for your input

Municipal Class EA Process

MUNICIPAL CLASS EA PLANNING AND DESIGN PROCESS



NOTE: This flow chart is to be read in conjunction with Part A of the Municipal Class EA

Beyond the Municipal Class EA Process

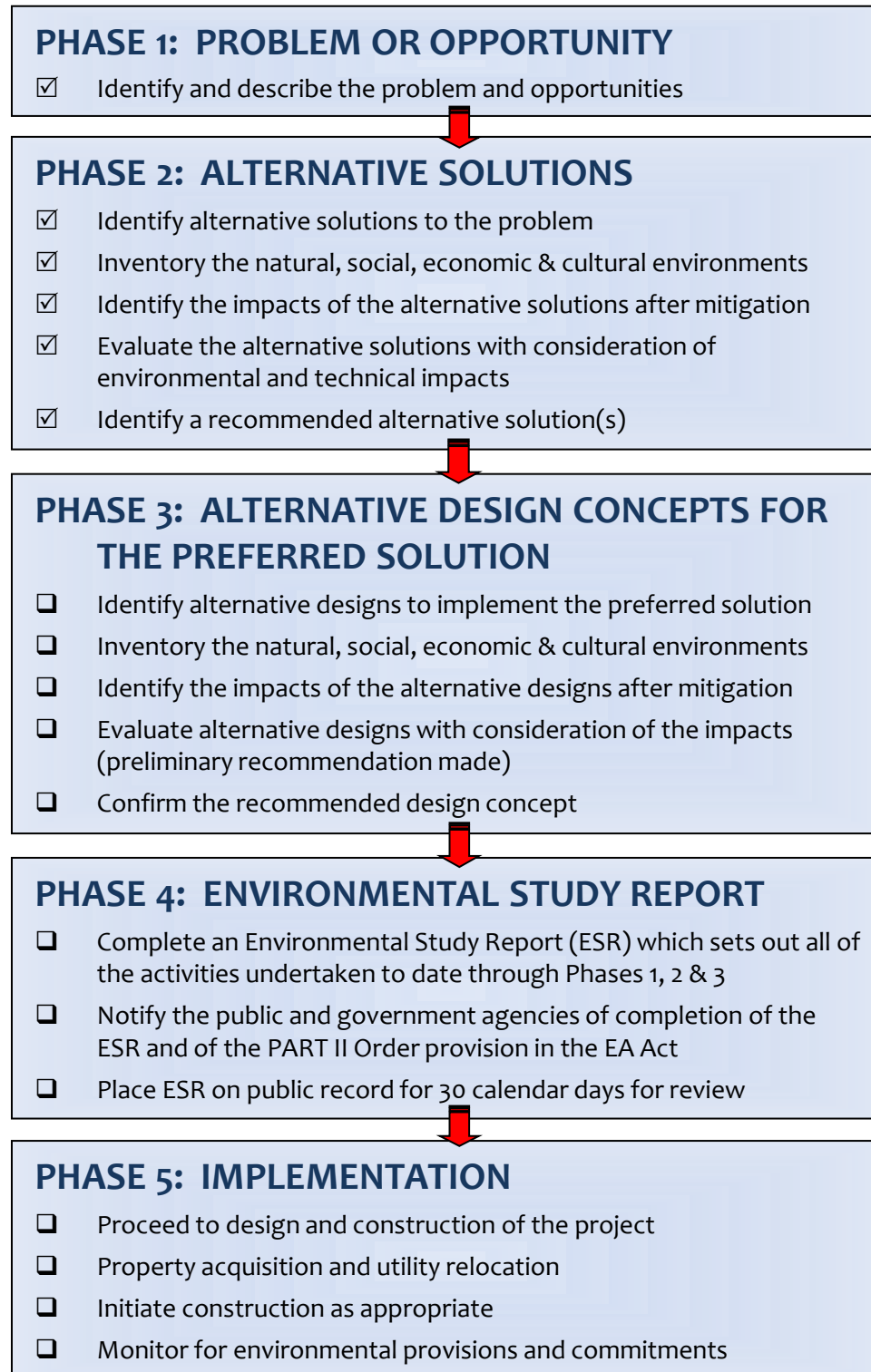
Additional Points of Public Contact

CWG Meeting Fall 2012 →
Open House Fall 2012 →

CWG Meeting Spring 2013 →

CWG Meeting Fall 2013 →

Phases



Mandated Points of Public Contact

Notice of Study Commencement

PIC#1 Spring 2013

- Needs and Justification
- Planning Alternative Solutions
- Evaluation of Planning Alternative Solutions
- Preliminary Recommended Solution

PIC#2 Fall 2013

- Alternative designs for the preferred solution
- Evaluation of alternative design concepts
- Preliminary recommended design concept

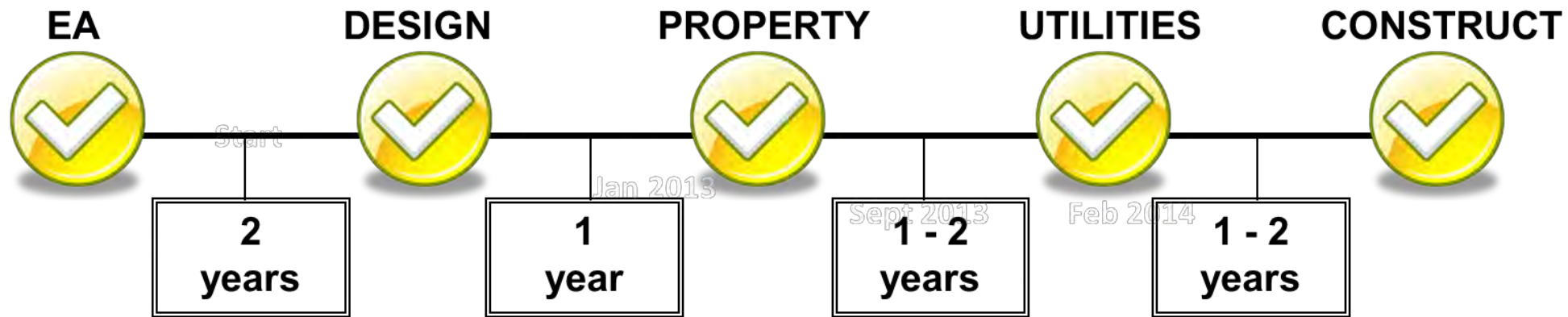
Notice of Study Completion and Filing the ESR



← We are here

In addition to the mandated points of public contact, the Region has chosen to organize a **Community Working Group (CWG)** for the study.

Process from EA Study to Construction



Typical Timeline from EA to Construction is 5 to 7 years

This EA will include 30% design, which consists of:

- Design Criteria
- Topographic Survey
- Cross-sections
- Preliminary Design Plan

Typical considerations for road design include:

Regional Standards

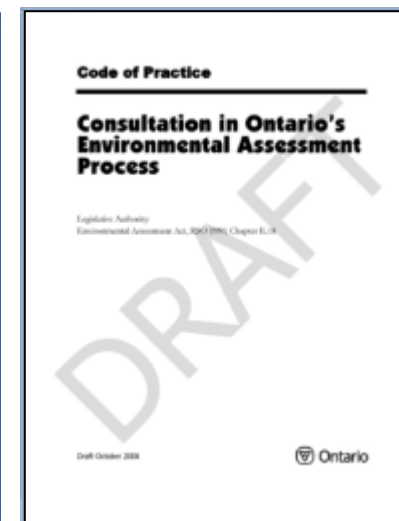
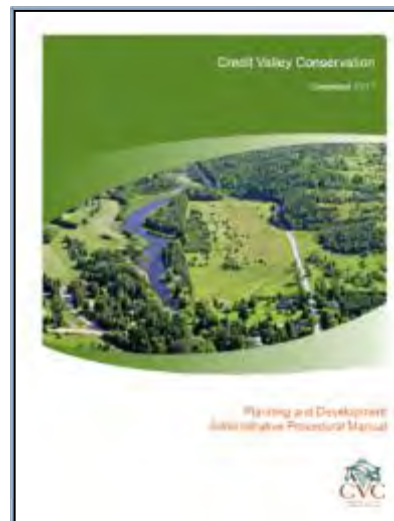
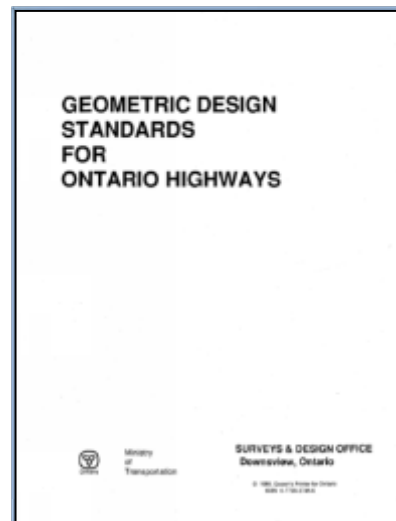
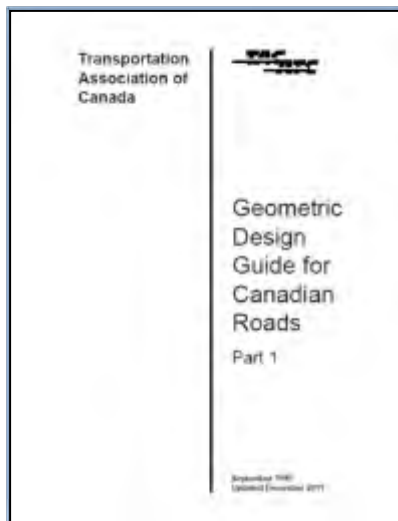
TAC Standards

MTO Standards

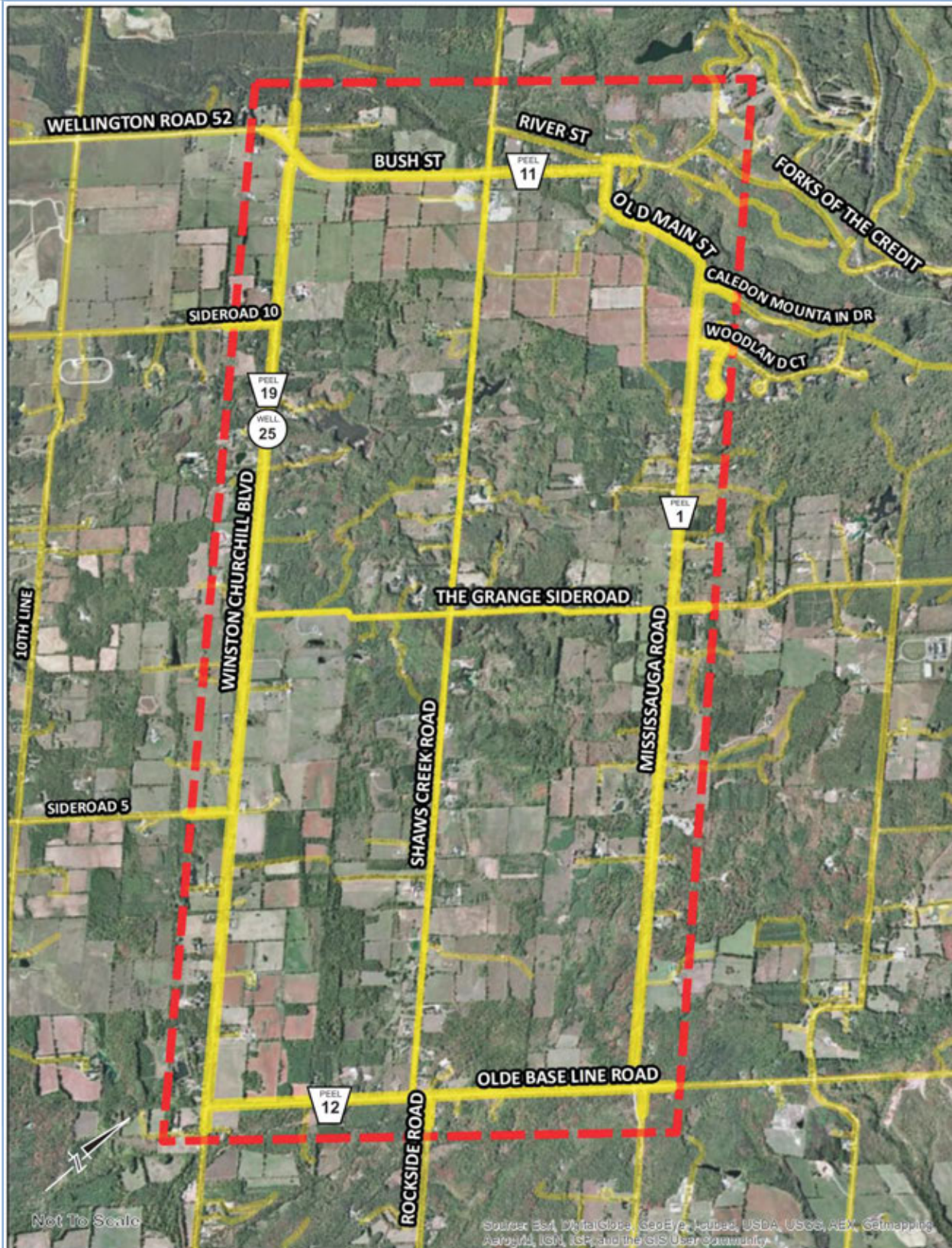
CVC Requirements

MNR Requirements

MOE Requirements



Study Area



The Study Area consists of:

- Bush Street
- Winston Churchill Boulevard
- Mississauga Road / Old Main Street
- Olde Base Line Road

Project Update

- Notice of Study Limits Expansion & Project Bulletin July 2012
- Established Community Working Group (CWG)
 - CWG Meeting #1 held on October 23, 2012
 - CWG Meeting #2 held on April 4, 2013
- Public Open House held on October 30, 2012
 - To gain insight from public on transportation issues
 - To solicit participation in CWG

What we have heard

- **Maintain rural character & countryside scenic quality**
- Preserve historic fences and features
- Preserve natural environment
- Maintain existing vertical alignment and cross-section
- Balance interests of all residents
- Address poor pavement conditions
- Improve pedestrian safety
- Address excessive speeds
- Address issues arising from trucks
- Accommodate cyclists outside travelling lane of traffic
- Improve sightlines in some locations
- Address parking congestion on weekends
- Address issues with motorcycle traffic

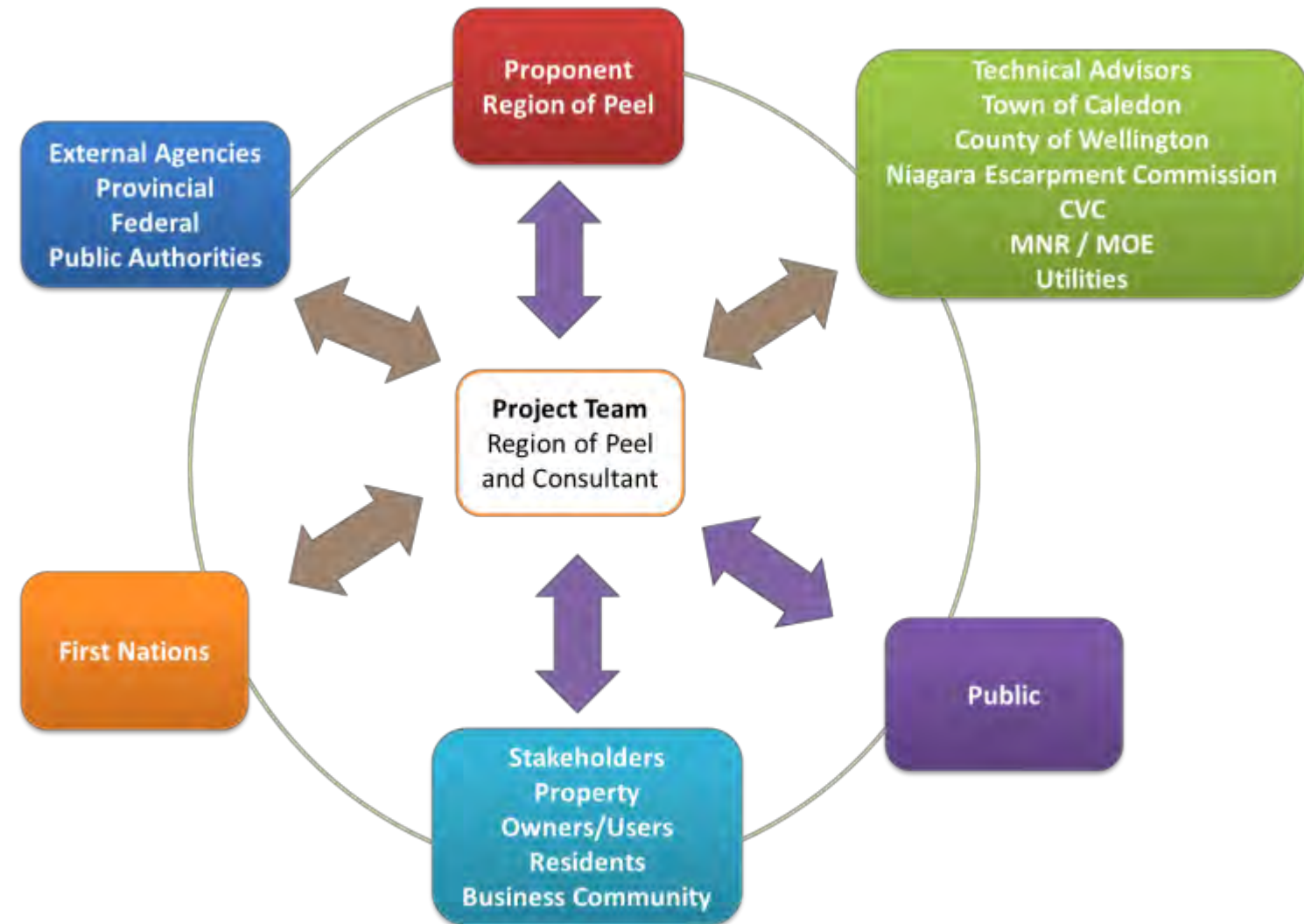
Study Objectives and Organization

Study Objectives

- Identify problems and opportunities
- Develop context-sensitive planning alternative solutions and a preferred solution
- Complete a functional design for the preferred concept
- Prepare a formal Environmental Study Report (ESR) documenting the study findings and recommendations

Goals

- Community and Stakeholder engagement
- Opportunities for Active Transportation
- Increase safety for all users
- Improve asset condition
- Improve natural environmental condition



Vision

Improvements through Context Sensitive planning and design will enhance experience for all users and reflect the character of the community

Strategic Plan and Term of Council Priorities for 2011-2014

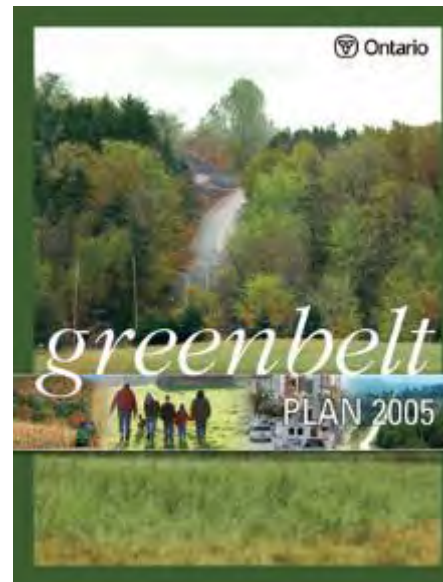
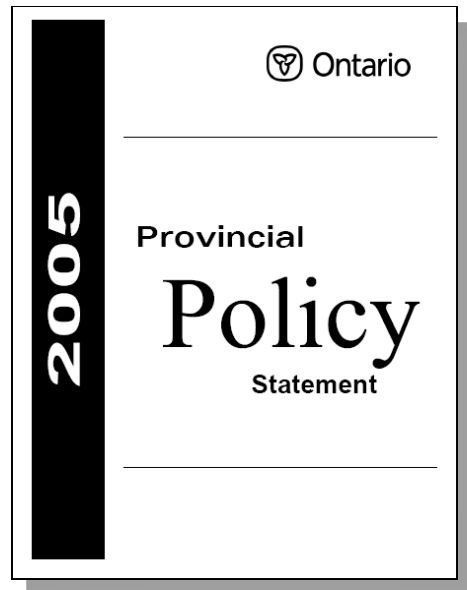
This Environmental Assessment supports a number of actions and initiatives related to transportation and the environment in the **Region's Strategic Plan Goals and Actions and the Term of Council Priorities**



- **Environment**
Protect, enhance and restore the environment
- **Transportation**
Support and influence sustainable transportation systems
- **Public Safety**
Ensure a safe Peel community

Planning and Policy Context

Guiding Policies



- Official Plan

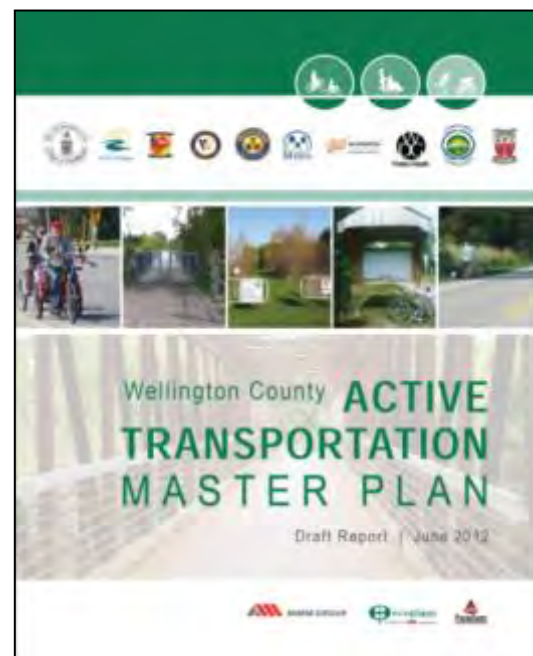
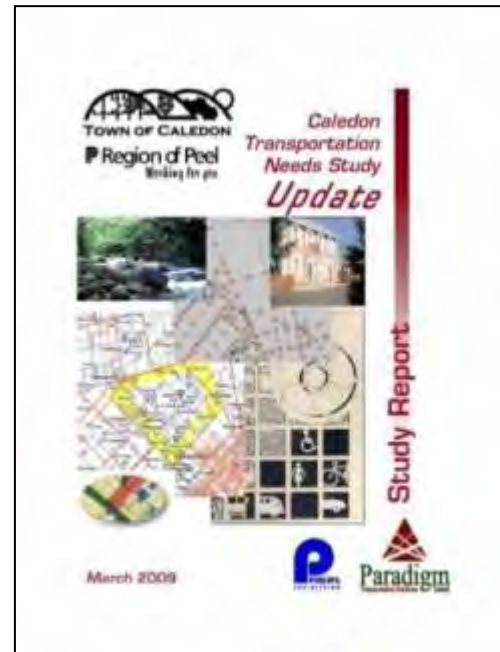


- Official Plan



- Official Plan

Transportation Studies and Initiatives



Draft Problem Statement

Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the study area roads (Mississauga Road/Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road) consist of:

- Deficient pavement conditions and drainage
- Deficient sightlines
- Safety for all road users, including safety of wildlife
- Motor vehicle accidents involving wildlife

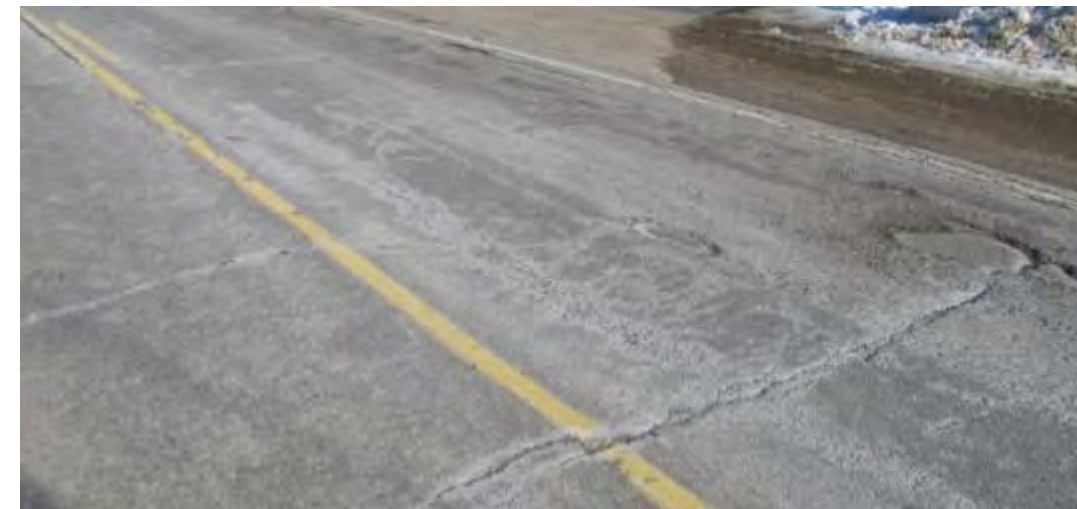


Needs Assessment Summary

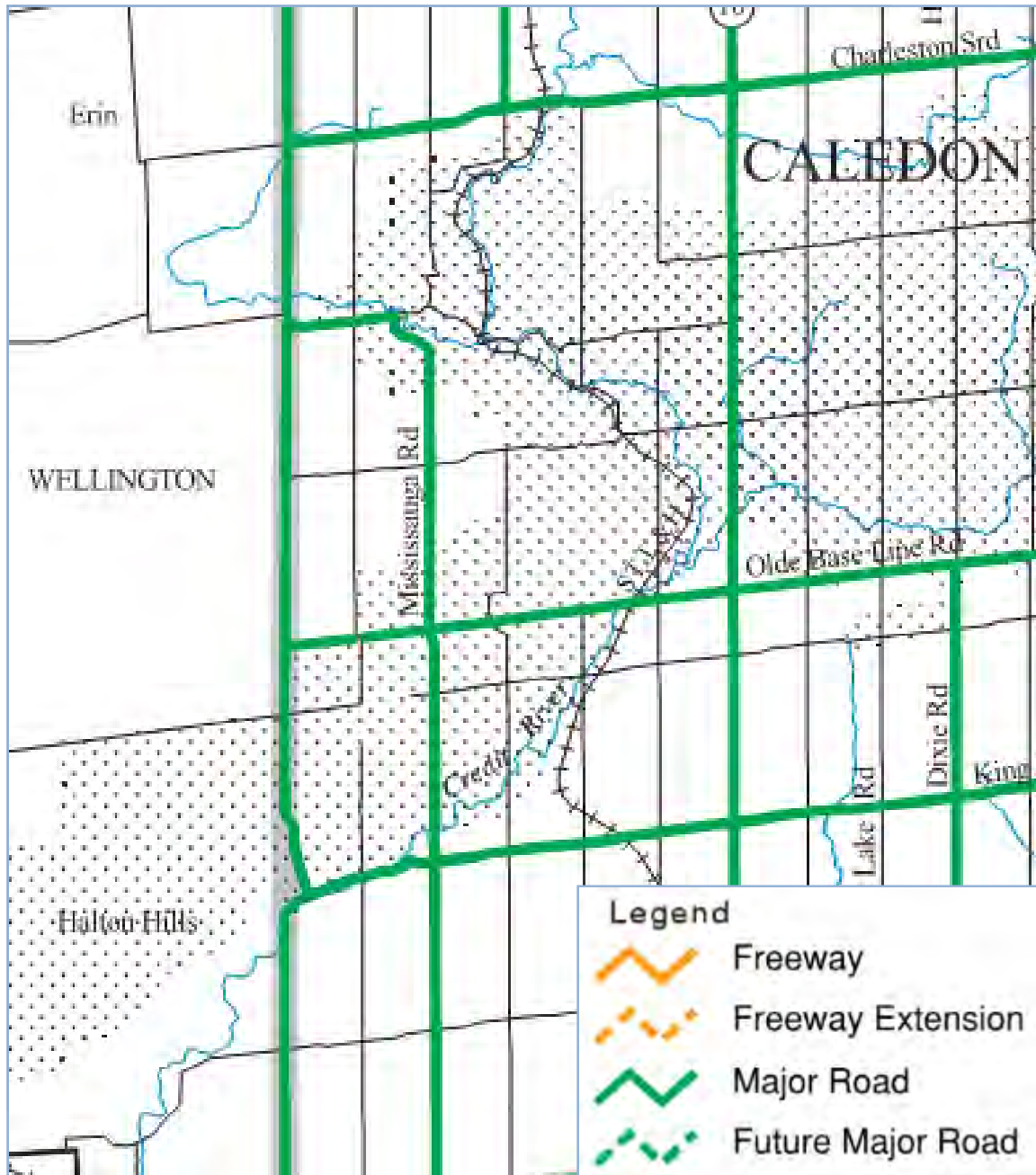
Based on the identified problems and issues, there is the requirement to address the following needs:

- Pavement Conditions and Drainage
- Sightlines
- Improving Safety for All Users
- Pedestrian and Cyclist Needs

Details on the Needs Assessment are provided in the following display panels.



Role and Function of Regional Arterial Roads

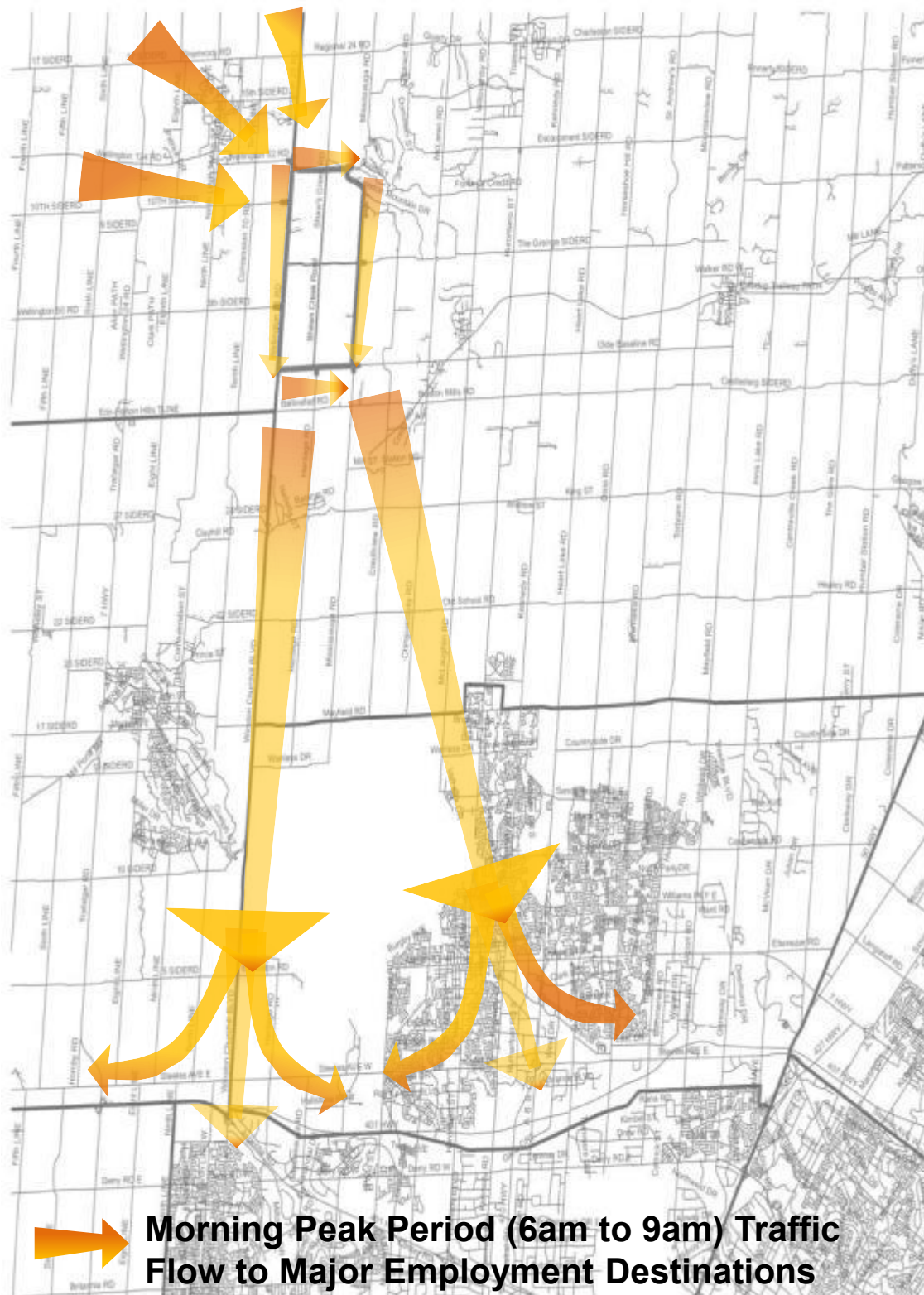


General Objectives of Peel's Transportation System

- To achieve a safe convenient and efficient movement of people and goods in the Region
Support the provision of improved transportation mobility to all residents, employees and visitors
- Promote and encourage all sustainable modes of transportation to provide mobility and choice
- Minimize adverse environmental impacts caused by transportation
- Support a transportation system that enhances economic growth in the Region
- Ensure Regional transportation infrastructure is sustainable and that practices and performance measures are in place to maintain a safe and efficient Regional transportation network

Region of Peel Official Plan – Schedule E

Broader Transportation Network



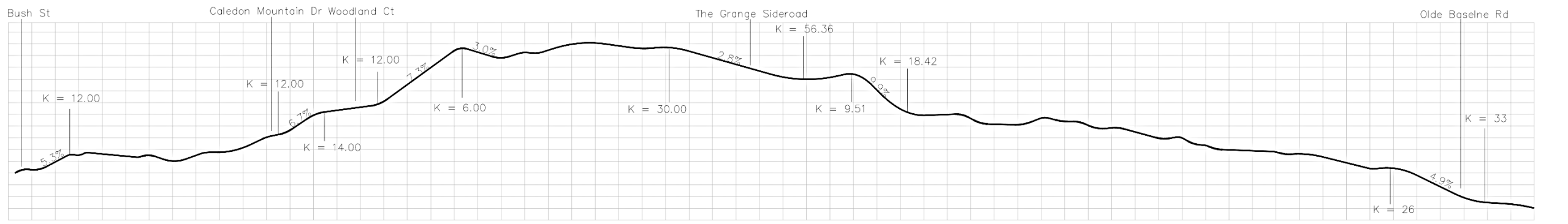
The roads in the study area provide connections to major employment destinations.

Bush Street (Regional Road 11), Mississauga Road / Old Main Street (Regional Road 1), Winston Churchill Boulevard (Regional Road 19), and Olde Base Line Road (Regional Road 12) are all part of the Peel Region arterial road network.

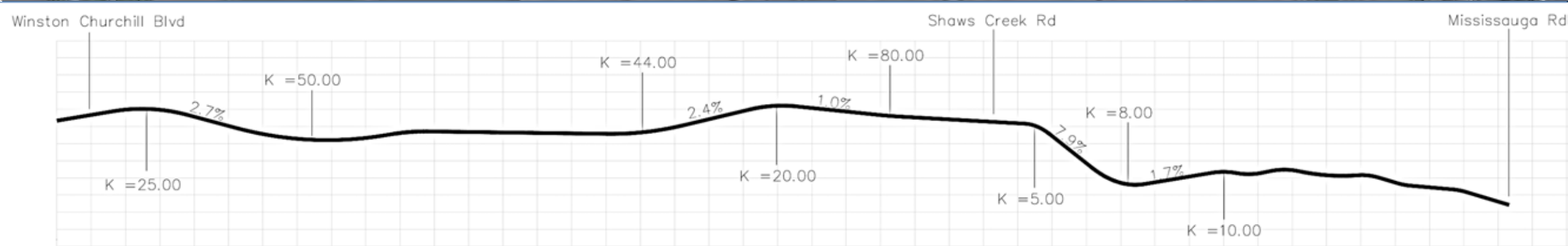
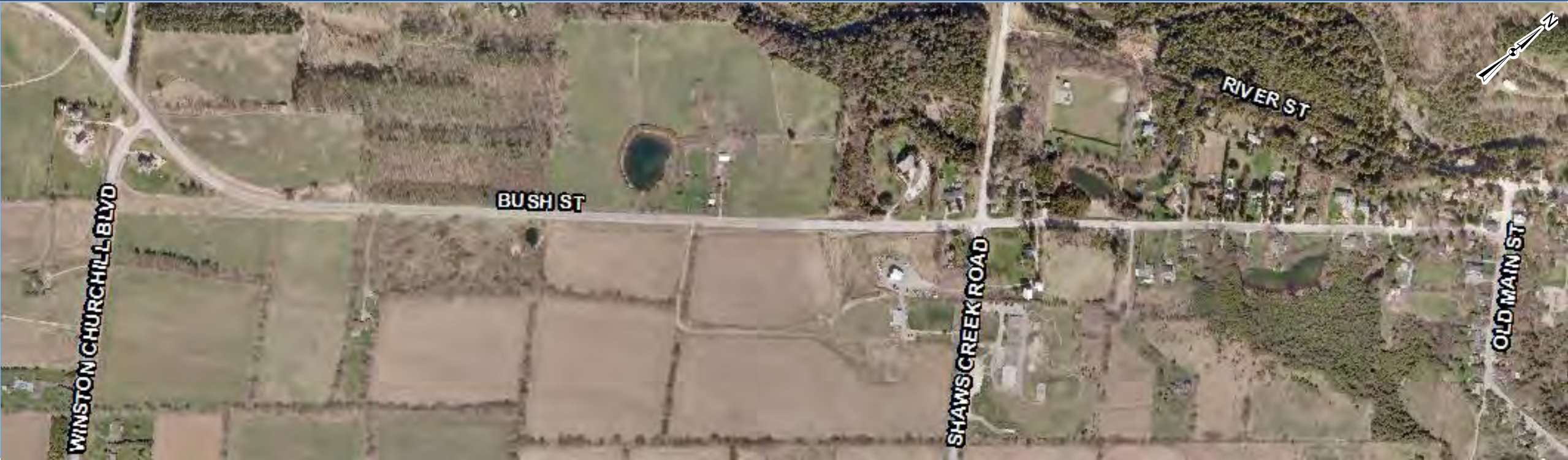
Winston Churchill Boulevard (Regional Road 25) is also part of the Wellington County arterial road network.

The roads in the study area also provide connections to tourist destinations in the immediate and surrounding area.

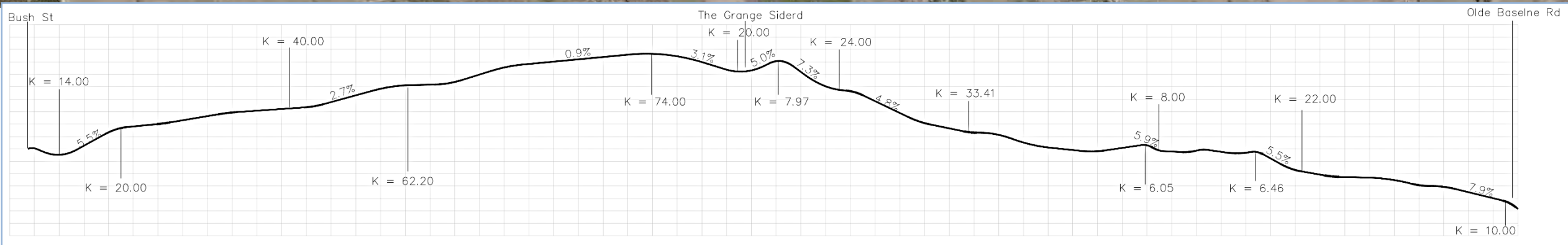
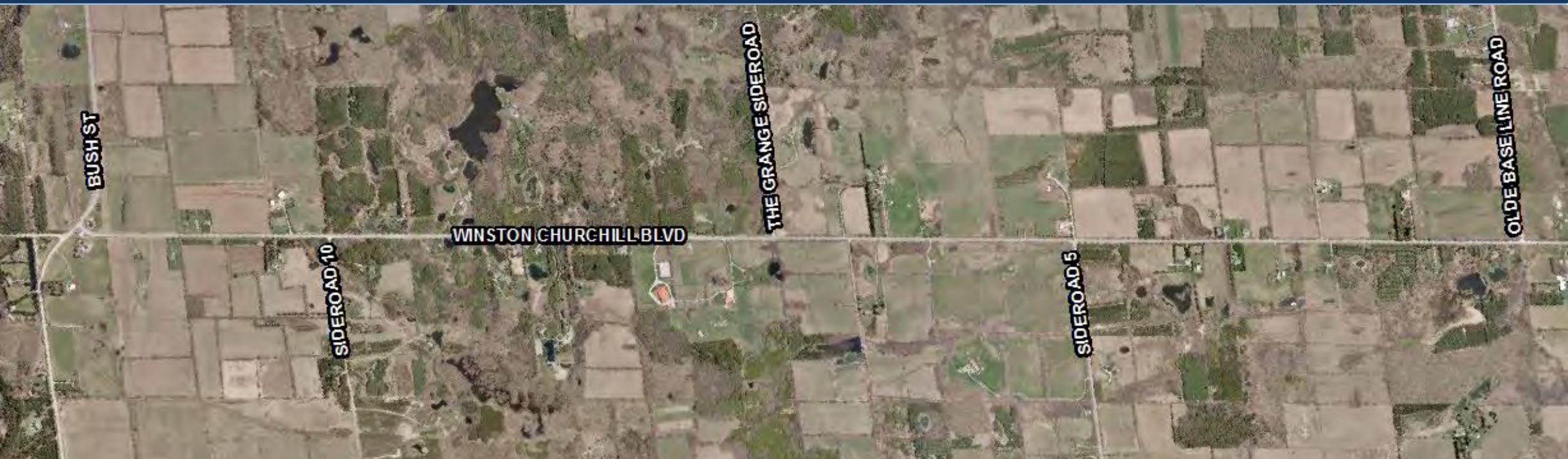
Old Main St - Mississauga Rd



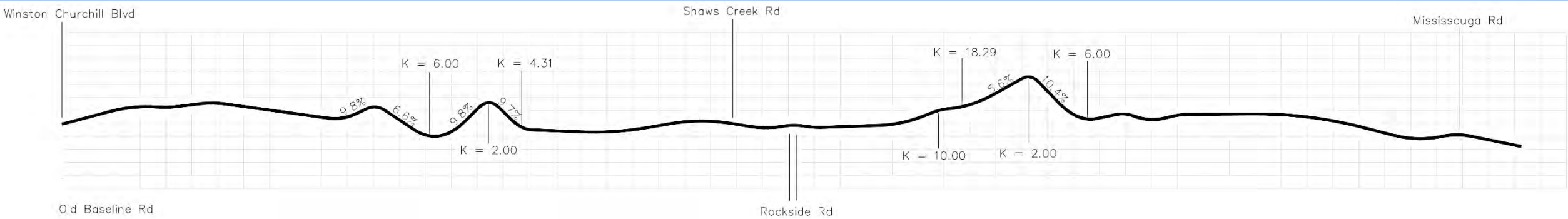
Bush Street (WCB-Old Main St)



Winston Churchill (Bush-OBL)



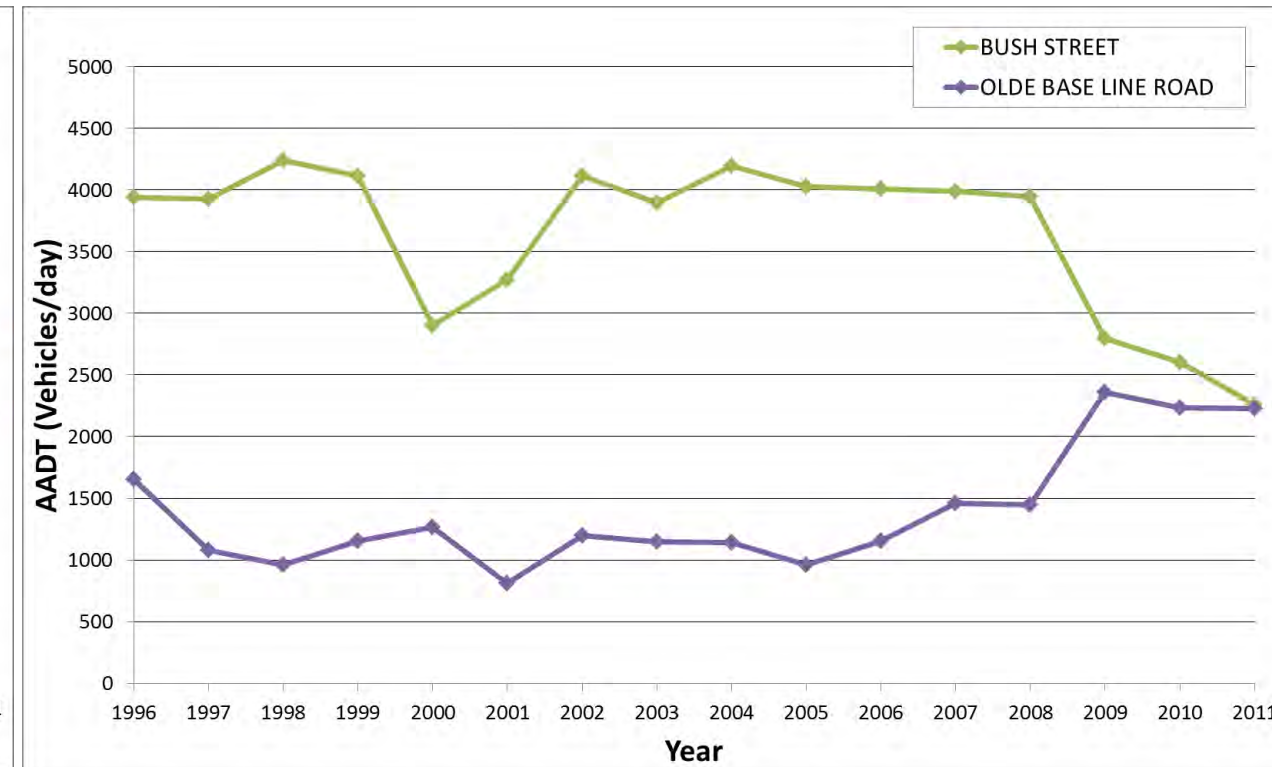
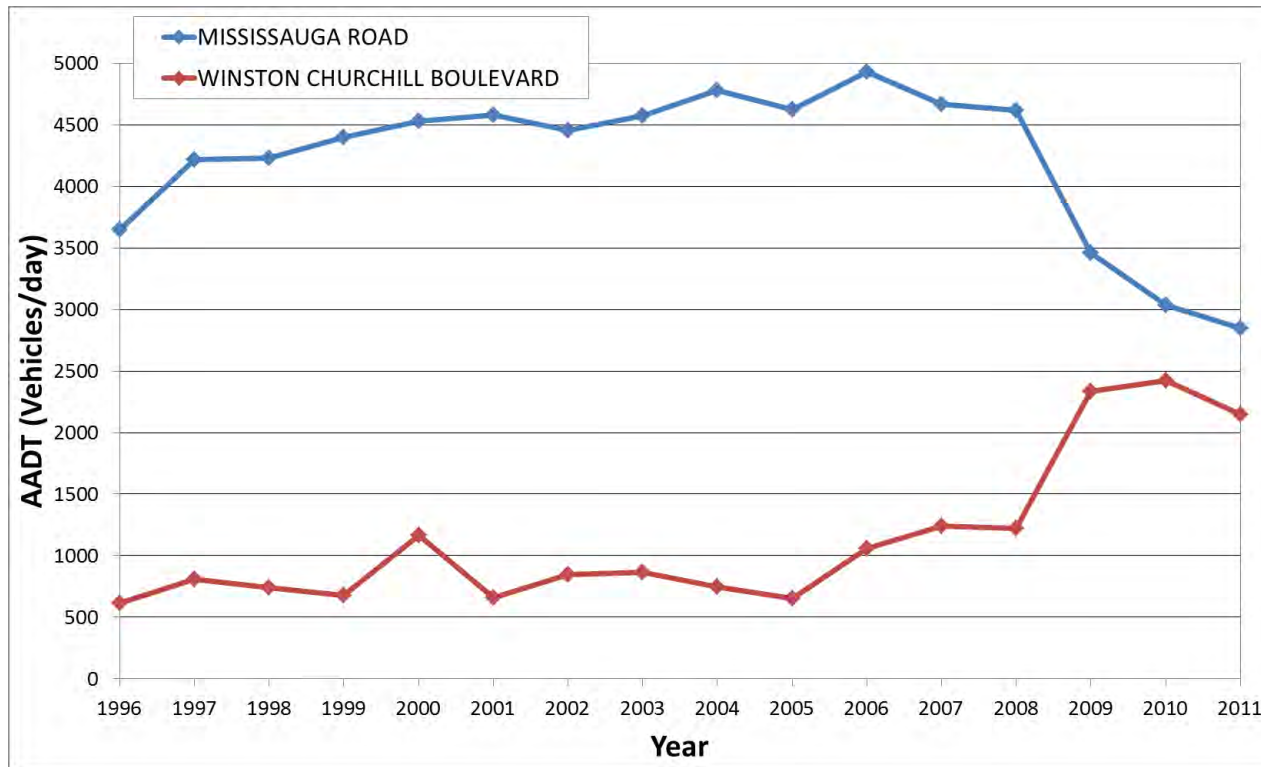
Olde Base Line (WCB-Mississauga)



Traffic Trends

Mississauga Road and Winston Churchill

Bush Street and Olde Base Line Road



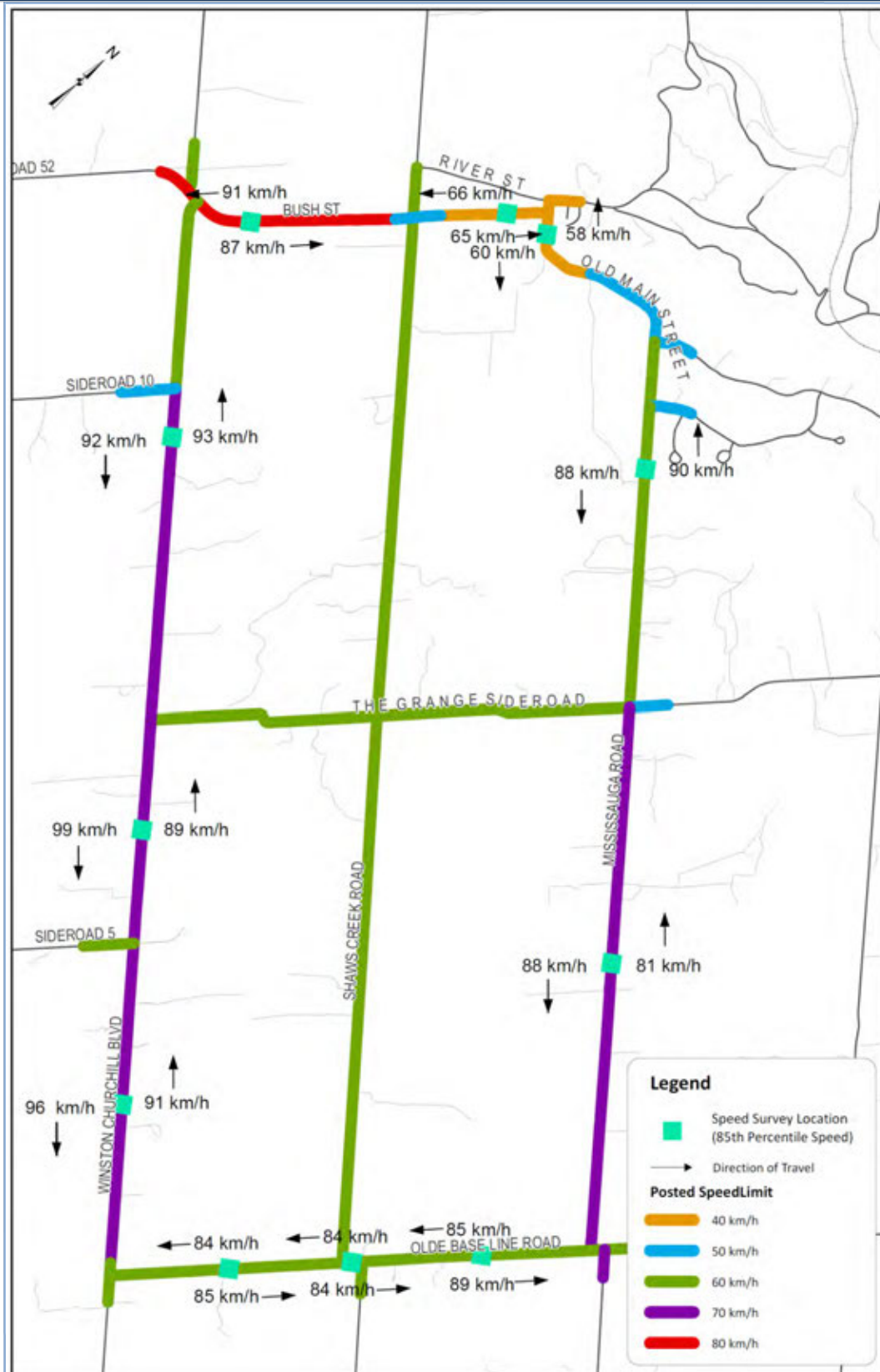
Over the last 15 years, overall traffic on the four arterials has been relatively stable, with an average of 9,500 to 10,000 vehicles per day combined for the four arterial roads

Traffic Speeds

Speed surveys indicate that traffic generally travels at higher speeds than the posted speed limits.

Road segments where the 85th percentile speeds are more than 20 km/h over the posted speed limits include:

- Olde Base Line Road
- Mississauga Road between The Grange Sideroad and Woodlands Court
- Winston Churchill Boulevard
- Bush Street in the Village of Belfountain



Collisions by Road Segment

Segment	AADT	Segment Length	Number of Collisions
Mississauga Road between Olde Base Line Road & The Grange Sideroad*	2750	3.0 km	11
Mississauga Road between The Grange Sideroad & Bush Street	2660	3.3 km	9
Bush Street between Mississauga Road & Winston Churchill Boulevard	2100	2.0 km	1
Winston Churchill Boulevard between Bush Street & Olde Base Line Road	2500	6.0 km	8
Olde Base Line Road between Winston Churchill Boulevard & Mississauga Road*	2370	2.7 km	9

Source: Collision information provided by Peel Region's Safety group.

* Average AADT along corridor. AADT estimated from TMC data provided by Region's Traffic Group

Highest number of collisions are on:

- Mississauga Road between Olde Base Line Road and The Grange Sideroad
- Olde Base Line between Winston Churchill Boulevard and Mississauga Road

Consider options to improve safety on the Study Area roads

Collisions by Severity

Property Damage Only	62	91%
Non Fatal Injury	6	9%
Fatality	0	0%
TOTAL	68	100%

(Based on 2006-2010 data)

0 collisions involving a pedestrian

1 collision involving a cyclist (sideswipe)

Non-fatal injury collisions evenly distributed

30 collisions at intersections

38 collisions at midblock (non-intersections)

Collisions by Impact Type

Animal	37%	
Single Vehicle	34%	(includes avoiding animals)
Angle	10%	
Sideswipe	7%	
Rear End	4%	
Turning Movement	4%	
Approach	3%	
TOTAL	100%	

(Based on 2006-2010 data)

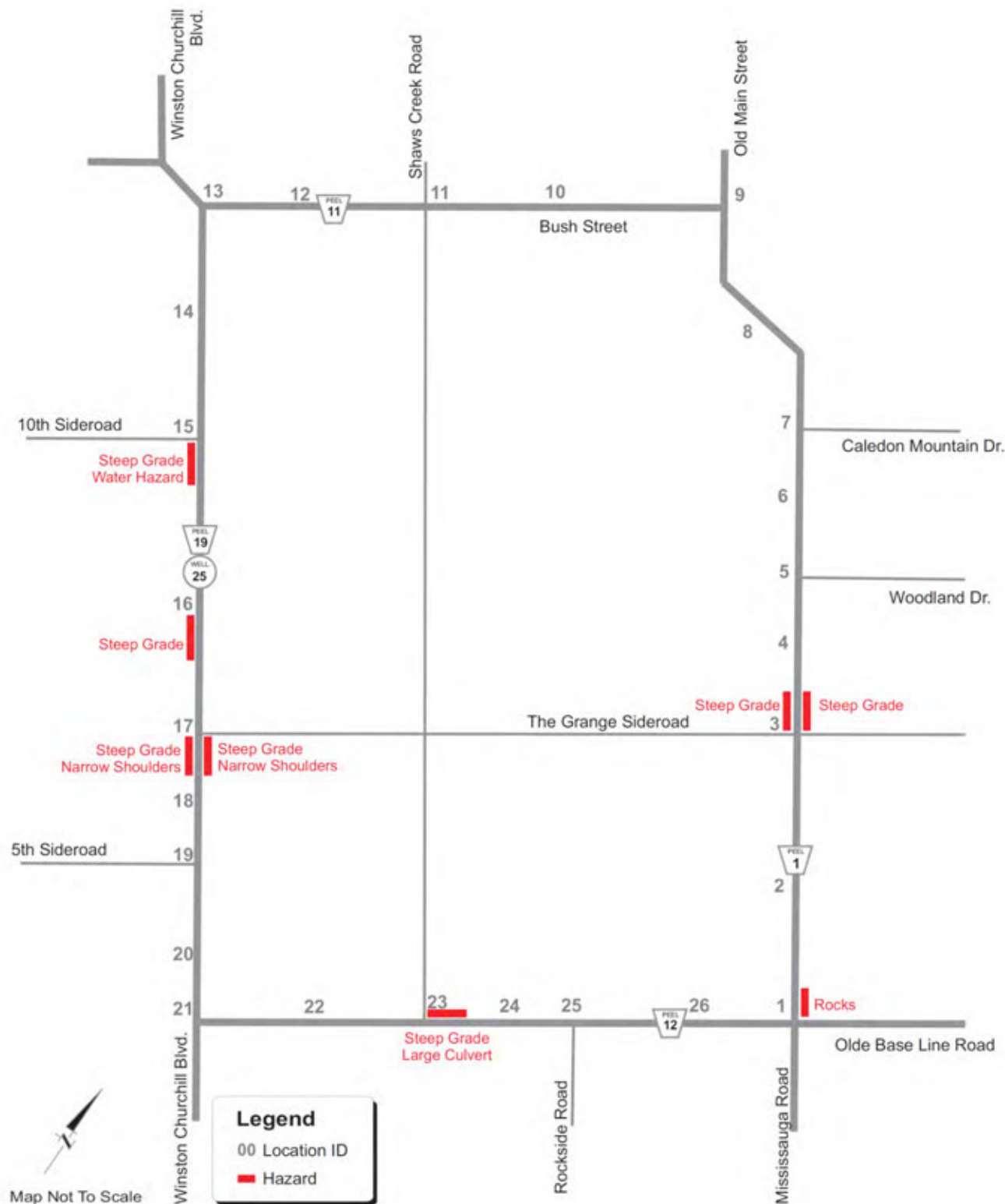
44% of collisions involve animals

Second highest – single vehicles with drivers losing control

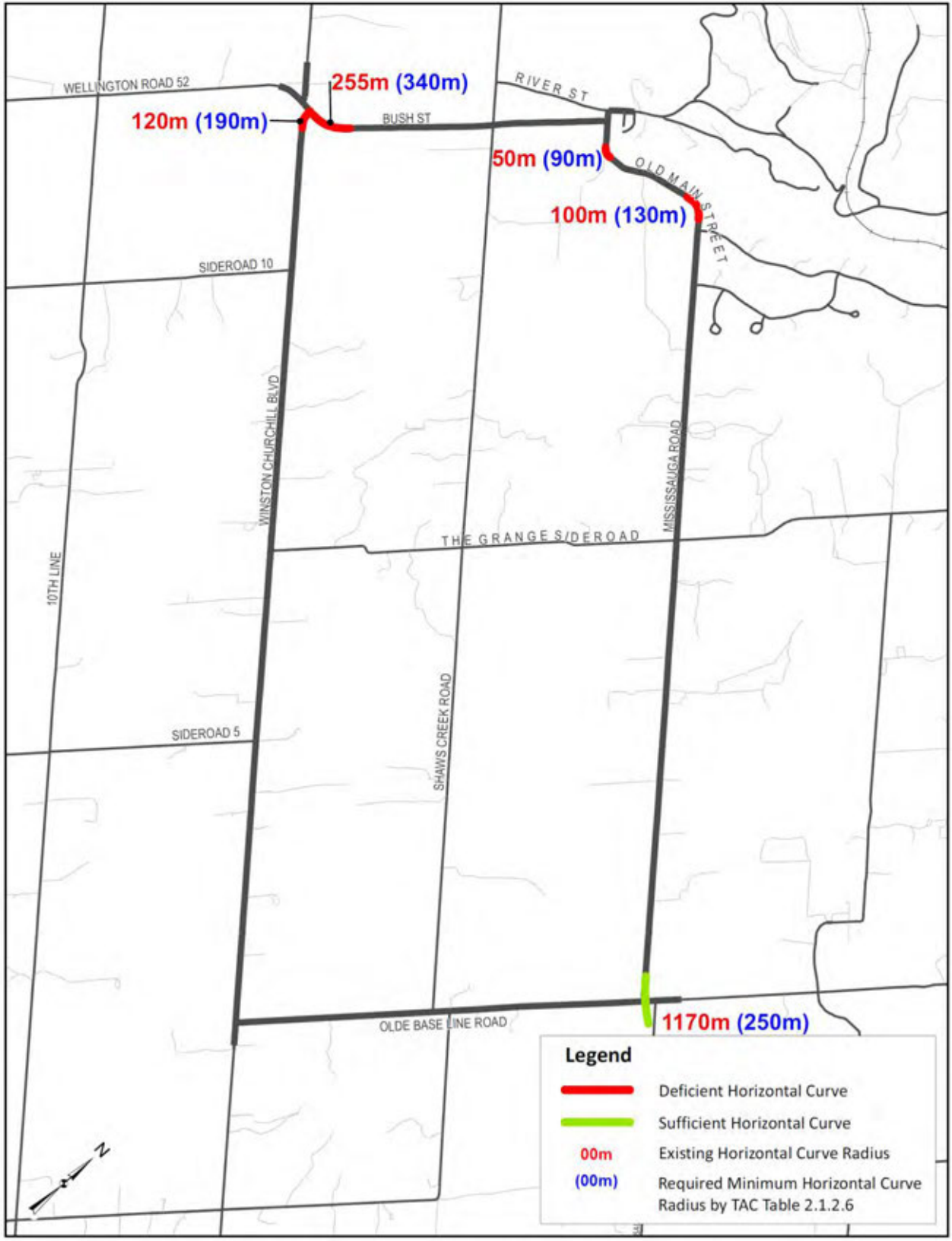
Consider options to reduce animal-related collisions

Roadside Hazards

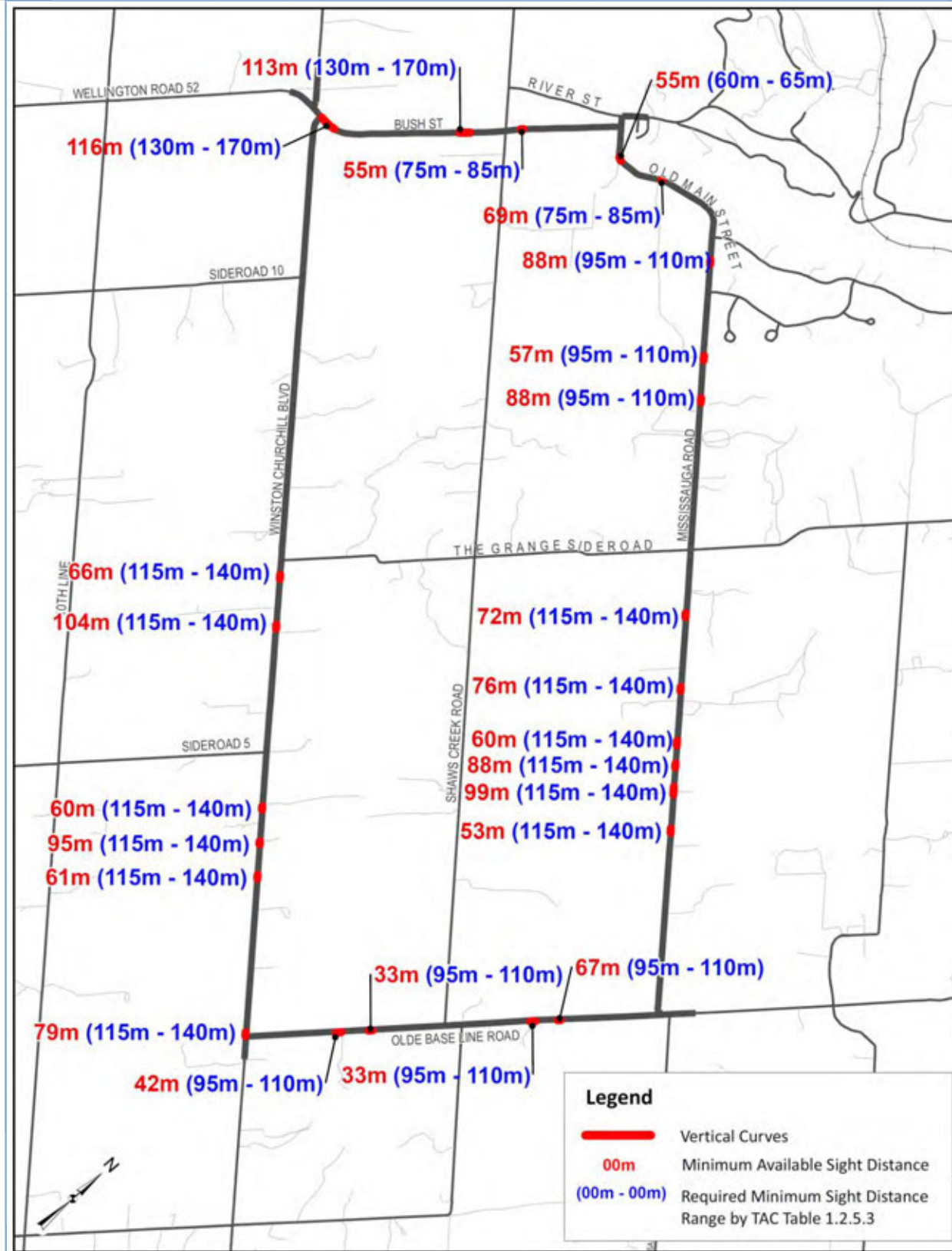
Roadside hazards include hydro poles, steep slopes, and rock cuts. The Study will consider options to improve safety at roadside hazard locations.



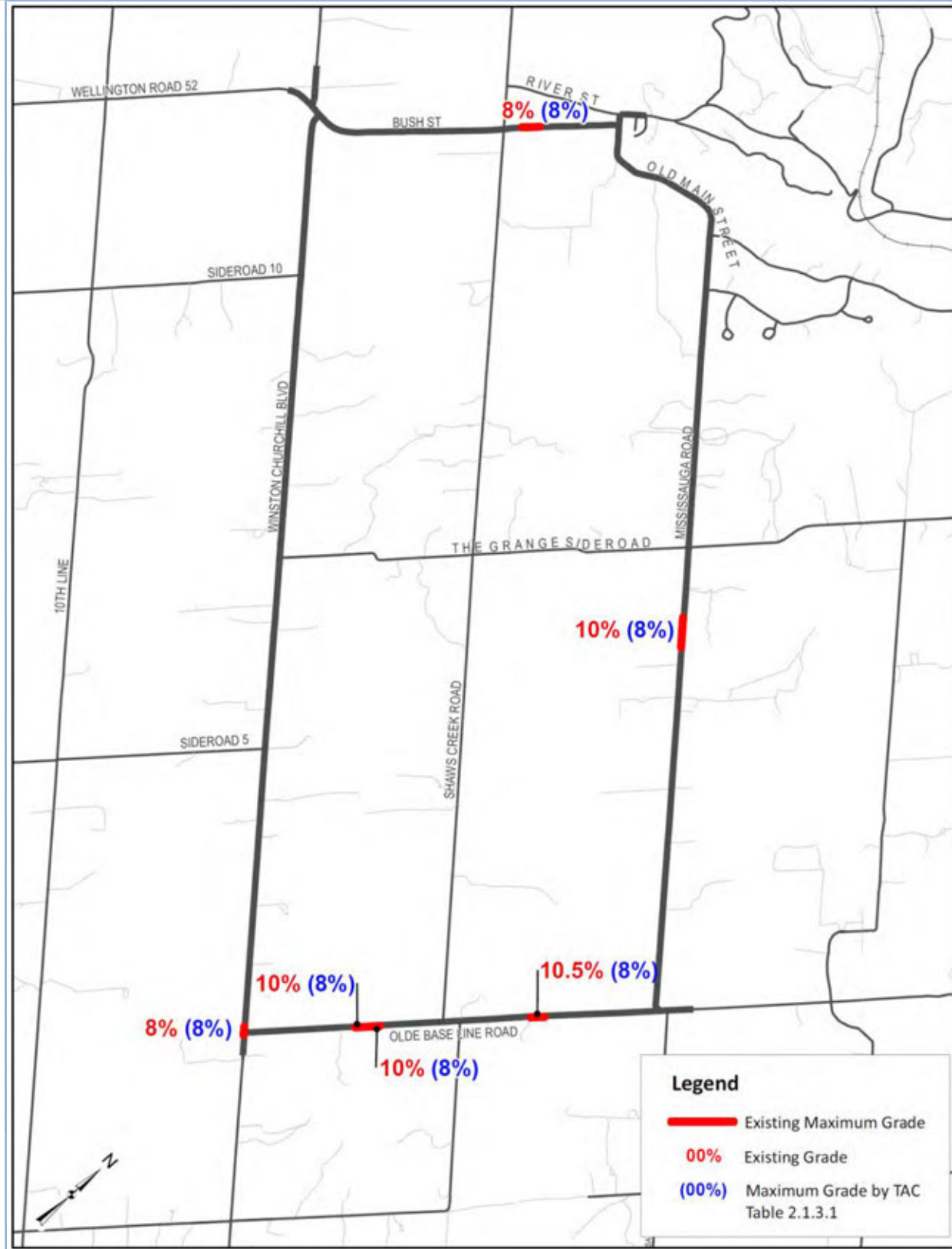
Horizontal Alignment Deficiencies



Vertical Alignment: Stopping Sight Distance Deficiencies



Vertical Alignment: Grade Deficiencies



Sight Distances at Driveways

At many driveways, sight distances are inadequate.

	Yes	No	TOTAL
Fully Meets Minimum Standards	75 (41%)	109 (59%)	184 (100%)
Fully Meets Minimum Desirable Standards	51 (28%)	133 (72%)	184 (100%)

Based on Transportation Association of Canada design standards.
Based on drivers turning left or right from their driveways.



Pedestrians and Cyclists



The Peel Active Transportation Study shows Winston Churchill Boulevard, Olde Base Line Road, Mississauga Road, and Bush Street as part of the Regional cycling network, with proposed and/or planned paved shoulders for cyclists

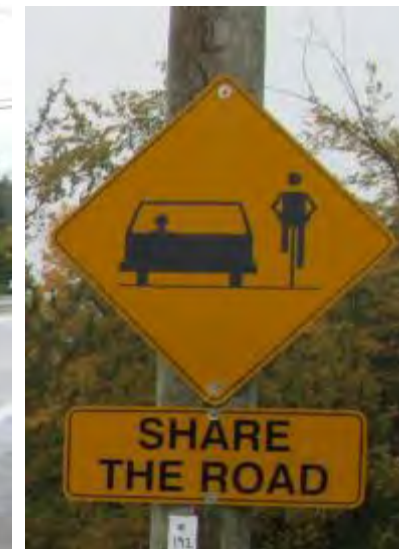
Summary of previous public input

Pedestrians:

- While there is an interest in improving pedestrian safety, there are mixed opinions on how that should be done

Cyclists:

- Safety concerns for cyclists
- While there is a desire for improved cycling infrastructure, mixed opinions as to how best to accommodate cyclists



Pavement and Drainage Conditions

- The preliminary findings from the geotechnical investigations completed to date reveal that structural capacity and strength of all roads are in poor condition and are expected to continuously deteriorate.
- The main cause to pavement distress is attributed to variable granular thickness along roadways with a non-uniform base and sub-base materials.
- Shoulder granular is also thinner than the sub-base below the roadway which affects the drainage of the base leading to frost heave and rutting.
- Some of the pavement deficiencies identified throughout the study area include:
 - Wheel tracking and rutting
 - Transverse and longitudinal meander and mid-lane cracking
 - Alligator pavement edge cracking

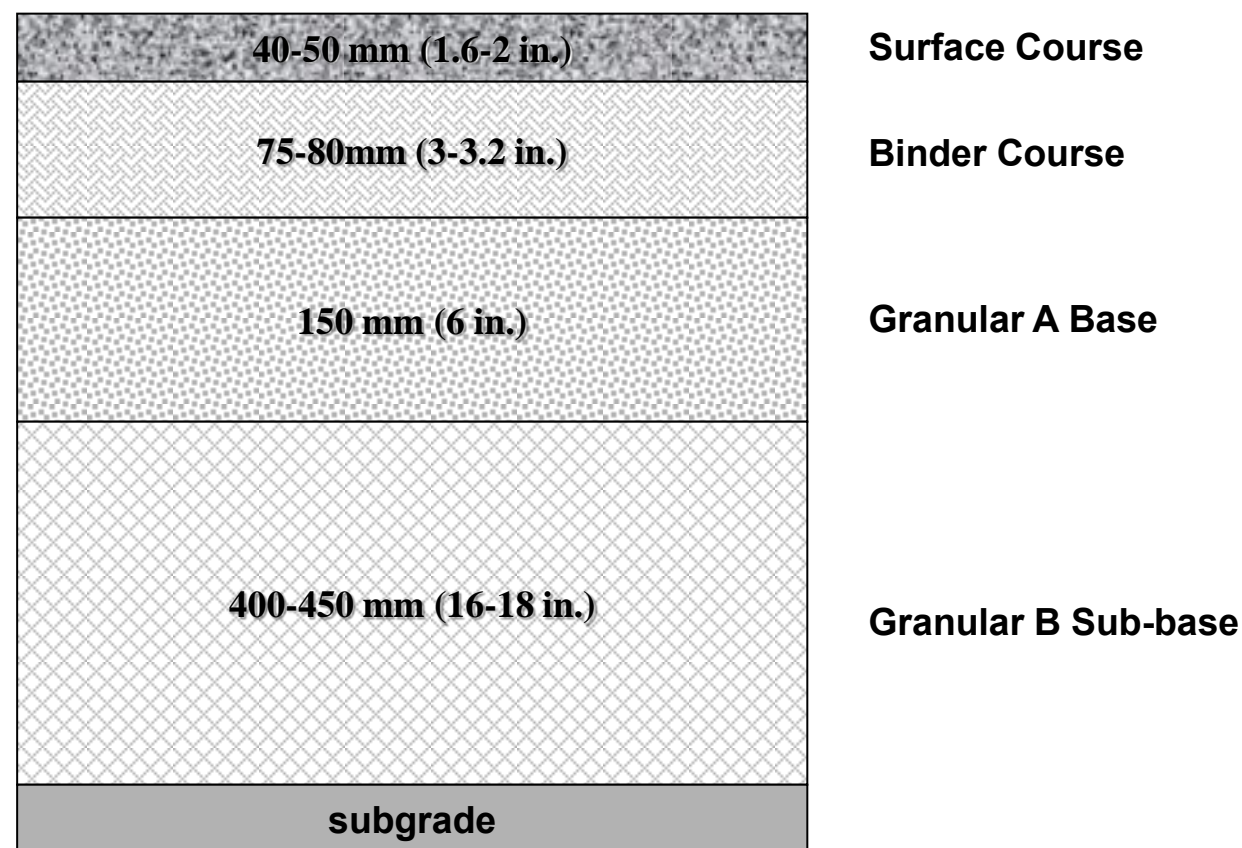


Pavement and Drainage Conditions by Roadway

Roadway	Existing Conditions / Pavement Deficiencies	
Mississauga Road/Old Main Street	<ul style="list-style-type: none"> •Granular thickness of base and sub-base highly variable •“Bathtub” construction –granular under shoulder is thinner than under the roadway •Wheel tracking rutting •Slight alligator pavement edge cracking 	<ul style="list-style-type: none"> •Moderate alligator transverse cracking •Longitudinal meander and mid-lane cracking •Inadequate / sub-standard ditches •Ponding and vegetation along shoulders
Bush Street	<ul style="list-style-type: none"> •Granular thickness of base and sub-base highly variable •Pot holes 	<ul style="list-style-type: none"> •Centreline and transverse cracking •Deficient structural capacity and stability
Winston Churchill Boulevard	<ul style="list-style-type: none"> •Deficient structural capacity and stability •Medium severity raveling •Localized depressions 	<ul style="list-style-type: none"> •Shallow bedrock does not allow for drainage under roadway
Olde Base Line Road	<ul style="list-style-type: none"> •High severity loss of aggregates •Pot holes •Frost heave and temperature related deterioration 	<ul style="list-style-type: none"> •Water logging due to top permeable layers and bottom relatively impermeable silty clay •Shallow bedrock does not allow for drainage under roadway

Recommended Pavement Structures by Roadway

Geotechnical Investigations completed to date have recommended the following pavement structure to address the deficient pavement conditions:



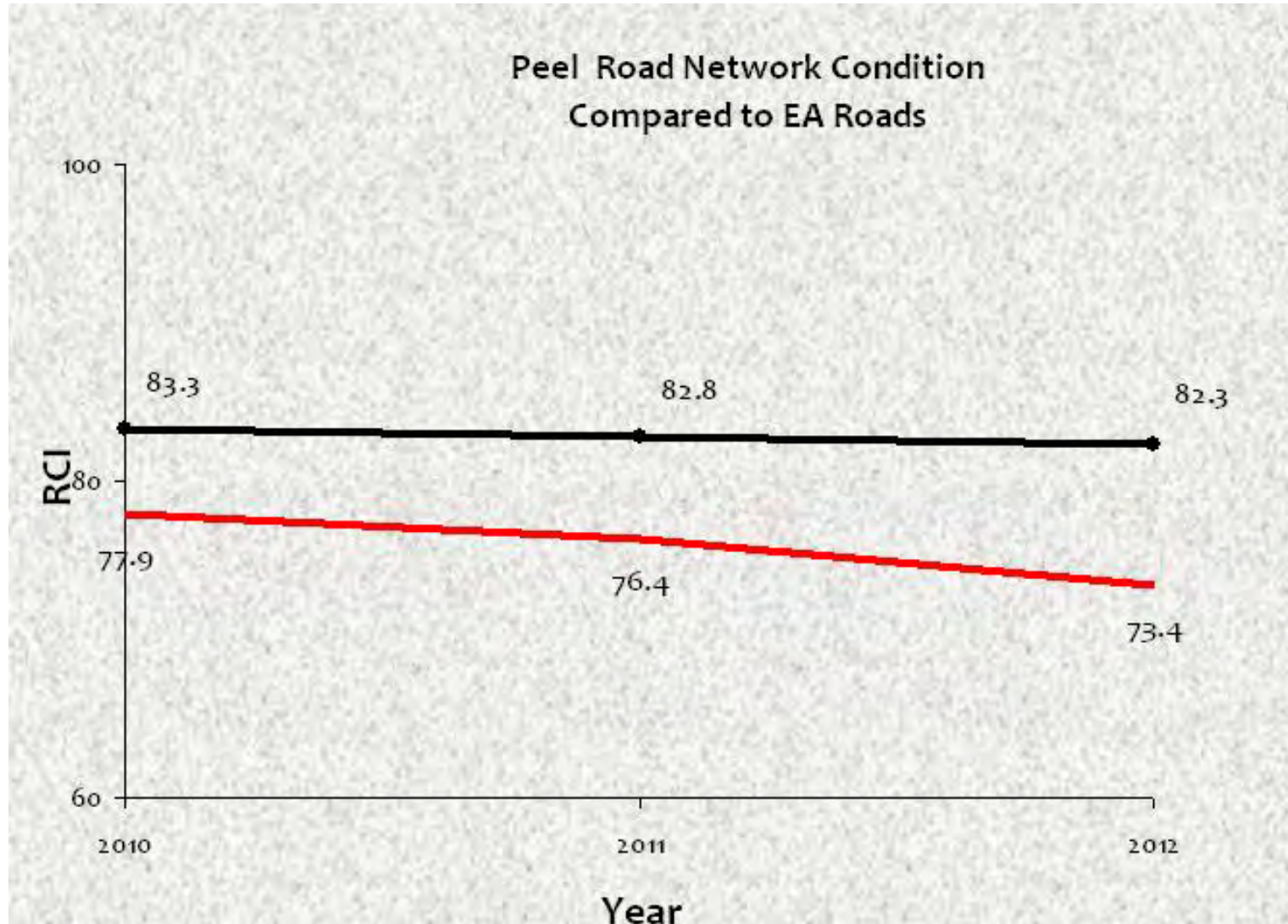
* Note: The pavement recommendations will be confirmed in the subsequent phases of this study.

Pavement structure granular materials must conform to OPSS (Ontario Provincial Standard Specification) specifications.

Preliminary Geotechnical Recommendations



Ride Condition Index (RCI)



- Black line represents projected network RCI for all Regional Roads.
- Red line represents projected RCI for the roads in the study area.

- The Ride Condition Index (RCI) is a quantitative number that represents the overall condition and quality of a Regional road network.
- The RCI aggregates the rating of many types of road defects including cracking, rutting, potholes and surface quality into one measurable number.
- Study area roads are below the network average and are deteriorating faster than the network average and will likely be below the level of service (72) for roads in the next 3- 5 years.

Needs Assessment

- Reduce collisions and improve safety, particularly in areas where there are steep grades, sharp curves, vertical crests, and driveways
- Accommodate pedestrians in areas of high pedestrian activity
- Improve pedestrian safety
- Reduce conflicts between cyclists and motorized vehicles
- Address substandard sightlines for the prevailing traffic speeds
- Address roadside hazards
- Reduce collisions with animals
- Improve traffic signage
- Minimize impact of increase in traffic volumes
- Address excessive speeds – cars, trucks, motorcycles
- Address poor conditions of the roadway pavement
- Address parking congestion in Belfountain experienced on weekends
- **There is no need for additional travel lanes**

There is a recognition that users may have competing interests and needs












Guiding Principles for Solutions

These principles will provide guidance to the study team when generating alternative solutions to address the problem statement:

- **Maintain the rural character and countryside scenic quality**
- Preserve historic fences and heritage / cultural / archaeological features
- Preserve / enhance the natural environment
- Protect the Niagara Escarpment
- Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife
- Provide a context sensitive design
- Enhance local tourism and economic development of the area

It is recognized that different users may have competing interests

Preliminary Alternative Solutions

Problem or Opportunity	Mississauga Rd. / Old Main St.	Bush St.	Winston Churchill Blvd.	Olde Base Line Rd.
1. Excessive Speeds				
Alternative Solutions:	iii.	Pavement markings (e.g. SLOW)	v.	Enforcement
i. Do nothing	iv.	Narrowing travel lanes	vi.	Combinations of the above
ii. Lower Design Speeds	v.	Traffic Signage		
2. Deteriorating Pavement Conditions and Drainage Deficiencies				
Alternative Solutions:	iii.	Road Rehabilitation		
i. Do nothing	iv.	Combinations of the above		
ii. Road Reconstruction				
3. Horizontal Alignment Deficiencies				OK
Alternative Solutions:	iii.	Pavement markings (e.g. SLOW)		
i. Do nothing	iv.	Lower Design Speeds		
ii. Traffic signage	v.	Combinations of the above		




Preliminary Alternative Solutions – cont'd

Problem or Opportunity	Mississauga Rd. / Old Main St.	Bush St.	Winston Churchill Blvd.	Olde Base Line Rd.
4. Vertical Alignment Deficiencies •Deficient Stopping Sight Distance •Deficient Grade	✓ ✓	✓ ✓	✓ ✓	✓ ✓
Alternative Solutions: i. Do nothing ii. Lower Design Speeds	iii. Traffic Signage iv. Road Reconstruction to reduce grades and bring vertical curves up to standard		v. Combinations of the above	
5. Pedestrian and Cycling Needs •Regional Active Transportation Plan identifies all study roads as proposed or planned cycling facilities with paved shoulders	✓	✓	✓	✓
Alternative Solutions: i. Do nothing ii. Widening / partially paving shoulders	iii. Sidewalks iv. Bike racks at local businesses v. Landscaping to shelter pedestrians from traffic		vi. Combinations of the above	

Preliminary Alternative Solutions – cont'd

Problem or Opportunity	Mississauga Rd. / Old Main St.	Bush St.	Winston Churchill Blvd.	Olde Base Line Rd.
6. Safety				
•Collisions by location	✓	OK	✓	✓
•Collisions by type	•Almost half of all collisions involve animals			
Alternative Solutions:	iii. Pavement markings		v. Seasonal communications and education regarding deer activity	
i. Do nothing	iv. Traffic Signage including animal crossing warning		vi. Combinations of the above	
ii. Lower Design Speeds				
7. Intersection Deficiencies				
•Deficient Sight lines	✓	✓	✓	✓
Alternative Solutions:	iii. Remove overgrown vegetation		v. Road Reconstruction to reduce grades and bring vertical curves up to standard	
i. Do nothing	iv. Lower Design Speeds		vi. Combinations of the above	
ii. Roundabout				

Preliminary Alternative Solutions – cont'd

Problem or Opportunity	Mississauga Rd. / Old Main St.	Bush St.	Winston Churchill Blvd.	Olde Base Line Rd.
8. Driveway Deficiencies •Deficient Turning Sight Distance	•More than half of driveways have deficient turning sight distance (approximately 70%)			
Alternative Solutions: i. Do nothing ii. Lower Design Speeds	iii. Traffic Signage iv. Road Reconstruction to reduce grades and bring vertical curves up to standard		v. Combinations of the above	
9. Roadside Hazards •Trees, ditches, steep slopes, poles, etc.		OK		
Alternative Solutions: i. Do nothing ii. Remove hazards	iii. Install guide rails iv. Combinations of the above			

Preliminary Evaluation Criteria

Maintain the rural character and countryside scenic quality

Transportation

- Geometric alignment
- Traffic operations
- Driveway operations
- Accommodation of all road users
- Safety
- Stormwater quality and quantity

Socio-Economic Environment

- Residential properties
- Farm operations
- Businesses
- Archaeological resources
- Built and cultural heritage resources
- Air, noise, vibration impacts

Natural Environment

- Vegetation and wildlife habitat
- Aquatic habitat
- Terrestrial habitat
- Species at Risk
- Wildlife movement and safety
- Wetlands and watercourses
- Natural hazards

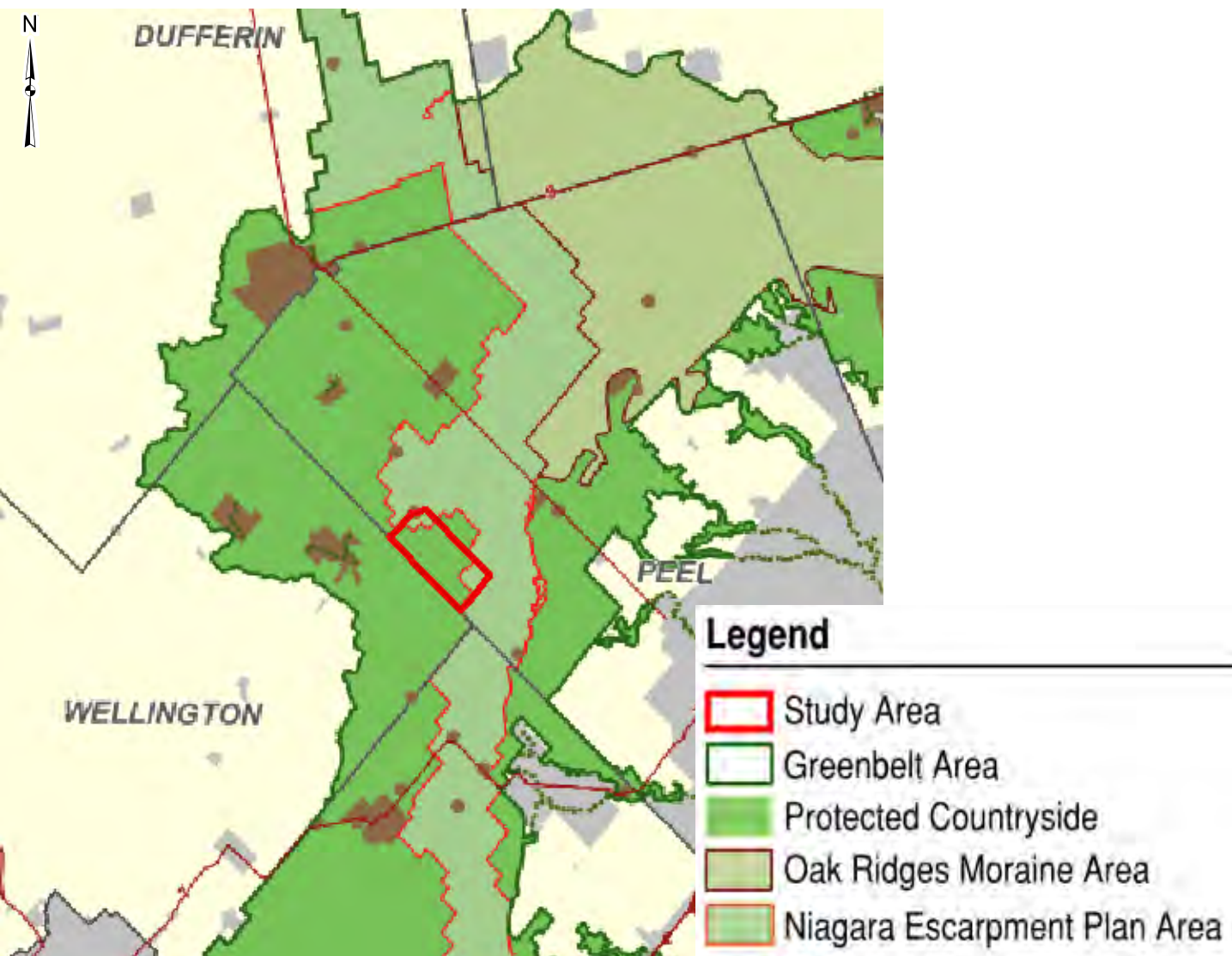
Capital Costs

- Construction costs
- Property acquisition

Inventory of Existing Natural, Archaeological and Built/Cultural Heritage Conditions

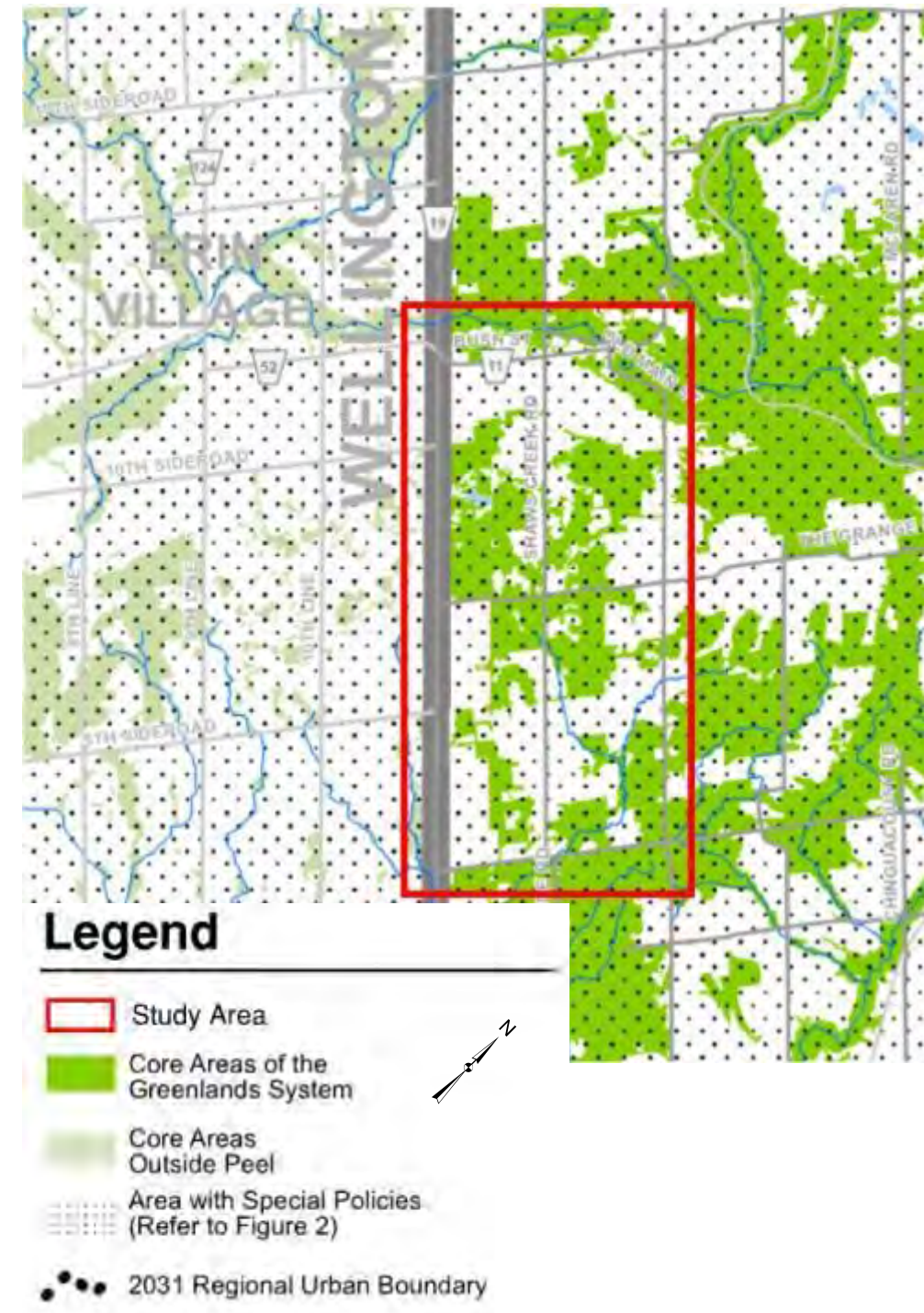


Greenbelt and Greenlands

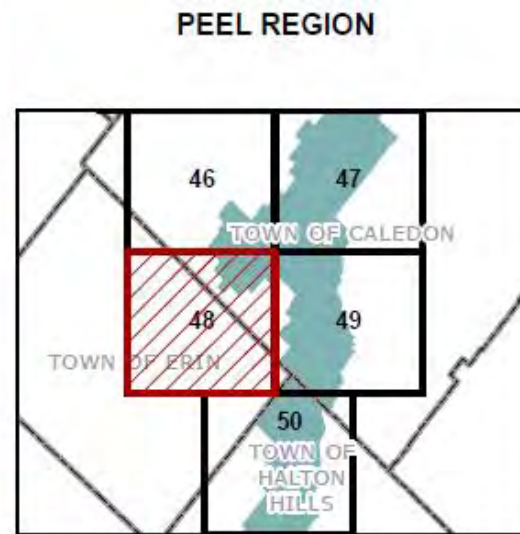
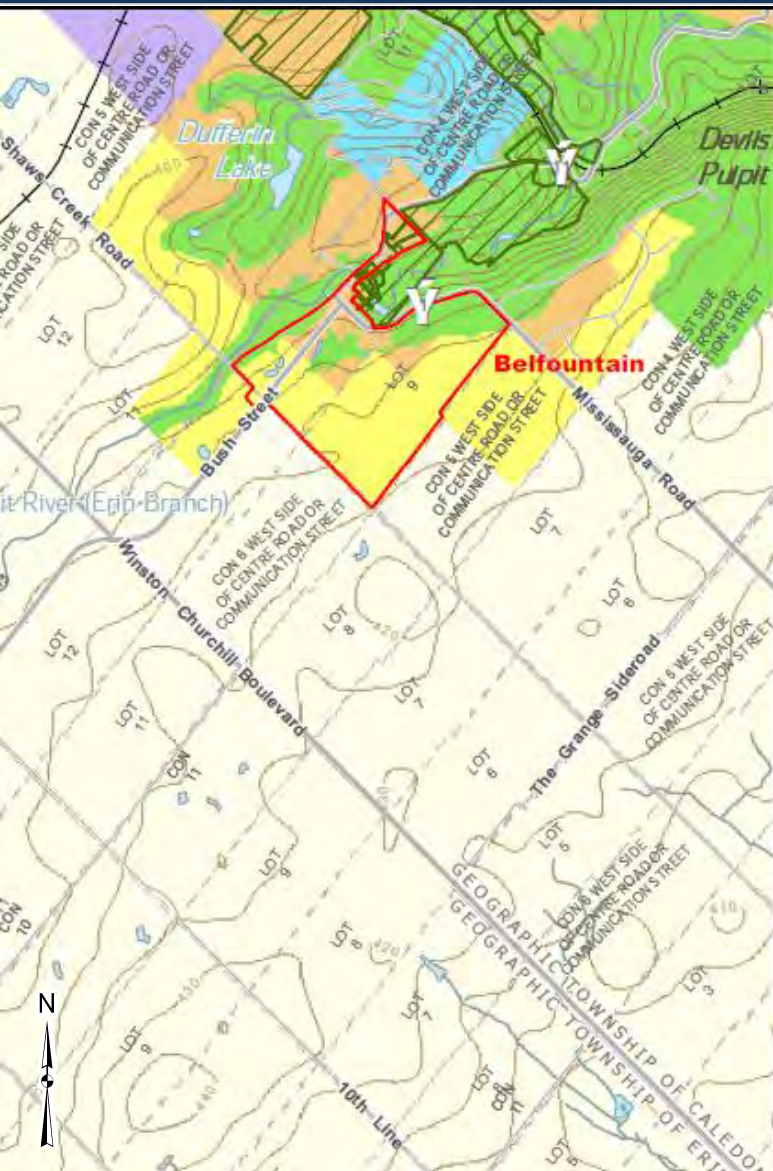


Policy Areas Map

- Greenbelt
- Region of Peel Core Greenlands



Niagara Escarpment



- Policy Areas Map
 - Niagara Escarpment Plan
 - NE and SE corners of study area

ELC Communities



• Ecological Land Classification
Vegetation Communities

Peel Region EA Existing ELC Communities and Locations of Natural Features



November 8, 2012, Project No: NRS1-1337
UTM Zone 17, NAD 83 Scale: 1:17,000 (at 11x17")
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Legend

- Study Area
- Waterbody
- Permanent Watercourse
- Intermittent Watercourse

<ul style="list-style-type: none"> Ecological Land Classification (CVR) Residential (CVR_3) Single Family Residential (CVR_4) Rural Property (FODM1) Dry - Fresh White Cedar Coniferous Forest Type (FOD) Deciduous Forest (FODM11) Naturalized Deciduous Hedge-row Ecosite (FOM) Mixed Forest 	<ul style="list-style-type: none"> (FOCM2-2) Dry - Fresh Upland Deciduous Forest Ecosite (FOCM5-5) Dry - Fresh Sugar Maple - Red Maple Deciduous Forest Type (FOCM6) Fresh - Most Sugar Maple Deciduous Forest Ecosite (FODM7.1) Fresh - Most White Elm Lowland Deciduous Forest Type (FODM7.2) Fresh - Most Green Ash - Hardwood Lowland Deciduous Forest Type (FMM) Mixed Forest (FOCM2-2) Dry - Fresh Sugar Maple - Hemlock Mixed Forest Type (FOCM3-3) Dry - Fresh White Cedar - Hardwood Mixed Forest Type (MAMM1-2) Catabol Graminoid Mineral Meadow Marsh Type (MAMM1-3) Reed-canary Grass Graminoid Mineral Meadow Marsh Type (MAMM3-1) Mixed Mineral Meadow Marsh Type (ME) Meadow 	<ul style="list-style-type: none"> (MECM3-5) Smooth Brome Graminoid Meadow Type (MEM3-8) Dry - Fresh Reed Canary Grass Graminoid Meadow Type (MEMM4) Fresh - Most Mixed Meadow Ecosite (OAG) Open Agriculture (OAGM1) Annual Row Crops (OAGM2) Perennial Cover Crops (OAGM4) Open Pasture (OAW) Open water (SWC) Coniferous Swamp (SWCM1-1) White Cedar Mineral Coniferous Swamp Type (SWCM1-1) White Cedar Mineral Coniferous Swamp (SWCO1-1) White Cedar Organic Coniferous Swamp Type (SWD) Deciduous Swamp (SWDM3-3) Swamp Maple Mineral Deciduous Swamp Type (SWDM4-5) Poplar Mineral Deciduous Swamp Type (SWTM2-1) Red-osier Dogwood Mineral Deciduous Thicket Swamp Type (SWTM3) Willow Mineral Deciduous Thicket Swamp Ecosite (SWTM3-5) Pussy Willow Mineral Deciduous Thicket Swamp Type (TAG) Treed Agriculture; (TAG) Coniferous Plantation (THD) Deciduous Thicket (THMM1-1) Native Mixed Regeneration Thicket Type (WOD) Deciduous Woodland (WODM4-3) Sugar Maple Deciduous Woodland Type
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Natural Areas and Features



Source: Esri, Intel, USDA, USGS, AeroX, GeoEye, Geomapping AeroGRID, IGN, ICS, and the GIS User

Peel Region EA Natural Areas and Features

0 100 200 300 400 500 Meters

March 20, 2013, Project No: NRSI-1337,
UTM Zone 17, NAD 83 Scale: 1:17,000 (at 11x17")

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NATURAL RESOURCE SOLUTIONS INC.
Aquatic, Terrestrial and Wetland Biologists

Legend

- Study
- Highway
- Primary Road
- Secondary Road
- Watercourse
- Waterbody
- Wooded
- ANSI, Life Science
- Provincially Significant Wetland (PSW)
- Non-Provincially Significant Wetland (PSW)
- Environmentally Sensitive Area (ESA)
- Evaluated Wetland (CVC)



Natural Environment



- ESA's, ANSI's, wetlands, woodlands, and watercourses
- Watercourses are tributaries to Credit River (Erin Branch) including Second Creek and Roger's Creek.
- All tributaries are managed as coldwater fish habitat



May 9, 2013

Natural Environment

- 30 Species at Risk Habitat & Species of Conservation Concern
 - 4 vegetation species
 - 10 bird species
 - 5 herpetofauna species
 - 3 mammals species
 - 5 insect species
 - 3 fish species



Western Chorus Frog



Jefferson Salamander



Butternut Tree



Bobolink



White-tailed Deer

Archaeological Assessment



Stone wall, cedar fence on Winston Churchill Boulevard



Boulder fence and cedar rail fence, Shaw's Creek and Olde Base Line



Cedar fence along Bush



Stone fence - Mississauga Road at The Grange



Winston Churchill Boulevard – low/wet and sloping beyond the road



Northwest view towards Bush – potential for archaeological significance in field

Built / Cultural Heritage

Mississauga / Bush

- 48 identified resources
- 4 designated under Ontario Heritage Act
- 23 listed by Town of Caledon
- Another 11 listed with high significance

Olde Baseline / Winston Churchill

- 21 identified resources
- None designated under Ontario Heritage Act



Belfountain Community Cemetery



Modern fence and fields adjacent to Olde Base Line Rd looking west

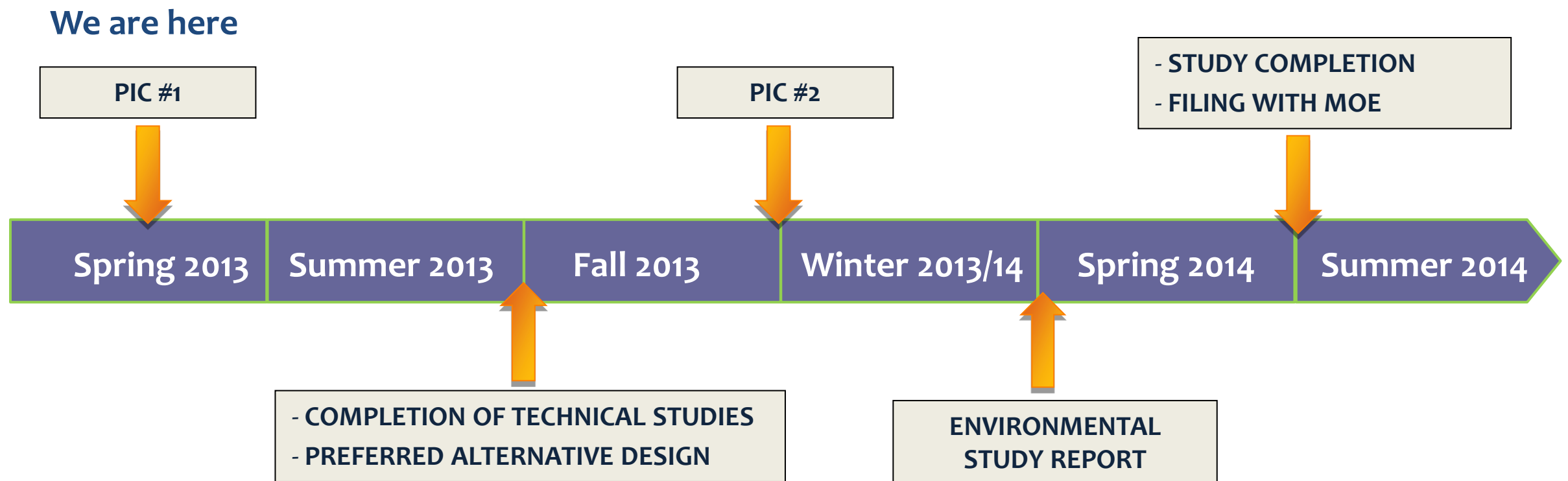


Belfountain Village Church



Belfountain Community Hall

Next Steps / Schedule



Thank You

Please complete your feedback form and place it in the Comment Box, or send your comments by email/fax/mail to any of the following team members by **Thursday, May 23, 2013.**

You can view tonight's information boards again on our website:

<http://www.peelregion.ca/pw/transportation/environ-assess/mississauga-road-bush.htm>

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Thank you for your participation



Environmental Assessment Study
Public Information Centre # 1
May 9, 2013
Feedback Form

1. Draft Problem Statement

The Project Team has developed the following problem statement:

Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the study area roads (Mississauga Road/Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road) consist of:

- Poor pavement conditions and drainage
- Deficient sightlines
- Safety for all road users, including safety of wildlife
- Motor vehicle accidents involving wildlife

Your comments on the draft problem statement:

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- Maintain the rural character and countryside scenic quality
- Preserve historic fences and heritage / cultural / archaeological features
- Preserve / enhance the natural environment
- Protect the Niagara Escarpment
- Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife
- Provide a context sensitive design
- Enhance local tourism and economic development of the area

Do these principles reflect your views Yes No Don't Know Tell us why you feel this way

I feel most of the above do reflect my views except for any potential increase in usage of Winston Churchill Blvd, in particular, for truck traffic.

I also strongly feel that economic development of the area is likely to affect most of the principles above mentioned.



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- ① Poor pavement conditions and drainage
- ② Deficient sightlines
- ③ Safety for all road users, including safety of wildlife
- ④ Motor vehicle accidents involving wildlife

Your comments on the draft problem statement:

- ① Drainage - 35 years of travelling all these roads have never presented drainage issues
- ② "deficient" sightlines - flat roads will not reduce accidents, only increase speed & therefore ~~more~~ accident
- ③ Wildlife are an integral part of our environment - It is driver awareness, not road conditions (except weather) which is important

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- ✓ Maintain the rural character and countryside scenic quality
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- ✓ Preserve / enhance the natural environment
- ✓ Protect the Niagara Escarpment
- ✓ Balance interests and meet needs of all road users - motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife
- ✓ Provide a context sensitive design
- NO Enhance local tourism and economic development of the area

Do these principles reflect your views: Yes No Don't Know Tell us why you feel this way:

Local economic development is not the focus of our NEC area - we love to have others enjoy & respect our trails, etc.

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

- a) Are there other operational and/or physical improvement options that you would like to see considered? Yes No Don't Know Please describe these

We are not looking for pristine road conditions - roads in adequate condition will force reduced speed -

Strict enforcement of using Hwy 124 for truck traffic

- b) Are there additional evaluation criteria that you would like to see considered? Yes No Don't Know Please describe these

Recognize and respect the need to preserve the uniqueness and recognized value of this world biosphere. The NEC needs to play a very important role in this process, as well as CVC.

4. Please share other comments about this study here

- Much of what is being suggested can be construed as "fear-mongering". Statistics apparently support very few, if any, serious accidents during a specified time frame.

So... where are these safety concerns coming from? Are roads that are flat going to reduce already low accident rates??

Our community would like transparency & open-ness - if you have a lot of money to spend, please consider what the residents feel is significant.



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- Poor pavement conditions and drainage
- Deficient sightlines
- Safety for all road users, including safety of wildlife
- Motor vehicle accidents involving wildlife

Your comments on the draft problem statement

No road is perfect. Since the these roads are not among (PST) ranking the top 150 riskiest roads, then we should do the BARE MINIMUM, such as rehabilitation of these roads but NOT reconstruction. Safety issues mostly can be addressed through lower speeds & enforcement of speed & enforcement of truck traffic. Preserving the environment is most important in this special area. LEAVE the hills Alone.

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- * ✓ Maintain the rural character and countryside scenic quality
- ✓ Preserve historic fences and heritage / cultural / archaeological features
- * ✓ Preserve / enhance the natural environment
- * ✓ Protect the Niagara Escarpment
 - Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife LOWERING SPEEDS WILL HELP ALL of these
 - Provide a context sensitive design
 - Enhance local tourism and economic development of the area

Do these principles reflect your views: Yes No ___ Don't Know ___ Tell us why you feel this way
in part

- But the other part is you can't do the first four issues well, if you make win/churchill & OLDE Baseline a primary trucking route, then those first four issues will be negatively impacted. This special area is more important to preserve than for flow of traffic, which has remained relatively stable for last 15 years.

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

a) Are there other operational and/or physical improvement options that you would like to see considered? Yes ___ No ___ Don't Know ___ Please describe these

Only resurface & rehabilitate - do not take out the curves or the hills of the study area

b) Are there additional evaluation criteria that you would like to see considered? Yes ___ No ___ Don't Know ___ Please describe these

I would like ~~the road~~ Winchell & QD Line to be designated Scenic Roads.

4. Please share other comments about this study here

The problem is speeding, not that the road needs to be rebuilt.



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- Poor pavement conditions and drainage
- Deficient sign/lighting
- Safety for all road users, including safety of wildlife
- Motor vehicle accidents involving wildlife

Your comments on the draft problem statement:

MISSISSAUGA ROAD PORTION AT OLDE BASE LINE ROAD SHOULD HAVE A LARGE SIGN ARROW POINTING TO WINSTON CHURCHILL BLVD, IF PEOPLE WISHED TO BYPASS GOING THRU BELEFOUNTAIN, TURN LEFT - WEST BOUND TO W.C. - SLOW DOWN, LOOK OUT FOR WILDLIFE. A CERTAIN PERCENTAGE ARE HEADING TO BARBONE, ERIN HILLSBURGH

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- Maintain the rural character and countryside scenic quality
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- Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife
- Provide a context sensitive design
- Enhance local tourism and economic development of the area

Do these principles reflect your views Yes No Don't Know Tell us why you feel this way

I HEAD DOWN TO MY BROTHER IN LAW'S FARM IN CHELTENHAM, McLAUGHLIN ROAD/OLD SCHOOL ROAD - I LIVE IN ERIN, I ALWAYS GO DOWN W.C. BLVD, TO OLDE BASE LINE, EAST ON OLDE BASE LINE TO MISS RD. - MISS RD S TO HUNST I MYSELF + MY WIFE AND SON ALWAYS GO THE ROUTE. HAVE DONE SINCE 1984. PRESERVE / PROTECT RURAL CHARACTER / HERITAGE, *POST MORE SIGN / WILDLIFE / DEER. WARNINGS

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

- a) Are there other operational and/or physical improvement options that you would like to see considered? Yes ___ No ___ Don't Know Please describe these

- PRIMARY RECOURSE - CORRECT CURRENT DEFICIENCIES - DO NOT RAISE SPEED LIMITS, "MORE SIGNAGE & OPP ENFORCEMENT" "IMPROVE SAFETY CONDITIONS FOR ROADWAY"

- b) Are there additional evaluation criteria that you would like to see considered?

Yes No ___ Don't Know ___ Please describe these

MISSISSAUGA ROAD REQUIRES A GREAT DEAL OF ROAD WORK - OLD BASE OF CONROY ROAD NEEDS WORK, IMPROVING - TO IMPROVE SAFETY AT RESIDENTS DRIVEWAYS BUT DO NOT RAISE THE SPEED LIMIT.

4. Please share other comments about this study here

ENHANCE BIKE SAFETY USE LANES, USE OF BIKES, LESS VEHICULAR TRAFFIC NO LARGE TRUCKS,

- RESIDENTS TRAFFIC SAFETY, "IF BRAMPTON BRICK WISHES TO USE ROAD / KING ST TO MISS RD, - TO OLD BRICK YARD, CHARGE THEM A TOLL USAGE - \$2.00 PER TRUCK IN, \$2.00 OUT - EACH TRUCK - EACH LOAD, + HAVE THEM PAY 5, MILLION DOLLARS UP-FRONT TO PAY FOR IMPROVEMENTS TO KING ST MISS RD - \$5, MILLION DOLLARS A YEAR - UP FRONT, - EVERY YEAR.

"ALLOW ROADS FOR - NITE SPEED LIMIT - WILDLIFE SAFETY"

"APPROPRIATE SPEED PER SECTION OF ROAD"



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Public Information Centre # 1**

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- Poor pavement conditions and drainage
- Deficient sightlines
- Safety for all road users, including safety of wildlife
- Motor vehicle accidents involving wildlife

Your comments on the draft problem statement

The person's 'problem' is another person's 'feature'. Most accidents in our area result from excessive speed. Just sit on Olde Baseline Road between Winston Churchill and Mississauga during morning or evening and check the speeds. They are EXCESSIVE.
We do not want changes that make people feel like going faster is OK.

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- Maintain the rural character and countryside scenic quality
 - Preserve historic fences and heritage / cultural / archaeological features
 - Preserve / enhance the natural environment
 - Protect the Niagara Escarpment
 - Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife
 - Provide a context sensitive design
 - Enhance local tourism and economic development of the area
- no - this could be interpreted to mean quarry proposals.*

Do these principles reflect your views Yes ___ No Don't Know ___ Tell us why you feel this way

Trucks should not be using Olde Baseline Road. We strongly oppose any suggestion to permit truck usage or to increase usage. Already they try to use this route to 'gain time' in their trips.
The other principles make sense.

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

- a) Are there other operational and/or physical improvement options that you would like to see considered? Yes No Don't Know Please describe these

Would want all 'improvement' options that would be made for trucks to be eliminated. Another route should be provided for trucks.

If Winston Churchill and Old Basline Road had never been paved, trucks would not be using this route. 'Improvements' will bring increased speeds and more traffic from trucks. In our view, these are

- b) Are there additional evaluation criteria that you would like to see considered? not improvements, Yes No Don't Know Please describe these

4. Please share other comments about this study here

We have observed the increased traffic and speeds since Old Basline was paved. Straightening Old Basline brought increased speeds.

We don't want drivers to have a sense that driving faster is now safer. The OPP has no resources to regulate speeds on Old Basline even now.

Please look at ways to reduce speeds on Old Basline and to reduce its usage as a commuter road. That status did not exist until Winston Churchill and Old Basline were paved.



Mississauga Road, Olde Base Line Road, Bush Street, Old Main Street and Winston Churchill Boulevard



Environmental Assessment Study Public Information Centre # 1 May 9, 2013 Feedback Form

1. Draft Problem Statement

The Project Team has developed the following problem statement.

Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the study area roads (Mississauga Road/Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road) consist of:

- Poor pavement conditions and drainage
- Deficient sightlines
- Safety for all road users, including safety of wildlife
- Motor vehicle accidents involving wildlife

Your comments on the draft problem statement

Draft problem statement does not state that WCB + OBL will be a truck haul route & only mentioned to north

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- Maintain the rural character and countryside scenic quality
- Preserve historic fences and heritage / cultural / archaeological features
- Preserve / enhance the natural environment
- Protect the Niagara Escarpment
- Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife
- Provide a context-sensitive design
- Enhance local tourism and economic development of the area

Do these principles reflect your views Yes ___ No Don't Know ___ Tell us why you feel this way

7 points above are impossible if area to become truck route.

Should prioritize road users as follows: 1) motorists (not trucks) 2) truck haul route horse, trucks & farm vehicles. There are maps

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

a) Are there other operational and/or physical improvement options that you would like to see considered? Yes No Don't Know Please describe these

Widen #124 to 4 lanes to accommodate, aggregate trucks + commuter traffic. This will maintain the area to reduce people count.

b) Are there additional evaluation criteria that you would like to see considered? Yes No Don't Know Please describe these

4. Please share other comments about this study here

I note with interest the comments of the CWC. All believe that it was transparent. If it were transparent why did they not know about the truck route. See discussion WCB + ORL co truck haul route sent meeting tonight.



Environmental Assessment Study
Public Information Centre # 1
May 9, 2013
Feedback Form

1. Draft Problem Statement

The Project Team has developed the following problem statement.
Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the study area roads (Mississauga Road/Olde Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road) consist of:

- Poor pavement conditions and drainage
- Deficient sightlines
- Safety for all road users, including safety of wildlife
- Motor vehicle accidents involving wildlife

Your comments on the draft problem statement

Poor Pavement Conditions & drainage
are simply maintenance issues.
Deficient sightlines are what give these roads
character, removing crests & curves will spoil
their appeal and increase traffic speed. How
will that help residents or wildlife?

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- Maintain the rural character and countryside scenic quality
- Preserve historic fences and heritage / cultural / archaeological features
- Preserve / enhance the natural environment
- Protect the Niagara Escarpment
- Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife
- Provide a context sensitive design
- Enhance local tourism and economic development of the area

Do these principles reflect your views Yes No Don't Know Tell us why you feel this way

There are RURAL roads in a RURAL
area, whilst they are regional roads
they are not highways.

People choose to live in these areas and
along these roads because of what they
are, not what commuters or engineers
think they should be.

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

a) Are there other operational and/or physical improvement options that you would like to see considered? Yes ___ No ___ Don't Know ___ Please describe these

b) Are there additional evaluation criteria that you would like to see considered? Yes ___ No ___ Don't Know ___ Please describe these

4. Please share other comments about this study here

The Scale of this Study and the possible impacts of some of the proposals seem to be completely out of proportion to the reality of the situation.

- ① traffic has been more or less stable over the last 15 years, with a projected increase of only 2% per year into the future.
- ② Accident Statistics hardly seem to indicate a major problem. Yes there are some sightline challenges, wildlife will cross roads but perhaps it is these challenges along with the scenery that help to slow traffic down.

ONLY PAGE

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

- a) Are there other operational and/or physical improvement options that you would like to see considered? Yes No Don't Know Please describe these

SAFETY → SCHOOL BUSES
FOR → GARBAGE TRUCKS
→ FARMERS
→ BIKERS

- b) Are there additional evaluation criteria that you would like to see considered? Yes No Don't Know Please describe these

DO ANYTHING TO PREVENT LOSS OF LIFE
EVERYONE NEEDS TO USE THESE ROADS

4. Please share other comments about this study here

DON'T BE RAILROADED BY VOCAL
MINORITY WHO MAKE IT UNCOMFORTABLE
TO COMMENT OR DISCUSS
PERTINENT SAFETY + LOGISTICAL
TRAFFIC MOVEMENT ESSENTIAL
TO THE STUDY
THANK YOU,
FROM CONCERNED RESIDENT





Environmental Assessment Study
Public Information Centre # 1
May 9, 2013
Feedback Form

1. Draft Problem Statement

The Project Team has developed the following problem statement.

Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the study area roads (Mississauga Road/Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road) consist of:

Poor pavement conditions and drainage

~~Deficient sightlines~~

~~Safety for all road users, including safety of wildlife~~

~~Motor vehicle accidents involving wildlife~~

Your comments on the draft problem statement

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- Maintain the rural character and countryside scenic quality
- Preserve historic fences and heritage / cultural / archaeological features
- Preserve / ~~enhance~~ the natural environment
- Protect the Niagara Escarpment
- ~~Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife~~
- ~~Provide a context sensitive design~~
- Enhance local tourism and economic development of the area

Do these principles reflect your views Yes ____ No ____ Don't Know ____ Tell us why you feel this way



Environmental Assessment S
Public Information Centre :
May 9, 2013
Feedback Form

1. Draft Problem Statement

The Project Team has developed the following problem statement.

Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the roads (Mississauga Road/Old Main Street, Bush Street, Winston Churchill Boulevard and Olde B Road) consist of:

- Poor pavement conditions and drainage*
- Deficient sightlines*
- Safety for all road users, including safety of wildlife*
- Motor vehicle accidents involving wildlife*

Your comments on the draft problem statement

Of primary concern to me is Olde Base Line
 > all above deficiencies should be corrected/im.
 As a proposed truck route, school bus & cycl
 safety is critical.

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have different interests.

- Maintain the rural character and countryside scenic quality
- Preserve historic fences and heritage / cultural / archaeological features
- Preserve / enhance the natural environment
- Protect the Niagara Escarpment
- Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm horses, trucks, wildlife
- Provide a context sensitive design
- Enhance local tourism and economic development of the area

Do these principles reflect your views Yes ___ No Don't Know ___ Tell us why you feel

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements with preliminary evaluation criteria that would be used to evaluate potential solutions.

- a) Are there other operational and/or physical improvement options that you would like to see considered? Yes No ___ Don't Know ___ Please describe these

- consider intersection improvements
ie roundabouts

- special attention to make all driveway
entrances/exits safe

- b) Are there additional evaluation criteria that you would like to see considered? Yes No ___ Don't Know ___ Please describe these

- important to evaluate long-term solutions
not just short term

4. Please share other comments about this study here

DO NOT DELAY!



**Environmental Assessment Study
Public Information Centre # 1
May 9, 2013
Feedback Form**

1. Draft Problem Statement

The Project Team has developed the following problem statement.

Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the study area roads (Mississauga Road/Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road) consist of:

- Deficient sightlines*
- Safety for all road users, including safety of wildlife*
- Motor vehicle accidents involving wildlife*

Your comments on the draft problem statement

I agree with all of the above. I would include the accomodation of all road users, cars, trucks, school busses, farm machinery, bicycles and motorcycles.

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- Maintain the rural character and countryside scenic quality
- Preserve historic fences and heritage / cultural / archaeological features
- Preserve / enhance the natural environment
- Protect the Niagara Escarpment
- Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife
- Provide a context sensitive design
- Enhance local tourism and economic development of the area

Do these principles reflect your views Yes ___ No ___ Don't Know X Tell us why you feel this way

I believe that you can have your cake and eat it too. Balancing the interests of all road users can

be accomplished while still achieving all of the above. I do not agree with those who wish to

preserve the area as it is today by maintaining a road network that is clearly deficient, unsafe, and is not

fulfilling its planned function. A nice, safe, full service regional road will be attractive, pleasant to drive on

and will boost property values. All this while providing a safe and efficient route for people and goods.

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

- a) Are there other operational and/or physical improvement options that you would like to see considered? Yes No Don't Know Please describe these

I think the improvement of sightlines and the reconstruction of the paved surface are the most important improvements. A traffic circle could be implemented at Miss/OBL. Also a sweep corner with at the intersection could be considered at OBL/WCB. I also strongly believe that these roads should be improved to a full load Primary Truck Route as described in the Peel Road Network Study that designated OBL and WCB as Primary Truck Routes.

- b) Are there additional evaluation criteria that you would like to see considered? Yes No Don't Know Please describe these

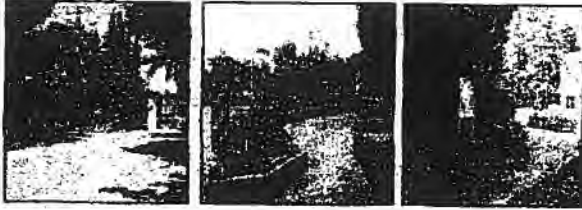
The overall savings in kilometers driven through Peel by trucks should be considered. Every round trip truck haul would save 28 km using this network. While it is important to get the local input into your study, at some point we need a regional network that some locals who are protecting their back yard may not agree with. I live on one of the major intersections in this study area and I support the truck use of these roads.

4. Please share other comments about this study here

The time is now to make these improvements and establish a Major Road Network in this area that is more than just a line on a map. Industry has been suffering over the decades that they have been denied an efficient route through this area. As a result millions of kilometers of unnecessary travel have taken place. With the big build to the south on our doorstep we must establish a full service road through this area to link Brampton with Caledon and Wellington County. In future Mississauga Road will have a key intersection on the GTA West Corridor and this link in the network will be ever more critical.



Mississauga Road, Olde Base Line Road, Bush Street,
Old Main Street and Winston Churchill Boulevard



Environmental Assessment Study
Public Information Centre # 1
May 9, 2013
Feedback Form

1. Draft Problem Statement

The Project Team has developed the following problem statement.

Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the study area roads (Mississauga Road/Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road) consist of:

- ① Poor pavement conditions and drainage
- ② Deficient sightlines
- ③ Safety for all road users, including safety of wildlife
- ④ Motor vehicle accidents involving wildlife

Your comments on the draft problem statement

- ① I am no expert but to my "eye" they are fine.
- ② If you're going the posted speed the sight lines are fine for rural living. Might be a few driveways that need to be improve.
- ③ The speed and quantity of vehicles have increased therefore wild life and road users are at higher risk.
- ④ This will be difficult to control.

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- Maintain the rural character and countryside scenic quality
- Preserve historic fences and heritage / cultural / archaeological features
- Preserve / enhance the natural environment
- Protect the Niagara Escarpment
- Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife
- Provide a context sensitive design
- Enhance local tourism and economic development of the area

Do these principles reflect your views Yes ___ No Don't Know ___ Tell us why you feel this way

- I would modify "trucks" to read "trucks for local delivery only"
- Enhance local tourism and economic development of the area should not be our concern. Tourism has always looked after itself. We don't want any economic development in our area as above principles would not be able to continue. There are other roads (eg. Hwy 24/Hwy 10) that can be used for any outside users.
- I would add another principle -- Do not make these roads a haul route for commercial vehicles.

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

a) Are there other operational and/or physical improvement options that you would like to see considered? Yes ___ No Don't Know ___ Please describe these

b) Are there additional evaluation criteria that you would like to see considered? Yes No Don't Know ___ Please describe these

It would benefit all parties if the Region would do a cost benefit analysis of continuing repairing these roads vs. reconstruction.

4. Please share other comments about this study here

We have lived here since 1973 (40 years) and my family has seen these four roads change and be repaired over this time. These changes have been done as a result of increase of traffic, public safety issues and Regional of Peel's standards. We would like ^{the} area to continue its rural character for generations to come. Doing a major overhaul of these roads you will risk public safety as the volume of traffic will increase and speed at which it travels will increase as well. We have watched this occur over the last 40 years!!

**Mississauga Road, Olde Base Line
Road, Winston Churchill
Boulevard, Bush Street and Old
Main Street, Environmental
Assessment Study**



May 9, 2013

**PUBLIC INFORMATION CENTRE #1
FEEDBACK REPORT**

This report was prepared by the Independent Facilitator and represents the feedback received at the Public Information Centre #1 held on May 9, 2013. It includes the themes and questions noted through the discussion and input received through the completed and returned Feedback Forms.

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For further information contact Asha Saddi, Technical Analyst, Infrastructure Programming & Studies, Transportation Division, Public Works, Region of Peel, Tel: 905-791-7800, extension 7794
Fax: 905-791-1442 Asha.Saddi@peelregion.ca

Comments about this report should be directed to Facilitator, Sue Cumming, MCIP RPP,
Cumming+Company at cumming1@total.net or Tel: 866-611-3715.

Mississauga Road, Olde Base Line Road, Winston Churchill Boulevard, Bush Street and Old Main Street, Environmental Assessment Study

PUBLIC INFORMATION CENTRE #1 FEEDBACK REPORT

1. ENVIRONMENTAL ASSESSMENT STUDY AND CONTEXT FOR PUBLIC INFORMATION CENTRE #1

In June 2009 Peel Region began a Schedule “C” Municipal Class Environmental Assessment (EA) for improvements to Mississauga Road from Olde Base Line Road to Bush Street and Bush Street from Mississauga Road to Winston Churchill Boulevard. The Region of Peel expanded the limits of the EA to include Olde Base Line Road from Mississauga Road to Winston Churchill Boulevard and Winston Churchill Boulevard from Olde Base Line Road to Bush Street (see map).

Why was the study area expanded?

Based on the feedback received for the Mississauga Road / Bush Street EA the Region of Peel expanded the study area to review road safety, sightlines, drainage, parking and pedestrian and cycling needs.

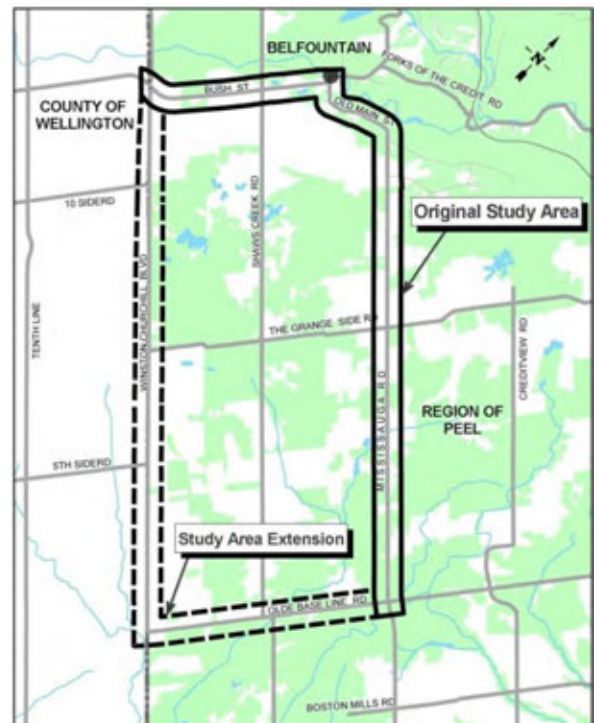
What is this project about?

Existing problems on the study area road network consist of:

- Deficient pavement conditions and drainage
- Deficient sightlines
- Safety for all road users
- Safety of wildlife
- Motor vehicle accidents

The purpose of the project is:

- Rehabilitation of the roads
- Enhancing safety
- Supporting bicycling and walking



As described at the Public Open House held on October 30, 2012 and the Public Information Centre #1 held on May 9, 2013 this EA study will **not** be considering road widening or increasing the number of lanes. The Project Team will build on the previous study information to develop a plan for the study area roads that meets the needs of all road users and maintains the rural character of the community.

The Process

The aim of the Environmental Assessment process is to provide everyone who has an interest or stake in the study area with the opportunity to create the best solution. The Project Team, with your input, will:

- define the problem statement (Winter/Spring 2013)
- develop and evaluate planning alternatives; (Summer 2013) and
- determine a preferred solution (Fall 2013/ Winter 2014)

Opportunities will be provided for the public to be involved through planned public meetings at key milestones as the study progresses. A Community Working Group (CWG) has also been established. The CWG is a representative broadly based group of interested stakeholders who will participate in focused discussion on project issues through workshops held over the course of the study. Two meetings of the CWG have been held – on October 23, 2012 and April 4, 2013. Meeting notes from these discussions are available on the project study website (peelregion.ca/pw/transportation/envIRON-assess/mississauga-road-bush.html). The CWG will meet again in the Fall of 2013.

Enhanced consultation and public involvement is being undertaken through the establishment of the Community Working Group and the holding of a Public Open House at the outset of the study, prior to the formal public meetings. The Open House was held so the Project Team could meet with community residents and stakeholders to discuss the process, and to learn about transportation issues and valued community characteristics. Over 100 attendees were at the Open House held from 6:30 p.m. to 9:00 p.m. at the Belfountain Public School. A workshop booklet was provided and tables were set up to allow people to provide their written responses contained in the booklet.

Your participation is important and your comments are valued. A second Public Information Centre will be held in the Fall 2013. The Region of Peel encourages you to provide input at future public meetings or by providing comment as the study develops. Contact information is found at page 8 of this report.

The Region of Peel is undertaking this environmental assessment study through a Context Sensitive Planning and Design Approach which focuses on improvements to enhance the experience for all road users and reflects the character of the community. This is important as it will ensure that the solutions fit with the rural and scenic quality of the area and are sensitive to the needs of the community.

Public Information Centre #1 held May 9, 2013

The first Public Information Centre was held on May 9, 2013 at the Belfountain Public School from 6:30 p.m. to 9:00 p.m. Over 70 people attended. Notice for the May 9, 2013 Public Information Centre was provided through the following:

- Mailing of notices to property owners fronting / backing along the study area corridors
- Project Study Web site
- Local newspaper advertisement:
 - Erin Advocate on April 24, May 1 and May 8, 2013
 - Caledon Enterprise/Caledon Citizen on April 25 and May 2, 2013
 - George Town Independent/Acton Free Press on April 25 and May 2, 2013
 - Wellington Advertiser on April 26 and May 3, 2013

The meeting was organized with a drop-in from 6:30 p.m. to 7:00 p.m. and the opportunity for viewing project background displays. This was followed with a presentation at 7:00 p.m. by Tyrone Gan, Project Manager, HDR Corporation lead transportation planning consultant for the project. The Project Team gave a presentation on the following:

- Information relative to Region of Peel Strategic Goods Movement Network Study and Regional Road Characterization Study approved at Regional Council on May 9, 2013.
- Purpose of the EA Study, overview of identified problems and results of needs assessment including traffic analysis and safety considerations, draft problem statement and principles for generating alternative solutions, proposed alternative operational and physical improvements that could be considered and the proposed draft evaluation criteria.

A public discussion followed. The meeting was facilitated by an independent facilitator, Sue Cumming, Cumming+Company public engagement lead for the project.

The Public Information Centre presentation and display board material is available on the project study website. A Feedback Form was provided to enable attendees to provide written responses.

This report, written by the Facilitator, is intended to provide a synthesis of the overall key messages heard and provide information on next steps. The Appendices contain the detailed public comments noted through the discussion at the meeting (Appendix A), responses in the Feedback Forms (Appendix B) and comments received through emails and letters (Appendix C). The comments received through Public Information Centre #1 will be considered by the Project Team and will help in informing the Project Team as the project moves forward. This report will be posted on the project study website and a letter will be forwarded to all residents in the area advising of the availability of the report. Copies of the Feedback Report are also available for viewing at the Belfountain Public Library.

2. KEY MESSAGES HEARD

There is significant community interest in the environmental assessment study. The residents and stakeholders who attended the May 9, 2013 Public Information Centre had many comments about the study scope and process, transportation review findings, regional standards and types of solutions that would be considered. Many indicated that they had attended previous meetings.

A number of residents question the Region of Peel's characterization of safety considerations expressing concern that the Region's safety standards could lead to dramatic changes in the profile of the roads which is not supported by the community. Residents indicated that the information shows that there has not been an increase in accidents in the area, that there have been no fatalities and that animal fatalities are part of living in a rural community. Many noted that they felt the pavement conditions could be addressed through patching and repaving in sections where needed and question the rationale for what they perceive to be unnecessary and costly reconstruction believing that there is no need to make changes to the roadway structure.

There is also concern that the Region of Peel will make changes that will increase the likelihood of these roads being used for more trucks. They believe that truck traffic through the area, particularly on Olde Base Line Road and Winston Churchill Boulevard, is an issue today. The majority of the residents oppose the establishment of truck priority routes through the Regional Strategic Goods Movement Network and object to having Winston Churchill Boulevard and Olde Base Line Road identified as such. Concerns expressed included noise, safety, speeding and incompatibility of tranquil character of area. Some of the written comments expressed support for improving truck routes and that trucks could and should be accommodated to service industry.

The majority of those that attended provided comments (written and through the discussion) conveying the importance of ensuring that the study outcomes do not impact the historic and much valued countryside and scenic character of the roads through the community and Village of Belfountain. While many identified concerns about pavement conditions, excessive speeding, proliferation of signage, increasing traffic from areas to the north, truck traffic, cyclists on the road and pedestrians, most residents are concerned about how solutions would be implemented. Many sought clarification on the stated commitment that no lane widening be considered.

The road profile is of significant importance to those in the community who want to see it maintained. The low increase in traffic volumes over the last 15 years together with low accident statistics lead many to question the motivation for the study and re-emphasized the need for care in identifying solutions. A key concern noted by many is the view that if "you fix it they will come" noting that flattening out the curves, smoothing out the roadway structure and taking away vegetation would lead to increased traffic and increased speeds.

Many residents appear to support the status quo or strategic rehabilitation of the roads. Other residents support reconstruction at key intersections, in areas where sightlines are problematic and cite the importance of improving the safety of roads for all users – including trucks.

The following is a synthesis of key messages heard.

a. **In developing operational and physical improvements, priority should be on maintaining the profile of the roads in the area.**

There is much concern that operational and physical improvements could change the road profile, flatten out certain areas, and reconstruct the roads which would significantly alter the much loved character of the area. The rural character and countryside scenic quality of the area is described as mature trees, natural vegetation, cedar rail, heritage fences, and hilly nature of the topography, extensive natural areas and curvature of the existing roadways. The road profile is what defines the area and residents are opposed to changes to the road profile that they believe would be made if the Region of Peel were to implement standards for road safety and road condition that might apply elsewhere.

b. **Only resurface and rehabilitate – do not take out the curves or hills of the study area. Residents prefer to see rehabilitation instead of reconstruction.**

Several residents question the road standards and asset management of the Region of Peel preferring more patching to reconstruction. Others want to better understand the condition of the roads in the study area today versus the Region of Peel's service level for all roads within Peel. Residents emphasized that this is a unique area with rural roads and that the hilly topography and natural environment are fundamental to the character of the area. Many stated that they are "okay" with having a road standard below that which may exist in other parts of Peel. Others want to see the road rehabilitated.

c. **Assess issue of truck usage on these roads relative to community impacts. Residents object to the creation of Truck Priority Routes along Winston Churchill Boulevard and Olde Base Line Road.**

There is opposition to the creation of Truck Priority Routes along Winston Churchill Boulevard and Old Base Line Road. Winston Churchill Boulevard is experiencing problems with trucks that travel too fast, are noisy, impact safety and the quality of life for the residents. Many are very concerned that there would be traffic accidents if this becomes a primary truck route.

Others, in written comments support truck travel along these roads for good movement through the area and would like to see the roads rehabilitated to accommodate trucks.

d. **Address speeding without making roads smoother and level which residents are concerned could make cars go faster and exacerbate existing speeding concerns.**

Many noted that making the road smoother and level will only make cars go faster. Residents are concerned that if the profile of the roads was flattened, this could lead to more speeding and collisions.

e. **Reduce posted speeds and increase enforcement on roads to reduce safety concerns.**

Residents support reduced posted speeds together with increased enforcement to address safety on the roads. Some would like to see the reduction of speeds for the whole area instead of having different speeds apply to different sections.

f. Assess the need for the Region of Peel to design and construct the roads to meet Regional safety standards.

Many residents could live with the roads as they are. They have seen the roads repaired over the years. They would like to see an approach that reflects the character of the area noting the relative stable nature of traffic over the past 15 years.

g. Review approaches for improvements to pavement condition.

The public would like to have more information at the next stage of the study on what good base/structure for the roadways would entail, and how this could be implemented without changes to the road profile. The Project Team will be addressing this through the provision of further information when the solutions are developed and reviewed with the public.

h. Improve sightlines by trimming back trees and overgrowth.

Residents do not support moving vegetation away from the road. There may be a small percentage of driveways where sightlines could be improved and this could be done by trimming back trees and growth in locations where it has overgrown.

i. Implement site specific improvements to address problem areas.

A number of residents support making improvements and feel that these roads need to be improved to address site specific concerns - for example, sightline deficiencies along Olde Base Line Road and the intersection of Olde Base Line Road and Mississauga Road.

j. Review potential property impacts affecting driveways, fences and vegetation.

Additional concerns have been noted about impacts to mature trees, cedar rail and heritage fences along the roadway.

k. Minimize impact from future growth north of the community.

Residents would like to see measures explored for minimizing the impact from future growth. Suggestions include improvement to Highway No. 124 to act as a by-pass. It was noted that good east/west truck routes already exist at King Street and Charleston Side Road (Highway No. 124) both leading to Highway No. 10.

l. Develop a realistic approach for accommodating pedestrians in the Village and for cyclists on major roads.

Residents would like to see a realistic focus on accommodation for pedestrians in the Village. They indicate that people do not walk on Mississauga Road or Winston Churchill Boulevard today. Some residents support paving the shoulder for cycling and pedestrian use.

m. Declutter signs.

Signage improvement is identified. Residents noted that there already are many signs today including animal crossing signs, watch for snow plough signs, different posted speed signs etc. A coordinated approach to signage would be appropriate.

3. NEXT STEPS

The comments received through the Public Information Centre will be considered by the Project Team as the project moves forward. After considering public comments on the evaluation criteria, guiding principles, and alternative solutions that were presented at the Public Information Centre, the next steps will be to evaluate the alternative solutions and determine preliminary recommendations.

The evaluation of the alternative solutions and preliminary recommendations will be undertaken during the Spring and Summer of 2013 and presented at the second Public Information Centre which will be held in the Fall of 2013. A notice for the second Public Information Centre will be mailed to all those on the project mailing list.

The next Community Working Group Meeting will be held in the Fall of 2013.

Progress on this study can be viewed on the project study website at:

peelregion.ca/pw/transportation/envIRON-ASSess/mississauga-road-bush.html

If you would like to comment on the study or be notified about future public meetings, please contact either one of the following Project Team members:

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Appendices

Appendix A Comments/ Questions noted through the discussion at PIC #1

Appendix B Public Comments noted through the Feedback Forms

Appendix C Comments received through emails and letters

Appendix A

COMMENTS/ QUESTIONS NOTED AT PUBLIC INFORMATION CENTRE #1

The following comments and questions were noted through the discussion:

Comments:

- a. This is a very special area. I fear that there would be more traffic accidents if this becomes a primary truck route. There are not many accidents right now!
- b. The presentation identified that many of the accidents were related to animal crossings. The natural environment, hilly terrain and topography are conducive to lots of wildlife. The issue is vehicle speeding along these roads. The more you flatten the profile of the roads, the more speeding will occur which could then lead to more collisions. It seems that this is counterproductive to what you are talking about. Making the road smoother and leveled will only make cars go faster.
- c. Any changes to the roads should not be about raising it to the top standards. This is a rural area which has a unique hilly topography. Very much opposed to changing this topography. Would like to see as little as possible when it comes to reconstruction.
- d. The Niagara Escarpment needs to be involved with this study and to protect the natural environment and uniqueness of the area.
- e. I read the newsletter and would like to hear from the Councillors why they think this study is necessary and to justify the comments made. Both Councillor Paterak and Councillor Thompson elaborated at the meeting on their comments underscoring their understanding of the character of the area and the need to balance any solution with level of service, connectivity of a Regional road network and having roads in good shape.
- f. I am not a resident of Belfountain and am here to ensure that the residents of this area are aware of issues that residents in our area have with Brampton Brick Yards with significant truck traffic – 5000 trucks, one per minute that are going through an area of the NEC. The natural environment and community character are being disregarded for industry and the site is being filled with 1.8 cubic metres of excess construction waste. The plan has already been endorsed by Council. This could happen here if you do not speak up now!
- g. Another individual spoke indicating that he resides on the Forks of the Credit and roads in that area were rebuilt five years ago. Every Friday and Saturday night there is racing along the roads. Speeding has become an even worse issue. If Mississauga Road is rebuilt and the hills taken out, you can expect to see more speeding here as well. Notwithstanding contact with the OPP there is no new enforcement coming. Our experience has shown that the OPP doesn't have the budget and manpower to enforce speeding on these rural roads.
- h. I appreciate that the Project Team is saying that they are trying to find the right balance of safety and not encouraging more speeding but there doesn't seem to be a safety

issue here. Do not take hills and valleys out of roadways within this study area as it will encourage more speeding.

- i. Would like to reinforce that part of the rural character is to not have increased traffic along these roads.
- j. Safety and traffic numbers don't support reconstruction in my view. Don't want to see the curves taken out. Without significant traffic increase it leaves us to wonder why a road reconstruction is being proposed. It is to service development from the Dufferin Aggregates Pit north in Erin off 10th Side Road. Have you looked at the proposed Solmar development? We fought the fight with the aggregates here and won. This study needs to respect that.
- k. Would like to understand why Winston Churchill Boulevard and Old Base Line Road and Bush Street are identified as a primary truck route. What types of trucks, how many, where are they coming from. People in this area do not want more trucks. I hear that you think there are a wide range of trucks that could use the roads, but this area is not compatible with larger trucks, with aggregate trucks and big loads. Highway No.124 should be used as a by-pass. Trucks are already on Highway No. 124, it is four lanes. Brampton Brick already uses it.
- l. There seems to be a focus in the presentation on hazards with poles and trees and poor sightlines. There may be a small percentage of driveways where sightlines could be improved and this could be done by trimming back trees and growth. If you move the vegetation away from the road, widen the roads and take out the hills you will effectively destroy the character of this area.
- m. There is a suggestion of signage as an improvement. We already have too many signs today. There are lots of animal crossing signs, watch for snow plough signs, different posted speed signs beyond which there is no benefit – we have a problem of visual pollution here. There is no net gain from putting up even more signs.
- n. I appreciate that you are saying that you want to take a fresh look at signage and would reduce sign clutter. You have also said that the prevailing speeds may be too high and that the posted speeds may not be appropriate. I don't want to see a different speed for each section of the road. This gets too confusing and people ignore the signs and go at a higher speed. If as you are saying you are going to determine the appropriate speed for each section of the road, could you not post it at the lower speed to provide consistency throughout the area? Why not post 50 or 60 kph on all roads?
- o. If you widen it, they will come. If you fix the roads, they will come. Fixing the roads could result in increased speeding along these roads.
- p. Need to accept that wildlife is going to be here and will be crossing roads. You should decide what speed is safe for night time for wildlife crossings (deer) and then compare that to what people are driving. The lower posted speed should become the regulated speed.

- q. Lots of taxpayers money goes to enforcement throughout the Region of Peel. We know that this is not a high priority area. If full time radar was used, five days a week that would slow people down. Set the limit and enforce it.
- r. The standards for the study area need to be compared to other roadways to further explain the meaning of the analysis/findings. The accidents noted do not show a high trend.
- s. No need for sidewalks, as pedestrian volumes are low. No one walks on Winston Churchill Boulevard or Mississauga Road. Can understand pedestrians may walk in the Village of Belfountain but not along the roads.
- t. By the way, there are no boulders in the roads.
- u. The Project Team should review the OMB decision on the quarry to better understand how significant the issue of trucks is to the people in the community.
- v. Would like to know more about what is happening at the Badlands. We see lots of people walking on the road and even a wedding party taking pictures. Parking is a big issue and there are safety concerns. How do I find out more? (Contact information was noted and Region of Peel staff involved with the Badlands will provide information to person who requested it).
- w. While overall traffic is low, there has been an increase in traffic on Winston Churchill Boulevard which needs to be addressed. Development in Wellington County will use these roads.
- x. Sightlines are an issue in some locations. We have many family members who live in the area. Sightlines on Olde Base Line Road are problematic. We see lots of cars come over the hill going too fast and going into ditches. There have been accidents. Someone did die here. We need to look at how to make the roads safer. I support road improvements for safety while maintaining the rural character. There are lots of things that could be considered – at Olde Base Line Road and Mississauga Road a traffic circle/ roundabout could be considered.
- y. I would like to see you consider holding an interim meeting before you come back with a solution at the next Public Information Centre. This would allow us to see what direction you are going in and to provide further input.

Questions:

- a. If safety is not a problem, why improve roads?
- b. The safety index is low but what can we compare to? Which standards are you trying to address.
- c. Collision impact by type shows animal type is the largest. Why would this be a reason to rebuild road?
- d. Why is this study being done and why now? Would like more clarification in light of low increase in traffic and low accident rate. Understand that you are referencing pavement conditions and safety concerns, but couldn't these be addressed by patching and other operational improvements?
- e. How does this area compare to safety in other areas? Is it in the bottom or top of areas identified for safety improvements?
- f. The Regional traffic forecast for all roads is 2% a year. What is driving the increase in traffic?
- g. If there is no vehicle traffic increase why is safety an issue? If the number of accidents is low, why do we need to fix roads?
- h. There are minimum design standards. These are rural roads and many roads in the Region of Peel fall below the standards. Are there liability issues for the Region of Peel? Is that what is driving this study? Why design to standard if there are no problems?
- i. Will you widen the roads?
- j. Data in traffic trend graphs in presentation (AADT) shows an increase in traffic; however, PIC in 2010 showed different data. Why the difference in data? Seems like there is an island of information without anything to compare it to.
- k. Will there be a need for the Region of Peel to acquire land?
- l. Will you be undertaking a cost /benefit analysis relative to safety? Will you be looking at asset management and how much it would cost to just patch vs. repaving or reconstructing.
- m. Would like to better understand road standards and requirements. When will the information on road standards requested at the Community Working Group Meeting be available?

Appendix B

COMMENTS FROM FEEDBACK FORMS

Feedback Forms were provided to obtain written responses. The following responses were received. These are verbatim comments transcribed from the individual forms.

1. Draft Problem Statement

The Project Team developed the following draft problem statement:

Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the study area roads (Mississauga Road/Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road) consist of: Poor pavement conditions and drainage, deficient sightlines, safety for all road users, including safety of wildlife, motor vehicle accidents involving wildlife.

The following were noted on the Feedback Forms. Each number represents a different response:

Table 1 – Comments on Draft Problem Statement

1. No road is perfect. Since these roads are not among (PSI ranking) the top 150 riskiest road, then we should do the bare minimum, such as rehabilitation of these roads not reconstruction. Safety issues mostly can be addressed through lower speeds and enforcement of speeds and enforcement of truck traffic. Preserving the environment is most important in this special area. Leave the hills alone!
2. First, with respect to drainage – 35 years of travelling these roads have never presented drainage issues. Second, with respect to deficient sightlines – flat roads will not reduce accidents, only increase speed and therefore accidents. Third, wildlife is an integral part of our environment. It is driver awareness, not road conditions, (except weather) which is important.
3. Mississauga Road Portion at Olde Base Line Road should have a large sign arrow pointing to Winston Churchill Boulevard. If people wished to by-pass going through Belfountain, turn left, west to Winston Churchill Boulevard. Slow down, look out for wildlife. A certain percentage is heading to Brisbane, Erin and Hillsburgh.
4. Draft problem statement does not state that Winston Churchill Boulevard and Olde Base Line Road will be a truck haul route – only mentioned tonight.
5. The draft problem statement is relative only in comparison to other roads in the area and are better than some; worse than others. With respect to drainage, I have not experienced road flooding in the study area. Deficient sightlines only occur at a few spots on the study and individual property owners are aware of the limitations in living on a somewhat hilly road. However, as a draft problem statement, it covers all possible issues.
6. Of primary concern to me is Olde Base Line Road – all above deficiencies should be corrected/improved. As a proposed truck route, school bus and cyclist safety is critical.
7. I agree with all of the above. I would like the accommodation of all road users, cars, trucks, school buses, farm machinery and motorcycles.
8. One person's problem is another person's feature. Most accidents in our area result from excessive speed. Just sit on Olde Base Line Road between Winston Churchill Boulevard

and Mississauga Road during morning or evenings and check the speeds – they are excessive! We don't want changes that make people feel like going faster is OK.

9. Poor pavement conditions and drainage are simply maintenance issues. Deficient sightlines are what give these roads character. Removing crests and curves will spoil their appeal and increase traffic speed. How will that help residents or wildlife?
10. I only see the poor pavement conditions and drainage problems as problem. Travelling at posted speed limits minimizes all other.
11. On pavement conditions and drainage – I am no expert but to my eye they are fine. If you are going the posted speed the sightlines are fine for rural living. Might be a few driveways that need to be improved. The speed and quantity of vehicles have increased therefore wildlife and road users are at higher risk and accident involving both will be difficult to control.
12. Problem is only poor pavement conditions and drainage.

2. Principles for guiding solutions to address the problem statement

The following principles have been developed to provide guidance to the study team when generating alternative solutions to address the problem statement. It is recognized that different users may have competing interests.

- *Preserve historic fences and heritage / cultural / archaeological features*
- *Preserve / enhance the natural environment*
- *Protect the Niagara Escarpment*
- *Balance interests and meet needs of all road users – motorists, pedestrians, cyclists, farm vehicles, horses, trucks, wildlife*
- *Maintain the rural character and countryside scenic quality*
- *Provide a context sensitive design*
- *Enhance local tourism and economic development of the area*

Do these principles reflect your views? Yes ____ No ____ Don't Know ____ Tell us why you feel this way

Table 2 – Comments on Principles for generating alternative solutions

The following responses indicated “Yes” and included the following comments:

1. Yes, in part. First four reflect my views. Lowering speeds will also help with balancing interests of all road users. But the other part is you can't do the first four issues well, if you make Winston Churchill and Olde Base Line Road primary trucking routes. Then these first four principles will be negatively impacted. This special area is more important to preserve than for the flow of traffic, which has remained relatively stable for the past 15 years.
2. Yes, to first six. No to enhance local tourism. Local economic development is not the focus of our NEC area. We love to have others enjoy and respect our trails etc.
3. Yes, I head down to by brother in-laws farm in Cheltenham, McLaughlin Road and Old School Road. I live in Erin. I always go down Winston Churchill Boulevard to Olde

Base Line Road, east on Olde Base Line Road to Mississauga Road then Mississauga Road to King Street. I myself and my wife and soon always go this route and have done so since 1984. Preserve, protect rural community and heritage. Post more signs to warn of wildlife/deer to protect wildlife.

4. Yes, these are rural roads in a rural area, whilst they are regional roads they are not highways. People choose to live in these areas and along these roads because of what they are, not what commuters or engineers think they should be.
5. Yes, we moved here 30 years ago just for these reasons.
6. The principles somewhat reflect my views. Mississauga and Main Street will remain a two lane road with only minor resurfacing of road. There should be increased emphasis on road quality on Winston Churchill and Olde Base Line Road with a light at Mississauga and Olde Base Line Road. Although there is emotional appeal to restrict trucks and economic development, the aggregate industry has been operational for over 150 years and has provided tax revenues for generations. What is the rate of accident relating specific to truck traffic in the area?

The following responses indicated “No” and included the following comments:

1. No, seven points above are impossible if area to become a truck route. Should prioritize road users as follows: motorists (no trucks), horse trailers, cyclists, farm vehicles, trucks, pedestrian and wildlife.
2. No, the EA team should be guided by the regional plan primarily.
3. No, trucks should not be using Olde Base Line Road. We strongly oppose any suggestion to permit truck usage or to increase usage. Already they try to use this route to gain time on their trips. No, to economic development as this could be interpreted to mean quarry proposals. The other principles are fine.
4. No, I would modify trucks to read “trucks for local delivery only”. Enhance local tourism and economic development of the area should not be our concern. Tourism as always looked after itself. We don’t want any economic development in our area as above. Principles would not be able to continue. There are other roads (i.e. Highway No. 124 and Highway No. 10 that could be used for any outside users. I would add another principle – do not make these roads a haul route for commercial vehicles.
5. Remove “enhance” natural environment, balancing interests and providing context sensitive design.

The following responses indicated “Don’t Know” and included the following comments:

1. Don’t know, I believe that you can have your cake and eat it too. Balancing the interests of all road users can be accomplished while still achieving all of the above. I do not agree with those who wish to preserve the area as it is today by maintaining a road network that is clearly deficient, unsafe, and is not fulfilling its planned function. A nice, safe, full service regional road will be attractive, pleasant to drive on and will boost property values. All the while providing a safe and efficient route for people and goods.
2. Don’t know. If feel most of the above do reflect my views except for any potential increase in usage of Winston Churchill Boulevard, in particular for truck traffic. I also feel strongly that the economic development of the area is likely to affect most of the principles above mentioned.

3. Preliminary Alternative Solutions and Evaluation Criteria

The Project Team presented preliminary operational improvements and physical improvements together with preliminary evaluation criteria that would be used to evaluate potential solutions.

- a) Are there other operational and/or physical improvement options that you would like to see considered? Yes___ No___ Don't Know___ Please describe these

Table 3 – Comments on Preliminary Alternative Solutions presented at PIC #1

1. Only resurface and rehabilitate – do not take out the curves or hills of the study area.
2. Yes, we are not looking for pristine road conditions – roads in adequate condition will force reduced speed. Strict enforcement of using Highway No. 124 for truck traffic.
3. Yes, widen Highway No. 124 to four lanes to accommodate aggregate trucks and commuter traffic. This will maintain this area to what people want.
4. Yes, school buses, garbage trucks, farmers, bikers.
5. Yes, consider intersection improvements i.e. roundabouts. Special attention to make all driveway entrances/exits safe.
6. Yes, I think the improvement of sightlines lines and the reconstruction of the paved surfaces are the most important improvements. A traffic circle could be implemented at Mississauga Road and Olde Base Line Road. Also a sweep corner with an intersection could be considered at Old Base Line road and Winston Churchill Boulevard. I strongly also believe that these roads should be improved to a full load Primary Truck Route as described in the Peel Road Network Study that designated Old Base Line Road and Winston Churchill Boulevard as Primary Truck Routes.
7. Yes, would want all “improvement” options that would be made for trucks to be eliminated. Another route should be provided for trucks. If Winston Churchill and Olde Base Line Road had never been paved, trucks would not be using this route. “Improvements” will bring increased speeds and more traffic from trucks. In our view, these are not improvements.
8. Improve Highway No. 124 to double lane both ways between Erin and Highway No. 10. Consider all of the way to Airport Road. This provides good alternatives to trucks and commuters. You will have to do this in the long term anyway.
9. Yes - resurface Winston Churchill and Olde Baseline for efficient truck/motor vehicle/bus traffic. It should be noted that the property on the northwest corner of Olde Base Line and Mississauga will be doubly impacted by having the increased mixed traffic corridor and stop light within its property vicinity. Our future development will see the access/egress from the driveway turning onto an upgraded, busy, mixed traffic corridor. It is anticipated that at certain times of day vehicles will sit idle passed the driveway while waiting for the light to change. Having elucidated the cons of upgrading Winston Churchill and Olde Base Line, it is still my belief that a secure, well managed truck/motor vehicle/bus route AND traffic light are necessary.
10. Correct current deficiencies – do not raise speed limits. More signage and OPP enforcement. Improve safety conditions for roadway.

- b) Are there additional evaluation criteria that you would like to see considered?
Yes ___ No ___ Don't Know ___ Please describe these

Table 4 – Comments on draft Evaluation Criteria presented at the PIC

1. I would like Winston Churchill Boulevard and Olde Base Line Road to be designated Scenic Roads.
2. Yes, recognize and respect the need to preserve the uniqueness and recognized value of this world biosphere. The NEC needs to play a very important role in this process, as well as CVC.
3. Yes, Mississauga Road requires a great deal of road work – old base of Corduroy Road needs work and improving to improve safety at Residents driveways. Do not raise the speed limit.
4. Other than addressing needs of users of roads, should also study the impact of the uses on local residents along these routes and their quality of life.
5. Yes, do anything that will prevent loss of life. Everyone needs to use these roads.
6. Yes, important to evaluate long term solutions not just short term.
7. Yes, the overall savings in kilometers driven through Peel by trucks should be considered. Every round trip truck haul would save 28 km using this network. While it is important to get local input into your study, at some point we need a regional network that some locals who are protecting their back yard may not agree with. I live on one of the major intersections in this study and support the truck use of these roads.
8. It would benefit all parties if the Region would do a cost benefit analysis of continuing repairing these roads vs. reconstruction.

4. Other comments noted on the Feedback Forms

The following are additional comments noted on the Feedback Forms. Each number represents a different individual's comment.

1. The problem is speeding, not that the roads need to be rebuilt.
2. Much of what is being suggested can be construed as “fear-mongering”. Statistics apparently support very few, if any serious accidents during a specified timeframe. So, where are these safety concerns coming from? Are roads that are flat going to reduce already low accident rates? Our community would like transparency and openness. If you have a lot of money to spend consider what the residents feel is significant.
3. Enhance bike safety use, NEC use of bikes, less vehicular traffic = no large trucks and residents traffic safety. if Brampton Brick wishes to use roads King Street to Mississauga Road to old Brick Yard charge them a toll usage of \$200 in and \$200 out per truck (for each truck, each load). Have them pay \$5 million dollars upfront to pay for improvements to King Street, Mississauga Road – \$5 million upfront a year – every year. Allow roads for night speed for wildlife safety. Post appropriate speed per section of road.

4. I note with interest the comments of the CWG. All believe that it was transparent. If it was transparent then why did they didn't know about the truck routes coming to Winston Churchill Boulevard and Olde Base Line Road until tonight.
5. Resurface Winston Churchill Boulevard and Olde Base Line Road for efficient truck/motor vehicle/bus traffic. It should be noted that the property on the northwest corner of Olde Base Line Road and Mississauga Road will be doubly impacted by having the increased mixed traffic corridor and stop light within its property vicinity. Our future development will see the access/egress from the driveway turning onto an upgraded, busy, mixed traffic corridor. It is anticipated that at certain times of day vehicles will sit idle passed the driveway while waiting for the light to change.
6. Having elucidated the cons of upgrading Winston Churchill Boulevard and Olde Base Line Road, it is still my belief that a secure, well managed truck/motor vehicle/bus route AND traffic light are necessary.
7. Don't be railroaded by vocal minority who make it uncomfortable to comment or discuss pertinent safety and logistical traffic movement essential to the study. Thank you from concerned resident.
8. Do not delay.
9. The time is now to make these improvements and establish a Major Road Network in this area that is more than just a line on the map. Industry has been suffering over the decades that they have been denied an efficient route through this area. As a result millions of kilometers of unnecessary travel have taken place. With the big build to the south on our doorstep we must establish a full service road through this area to link Brampton with Caledon and Wellington County. In future Mississauga Road will have a key intersection on the GTA West Corridor and this link in the network will be ever more critical.
10. We have observed the increased traffic and speeds since Olde Base Line Road was paved. Straightening Olde Base Line Road brought increased speeds. We don't want drivers to have a sense that driving faster is now safer. The OPP has no resources to regulate speeds on Olde Base Line Road even now. Please look at ways to reduce speeds on Olde Base Line Road and to reduce its usage as a commuter road. That status did not exist until Winston Churchill Boulevard and Olde Base Line Road were paved.
11. The scale of this study and the possible impacts of some of the proposals seem to be completely out of proportion to the reality of the situation. First, traffic has been more or less stable over the last 15 years, with a projected increase of only 2% into the future. Second, accident statistics hardly indicate a major problem. Yes, there are some sightline challenges, wildlife will cross roads but perhaps it is these challenges along with the scenery that would help slow traffic down.
12. We have lived here since 1973 (40 years) and my family has seen these four roads change and be repaired over this time. These changes have been done as a result of

increase of traffic, public safety issues and Region of Peel's standards. We would like this area to continue its rural character for generations to come. By doing a major overhaul of these roads you will risk public safety as the volume of traffic will increase and speed at which it travels will increase as well. We have watched this occur over the last 40 years.

Appendix C

COMMENTS RECEIVED BY LETTER AND EMAIL

The following responses were received. These are verbatim comments transcribed from the individual letters and emails. Each number represents a different individual's letter or email.

1. I would like to congratulate all of you for an excellent meeting on May 9th, 2013. I believe there was good discussion and a lot of points were cleared up. I did not have an opportunity to complete my statements so I thought I would share them with you now. I would personally like to thank Mr. Gan for the answers to some of my questions regarding the data for the traffic studies. I was concerned that 1 study indicated that the AADT was close to 726 vehicles and the other indicated 3450 vehicles. It was pointed out that I was incorrect in interpreting these numbers. Thank you again for showing me that the 726 vehicles was peak AM and PM only and not daily traffic.

By your confirmation that both of these studies are in fact correct, it brings up more questions and concerns. If we assume that peak AM and PM traffic is 6-9 AM and 4-7 PM respectively, that is 6 hours in total. This means that there is an average of $726/6$ or 121 vehicles/hr. This seems reasonable. This leaves ~2700 vehicles for the rest of the day. Now those of us that live in the area will recognize that there is virtually no traffic overnight, but for the benefit of doubt let's assume that the rest of the traffic is distributed throughout the rest of the day. This would mean that there are 2750 vehicles in 18 hours or 151 vehicles/hour. Is it plausible that this humble area is the only area in the entire world that has a higher hourly traffic rate during non-peak hours than peak hours?

Mr. Gan mentioned that you have an obligation to hold public safety paramount. I can certainly sympathize with you. You are in a difficult position as you are getting the numbers from Region of Peel, but as a traffic engineer you surely cannot believe these numbers to be accurate. As a traffic engineer, you certainly cannot believe that turning Winston Churchill Boulevard and Olde Base Line Road into truck routes will improve the safety of the road. I felt you were sincere when you spoke about how you would feel if you did nothing and someone in the future was hurt or killed in an accident in this area. The reciprocal is also true, what if you completed your task here and there are more accidents?

I have made a few assumptions in my calculations. I apologize if they are incorrect. If my numbers are flawed, I would welcome any input that can rectify them. It seems the Region of Peel is transparent in this Assessment, as none of my questions are being answered.

It is apparent that the facts and figures provided by Region of Peel are flawed, incomplete, and misleading. It is also apparent that these facts and figures are endorsed by HDR with little to no validation or verification. One can only conclude that there is a hidden agenda that we are not aware of yet. I for one would appreciate a direct and honest approach. Region of Peel has to stop being transparent and state what they want and why they want it and stop pretending they are not influencing the process.

2. I attended the meeting May 9th, and was stunned to learn, that council that very day, had endorsed the "good movement Initiative". Has the Region of Peel now aligned themselves with the aggregate companies? If so, shame, shame, shame! Really, was there ever any consideration for the residents, at any time during the processes, prior to the meeting on May 9th? As the meeting progressed it was apparent the Region of Peel's representatives that night, could only continue to reiterate what was on the slides in the presentation, often not being able to fully answer questions posed by the public. The serenity, the green space shared by man, and wildlife, is what enticed me to relocate from a fast paced life in Halton Hills 21 years ago. Neighbors in this area, being here well before me, were of the impression from the Region then (80's/early 90's) that, WCB would not become a truck route. As with the application for the proposed pit (WCB/OBL), was valiantly fought against and won, by the residents. So to, this new agenda of "good movement initiative" will not go away quickly or quietly...it is strongly opposed by the residents.
3. I live in the area being studied. I am glad that the road network in finally being upgraded to proper standards. I have several suggestions that I would appreciate being taken into account as you plan these road upgrades. Please fix the hilly areas where sightlines are cut down to an unsafe distance, and thrill seekers are more likely to drive above the set speed limits. Please upgrade Olde Base Line Road, Winston Churchill Boulevard and Mississauga Road south of Belfountain, so they are safe, smooth, and wide enough to be used as the designated Primary Truck Route. Please consider intersection improvements to The Grange Side road and Mississauga Road where a hill on The Grange Side road slopes steeply toward Mississauga Road. Attempting to stop before sliding onto Mississauga Road during winter driving conditions is rather treacherous.
4. I am a resident who lives in the area being studied. I use these roads on a daily basis. I have the following comments on the Environmental Assessment for the above roads: There are serious safety concerns regarding sightlines in the study area. Some of the hills should be smoothed out so that appropriate sight distances can be achieved. I am supportive of the utilization of Winston Churchill Boulevard and Olde Base Line Road as truck routes. This will keep trucks out of Belfountain and provide a suitable route through this area. Provisions should be made to keep cyclists off the traveled portions of the road perhaps by paving the shoulders.
5. I am a new driver who uses these roads on a daily basis. My grandparents also live in the area. There are some hills in the area that are too steep to see over. I sometimes ride my bike on these roads and it is dangerous to ride on those sections. I think that it would be a good idea to make a truck route on roads that are away from Belfountain Village. Winston Churchill Boulevard and Olde Base Line Road make sense to me for this purpose.
6. Thank you for your presentation the other night. My message is simple, do the responsible thing and fix the roads. Having an efficient route for all road users pays dividends for our society. The true environmental costs come when commuters and local goods are diverted

miles out of their way resulting in millions of tons of unnecessary gas emissions. More important to me is the cost in lives. Every extraneous mile travelled will statistically translate into loss of life. It is also a fact that unsafe road conditions inevitably result in loss of life. As such I whole heartedly support the improvements you are making to the road network. These roads are not the private driveways for a few select residents. They are part of a network that serves, really, all of Ontario. Having a strong economy allows us to enjoy the lives we do. With the coming population to the south it is important to put in place a safe and efficient road network that serves everyone. Thank you for your efforts in this important issue.

7. Overall, I was not at all happy with the responses provided by the presenters to the comments and questions from the public. It appeared that no matter what issues were raised; many of the responses were justifications as to why the so called “experts” were right and the public view point was flawed. I got the feeling that, although the public was heard, no one was listening to the point of understanding or caring about what the public opinion was. Is this just an exercise in placating the residents with an “opportunity” to speak and some small gestures of minor amendments to what the so called ‘experts” will decide anyway?

Specifically, the presenters indicated that traffic volume had remained steady however, some of the volume had transferred from Mississauga Road to Winston Churchill Boulevard; this is true, I live on Winston Churchill Boulevard and I have witnessed this. The reason for the shift is because Winston Churchill Boulevard was paved over the gravel. I understand the need to upgrade the road; it’s in poor condition and will get worse.

I am in favour of paving the shoulder for cycle and pedestrian use. This was the first time I recall any mention of Winston Churchill Boulevard and Olde Base Line Road being developed as a truck route; this answers a lot of questions about why there is all the fuss about sight lines and flattening parts of the road! I have no objection to increased commuter traffic, but object strongly to creating a truck route.

An Old Base Line Road/Winston Churchill Boulevard truck route makes absolutely no sense at all when you consider the “Principles for Guiding Solutions” outlined in the presentations including:

- Maintain the rural character and countryside scenic quality
- Preserve historic fences and heritage/culture/archaeological features
- Preserve /enhance the natural environment
- Protect the Niagara Escarpment
- Balance interests and meet the needs of all road users
- Provide a context sensitive design
- Enhance local tourism and economic development

Good East/West truck routes already exist at King Street and Charleston Side Road (Highway No. 124) both leading to Highway No.10. I saw no evidence of the need to create additional truck routes through this supposedly protected area and, I thought we

had been through all the reasons this area needs protecting from trucks during the James Dick quarry battle. There are already too many trucks using Winston Churchill illegally, creating noise, vibration and air pollution. In truth we were better off when the road was gravel and quiet! Please do all in your power to avoid an unnecessary truck route.

8. I live on Winston Churchill Boulevard within the study area. I would like to see the improvement of these roads. I firmly believe that the hummocky terrain along the area roads should be graded to allow for safe site distances. I know of at least two major accidents that have occurred due to the site distance issues along Olde Base Line Road. One involved an off duty police officer coming over one of the hills and having to ditch his car due to children crossing the road to a school bus. This occurred at 262 Olde Base Line Road. Clearly this could have been a tragedy and can easily be avoided in the future by properly reconstructing the road. I believe that the area needs a higher level of service route, one that accommodates all vehicles including trucks. This should be Olde Base Line Road and Winston Churchill Boulevard. Thank you for considering my input.
9. I was out of the country last week and missed the public meeting, but suffice it to say, I was so very disappointed to hear that the Region of Peel's new "good movement initiative" involves turning Winston Churchill Boulevard and Olde Base Line Road into truck haul routes. If the Region of Peel has been working on this "initiative" for some time, why were we not told about this at the Community Working Group meetings? You will recall that I and others repeatedly stated that we did not want Winston Churchill Boulevard and Olde Base Line Road to become haul routes – but yet no one from the Region of Peel mentioned this "good movement initiative". I know that you have heard from others on the same issue, and can expect this to be something that will be loudly and actively opposed by the residents.
10. I was unable to attend the May 9 session (due to a previous professional commitment). I too am extremely disappointed that the Region of Peel's new "good movement initiative" involves turning Winston Churchill Boulevard and Olde Base Line Road into truck haul routes. I cannot be more emphatic in raising objection to this.
11. I travel Mississauga Road to Olde Base Line Road every day. I would like to see a report on the collisions at this intersection as I have never seen one in the 16 years I have travelled it. I feel that installing any more roundabouts would be a waste of money, and I do not feel they are safe. I would like to know the cost of the one that went in at Dixie & Olde Base Line Road. You could reduce the speed on Mississauga Road, but drivers do not observe the current one. Many cars pass me doing highway speeds. This is a country road so I do not want to see paved shoulders or curbs, or guard rails were they are not warranted. I also don't support putting sidewalks in Belfountain.
12. At our recent Accessibility Advisory meeting here in Caledon, I brought this project to the committee's attention and found that our town's co-ordinator and chairperson were not completely aware of the current developments. There may be accessibility issues with the road enhancements proposed. Would you be able to include these people on your mailing lists and/or email updates concerning this project?

Public Information Centre #2
November 20, 2013

NOTICE OF PUBLIC INFORMATION CENTRE #2 MISSISSAUGA ROAD, OLD MAIN STREET, BUSH STREET, WINSTON CHURCHILL BOULEVARD AND OLDE BASE LINE ROAD

The Project

The Region of Peel is undertaking a Schedule 'C' Municipal Class Environmental Assessment (EA) to develop a road design that addresses safety and pavement condition deficiencies on Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road. The approximate limits of the project area are illustrated on the map.

The EA will review and recommend solutions to address:

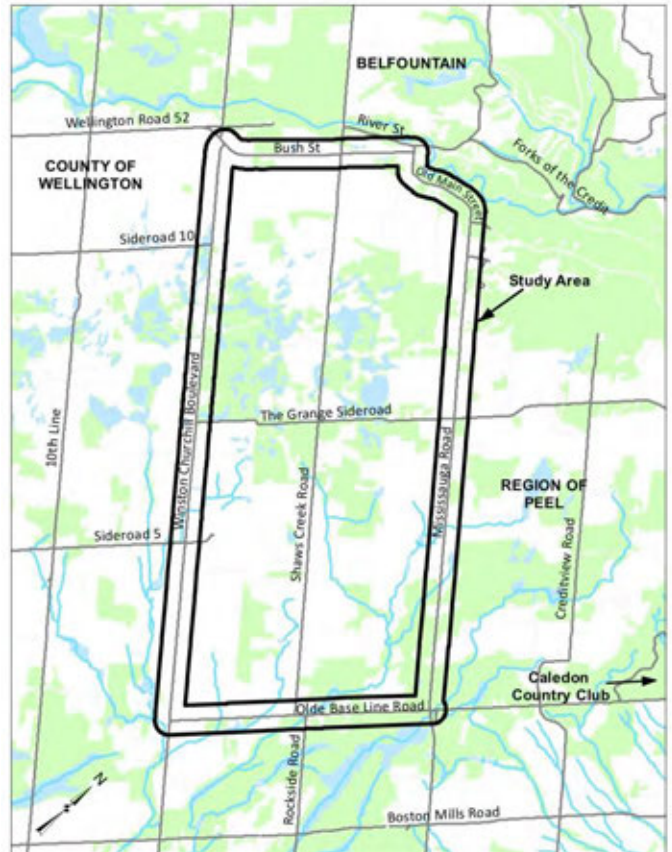
- road safety deficiencies
- sight lines that do not meet standards
- pavement condition deficiencies
- road drainage problems
- parking
- pedestrian and cycling needs

This EA will not be considering road widening or increasing the number of lanes and will provide a solution that meets the needs of all road users and maintains the rural character of the community.

The Process

The Class EA process includes:

- public and agency consultation
- an evaluation of road improvement alternatives
- an assessment of the possible environmental effects of the improvements
- the identification of reasonable means to mitigate any adverse impacts



Public Information Centre

Two Public Information Centres (PICs) have been planned for this study. The first PIC (May 9, 2013) sought feedback on the Draft Problem Statement, Preliminary Alternative Solutions and Preliminary Evaluation Criteria. The second PIC will consist of an Open House, where you can speak with different team members about your specific concerns. The PIC will provide an overview of what we have heard and the technical work completed to date. Please join us at this PIC to share your opinions on:

- Alternative design concepts developed by the study team
- Evaluation of alternative design concepts
- Preliminary recommended design concept

The Project Team members will be present to answer questions and discuss the next steps of the study. The second PIC is scheduled for:

Date: Wednesday, November 20, 2013
Time: Open House 4:30 p.m. - 8:30 p.m.
Location: Caledon Country Club, 2121 Olde Base Line Road, Caledon

Comments and Information

Please visit our website: <http://www.peelregion.ca/pw/roads/enviro-assess/mississauga-road-bush.htm> for updates on this project. Comments and information regarding the study are being collected to assist the Region of Peel in meeting the requirements of the EA Act. This material will be maintained on file for use during the project and may be included in project documentation.

To provide comments or request additional information about this project, please contact any of the following team members:

Gino Dela Cruz

Project Manager, Region of Peel
 10 Peel Centre Drive, Suite B, 4th Floor
 Brampton, ON L6T 4B9
 Tel: 905-791-7800 ext. 7805
 Fax: 905-791-1442
 Email: Gino.DelaCruz@peelregion.ca

Asha Saddi

Technical Analyst, Region of Peel
 10 Peel Centre Drive, Suite B, 4th Floor
 Brampton, ON L6T 4B9
 Tel: 905-791-7800 ext. 7794
 Fax: 905-791-1442
 Email: asha.saddi@peelregion.ca

Tyrone Gan

Project Manager, HDR
 100 York Boulevard, Suite 300
 Richmond Hill, ON L4B 1J8
 Tel: 289-695-4622
 Fax: 289-695-4601
 Email: Tyrone.Gan@hdrinc.com

The Region of Peel is committed to ensure that all Regional services, programs and facilities are inclusive and accessible for persons with disabilities. Please contact the Project Manager if you need any disability accommodations to participate in the PIC.

This notice first issued November 4, 2013

Mississauga Road, Old Main Street, Bush Street, Olde Base Line Road, and Winston Churchill Boulevard Class EA

Public Information Centre #2

Wednesday, November 20, 2013

Caledon Country Club, 2121 Olde Base Line Road, Caledon



Welcome

- Please sign in and take a feedback form
- If you have any questions our team is available to help you
- Place your completed feedback form in the Comment Box, or send it to:

Gino Dela Cruz
Project Manager

Gino.DelaCruz@peelregion.ca

Asha Saddi
Technical Analyst

asha.saddi@peelregion.ca

by Wednesday, December 4, 2013.

What we heard at PIC#1

- **Maintain rural character & countryside scenic quality**
- Minimize potential property impacts
- Preserve historic fences and features
- Preserve natural environment
- Maintain existing vertical alignment and cross-section
- Address poor pavement conditions
- Address excessive speeds
- Address signage clutter
- Accommodate all road users through a multi-modal approach
- Improve sightlines

Purpose of PIC #2

The purpose of this Public Information Centre (PIC) is to:

- Provide a project update on
 - What has been done to date
 - What we have heard
- Present the
 - Alternative design concepts developed by the study team
 - Evaluation of alternative design concepts
 - Preliminary recommended design concept
- Discuss Next Steps
- Ask for your input

Beyond the Municipal Class EA Process

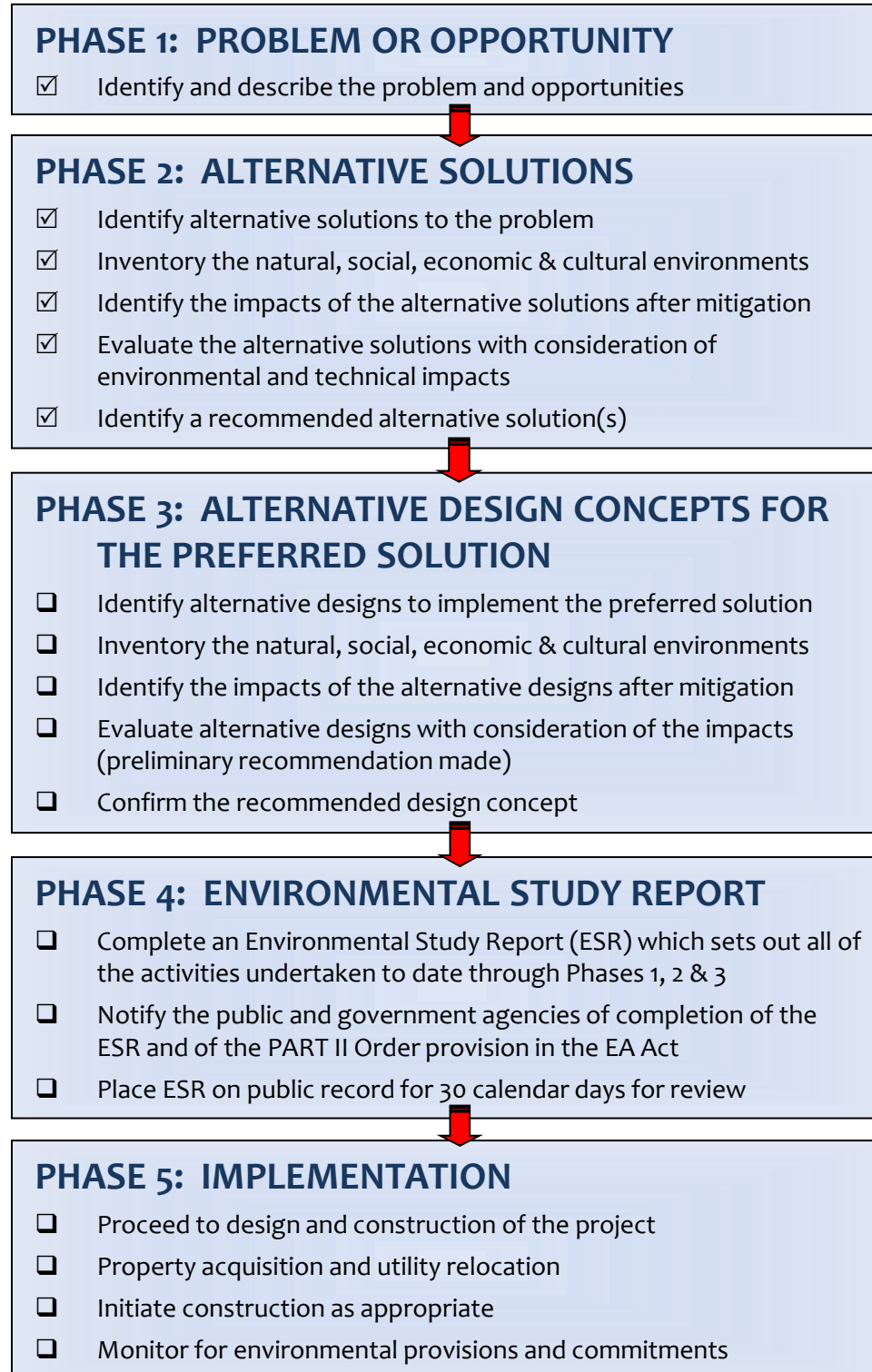
Additional Points of Public Contact

- CWG Meeting Fall 2012 →
- Open House Fall 2012 →

CWG Meeting Spring 2013 →

CWG Meeting Fall 2013 →

Phases



Mandated Points of Public Contact

Notice of Study Commencement

- PIC#1 Spring 2013
- Needs and Justification
 - Planning Alternative Solutions
 - Evaluation of Planning Alternative Solutions
 - Preliminary Recommended Solution

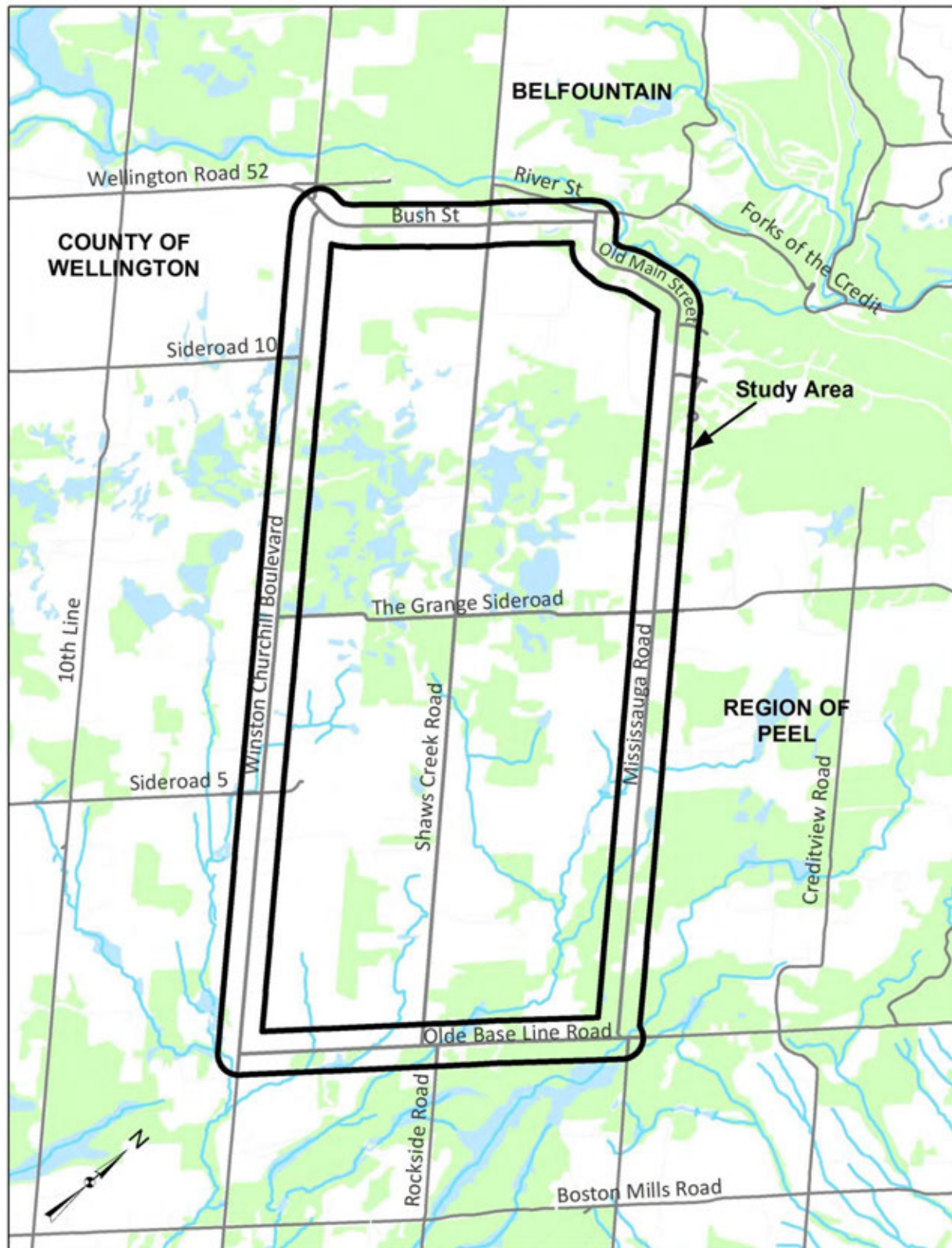
- PIC#2 Fall 2013
- Alternative designs for the preferred solution
 - Evaluation of alternative design concepts
 - Preliminary recommended design concept
- ← We are here

Notice of Study Completion and Filing the ESR



In addition to the mandated points of public contact, the Region has chosen to organize a **Community Working Group (CWG)** for the study.

Study Area



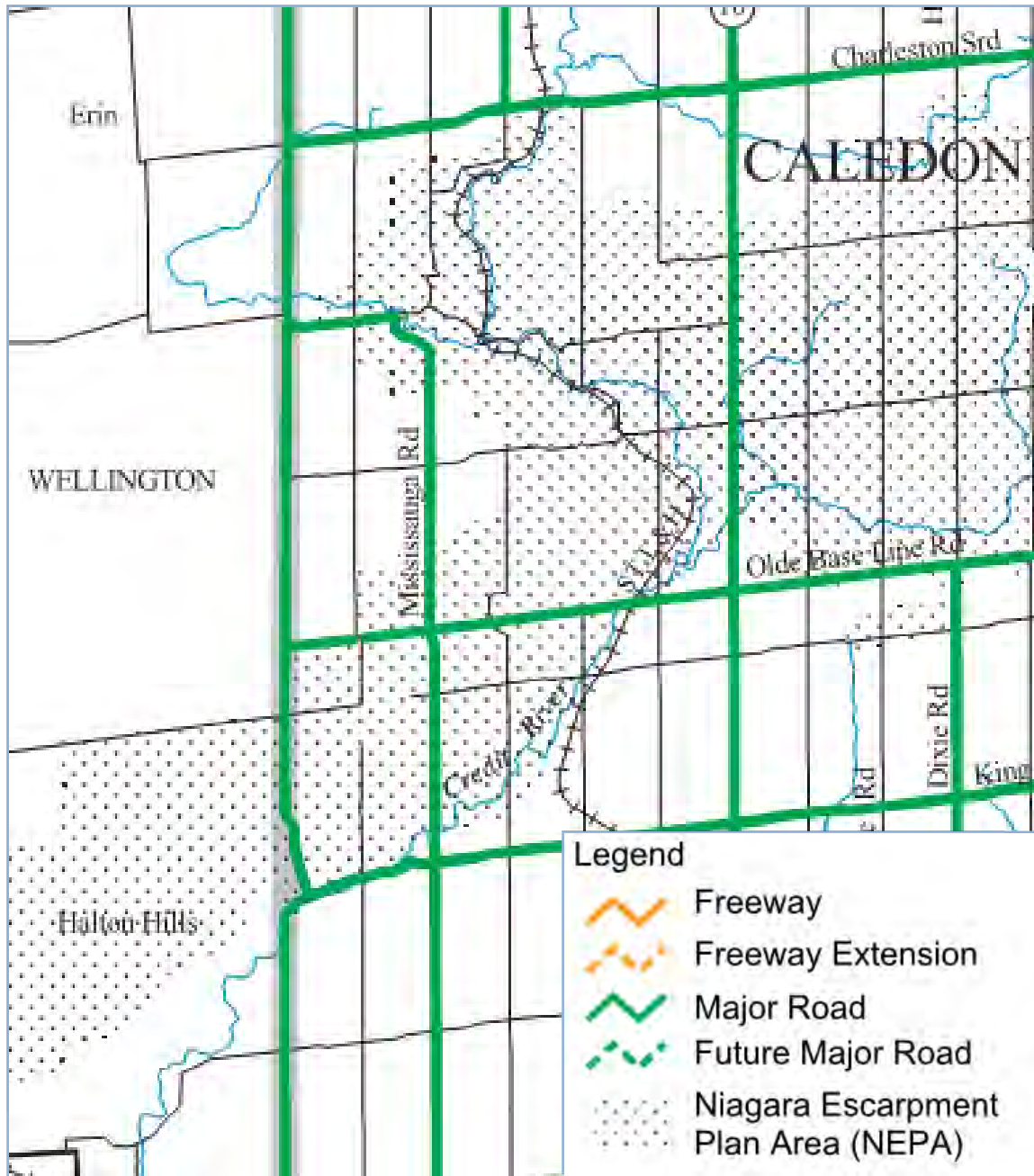
The Study Area consists of:

- Bush Street
- Winston Churchill Boulevard
- Mississauga Road / Old Main Street
- Olde Base Line Road

Our Vision

The Region is focused on ensuring the existing and future road network meets the changing needs of all users in a safe, efficient, sustainable and environmentally friendly manner

Role and Function of Regional Arterial Roads

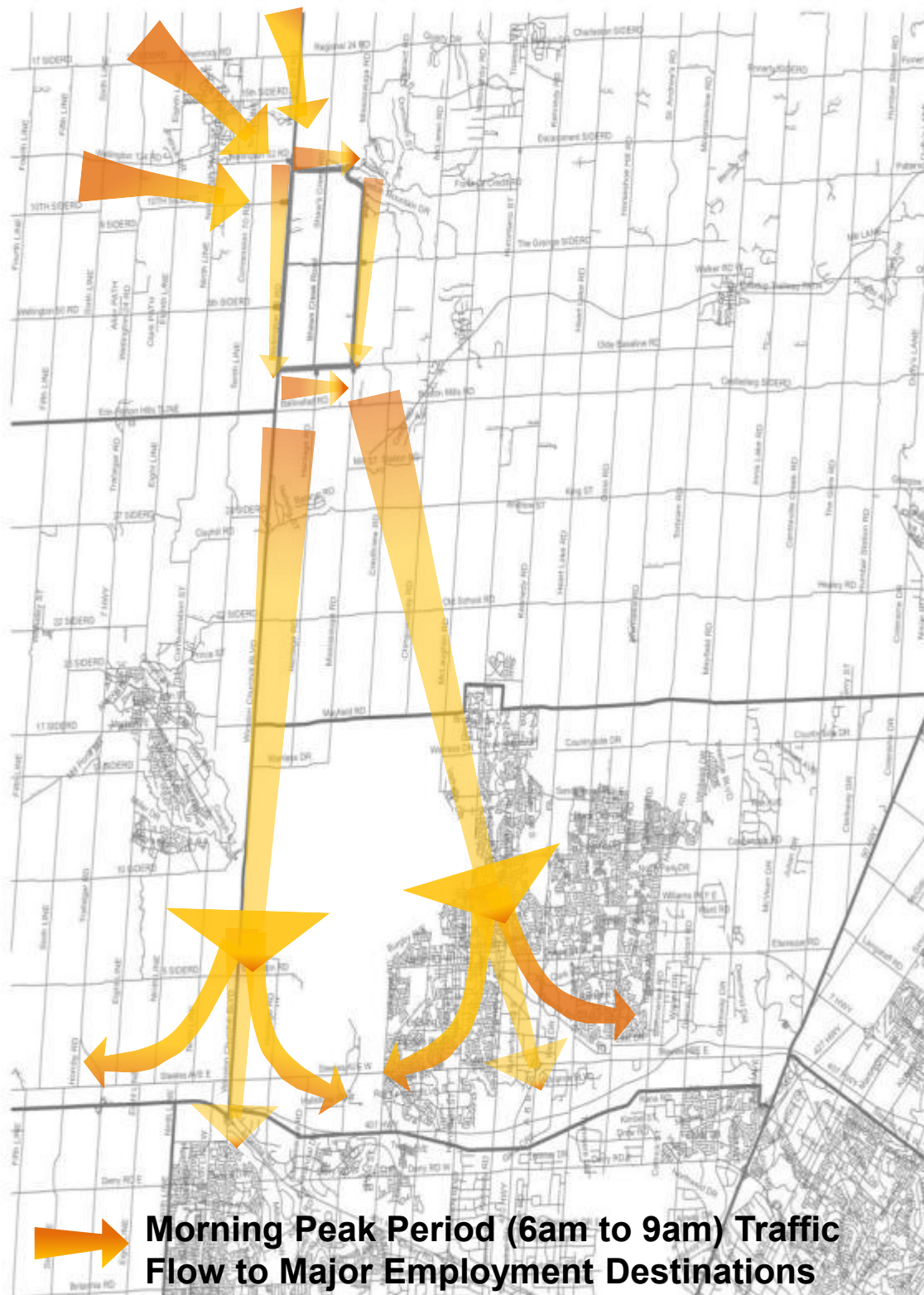


General Objectives of Peel's Transportation System

- To achieve a safe convenient and efficient movement of people and goods in the Region
Support the provision of improved transportation mobility to all residents, employees and visitors
- Promote and encourage all sustainable modes of transportation to provide mobility and choice
- Minimize adverse environmental impacts caused by transportation
- Support a transportation system that enhances economic growth in the Region
- Ensure Regional transportation infrastructure is sustainable and that practices and performance measures are in place to maintain a safe and efficient Regional transportation network

Region of Peel Official Plan – Schedule E

Broader Transportation Network



The roads in the study area provide connections to major employment destinations.

Bush Street (Regional Road 11), Mississauga Road / Old Main Street (Regional Road 1), Winston Churchill Boulevard (Regional Road 19), and Olde Base Line Road (Regional Road 12) are all part of the Peel Region arterial road network.

Winston Churchill Boulevard (Regional Road 25) is also part of the Wellington County arterial road network.

The roads in the study area also provide connections to tourist destinations in the immediate and surrounding area.

Problem Statement

Work to date has confirmed similar issues identified in the 2010 study. Existing problems on the study area roads (Mississauga Road/Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road) consist of:

- Deficient pavement conditions
- Deficient drainage
- Deficient sightlines
- Safety for all road users, including safety of wildlife



Needs Assessment

Assessment done to date has identified issues in the following theme areas:

- Traffic and Road Safety
 - Improve safety for all road users – motorists, cyclists, pedestrians
 - Reduce collisions with animals
 - Address excessive speeds – cars, trucks, motorcycles
- Asset Management and State of Good Repair
 - Address poor conditions of the roadway pavement
 - Address drainage deficiencies
- Maintain Existing Character
 - Retain existing number of travel lanes
 - Retain existing vertical alignments where safe
 - Minimize impacts on natural, heritage, and cultural features

There is a recognition that users may have competing interests and needs

Theme #1

Traffic and Road Safety

Traffic Speeds



Speed surveys indicate that traffic generally travels at higher speeds than the posted speed limits.

Road segments where the 85th percentile speeds are more than 20 km/h over the posted speed limits include:

- Olde Base Line Road
- Mississauga Road between The Grange Sideroad and Woodlands Court
- Winston Churchill Boulevard
- Bush Street in the Village of Belfountain

Collisions by Road Segment / Intersection

Number of Collisions by causal factor from January 1, 2006 to December 31, 2010 within Study Area

Location	Multiple Motor Vehicles ¹			Single Motor Vehicle and Cyclist(s) ²			Single Motor Vehicle and Pedestrian(s) ²			Single Motor Vehicle and Animal(s) ²			Single Motor Vehicle Only, Involving Off-Road Objects ³		
	Property Damage Only	Non-Fatal Injury	Fatal	Property Damage Only	Non-Fatal Injury	Fatal	Property Damage Only	Non-Fatal Injury	Fatal	Property Damage Only	Non-Fatal Injury	Fatal	Property Damage Only	Non-Fatal Injury	Fatal
Intersection of Olde Base Line Rd and Mississauga Rd	6	1	-	-	-	-	-	-	-	2	-	-	1	-	-
Mississauga Rd Between Olde Base Line Rd and Bush St	7	-	-	-	1	-	-	-	-	8	-	-	5	1	-
Intersection of Mississauga Rd and Bush St	2	-	-	-	-	-	-	-	-	2	-	-	-	-	-
Bush St Between Mississauga Rd and Winston Churchill Blvd	1	-	-	-	-	-	-	-	-	2	-	-	-	-	-
Intersection of Bush St and Winston Churchill Blvd	1	-	-	-	-	-	-	-	-	1	-	-	2	1	-
Winston Churchill Blvd Between Bush St and Olde Base Line Rd	1	1	-	-	-	-	-	-	-	5	-	-	2	-	-
Intersection of Winston Churchill Blvd and Olde Base Line Rd	1	-	-	-	-	-	-	-	-	2	-	-	-	1	-
Olde Base Line Rd Between Winston Churchill Blvd and Mississauga Rd	1	-	-	-	-	-	-	-	-	8	-	-	2	-	-
Total Collisions	20	2	-	-	1	-	-	-	-	30	-	-	12	3	-
	22			1			0			30			15		

Notes:

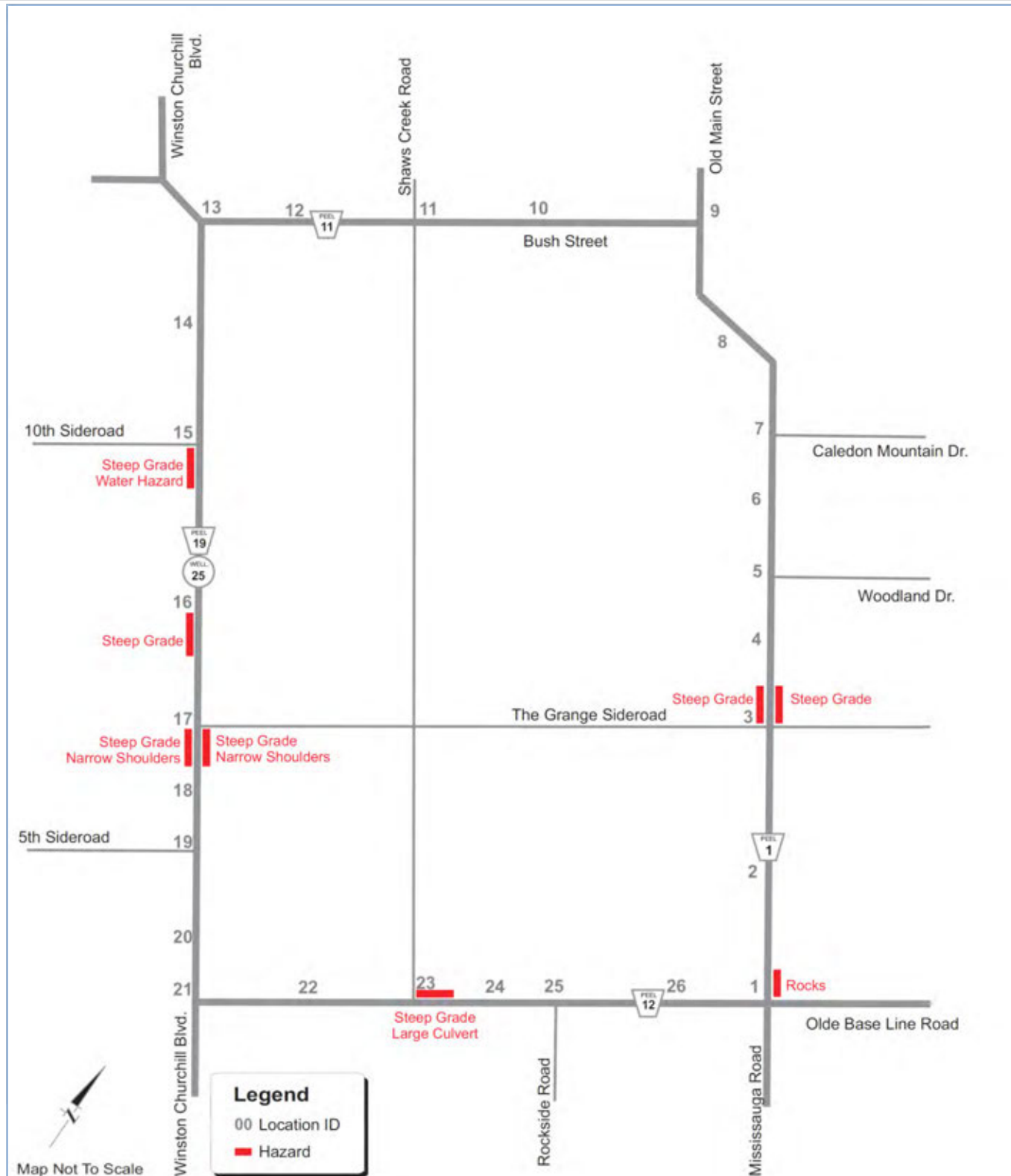
- 1) 'Multiple Motor Vehicles' collisions include collisions caused by, but did not necessarily collide with multiple motor vehicles.
- 2) 'Single Motor Vehicle' collisions involving cyclists, pedestrians, or animals, include collisions caused by, but did not necessarily collide with the external factor. (Ex. A collision in which a vehicle swerved to avoid an animal and thus collided with the guardrail, was considered a 'Single Motor Vehicle and Animal' collision).
- 3) 'Single Motor Vehicle Only, Involving Off-Road Objects' collisions include collisions in which vehicles lost control due to external factors (not including motor vehicles, cyclists, pedestrians, or animals), and the single motor vehicles ended up in the ditch, or collided with stationary objects such as guiderails or posts.

Source: Collision information provided by Peel Region's Safety group.

Highest number of collisions are on:

- Mississauga Road between Olde Base Line Road and The Grange Sideroad
- Olde Base Line Road between Winston Churchill Boulevard and Mississauga Road

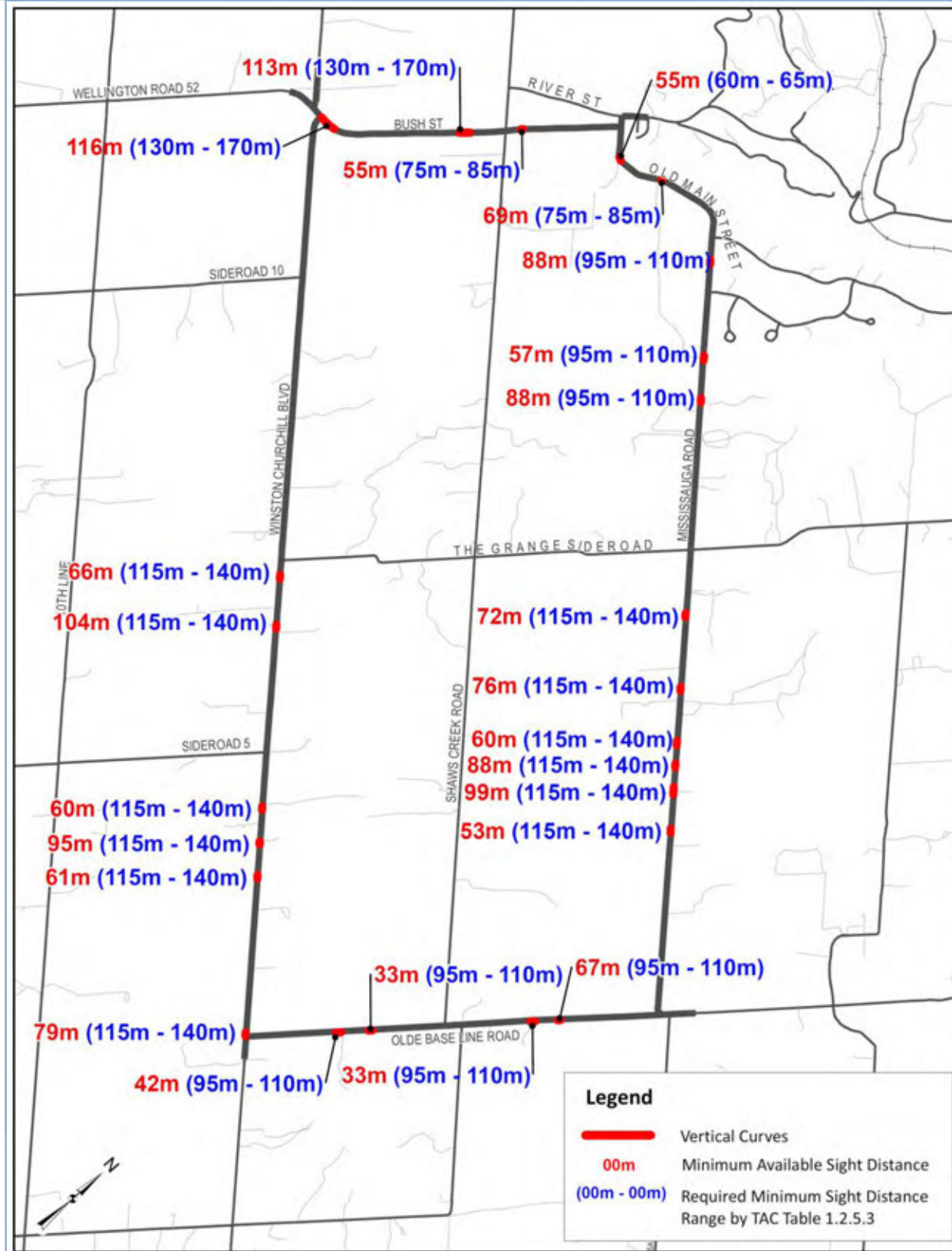
Roadside Hazards



Roadside hazards include hydro poles, steep slopes, and rock cuts. The Study will consider options to improve safety at roadside hazard locations.



Vertical Alignment: Stopping Sight Distance Deficiencies



Sight Distances at Driveways

At many driveways, sight distances are inadequate.

Fully Meets Minimum Standards	Yes	No	TOTAL
Stopping Sight Distance	163 (88%)	21 (12%)	184 (100%)
Minimum Turning Sight Distance	83 (45%)	101 (55%)	184 (100%)
Desirable Turning Sight Distance	60 (33%)	124 (67%)	184 (100%)

Based on Transportation Association of Canada (TAC) design standards.

Stopping Sight Distance is based on drivers on the main road approaching driveways.

Turning Sight Distance is based on drivers turning left or right from their driveways.



Theme #2

Asset Management and State of Good Repair

Pavement and Drainage Conditions

- The preliminary findings from the geotechnical investigations completed to date reveal that structural capacity and strength of all roads are in poor condition and are expected to continuously deteriorate.
- The main cause to pavement distress is attributed to variable granular thickness along roadways with a non-uniform base and sub-base materials.
- Shoulder granular is also thinner than the sub-base below the roadway which affects the drainage of the base leading to frost heave and rutting.
- Some of the pavement deficiencies identified throughout the study area include:
 - Wheel tracking and rutting
 - Transverse and longitudinal meander and mid-lane cracking
 - Alligator pavement edge cracking



Pavement and Drainage Conditions by Roadway

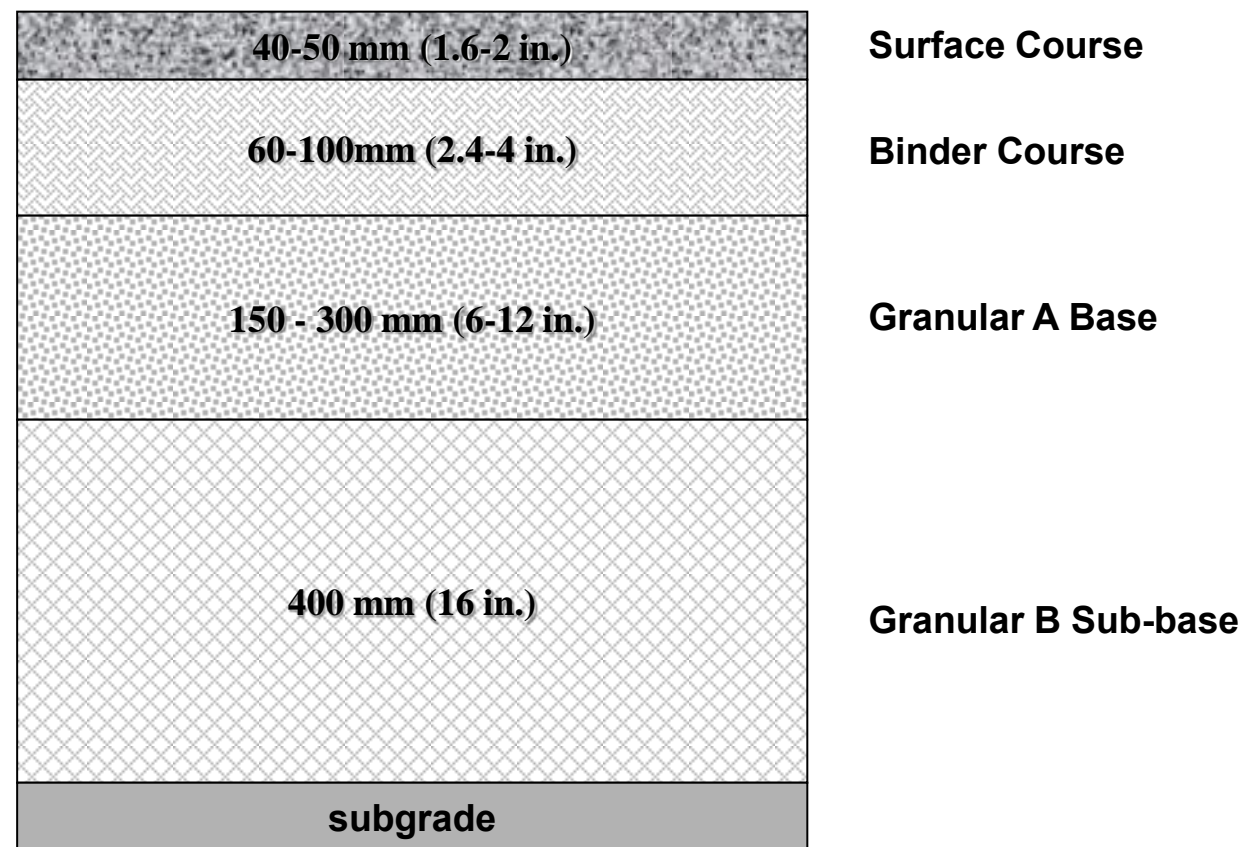
Roadway	Existing Conditions / Pavement Deficiencies	
Mississauga Road/Old Main Street	<ul style="list-style-type: none"> •Granular thickness of base and sub-base highly variable •“Bathtub” construction – granular under shoulder is thinner than under the roadway •Wheel tracking rutting •Slight alligator pavement edge cracking 	<ul style="list-style-type: none"> •Moderate alligator transverse cracking •Longitudinal meander and mid-lane cracking •Inadequate / sub-standard ditches •Ponding and vegetation along shoulders
Bush Street	<ul style="list-style-type: none"> •Granular thickness of base and sub-base highly variable 	<ul style="list-style-type: none"> •Centreline and transverse cracking •Deficient structural capacity and stability
Winston Churchill Boulevard	<ul style="list-style-type: none"> •Granular thickness of base and sub-base highly variable •Deficient structural capacity and stability •Medium severity raveling 	<ul style="list-style-type: none"> •High severity large area alligator cracking •Localized depressions •Shallow bedrock does not allow for drainage under roadway
Olde Base Line Road	<ul style="list-style-type: none"> •Granular thickness of base and sub-base highly variable •Medium and high severity cracking •Frost heave and temperature related deterioration 	<ul style="list-style-type: none"> •Localized depressions •Water logging due to top permeable layers and bottom relatively impermeable silty clay •Shallow bedrock does not allow for drainage under roadway

Geotechnical Recommendations



Recommended Pavement Structures

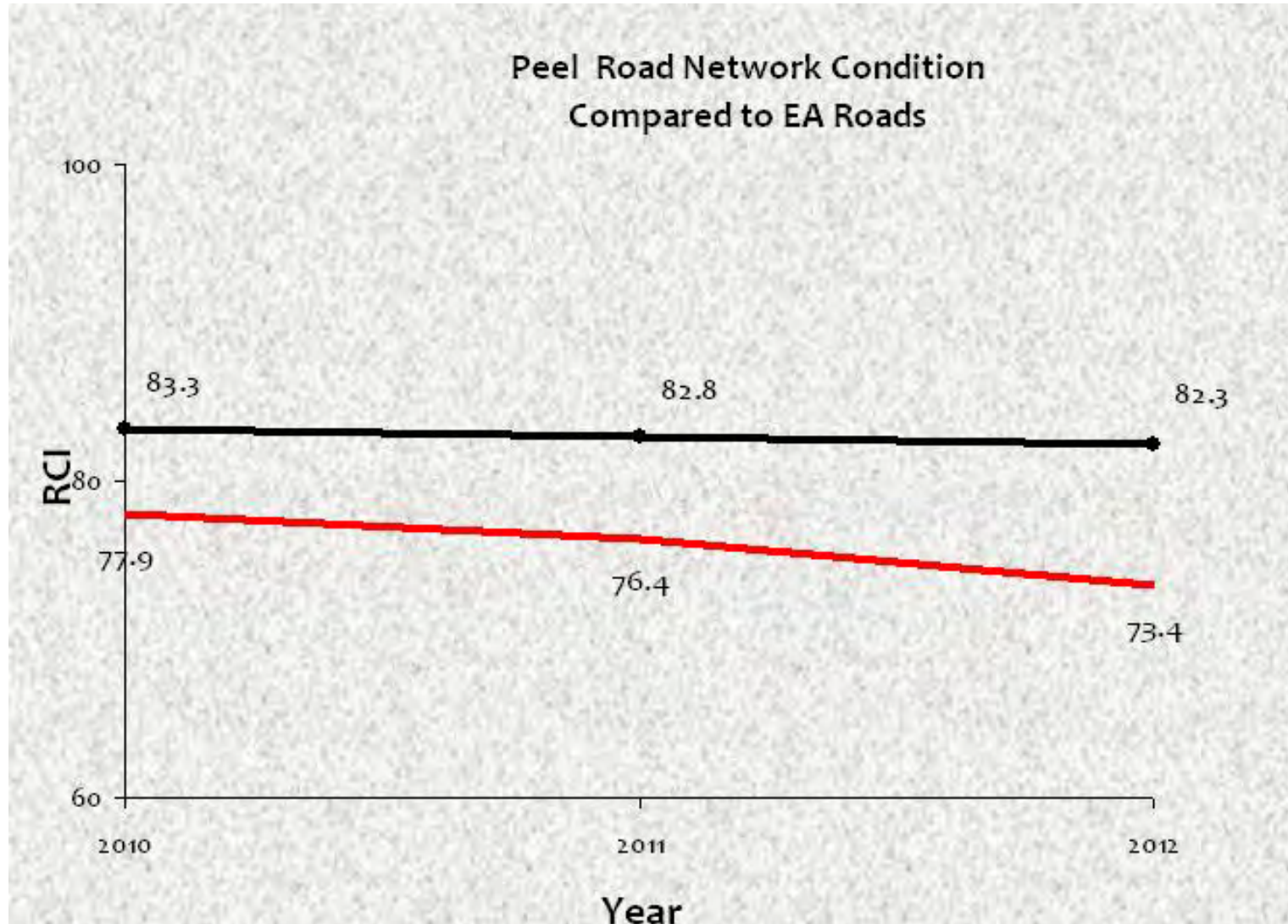
Geotechnical Investigations completed to date have recommended the following typical pavement structure to address the deficient pavement conditions:



* Note: The pavement recommendations will be confirmed in the subsequent phases of this study.

Pavement structure granular materials must conform to OPSS (Ontario Provincial Standard Specification) specifications.

Ride Condition Index (RCI)



- Black line represents projected network RCI for all Regional Roads.
- Red line represents projected RCI for the roads in the study area.

- The Ride Condition Index (RCI) is a quantitative number that represents the overall condition and quality of a Regional road network.
- The RCI aggregates the rating of many types of road defects including cracking, rutting, potholes and surface quality into one measurable number.
- Study area roads are below the network average and are deteriorating faster than the network average and will likely be below the level of service (72) for roads in the next 3- 5 years.

Theme #3

Maintain Existing Character

Key Design Principles

- Maintain two lane cross-section
- Minimize profile changes
- Maximize utilization of right-of-way space
- Minimize property impacts
- Minimize impacts to existing driveways
- Promote Active Transportation



Existing and Proposed Posted Speeds



In general, a reduction in posted speeds throughout the study area is proposed.

This will address deficient stopping sight distance and driveway sightlines, while minimizing changes to the existing profiles.

Belfountain Village Design Domain

Design Element	TAC Standards	Existing	Recommended Cross-Section
Speed Limit	Minimum 50 km/h design speed required for 40 km/h posted speed	Existing alignment generally conforms with design standards	Retain 40 km/h posted speed limit. Design conforms with design standards
Number of lanes based on existing and future traffic	2	2	2
Travel Lane width	3.3 - 3.7 m	3.2-3.7 m	3.3m
Shoulder / buffer width	1.5 m paved shoulder	0.5-2.7m shoulder (of which 0.2-2.0 m is paved)	1.7 m paved shoulder on Old Main Street, east of the Community Centre; 0.5 m mountable curb separates 1.7 m sidewalk from vehicle zones on Bush Street and Old Main Street north of the Community Centre
Cycling facility	1.5 m minimum (paved)	None	1.7 m paved shoulder east of the Community Centre
Drainage	Adequate drainage is required	Inadequate drainage	Underground infrastructure to provide adequate drainage

Bush Street Design Domain

Design Element	TAC Standards	Existing	Recommended Design
Speed Limit	Minimum 60-90 km/h design speed required for 50-80 km/h posted speed	Deficient. Vertical alignment provides design speed of 50 km/h	50-70 km/h posted speed limit with a 60-80 km/h design speed
Number of lanes based on existing and future traffic	2	2	2
Travel Lane width	3.5 - 3.7 m	3.2-3.8 m	3.5 m
Shoulder / buffer width	1.5 m paved shoulder	1.3-3.5 m shoulder (of which 0.2-1.5 m is paved)	1.7 m paved shoulder
Cycling facility	1.5 m wide (paved)	None	1.7 m paved shoulder
Drainage	Adequate drainage is required	Substandard ditches are damaging the pavement	Proper ditches to provide adequate drainage and protect the pavement

Mississauga Road Design Domain

Design Element	TAC Standards	Existing	Recommended Design
Speed Limit	Minimum 60-80 km/h design speed required for 50-70 km/h posted speed	Deficient. Vertical alignment provides design speed of 30 – 50 km/h	50-60 km/h posted speed limit with a 60-70 km/h design speed
Number of lanes based on existing and future traffic	2	2	2
Travel Lane width	3.5 - 3.7 m	3.3-3.5 m	3.5 m
Shoulder / buffer width	1.5 m paved shoulder	0.5-2.3 m shoulder (of which 0-2.3 m is paved)	1.7 m paved shoulder
Cycling facility	1.5 m wide (paved)	None	1.7 m paved shoulder
Drainage	Adequate drainage is required	Substandard ditches are damaging the pavement	Proper ditches or underground infrastructure to provide adequate drainage

Winston Churchill Boulevard Design Domain

Design Element	TAC Standards	Existing	Recommended Design
Speed Limit	Minimum 70-80 km/h design speed required for 60-70 km/h posted speed	Deficient. Vertical alignment provides design speed of 40 – 60 km/h	60 km/h posted speed limit with a 70 km/h design speed
Number of lanes based on existing and future traffic	2	2	2
Travel Lane width	3.5 - 3.7 m	3.1-3.6 m	3.5 m
Shoulder / buffer width	1.5 m paved shoulder	1.2-3.0 m shoulder (of which 0-1.0 m is paved)	1.7 m paved shoulder
Cycling facility	1.5 m wide (paved)	None	1.7 m paved shoulder
Drainage	Adequate drainage is required	Substandard ditches are damaging the pavement	Proper ditches or underground infrastructure to provide adequate drainage

Olde Base Line Road Design Domain

Design Element	TAC Standards	Existing	Recommended Design
Speed Limit	Minimum 70 km/h design speed required for 60 km/h posted speed	Deficient. Vertical alignment provides design speed of 30 – 50 km/h	50 km/h posted speed limit with a 60 km/h design speed
Number of lanes based on existing and future traffic	2	2	2
Travel Lane width	3.3 - 3.7 m	3.4-3.5 m	3.5 m
Shoulder / buffer width	1.5 m paved shoulder	0.4-0.8 m unpaved shoulder	1.7 m paved shoulder
Cycling facility	1.5 m wide (paved)	None	1.7 m paved shoulder
Drainage	Adequate drainage is required	Substandard ditches are damaging the pavement	Proper ditches or underground infrastructure to provide adequate drainage

Natural Feature Constraints - Woodlands, Wetlands and Designated Policy Areas



Source: Esri, DeLorme, USGS, USGS, AEX, GeoEye, Getmapping, Aerogrid, IGN, IGP, and the GIS User

Figure 5a
Belfountain Transportation EA
Natural Feature Constraints - Woodlands, Wetlands and Designated Policy Areas



0 100 200 300 400 500 Meters

November 18, 2013, Project No: NRSI-1337
 UTM Zone 17, NAD 83 Scale: 1:17,000 (at 11x17")

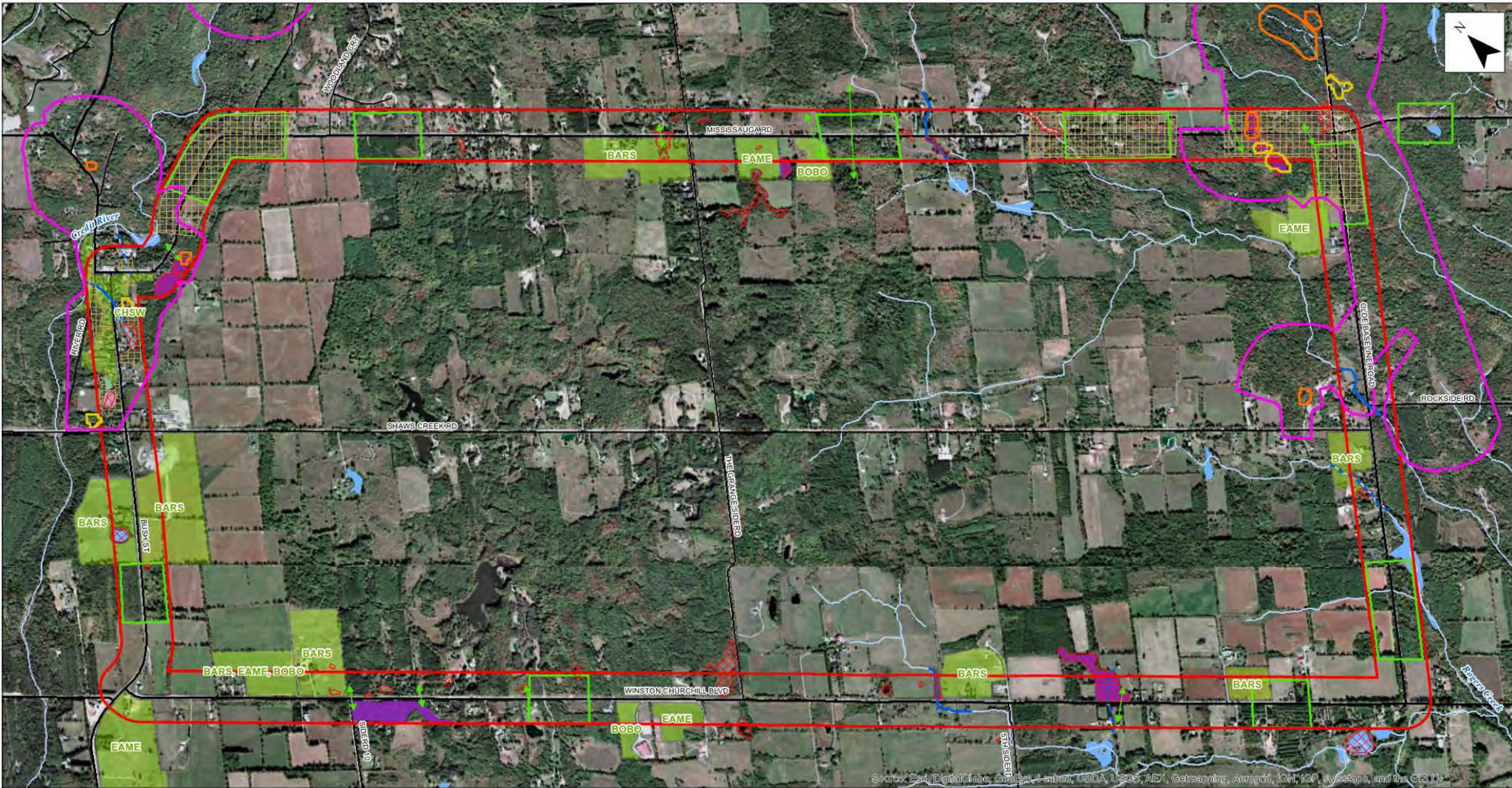
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Legend

- Study Area
- Primary Road
- Secondary Road
- Permanent Watercourse
- Intermittent Watercourse
- ~ Waterbody
- Significant Woodland (>10ha)
- Core Area Woodland (>16ha)
- ANSI, Life Science
- Environmentally Sensitive Area (ESA)
- Provincially Significant Wetland (PSW)
- Non-Provincially Significant Wetland (PSW)
- Butternut Observation



Natural Feature Constraints - Fish and Wildlife Habitat



Source: Esri, DigitalGlobe, GeoEye, Earthstar, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User

Figure 5b
Belfountain Transportation EA
Natural Feature Constraints - Fish and Wildlife Habitat

NATURAL RESOURCE SOLUTIONS INC.
 Aquatic, Terrestrial and Wetland Biologists

0 100 200 300 400 500 Meters

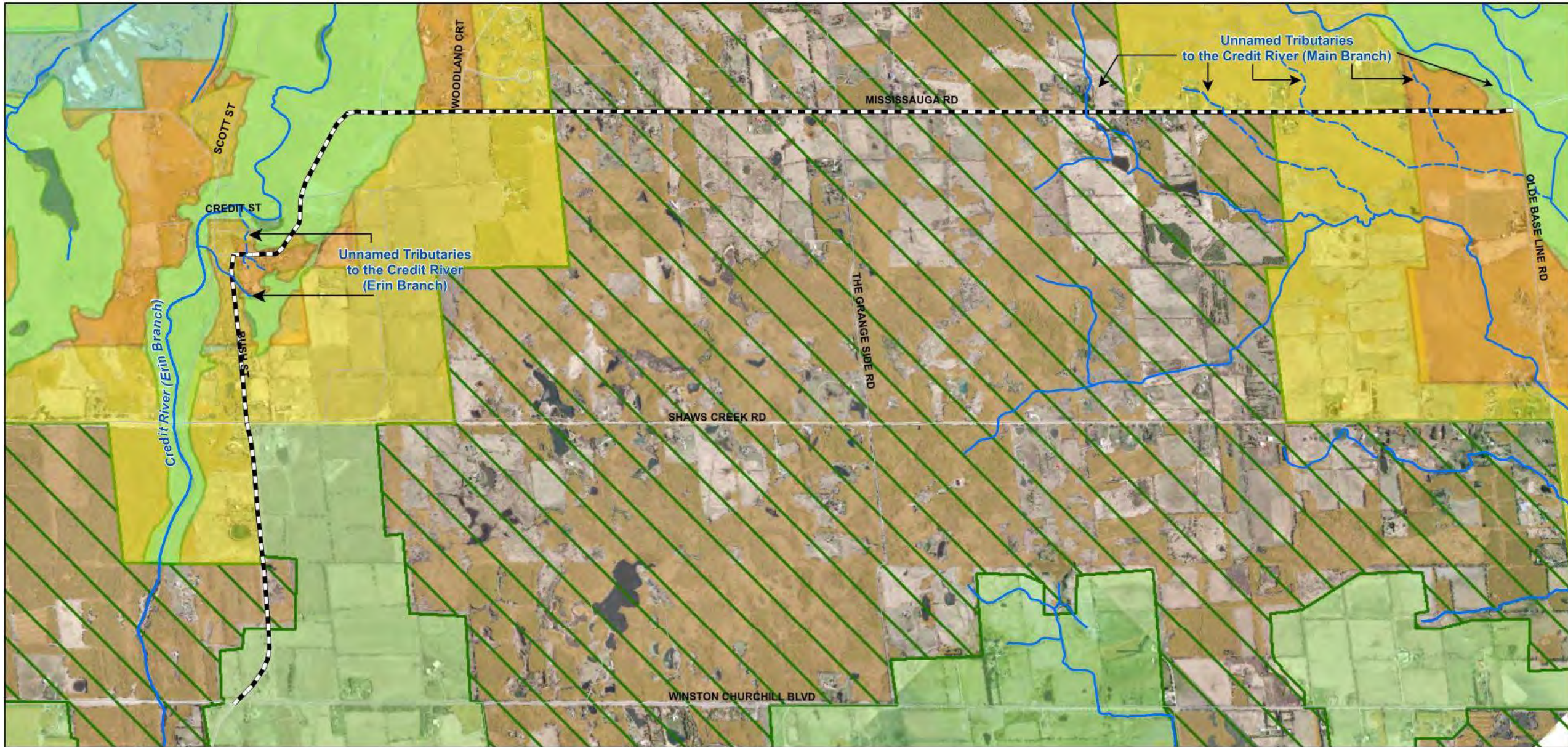
November 15, 2013, Project No: NRSI-1337
 UTM Zone 17, NAD 83 Scale: 1:17,000 (at 11x17")
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Legend

- Study Area
- Primary Road
- Secondary Road
- Permanent Watercourse
- Intermittent Watercourse
- Waterbody
- Northern Flying Squirrel High-Density Areas
- ▶ Amphibian Crossing
- ▶ Direct Fish Habitat
- - - Indirect Fish Habitat
- Amphibian Breeding SWH
- Species at Risk (SAR)
- Jefferson Salamander Habitat Confirmed
- Potential
- Regulated Habitat
- Significant Wildlife Habitat Western Chorus Frog Habitat
- Turtle Overwintering SWH
- Deer Movement Corridor SWH



Environmental Policy Areas



Bush Street and Mississauga Road Class EA
Figure 4: Environmental Policy Areas

- | | | | | | |
|---------------|-----------------------|--------------------------------|------------------------------|-------------------------------|------------------------------------|
| Legend | | Niagara Escarpment Plan | | Greenbelt Designations | |
| — | Roads | | Escarpment Natural Area | | Greenbelt Protected Countryside |
| | Watercourse | | Escarpment Protected Area | | Greenbelt Natural Heritage System |
| | Ephemeral Watercourse | | Escarpment Rural Area | | Core Areas of the Greenland System |
| | Study Alignment | | Escarpment Recreational Area | | |

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 Date Created: 060710
 Date Modified: 110210
 File Path: I:\GIS\1103121 -
 Bush Street & Mississauga Rd
 Mapping\Natural Features Report
 Environmental Policy Areas.mxd

Archaeological Assessment



Stone wall, cedar fence on Winston Churchill Boulevard



Boulder fence and cedar rail fence, Shaw's Creek and Olde Base Line



Cedar fence along Bush



Stone fence - Mississauga Road at The Grange



Winston Churchill Boulevard – low/wet and sloping beyond the road



Northwest view towards Bush – potential for archaeological significance in field

Built / Cultural Heritage

Mississauga / Bush

- 48 identified resources
- 4 designated under Ontario Heritage Act
- 23 listed by Town of Caledon
- Another 11 listed with high significance

Olde Baseline / Winston Churchill

- 21 identified resources
- None designated under Ontario Heritage Act



Belfountain Community Cemetery



Modern fence and fields adjacent to Olde Base Line Rd looking west



Belfountain Village Church



Belfountain Community Hall

Preliminary Design / Evaluations

Refer to corridor-specific stations

Design Criteria

	DESIGN STANDARDS	DESIGN STANDARDS	DESIGN STANDARDS	DESIGN STANDARDS	DESIRED DESIGN STANDARDS (highly unlikely to be achieved)	REFERENCE
	RAU 50	RAU 60	RAU 70	RAU 80	RAU 90	
HIGHWAY CLASSIFICATION						
MINIMUM STOPPING SIGHT DISTANCE	60-65 m	75-85 m	95-110 m	115-140 m	130-170 m	(TAC – page 1.2.5.4 Table 1.2.5.3)
MIN. EQUIV. VERTICAL CURVE (WITH ILLUMINATION) ¹	6-7 - CREST 5-6 -SAG (Comfort)	10-13 - CREST 8-9 -SAG (Comfort)	16-23 - CREST 10-12 -SAG (Comfort)	24-26 - CREST 12-16 -SAG (Comfort)	32-53 - CREST 15-20 -SAG (Comfort)	(TAC – page 2.1.3.6 Table 2.1.3.2) (TAC-Page 2.1.3.9. Table 2.1.3.4)
MIN. EQUIV. VERTICAL CURVE (WITHOUT ILLUMINATION) ²	6-7 - CREST 11-12 -SAG (Headlight Control)	10-13 - CREST 15-18 -SAG (Headlight Control)	16-23 - CREST 20-25 -SAG (Headlight Control)	24-26 - CREST 25-32 -SAG (Headlight Control)	32-53- CREST 30-40 -SAG (Headlight Control)	(TAC – page 2.1.3.6 Table 2.1.3.2) (TAC-Page 2.1.3.9. Table 2.1.3.4)
MAXIMUM GRADIENT	8-10%	8-10%	8-10%	8-10%	8-10%	(To reflect prevailing conditions and maintain existing rural character)
MINIMUM CURVATURE	90 m	130 m	190 m	250 m	340 m	(TAC – page 2.1.2.13 Table 2.1.2.6)
SUPERELEVATION (ON CURVE)	6%	6%	6%	6%	6%	(TAC – page 2.1.2.3)
LANE WIDTH	3.3-3.7 m	3.3-3.7 m	3.5-3.7 m	3.5-3.7 m	3.5-3.7 m	(TAC – page 2.2.2.1 Table 2.2.2.1)
SHOULDER WIDTH	1.50 m min (Paved) 2.0 m (Unpaved)	1.50 m min (Paved) 2.0 m (Unpaved)	1.50 m min (Paved) 2.0 m (Unpaved)	1.50 m min (Paved) 2.0 m (Unpaved)	1.50 m min (Paved) 2.0 m (Unpaved)	(Region of Peel’s Road Characterization Study, Rural Road with 30 m ROW)
SHOULDER WIDTH ON SIGNED BICYCLE ROUTE	2.0 m desirable 1.2 m minimum	2.0 m desirable 1.2 m minimum	2.0 m desirable 1.2 m minimum	2.0 m desirable 1.2 m minimum	2.0 m desirable 1.2 m minimum	(OTM BOOK 18 Table 4.2)
DRAINAGE ZONE	8.0 m	8.0 m	8.0 m	8.0 m	8.0 m	(Region of Peel’s Road Characterization Study, Rural Road with 30 m ROW)
DESIGN SPEED	50 km/h	60 km/h	70 km/h	80 km/h	90 km/h	
POSTED SPEED	40 km/h	50 km/h	60 km/h	70 km/h	80 km/h	

NOTE 1: CROSS-SECTION ELEMENT WIDTHS MAY CHANGE DEPENDING ON AVAILABLE ROW WIDTHS

NOTE 2: ALTHOUGH HIGHER DESIGN SPEEDS ARE DESIRABLE, THEY MAY NOT BE ACHIEVABLE DUE TO EXISTING TERRAIN

¹ Applies only at some locations

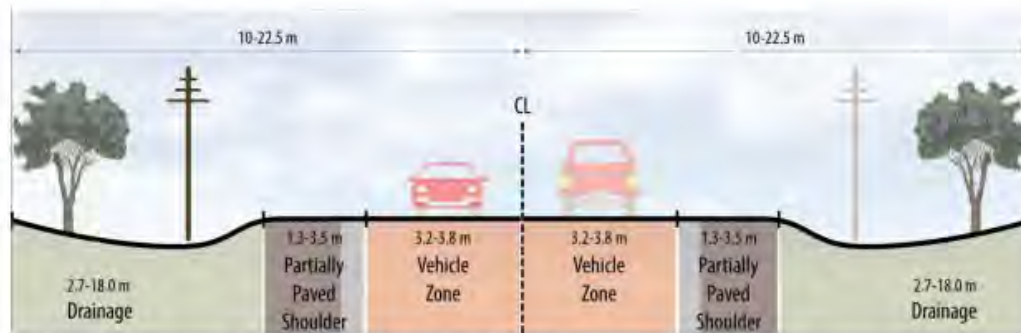
² Applies for the majority of the study area

Belfountain Village Cross-Sections

<p>DO NOTHING</p>	<p>Note: Total right-of-way is predominantly 20 m; paved portion of shoulder ranges from 0.2-2.0 m; majority of above ground utilities run on one side of the road and cross over between sides</p>	<p>Typical existing cross-section</p> <ul style="list-style-type: none"> ✓ No impacts to properties, natural environment, or built/cultural heritage features ✗ Does not accommodate all road users on narrow paved shoulder ✗ Inadequate drainage
<p>9.3 m PLATFORM SEMI-RURAL ROAD WITH SIDEWALK</p>	<p>PREFERRED (Bush between Shaws Creek and Old Main Street)</p>	<p>Semi-rural cross-section with sidewalk to accommodate pedestrians on constrained ROW</p> <ul style="list-style-type: none"> ✓ Minimized impacts to properties, natural environment, and built/cultural heritage features ✗ Higher construction cost than Do Nothing but comparable to other semi-rural options ✗ Does not accommodate parking ✗ No dedicated bicycle zones ✓ Dedicated pedestrian zones on one side of the street ✓ Safer pedestrian environment from dedicated zones ✓ Addresses drainage deficiencies through underground infrastructure (catch basin and subdrain under curb)
<p>9.3 m PLATFORM SEMI-RURAL ROAD WITH PAVED SHOULDER</p>	<p>PREFERRED (Old Main Street east of the Community Centre)</p>	<p>Semi-rural cross-section with paved shoulder to accommodate all road users on constrained ROW</p> <ul style="list-style-type: none"> ✓ Minimized impacts to properties, natural environment, and built/cultural heritage features ✗ Higher construction cost than Do Nothing but comparable to other semi-rural options ✗ Does not accommodate parking ✓ Dedicated bicycle and pedestrian zones, but only on one side of the street ✓ Safer cycling and pedestrian environment from dedicated zones ✓ Addresses drainage deficiencies through underground infrastructure (catch basin and subdrain under curb)
<p>9.3 m PLATFORM SEMI-RURAL ROAD WITH NARROW PAVED BUFFER</p>		<p>Semi-rural cross-section with narrow paved buffers on constrained ROW</p> <ul style="list-style-type: none"> ✓ Minimized impacts to properties, natural environment, and built/cultural heritage features ✗ Higher construction cost than Do Nothing but comparable to other semi-rural options ✗ Does not accommodate parking ✗ Does not accommodate all road users on narrow paved buffers ✓ Addresses drainage deficiencies through underground infrastructure (catch basin and subdrain under curb)
<p>10.6 m PLATFORM SEMI-RURAL ROAD WITH MULTI-USE TRAIL</p>		<p>Semi-rural cross-section with paved multi-use trail to accommodate all road users</p> <ul style="list-style-type: none"> ✗ Potential impacts to properties, natural environment, and built/cultural heritage features ✗ Higher construction cost than Do Nothing but comparable to other semi-rural options ✗ Does not accommodate parking ✓ Dedicated bicycle and pedestrian zones ✓ Safer cycling and pedestrian environment from dedicated zones ✓ Addresses drainage deficiencies through underground infrastructure (catch basin and subdrain under curb)
<p>11.7 m PLATFORM SEMI-RURAL ROAD WITH SIDEWALK AND PARKING</p>	<p>PREFERRED (Old Main Street between Bush and the Community Centre)</p>	<p>Semi-rural cross-section with sidewalk and parking</p> <ul style="list-style-type: none"> ✗ Potential impacts to properties, natural environment, and built/cultural heritage features, but minimized where possible ✗ Higher construction cost than Do Nothing but comparable to other semi-rural options ✓ Accommodates parking ✗ No dedicated bicycle zones ✓ Dedicated pedestrian zones on one side of the street ✓ Safer pedestrian environment from dedicated zones ✓ Addresses drainage deficiencies through underground infrastructure (catch basin and subdrain under curb)

Bush Street Cross-Sections

DO NOTHING

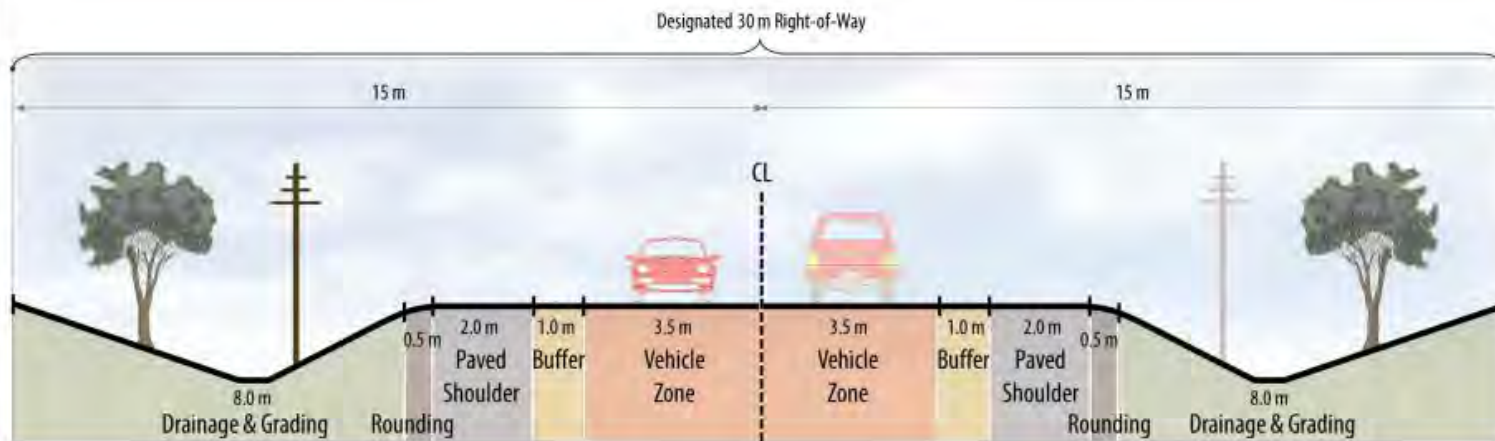


Note: Total right-of-way is predominantly 30 m; paved portion of shoulder ranges from 0.2-1.5 m; majority of above ground utilities run on north side of the road and crosses over between sides

Typical existing cross-section

- ✓ No impacts to properties, natural environment, or built/cultural heritage features
- ✗ Does not accommodate all road users on unpaved shoulders
- ✗ Inadequate drainage

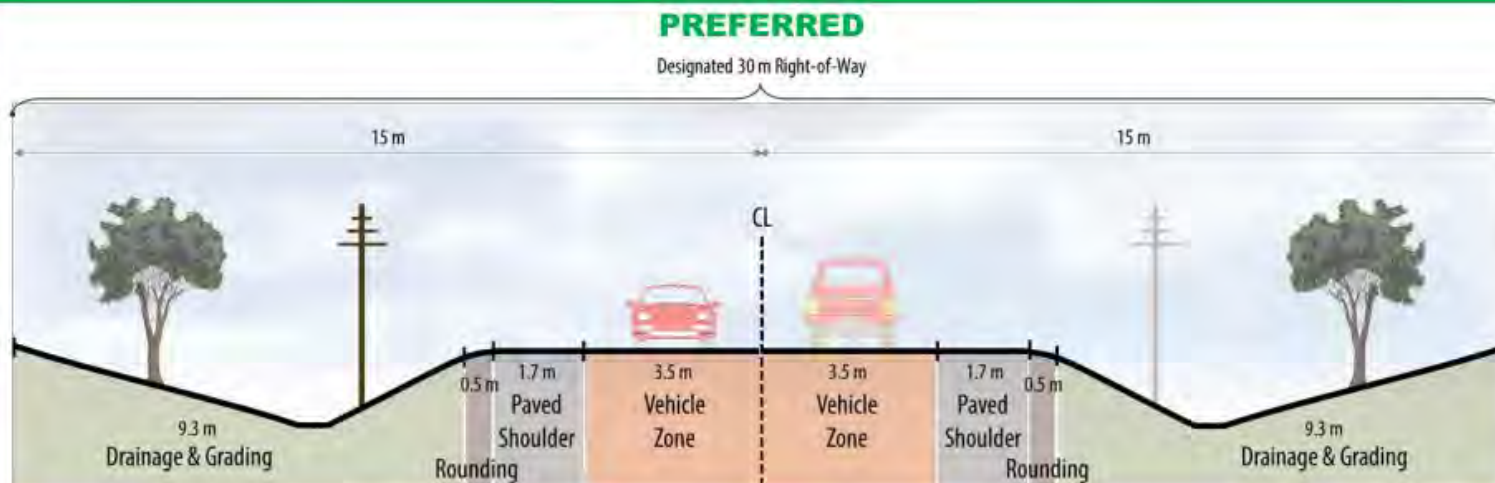
14m PLATFORM RURAL ROAD



Accommodates all road users on 30m ROW with paved shoulder and buffer

- ✗ Potential impacts to properties, natural environment, and built/cultural heritage features along some segments of the corridor
- ✗ Higher construction cost than Do Nothing and 11.4m Platform Rural Road
- ✓ Accommodates all road users on wider paved shoulder
- ✓ Safer cycling and pedestrian environment as a result of buffer zone
- ✓ Accommodates agricultural vehicles on wider pavement
- ✓ Retains rural character and countryside scenic quality
- ✓ Addresses drainage deficiencies

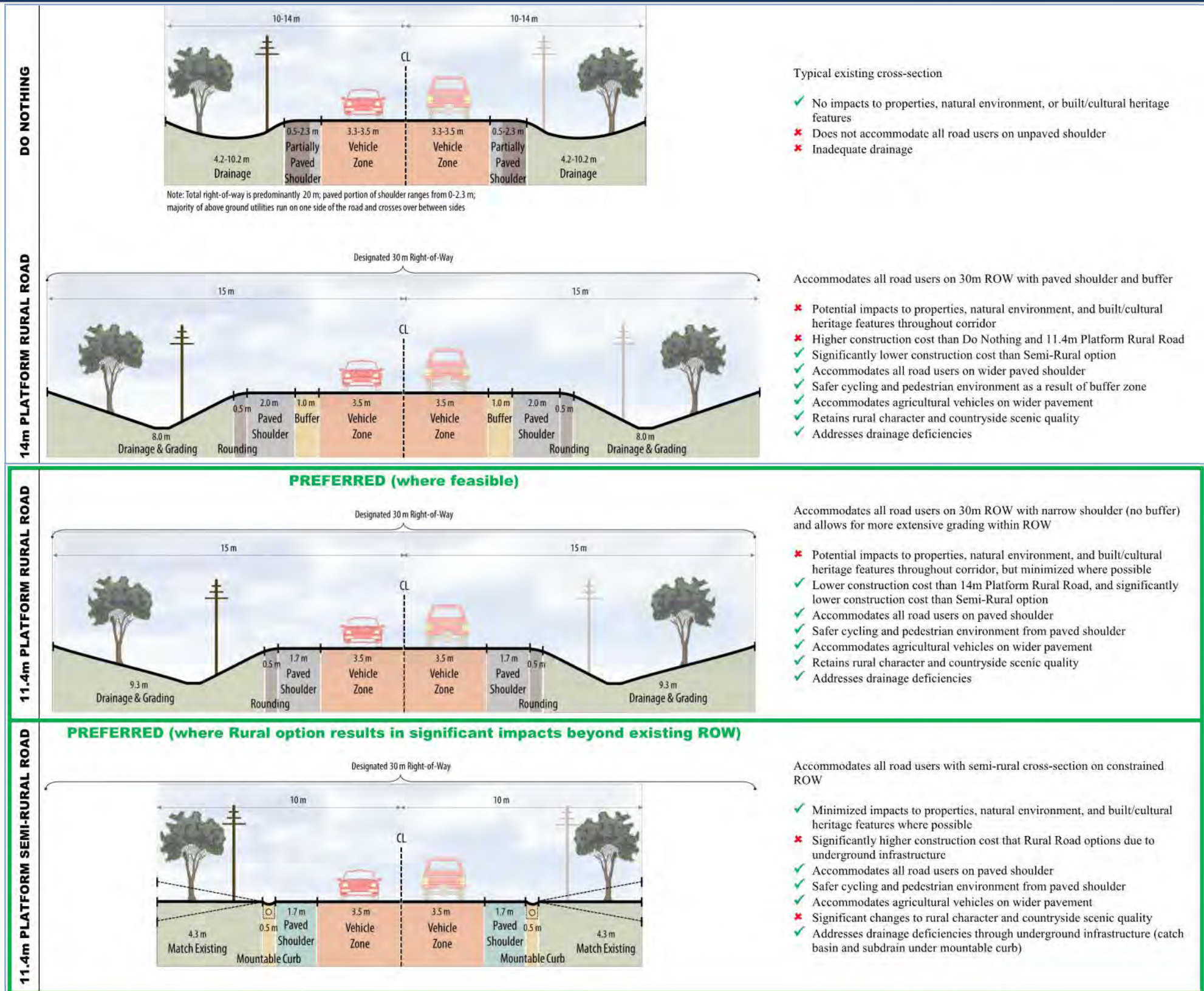
11.4m PLATFORM RURAL ROAD



Accommodates all road users on 30m ROW with narrow shoulder (no buffer) and allows for more extensive grading within ROW

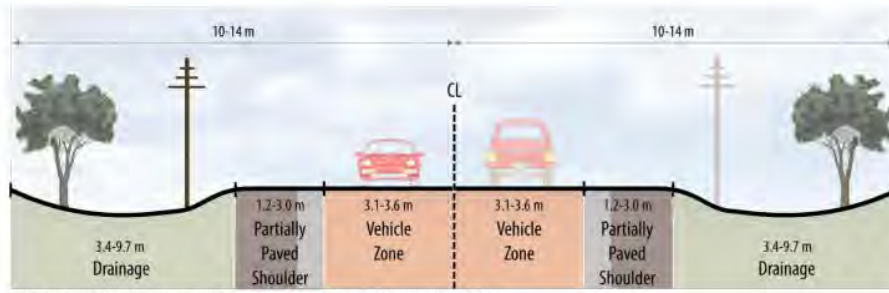
- ✓ Some potential impacts to properties, natural environment, and built/cultural heritage features along some segments of the corridor, but minimized where possible
- ✓ Lower construction cost than 14m Platform Rural Road
- ✓ Accommodates all road users on paved shoulder
- ✓ Safer cycling and pedestrian environment from paved shoulder
- ✓ Accommodates agricultural vehicles on wider pavement
- ✓ Retains rural character and countryside scenic quality
- ✓ Addresses drainage deficiencies

Mississauga Road Cross-Sections



Winston Churchill Boulevard Cross-Sections

DO NOTHING

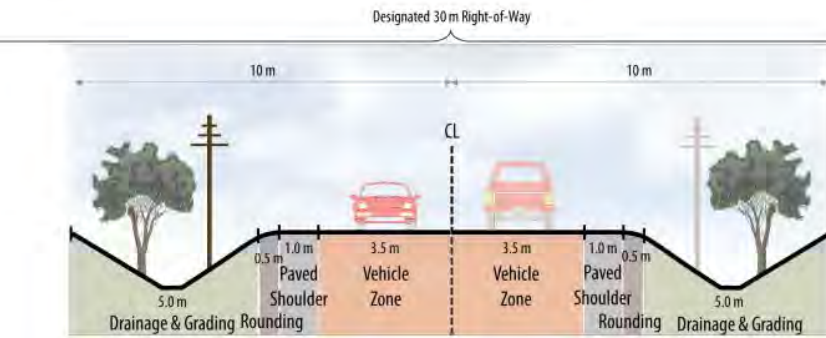


Note: Total right-of-way is predominantly 20-23 m; paved portion of shoulder ranges from 0-1.0 m; majority of above ground utilities run on east side of the road and crosses over between sides

Typical existing cross-section

- ✓ No impacts to properties, natural environment, or built/cultural heritage features
- ✗ Does not accommodate all road users on unpaved shoulders
- ✗ Inadequate drainage

10m PLATFORM RURAL ROAD

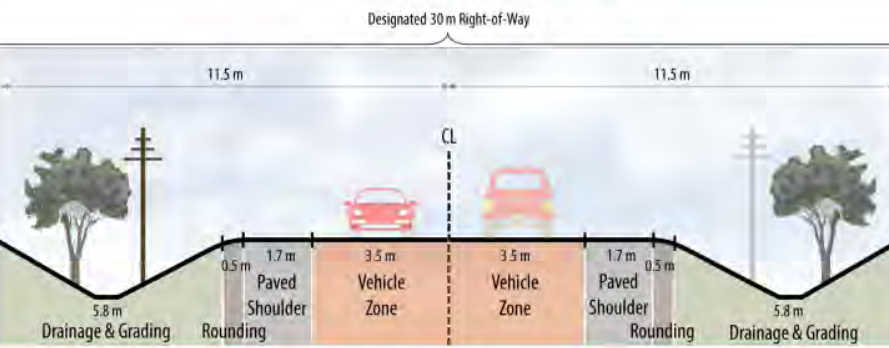


Constrained ROW with narrow paved shoulder (no buffer) and allows for moderate grading within ROW

- ✓ Some impacts to properties, natural environment, and built/cultural heritage features along segments of the corridor, but minimized where possible
- ✓ Higher construction cost than Do Nothing, but lower than 11.4 Platform Rural Road, and significantly lower than Semi-Rural options
- ✗ Does not accommodate all road users on narrow paved shoulder
- ✗ Does not accommodate agricultural vehicles as well as other options
- ✓ Retains rural character and countryside scenic quality
- ✗ Addresses drainage deficiencies, but drainage/grading might extend beyond existing ROW

11.4m PLATFORM RURAL ROAD

PREFERRED (where feasible)

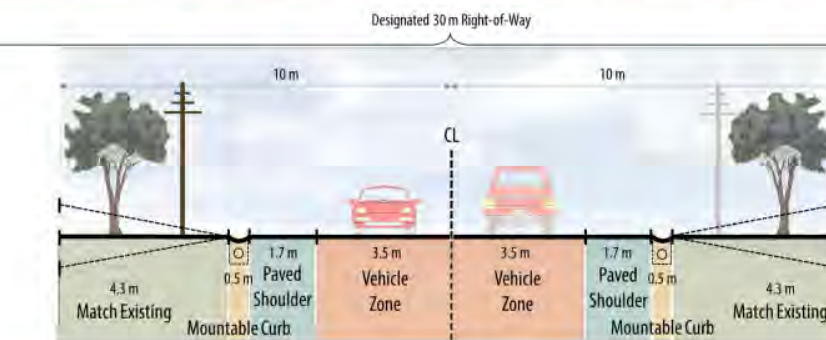


Accommodates all road users with paved shoulder (no buffer) and allows for moderate grading within ROW

- ✗ Impacts to properties, natural environment, and built/cultural heritage features throughout corridor
- ✓ Higher construction cost than Do Nothing, 10m Platform Rural Road, but significantly lower than Semi-Rural options
- ✓ Accommodates all road users on paved shoulder
- ✓ Safer cycling and pedestrian environment from paved shoulder
- ✓ Accommodates agricultural vehicles on wider pavement
- ✓ Retains rural character and countryside scenic quality
- ✗ Addresses drainage deficiencies, but drainage/grading might extend beyond existing ROW

11.4m PLATFORM SEMI-RURAL ROAD

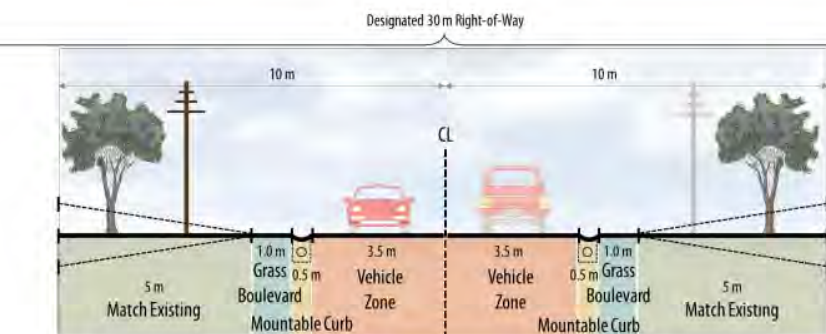
PREFERRED (where Rural option results in significant impacts beyond existing ROW)



Semi-rural cross-section with paved shoulder to accommodate all road users on constrained ROW

- ✓ Minimized impact to properties, natural environment, and built/cultural heritage features where possible
- ✗ Significantly higher construction cost than Rural Road options due to underground infrastructure, and higher than 10m Platform Semi-Rural Road
- ✓ Accommodates all road users on wider paved shoulder
- ✓ Safer cycling and pedestrian environment from paved shoulder
- ✓ Accommodates agricultural vehicles on wider pavement
- ✗ Significant changes to rural character and countryside scenic quality
- ✓ Addresses drainage deficiencies through underground infrastructure (catch basin and subdrain under mountable curb)

10m PLATFORM SEMI-RURAL ROAD

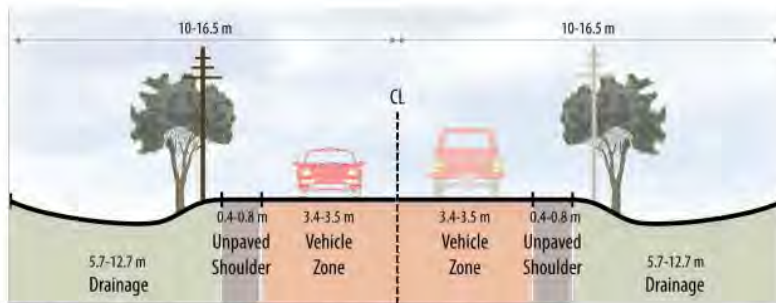


Semi-rural cross-section with narrow grass boulevard on constrained ROW

- ✓ Minimized impact to properties, natural environment, and built/cultural heritage features where possible
- ✗ Significantly higher construction cost than Rural Road options due to underground infrastructure, but lower than 11.4m Platform Semi-Rural Road
- ✗ Does not accommodate all road users on narrow grass boulevard
- ✗ Does not accommodate agricultural vehicles
- ✗ Significant changes to rural character and countryside scenic quality
- ✓ Addresses drainage deficiencies through underground infrastructure (catch basin and subdrain under mountable curb)

Olde Base Line Road Cross-Sections

DO NOTHING

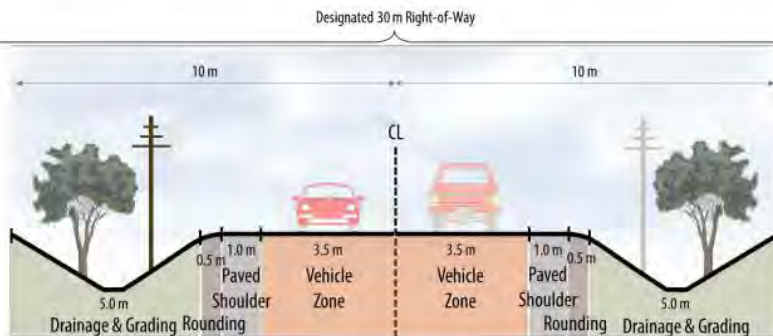


Note: Total right-of-way is predominantly 20-25 m; no paved portion of shoulder exists; majority of above ground utilities run on one side of the road and cross over between sides

Typical existing cross-section

- ✓ No impacts to properties, natural environment, or built/cultural heritage features
- ✗ Does not accommodate all road users on unpaved shoulders
- ✗ Inadequate drainage

10m PLATFORM RURAL ROAD

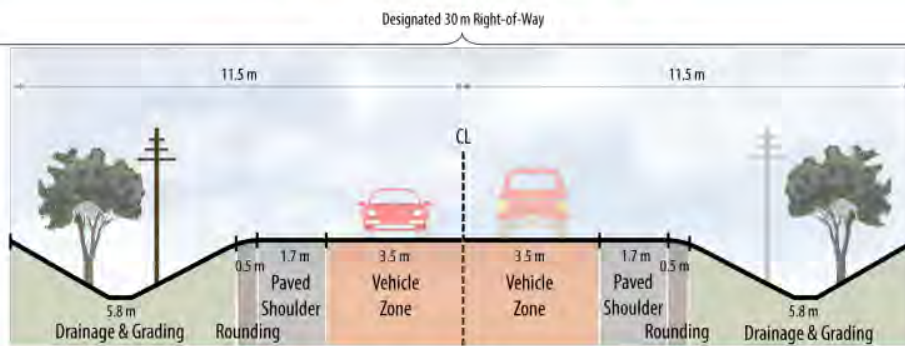


Constrained ROW with narrow paved shoulder (no buffer) and allows for moderate grading within ROW

- ✓ Some impacts to properties, natural environment, and built/cultural heritage features along segments of the corridor, but minimized where possible
- ✓ Higher construction cost than Do Nothing, but lower than 11.4 Platform Rural Road, and significantly lower than Semi-Rural options
- ✗ Does not accommodate all road users on narrow paved shoulder
- ✗ Does not accommodate agricultural vehicles as well as other options
- ✓ Retains rural character and countryside scenic quality
- ✗ Addresses drainage deficiencies, but drainage/grading might extend beyond existing ROW

11.4m PLATFORM RURAL ROAD

PREFERRED (where feasible)

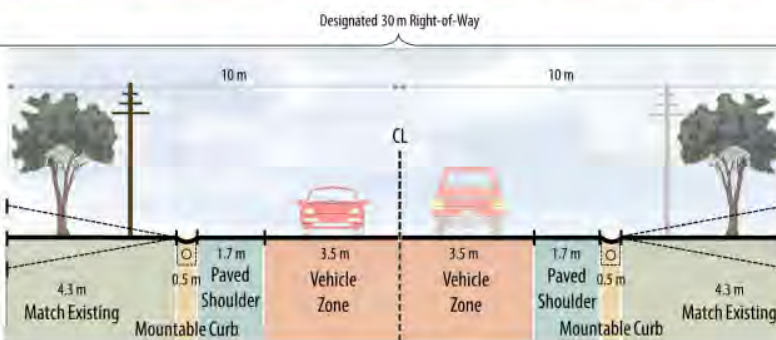


Accommodates all road users with paved shoulder (no buffer) and allows for moderate grading within ROW

- ✗ Impacts to properties, natural environment, and built/cultural heritage features throughout corridor
- ✓ Higher construction cost than Do Nothing, 10m Platform Rural Road, but significantly lower than Semi-Rural options
- ✓ Accommodates all road users on paved shoulder
- ✓ Safer cycling and pedestrian environment from paved shoulder
- ✓ Accommodates agricultural vehicles on wider pavement
- ✓ Retains rural character and countryside scenic quality
- ✗ Addresses drainage deficiencies, but drainage/grading might extend beyond existing ROW

11.4m PLATFORM SEMI-RURAL ROAD

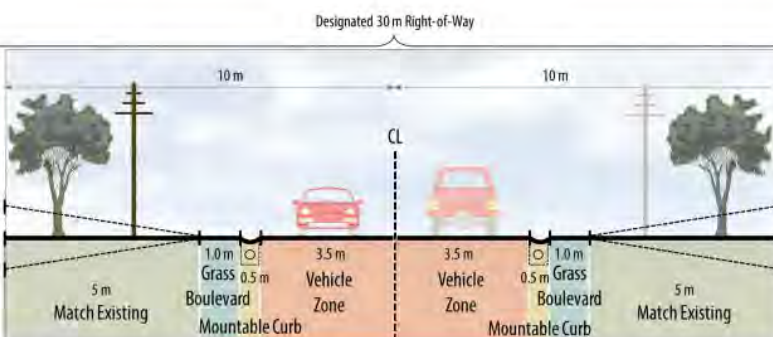
PREFERRED (where Rural option results in significant impacts beyond existing ROW)



Semi-rural cross-section with paved shoulder to accommodate all road users on constrained ROW

- ✓ Minimized impact to properties, natural environment, and built/cultural heritage features where possible
- ✗ Significantly higher construction cost than Rural Road options due to underground infrastructure, and higher than 10m Platform Semi-Rural Road
- ✓ Accommodates all road users on wider paved shoulder
- ✓ Safer cycling and pedestrian environment from paved shoulder
- ✓ Accommodates agricultural vehicles on wider pavement
- ✗ Significant changes to rural character and countryside scenic quality
- ✓ Addresses drainage deficiencies through underground infrastructure (catch basin and subdrain under mountable curb)

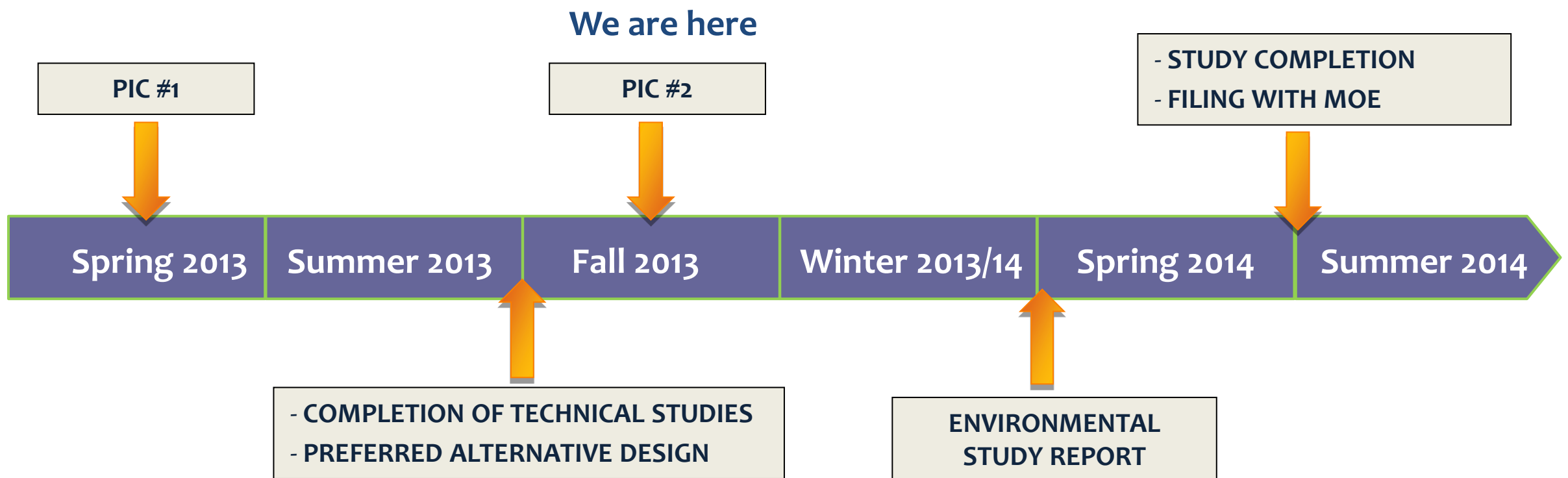
10m PLATFORM SEMI-RURAL ROAD



Semi-rural cross-section with narrow grass boulevard on constrained ROW

- ✓ Minimized impact to properties, natural environment, and built/cultural heritage features where possible
- ✗ Significantly higher construction cost than Rural Road options due to underground infrastructure, but lower than 11.4m Platform Semi-Rural Road
- ✗ Does not accommodate all road users on narrow grass boulevard
- ✗ Does not accommodate agricultural vehicles
- ✗ Significant changes to rural character and countryside scenic quality
- ✓ Addresses drainage deficiencies through underground infrastructure (catch basin and subdrain under mountable curb)

Next Steps / Schedule



Thank You

Please complete your feedback form and place it in the Comment Box, or send your comments by email/fax/mail to any of the following team members by **Wednesday, December 4, 2013.**

You can view tonight's information boards again on our website:

<http://www.peelregion.ca/pw/transportation/environ-assess/mississauga-road-bush.htm>

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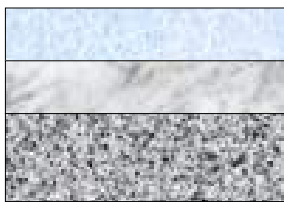
Thank you for your participation

What is the difference between the design for the Olde Base Line Road and Winston Churchill Boulevard roads vs typical high truck volume roads in the Region of Peel?

This area is not designated for significant future growth, therefore the volumes will remain relatively constant. There is no need to construct the pavement to handle significant volumes of truck traffic.

Current EA Design

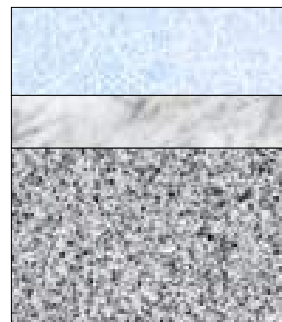
1. Pavement Structure



100mm (4in.) Asphalt
150mm (6in.) Granular A
400mm (16in.) Granular B
650mm (26in.) TOTAL THICKNESS

Typical Pavement Design for High Truck Volume Regional Roads

e.g. Mayfield Road from Chinguacousy Road to Heart Lake Road

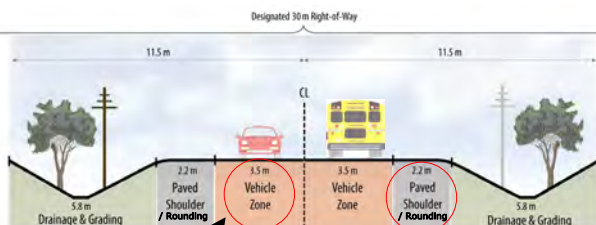


220mm (9in.) Asphalt
150mm (6in.) Granular A
830mm (33in.) Granular B

1200mm (48in.) TOTAL THICKNESS

Current EA Design

2. Lane Widths

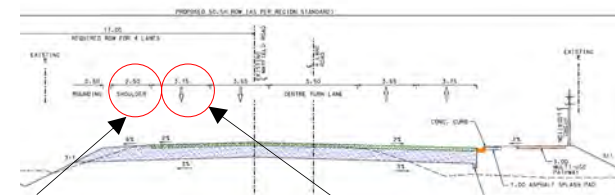


3.5m lane width

2.2m paved shoulder / rounding

The paved shoulders will allow for safe movement of cyclists, pull over for emergency parking, and oversized agricultural equipment.

Typical Lane Widths for High Truck Volume Regional Roads



2.5m paved shoulder

3.75m lane width

The 3.75m lane widths permit wide truck dimensions.

This EA is not recommending a pavement structure for high volume trucks

Current EA Design

3. Speed

Proposed posted speeds



Proposed 60 km/h for
Winston Churchill Boulevard



Proposed 50 km/h for
Olde Base Line Road

Typical Posted Speed for High Truck Volume Regional Roads

e.g. Mayfield Road, Airport Road, and,
Highway 50



Contact Us

If you have any questions or concerns about the project, please contact **Gino Dela Cruz, Project Manager, Region of Peel** at 905-791-7800 x7805 or email at Gino.Dela_Cruz@peelregion.ca.

Further Information

To obtain more information about the study please visit our study website at:

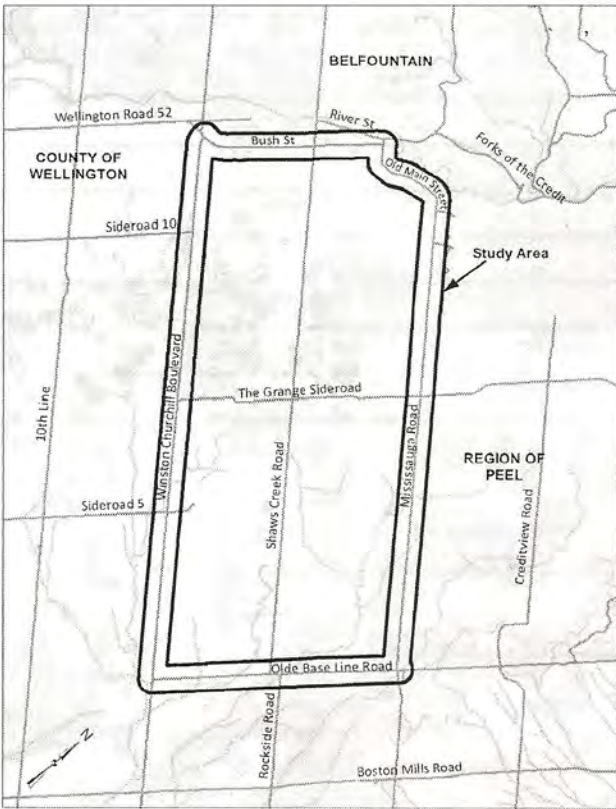
<http://www.peelregion.ca/pw/transportation/enviro-assess/mississauga-road-bush.htm>



**Environmental Assessment Study
Public Information Centre # 2
November 20, 2013
Feedback Form**

1. Recommended Designs for each of the roads

The study team presented the recommended designs for each of the roads and other alternatives that were considered. Do you agree with the recommended designs? Please share any comments or suggestions about the recommended designs for each of the roads that concern you. Circle or mark the map to identify the areas that your comments refer to.



@ 23+000 HERITAGE STONE WALL AT MISSISSAUGA RD.

- RELOCATE CULVERT IN ROAD TO MATCH NATURAL DRAINAGE THROUGH STONE WALL (@ STONE ARCH)
FROM THERE IT GOES INTO AN OLD CONCRETE CULVERT FOR ~300'

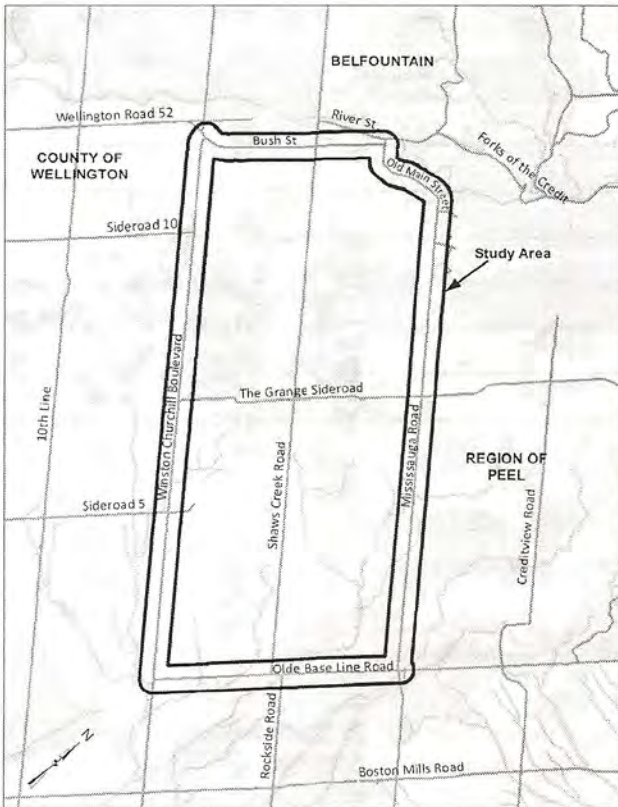
- GRADE AWAY FROM STONE WALL & MATCH GRADE WITH MOUNTABLE CURB



**Environmental Assessment Study
Public Information Centre # 2
November 20, 2013
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1. Recommended Designs for each of the roads

The study team presented the recommended designs for each of the roads and other alternatives that were considered. Do you agree with the recommended designs? Please share any comments or suggestions about the recommended designs for each of the roads that concern you. Circle or mark the map to identify the areas that your comments refer to.



Recommended designs look acceptable. Would like to know which areas of the study roads will have mountable curbs.

Consider stop signs on WCB at 5 sideroad + 10 sideroad as a way to enforce speed limits.

Thank you for your hard work & patience!

2. Alternative Design Concepts and the Evaluation Process

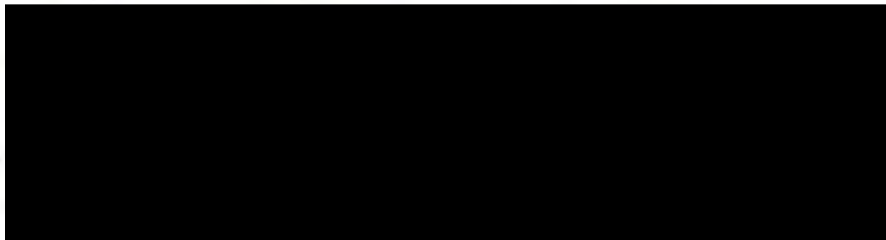
The Study Team evaluated alternative design concepts based on evaluation criteria developed with public input. The preferred designs aim to accommodate the existing mix of traffic while maintaining the rolling terrain, retaining the rural character of the area, and minimizing impact to adjacent properties and landscapes. What comments do you have about how the alternative design concepts were evaluated?

3. Please share other comments about this study here

Pleased to hear that WCB+OLB will not be designated as priority truck haul routes!

Thank You

Your Name:
Address:
Phone Number:
Email:



Please complete your feedback form and place it in the Comment Box, or send your comments by email/fax/mail to one of the following team members by **Wednesday, December 4, 2013**.

You can view today's information boards again on our website:

<http://www.peelregion.ca/pw/transportation/envirom-assess/mississauga-road-bush.htm>

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November 20, 2013

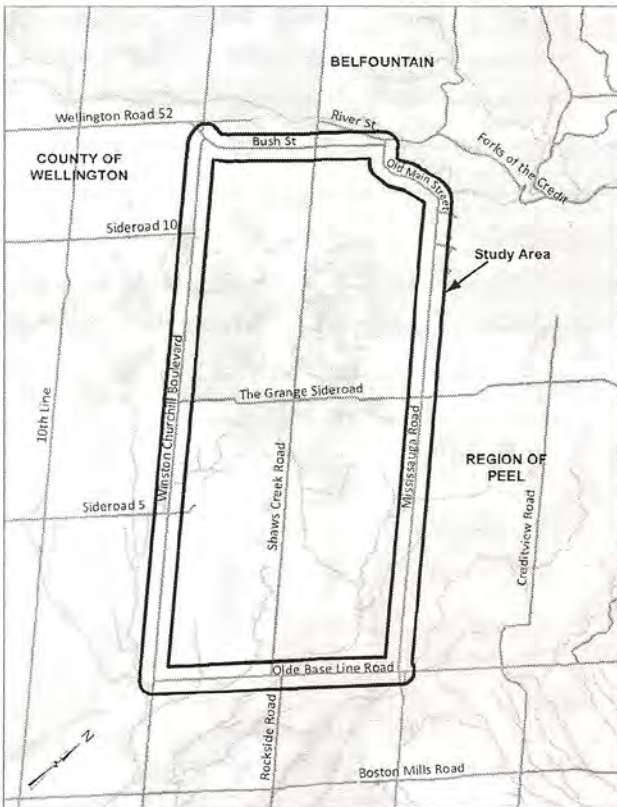




Environmental Assessment Study
Public Information Centre # 2
November 20, 2013
Feedback Form

1. Recommended Designs for each of the roads

The study team presented the recommended designs for each of the roads and other alternatives that were considered. Do you agree with the recommended designs? Please share any comments or suggestions about the recommended designs for each of the roads that concern you. Circle or mark the map to identify the areas that your comments refer to.



- Absolutely no sidewalk or road widening inside Belfountain hamlet - the houses are too close to the road already

- Do not dump project rubble (as you did 1000+ truck loads from the Forks of Credit Rd project) in Belfountain

- Please, do nothing

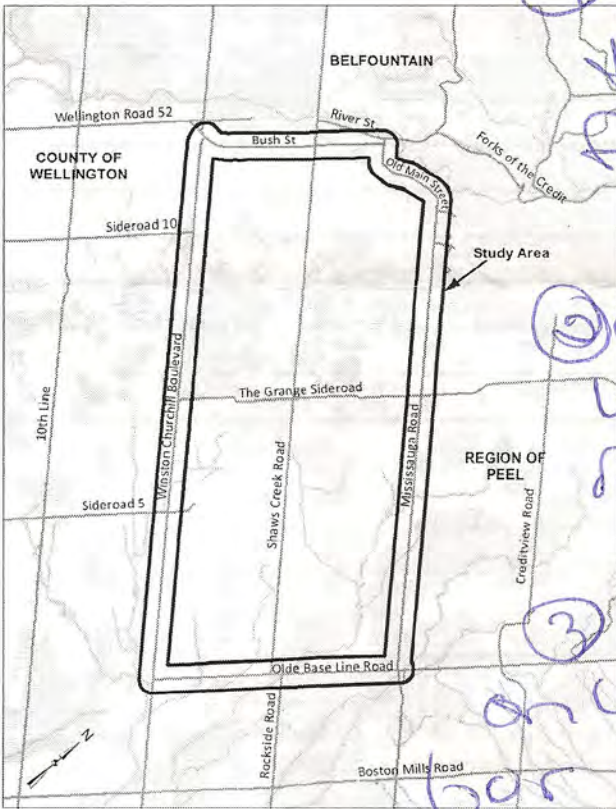
- in widening the Mississauga Rd entering Belfountain, you will prevent flying squirrels from crossing the road.



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① Bike signs reminding them to ride single file. NOT 5 abreast

② Excessive speed on Winston Churchill south of Olde Base.

③ Police coverage on Winston Churchill for excessive speed

Heavy truck activity & gravel trucks.

2. Alternative Design Concepts and the Evaluation Process

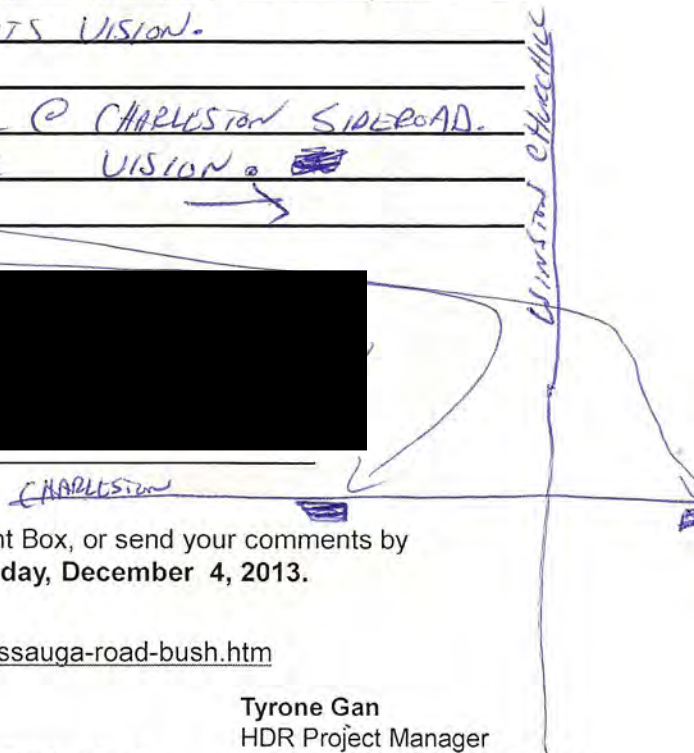
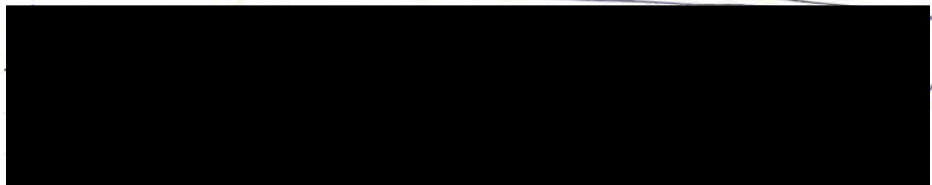
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3. Please share other comments about this study here

- ① THE SPEED LIMIT ON BUSH STREET @ WINSTON CHURCHILL IS 80km/h. CROSSING IT IS A RISK!! THE CURVE BEFORE AND AFTER LIMITS VISION.
- ② HEADING NORTH ON WINSTON CHURCHILL @ CHARLESTON SIDEROAD. THE SIGNS IN THE DITCH BLOCK VISION.

Thank You

Your Name:
 Address:
 Phone Number:
 Email:



Please complete your feedback form and place it in the Comment Box, or send your comments by email/fax/mail to one of the following team members by **Wednesday, December 4, 2013**.

You can view today's information boards again on our website:

<http://www.peelregion.ca/pw/transportation/envirom-assess/mississauga-road-bush.htm>

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 Fax: (289) 695-4601

November 20, 2013

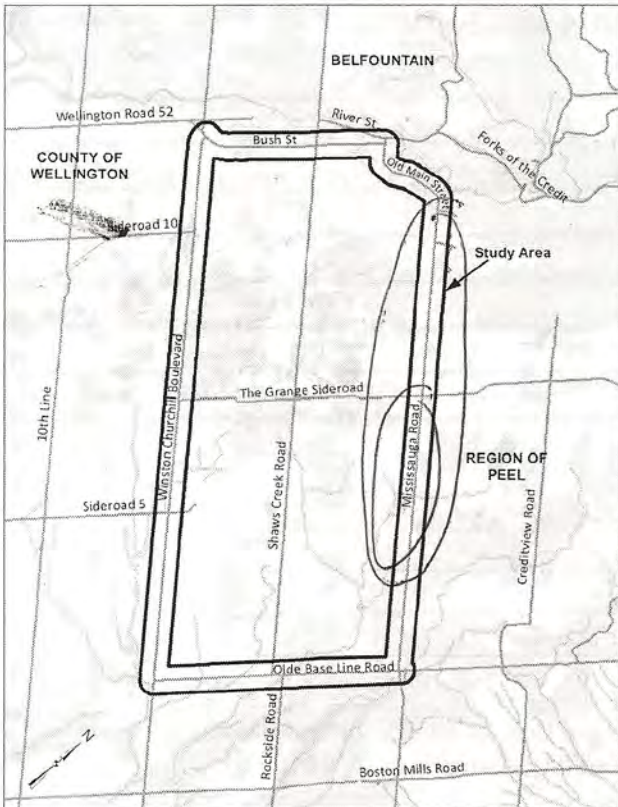




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*We agree with the recommendations
that you have proposed & wish
that you could start ASAP.*

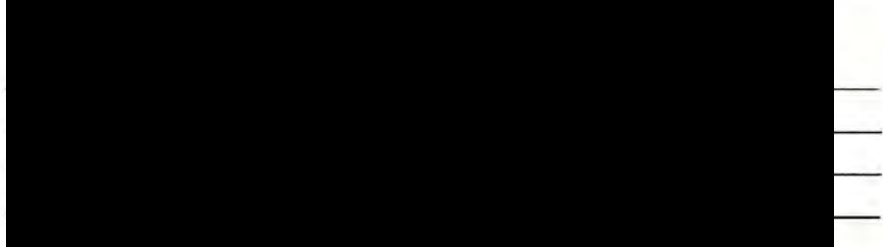
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3. Please share other comments about this study here

We wish you could expedite the addition of paved shoulders to improve ~~the~~ safety for walking & biking on Mississauga Rd.

Thank You
Your Name:
Address:
Phone Number:
Email:



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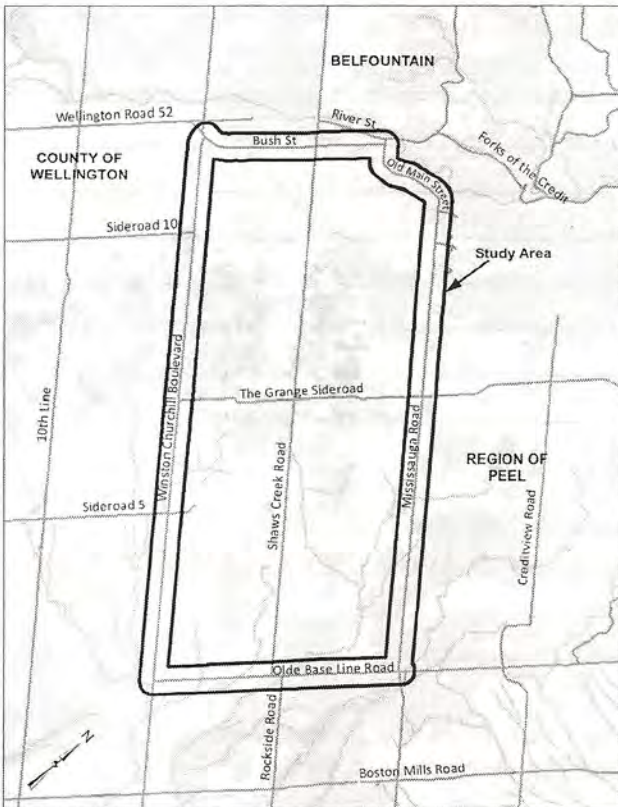




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I reviewed the Winston Churchill design - I like it - especially the recommended design - the paved shoulder is an important safety improvement

Please try to do something to slow traffic
I live south of the study area (below the gravel portion) + the speed of vehicles is very high - if it is all paved it will exponentially increase the volume as well

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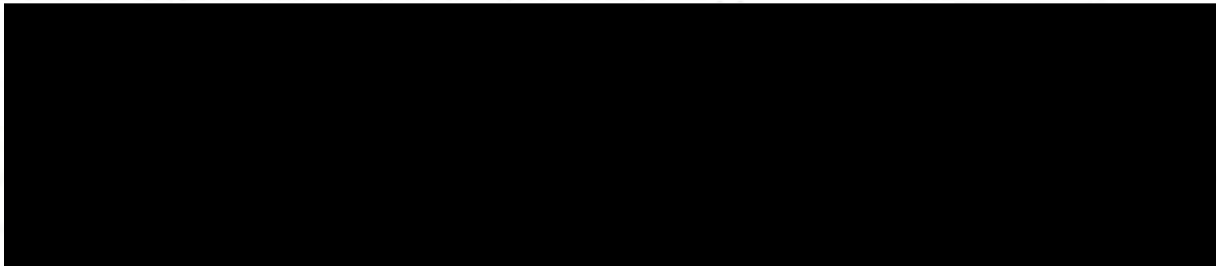
see #1

3. Please share other comments about this study here

I like the pavement/asset management approach. 'Pavement shaving' seems to be a fiscally responsible method of maintenance

Thank You

Your Name:
Address:
Phone Number:
Email:



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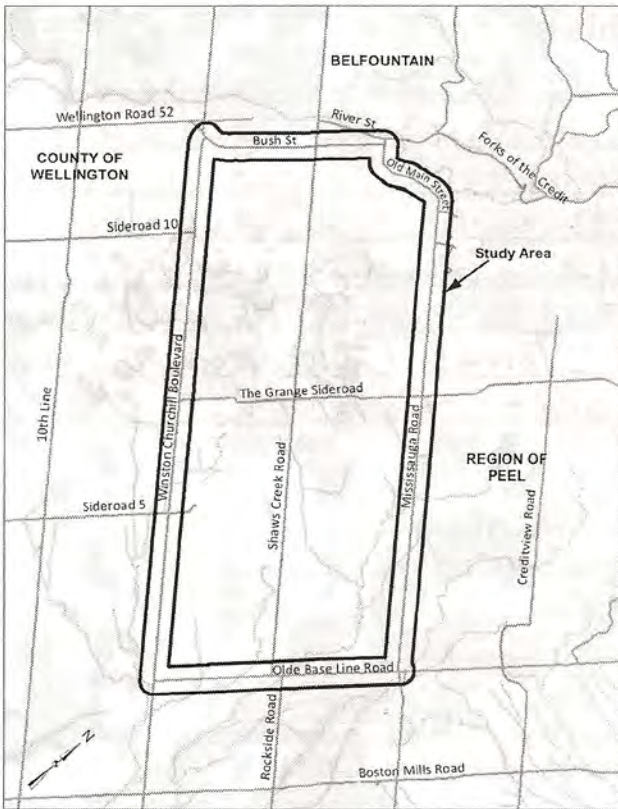




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Adding bike lanes would be great.

We are supportive of lower speed limits and greater enforcement to improve safety.

Smoothing out the hills may only encourage greater speeds and defeat the original purpose.

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3. Please share other comments about this study here

We are strongly opposed to any proposed changes to the weight restrictions on any of these roads. We CANNOT have gravel trucks going through these roads due to safety, heritage and quality of life reasons. We've spent 21 years fighting against the proposed James Dick quarry and do not want a repeat fight!

Thank You

Your Name:

Address:

Phone Number:

Email:



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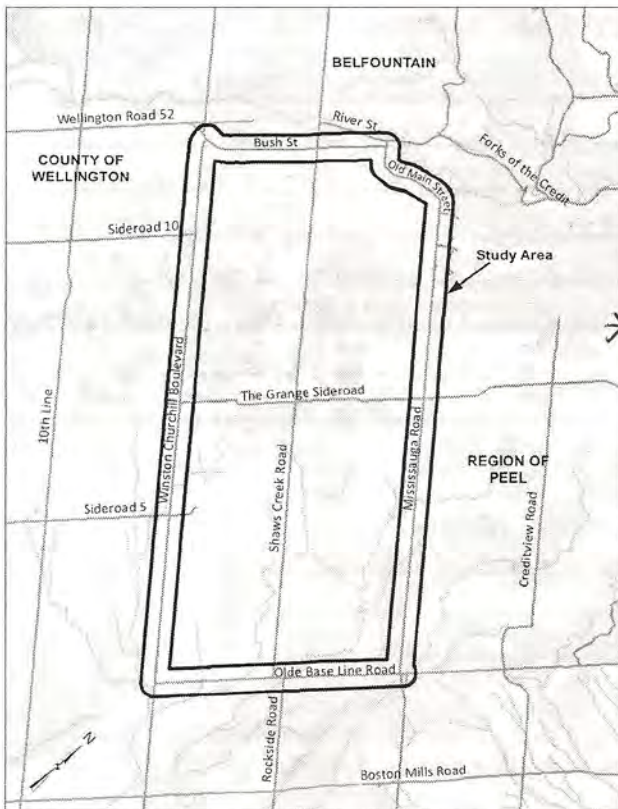




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I AGREE THAT THE ROAD SYSTEM IDENTIFIED SHOULD BE UPGRADED TO THE CURRENT E A DESIGN, FOR ① PAVEMENT STRUCTURE ② LANE WIDTHS * FOR ③ POSTED SPEED THE NEW SIGHT LINE DESIGN SHOULD ACCOMODATE 90 KM PER HOUR. — IT DOES NOT MAKE ECONOMIC SENSE TO BUILD TO 60-70 KMS/HR. AND THEN IN A FEW YEARS NEED TO RECONSTRUCT. THE PRESENT TRAFFIC FLOW BETWEEN 6-9 AM AND 3.30-6.30 PM DEMANDS

THAT OLD BASE LINE TO WINSTON CHURCHILL AND WINSTON CHURCHILL NORTH ACCOMODATE HIGH VOLUME COMMUTER TRAFFIC. A GROWING POPULATION IN THE ERIN AREA ADDS TO THE URGENCY OF THE MAXIMUM UPGRADE FOR COMMUTER TRAFFIC

2. Alternative Design Concepts and the Evaluation Process

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SEE H 10 GO TO 90 KMS / HR DESIGN

3. Please share other comments about this study here

THIS STUDY HAS A SERIOUS FLAW IN THAT IT DOES NOT INDICATE A MAJOR UPGRADE IN WINSTON CHURCHILL FROM OLD BASE LINES SOUTH TO BALNEAR ROAD. THIS WOULD BEST ACCOMMODATE WESTBOUND TRAFFIC FROM THE STUDY AREA AND EAST TO THE GUELPH AREA AND SOUTH TO 401 VIA TRAFALGAR ROAD.

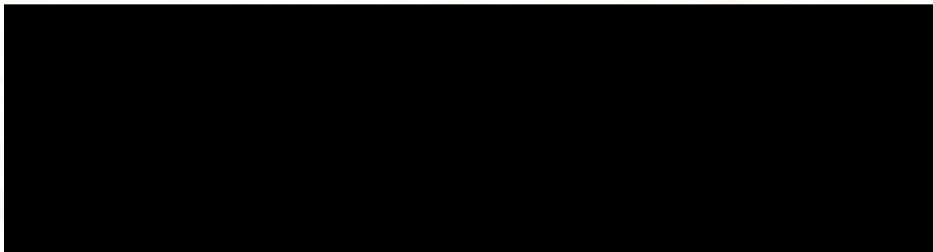
Thank You

Your Name:

Address:

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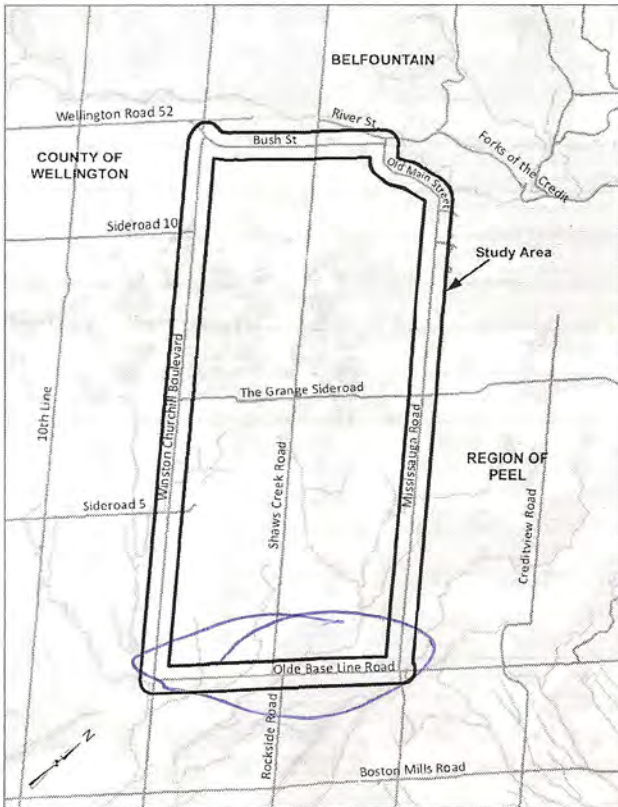
 Region of Peel
Working for you



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There is no need for BASE LINE
TO BE A HEAVY TRUCK ROUTE
GRAVEL TRUCKS FROM FUTURE
JAMES DICK QUARRY LOCATED
NORTH OF BUSH ON WINSTON CHURCHILL
CAN USE WELLINGTON #52 OVER
TO TRAFALGAR RD. AS THERE
IS AN EXISTING TRUCK ROUTE
FROM THE ERIN PIT.

KEEP BASE LINE AS A RURAL ROAD.

Mississauga Rd., Old Main St., Bush St., Olde Base Line Rd. and Winston Churchill Blvd. EA
 Summary of PIC #2 Comments noted on Plan & Profile Drawings

Station	Comment	Response
Mississauga Road / Old Main Street		
20+300	Review warrants for all-way stop at Mississauga Road/Olde Base Line Road intersection	Intersection does not meet warrant for all-way stop control based on the minimum volume warrant (arterial and major roads), and the collision warrant
22+450	Additional culvert at driveway on east side	Comment noted; however, outcome of proposed design is not affected by this culvert (approx. 25m beyond existing ROW)
23+060	Please grade from heritage stone wall to mountable curb. Very important that wall has good drainage and will help re: maintenance. Move culvert south in line with stone arch in wall.	Culvert has been relocated to align with stone arch in wall. Grading does not impact stone wall.
23+340	Consider reducing the slope on The Grange Side Road approach to the intersection (school bus has slid onto Mississauga Road).	Design will pave to curb return, but grading on The Grange Side Road is outside of current scope
23+500	Lay some fiber optic cable for high speed internet service	Comment noted; however not in current scope
23+700	Allow for natural gas	Comment noted; however not in current scope
24+520	Existing rock cut or hill on both sides	Noted on plans
24+575	Save tree	Reviewed options; however tree removals are required to accommodate design
24+600 to 25+000	Will there be passing lanes on this uphill section?	Passing lanes are not proposed as they would require road widening and are not required based on low volumes
24+900	Land for potential acquisition is valued Concern over property – consider curb	Design has been revised (rural cross-section was replaced with semi-rural cross-section) to avoid property acquisition at this location
24+960	Like shoulder for bike lanes	Comment noted
25+680	Please do not impact the fence	Revised design does not indicate impacts to the fence

Mississauga Rd., Old Main St., Bush St., Olde Base Line Rd. and Winston Churchill Blvd. EA
 Summary of PIC #2 Comments noted on Plan & Profile Drawings

Station	Comment	Response
25+800	Please do not widen the road – no sidewalk	Paved shoulders (not sidewalks) are proposed at this location. Any pavement widening is being minimized and kept within the Regional ROW.
26+100	Speeding issue – add sign warning of pedestrians	Comment noted – enforcement required
26+260 to 26+430	Prefer retain narrow shoulders and no sidewalks	Comment noted; however design recommends sidewalk on the west side to connect to south side of Bush
26+400	Property boundaries not accurate to title – off by 7+ft	Property boundaries were provided by the Region. Outcome of proposed design is not affected by property lines at this location.
26+430	Sidewalks? Place to park if walkways? Emergency?	Current design provides parking and sidewalk. Vehicles can pull over onto parking area in an emergency.
Bush Street		
12+120	Retain parking	Design revised to include parking on Old Main Street immediately north of Bush Street
12+110	Investigate for sidewalk passage	Design revised to connect sidewalk on Bush Street and Old Main Street through sidewalk passage
12+010	Culvert replaced 7-8 years ago	Comment noted – existing culvert is undersized
11+360 to 12+100	Some residents support sidewalks, others strongly oppose them	Comments noted. Design recommends sidewalk on the south side of Bush to connect to west side of Old Main Street
11+300	Speeding problem	Proposed reduction of speed limit west of Shaws Creek – enforcement required
11+100	Please don't damage all the new evergreen trees (planted fall 2013 along old fence line)	Current design avoids impacts to fence and trees
11+100	Like that the road is being shifted to the south where it used to be – lots of room!	Comment noted
11+000	Replace our mailbox if moved back to the south	To be reviewed during detailed design

Mississauga Rd., Old Main St., Bush St., Olde Base Line Rd. and Winston Churchill Blvd. EA
 Summary of PIC #2 Comments noted on Plan & Profile Drawings

Station	Comment	Response
10+990	Don't feel this (driveway) culvert is required	Based on proposed drainage section, a culvert will be required for roadside ditch
10+240	Trim vegetation	Comment noted; to be reviewed
10+100	Remove dangerous cement curb (at south jog of WCB intersection)	Current design replaces curb with shoulder and ditch
10+060	Dangerous intersection (north jog of WCB intersection)	Design recommends reducing posted speed limit through the intersection
Winston Churchill Boulevard		
44+960	Consider stop sign at 10 th Side Road as speed control measure	Intersection does not meet warrant for all-way stop control based on the minimum volume warrant (arterial and major roads), and the collision warrant
44+480	Too close to pond	Design has been revised (rural cross-section was replaced with semi-rural cross-section) to minimize impacts to pond and vegetation at this location
44+300	Protect trees; provide buffer	Comment noted
43+740	Beautiful trees	Design has been revised (rural cross-section was replaced with semi-rural cross-section) to minimize tree removals at this location
43+400	Resident noted drainage low area	Comment noted. Design has been revised (rural cross-section was replaced with semi-rural cross-section) to minimize impacts to adjacent areas
43+140	Stop sign would slow traffic down	Intersection does not meet warrant for all-way stop control based on the minimum volume warrant (arterial and major roads), and the collision warrant
41+870	Consider stop sign at 5 th Side Road as a means to reduce speed	Intersection does not meet warrant for all-way stop control based on the minimum volume warrant (arterial and major roads), and the collision warrant

Mississauga Rd., Old Main St., Bush St., Olde Base Line Rd. and Winston Churchill Blvd. EA
 Summary of PIC #2 Comments noted on Plan & Profile Drawings

Station	Comment	Response
41+320	Ditch requirement through wetland?	Design has been revised (rural cross-section was replaced with semi-rural cross-section) to minimize impacts to adjacent areas
40+000	What will happen to this area (OBL south of WCB)? – potholes, rough grading	Improvements to this section of road completed through separate study – ESR completed and detailed design currently underway
Between 10 th Side Road and The Grange Side Road	Likely turtle overwintering pond	Comment noted; to be reviewed along with NRSI's recommendations
Olde Base Line Road		
30+600	Driveway goes up. If profile lowered, driveway more leveled – who is responsible for grading?	Region would be responsible for regarding impacted driveways, as required
30+640 to 30+820	Noise concern	Proposed reduction in posted speed limit will reduce noise level
30+820	Suggest a deeper rock cut to lower top of knob to avoid (reduce) filling the bottom	Design must be sensitive to grade changes at driveways
31+000 to 31+160	Stone wall under wooden fence	Comment noted
32+280	Can hear trucks	Comment noted – increase in traffic volumes / change in mix of traffic not proposed and existing truck restrictions to remain
General comments		
WCB and OBL	Is it worth investment to have wide shoulders when there are few cyclists?	Goal of the study is to accommodate all road users

**Mississauga Road, Olde Base Line
Road, Winston Churchill
Boulevard, Bush Street and Old
Main Street Environmental
Assessment Study**



November 20, 2013

**PUBLIC INFORMATION CENTRE #2
FEEDBACK REPORT**

This report was prepared by the Independent Facilitator and represents the feedback received at the Public Information Centre #2 held on November 20, 2013. It includes the comments noted through informal discussion, on the plans and input received through Feedback Forms, letter and email.

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Appendices

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Appendix C Comments received by letter and email	Page 18

For further information contact Asha Saddi, Technical Analyst, Infrastructure Programming and Studies Transportation Division, Public Works, Region of Peel, Tel: 905-791-7800, extension 7794
Fax: 905-791-1442 Asha.Saddi@peelregion.ca.

Comments about this report should be directed to Facilitator, Sue Cumming, MCIP RPP,
Cumming+Company at cumming1@total.net or Tel: 866-611-3715.

Mississauga Road, Olde Base Line Road, Winston Churchill Boulevard, Bush Street and Old Main Street Environmental Assessment Study

PUBLIC INFORMATION CENTRE #2 FEEDBACK REPORT

1. ABOUT THE ENVIRONMENTAL ASSESSMENT STUDY AND CONTEXT FOR PUBLIC INFORMATION CENTRE #2

In June 2009, Region of Peel began a Schedule “C” Municipal Class Environmental Assessment (EA) for improvements to Mississauga Road from Olde Base Line Road to Bush Street and Bush Street from Mississauga Road to Winston Churchill Boulevard. The Region of Peel expanded the limits of the EA to include Olde Base Line Road from Mississauga Road to Winston Churchill Boulevard and Winston Churchill Boulevard from Olde Base Line Road to Bush Street (see map).

Why was the study area expanded?

Based on the feedback received for the Mississauga Road / Bush Street EA the Region of Peel expanded the study area to review road safety, sight lines, drainage, parking and pedestrian and cycling needs.

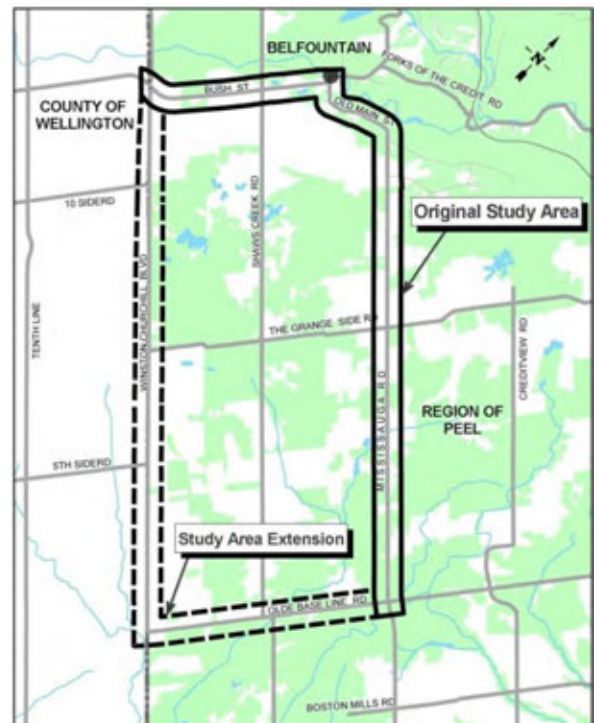
What is this project about?

Existing problems on the study area road network consist of:

- Deficient pavement conditions and drainage
- Deficient sightlines
- Safety for all road users
- Safety of wildlife
- Motor vehicle accidents

The purpose of the project is:

- Rehabilitation of the roads
- Enhancing safety
- Supporting active transportation



As described at the Public Open House held on October 30, 2012 and Public Information Centres held on May 9, 2013 and November 20, 2013 this EA study will **not** be considering road widening or increasing the number of lanes. The Project Team will build on the previous study information to develop a plan for the study area roads that meets the needs of all road users and maintains the rural character of the community.

The Process

The aim of the Environmental Assessment process is to provide everyone who has an interest or stake in the study area with the opportunity to create the best solution. The Project Team, with the Community Working Group and public input, has:

- defined the problem statement (Winter/Spring 2013)
- developed and evaluated planning alternatives; (Summer 2013) and
- determined a preferred solution (Fall 2013/ Winter 2014)

The Region of Peel is undertaking this Environmental Assessment study through a Context Sensitive Planning and Design Approach which focuses on improvements to enhance the experience for all road users and reflects the character of the community. This is important as it will ensure that the solutions fit with the rural and scenic quality of the area and are sensitive to the needs of the community.

Opportunities have been provided for the public to be involved through planned public meetings at key milestones as the study progressed. A Community Working Group (CWG) was also established. The CWG is a broadly based group of interested community stakeholders who have participated in focused discussion on project issues through meetings held over the course of the study. Three meetings of the CWG were held on October 23, 2012, April 4, 2013 and October 16, 2013.

A Public Open House was held at the outset of the study, prior to the formal public meetings, so the Project Team could meet with community residents and stakeholders to discuss the process, and to learn about transportation issues and valued community characteristics. Over 100 attendees were at the October 30, 2012 Open House.

Public Information Centre #1 was held on May 9, 2013 at the Belfountain Public School and was attended by 70 people. The meeting was organized with a short drop-in open house from 6:30 to 7:00 followed by a presentation by Tyrone Gan, Project Manager, HDR Corporation, lead transportation planning consultant for the project on the following:

- Purpose of the EA Study,
- Overview of identified problems and results of needs assessment including traffic analysis and safety considerations,
- Draft problem statement and principles for generating alternative solutions,
- Proposed alternative operational and physical improvements that could be considered, and
- Proposed draft evaluation criteria.

Meeting notes from the CWG discussions and Facilitators Feedback Reports from the Open House and Public Information Centre #1 are available on the project study website (peelregion.ca/pw/transportation/envIRON-ASSess/missISSauga-road-bush.html).

Public Information Centre #2 held November 20, 2013

Public Information Centre #2 was held on November 20, 2013 at the Caledon Country Club from 4:30 to 8:30 p.m. Over 105 people attended. Notice for the November 20, 2013 Public Information Centre was provided through the following:

- Mailing of notices to property owners fronting / backing along the study area corridors and all those who showed interest at previous consultation events
- Project Study Web site
- Local newspaper advertisement:
 - Erin Advocate on November 6 and November 13, 2013
 - Caledon Enterprise on November 7 and 14, 2013
 - George Town Independent on November 7 and 14, 2013
 - Wellington Advertiser on November 8 and 15, 2013

The purpose of the Public Information Centre was to present and receive public comments on the alternative design concepts and recommended designs. The input received is being reviewed to refine the designs and to determine the final recommendations. The PIC was organized as an open house with the opportunity for people to drop-in anytime from 4:30 p.m. to 8:30 p.m. to view plans and to discuss their input with the Project Team. The format of the PIC was designed to maximize the opportunity for each property owner and stakeholder to review the designs for each section of the road on large plan and profile drawings. These were displayed around the perimeter of the room on long tables at a large scale suitable for seeing how the designs would affect each property. A number of other information stations were set-up to provide information on study elements. In addition to the Project Team, other Region of Peel staff and staff representing the Ministry of Natural Resources (MNR) and Niagara Escarpment Commission were in attendance to provide information and participate. Information was shared at these stations through discussions with the Project Team, handouts and review of other related reports and studies.

The information stations included:

- Proposed Road Profile and Cross-section recommended designs for:
 - Mississauga Road/Old Main Street
 - Olde Base Line Road
 - Winston Churchill Boulevard
 - Bush Street
 - Belfountain Village
- Key Design Principles and Design Criteria
- Traffic and Road Safety (sight distances at driveways, collisions by road segment/intersection, roadway hazards, stopping sight distance deficiencies, existing and proposed posted speeds)
- Pavement/Assessment Management Approach and Specifications (Ride Condition Index)
- Peel Long Range Transportation Plan
- Goods Movement in Peel and Strategic Goods Movement Network Study
- Natural Environment Inventory
- Built and Cultural Heritage

The Public Information Centre material is available on the project website. A Feedback Form was provided to enable attendees to provide written responses.

This report, written by the Independent Facilitator, Sue Cumming, Cumming+Company public engagement lead for the project, is intended to provide a synthesis of the overall key messages heard and provide information on next steps. The Appendices contain the input from the Public Information Centre (Appendix A), responses in the Feedback Forms (Appendix B) and comments received through emails and letters (Appendix C). The comments received through the Public Information Centre will be considered by the Project Team and will help in informing the Project Team as the project moves forward.

This report will be posted on the project website and a letter will be forwarded to all residents in the area advising of the availability of the report.

Your participation is important and your comments are valued.

2. KEY MESSAGES HEARD

There is significant community interest in the Environmental Assessment study. The residents and stakeholders who attended the November 20, 2013 Public Information Centre were particularly interested in learning about the recommended designs for sections of the road that most affected them. They met with the Project Team to review the plan profile and cross-sections and to understand what was being recommended. People were encouraged to write comments on the aerial plans and many chose to do so identifying additional details and providing specific feedback.

A number of residents noted their concerns about the potential for any increase in truck usage of the study area roads. Some noted that they had been to meetings before the Public Information Centre set up by others in the community privately that had heightened concerns about goods movement through the area. After reviewing and learning of the recommendation to reduce the posted speeds on these roads and after discussion on the Strategic Goods Movement Study, some of these concerns were alleviated. There remains concern that the Region of Peel will make changes in the future that will increase the likelihood of these roads being used for more trucks. They believe that truck traffic through the area, particularly on Olde Base Line Road and Winston Churchill Boulevard, is an issue today. The majority of the residents oppose the designation of truck priority routes through the Regional Strategic Goods Movement Network and object to having Winston Churchill Boulevard and Olde Base Line Road identified as such. Concerns expressed included noise, safety, speeding and incompatibility of rural character of area.

The importance of ensuring that the study outcomes do not impact the historic and much valued countryside and scenic character of the roads through the community and Village of Belfountain was reiterated and confirmed. There remain a number of concerns pertaining to maintaining the rural character of the Village with mixed views on how to address connectivity, pedestrian and

cycling safety. Some expressed concern about the impacts to their properties of the proposed sidewalks on Bush Street, impact to hydro poles and front yards.

Given the detailed nature of the recommended designs, comments are notably specific to certain properties. The Project Team through their review of the input received will be refining the designs to address the input as part of the final recommendations. The detailed comments are noted on the plans and a summary is provided in Appendix A.

Through discussion and review of input received, there appears to be support for the approach undertaken and the recommended designs. Several commented that having reviewed the plan for Mississauga Road and Winston Churchill Boulevard that they like what is being put forward.

The following is a synthesis of comments and input received.

a. Reassess issue of truck usage on these roads relative to community impact. Residents continue to object to the designation of Truck Priority Routes on Winston Churchill Boulevard and Olde Base Line Road.

There is significant opposition to the designation of Truck Priority Routes along Winston Churchill Boulevard and Old Base Line Road. Many believe that the Region needs to rethink the approach of designating these roads for a truck route and for an increase in truck usage citing that such an outcome would destroy the community. While people who attended noted that they were pleased that Winston Churchill Boulevard and Olde Base Line Road will not be designated as priority truck haul routes as part of this study, there remains concern about future intent. This is a sensitive issue given the effort that the community has expended over a decade on addressing truck travel and other impacts relative to quarry proposals in the area with many feeling that their gains in this regard are being diminished by the Strategic Goods Movement Network Study. There are similar objections to any consideration of changed status for Mississauga Road and Bush Street.

A few others, in written comments, support truck travel along these roads for goods movement through the area and would like to see the roads rehabilitated to accommodate trucks as part of this study.

b. Reduce posted speeds and increase enforcement on roads to reduce safety concerns.

Residents support the reduced posted speeds being recommended and understand how the reduction of speeds affects the profile for each section of the road. In addition to the reduction of the posted speeds, the Region of Peel is encouraged to work with Police to increase enforcement.

c. In the final design, minimize road profile changes.

There appears to be an understanding of why some road profile changes are necessary. Residents emphasized that this is a unique area with rural roads and that the hilly topography and natural environment are fundamental to the character of the area. As the Project Team refines the plans, they would like to see the final recommendation minimize road profile changes wherever possible to maintain this important character.

- d. In developing the final recommendations, priority should be placed on ensuring the protection of historic fences, mature trees, natural vegetation, cedar rail and other features that define the unique character of this area.**

Residents provided comments on the plans for the recommended design identifying where mature trees, heritage fences, ponds, etc. should be carefully addressed in the final recommendations to ensure that these are protected.

- e. Reassess design for the Village of Belfountain to maintain a rural streetscape and minimize impact to heritage features, hydro poles and front yards from proposed sidewalks and road width.**

Many who reside in the Village of Belfountain are concerned about the proposed urban streetscape with mountable curbs and object to sidewalks in front of their homes, citing concerns about removing hydro poles, historic fences, widening of the pavement width and impacts to front yards and the rural character of the Village. They feel that their input is being disregarded and ask that the Project Team reassess the recommended designs along Bush Street through the Village.

Others note that there are solutions that should be addressed through the Village including the talked about multi-use trail or potentially a paved shoulder. A sidewalk akin to what exists in Brampton or Mississauga is not a solution that people feel is acceptable for Belfountain.

- f. Sidewalk consideration along roadways received mixed reviews**

There remain mixed views throughout the study area about sidewalks. Some support the inclusion of pedestrian infrastructure while others feel that sidewalks are unnecessary and that the proposed designs are not in keeping with the rural character of the community. Some noted their support for cycling infrastructure as proposed while others feel that cyclists are being accommodated already and what may be needed is cyclist education and signs for cyclists reminding them to ride single file and not five abreast.

- g. Cycling on paved shoulders and cycling infrastructure received mixed reviews.**

Some noted their support for cycling infrastructure as proposed while others feel that cyclists are being accommodated already and what may be needed is cyclist education and signs for cyclists reminding them to ride single file and not five abreast.

h. Review potential property impacts affecting driveways, fences and vegetation in final design and work with homeowners to minimize impact and disruptions.

Additional concerns noted how the recommended design would impact mature trees, mailboxes, recent tree plantings, culverts and heritage fences along the roadway. Those affected would like to be assured that the Region will involve them in discussions about how these features will be affected, to discuss any potential property takings and minimize impact during the final design and construction stages.

i. Implement site specific improvements to address problem areas.

There appears to be support for improving sightline deficiencies and addressing issues related to the curvature of the roadway in specific areas to improve safety particularly at intersections. The recommended designs are felt to be addressing these. Some would like a further review and possible refinement in the final recommendations for key intersections in the study area. In some areas, additional stop signs are suggested by residents. There is an understanding that new posted speed signage would be placed throughout the study area.

j. Address condition of Winston Churchill Boulevard south of Olde Base Line Road.

While outside the EA Study Area, several noted concerns with speeding and poor surface of Winston Churchill Boulevard south of Olde Base Line Road. There are mixed views as to whether the road should be reconstructed. This is being addressed as part of a separate study.

3. NEXT STEPS

The comments received through the Public Information Centre will be considered by the Project Team as the project moves forward. After considering public comments on the alternative design concepts and recommended designs that were presented at the Public Information Centre, the next steps will be to refine the designs and determine the final recommendations.

The development and evaluation of alternative design concepts, along with the final recommendations, will be documented in an Environmentally Study Report (ESR). The ESR will be filed for public and agency review in late spring/early summer of 2014. A notice of study completion containing information about where the ESR can be reviewed will be mailed to all those on the project mailing list.

Progress on the study can be viewed on the website at:

<http://peelregion.ca/pw/transportation/enviro-assess/mississauga-road-bush.htm>

If you would like to comment on the study, please contact either one of the following Project Team members:

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Appendices

Appendix A Comments/ Questions noted at Public Information Centre #2

Appendix B Public Comments noted through the Feedback Forms

Appendix C Comments received through emails and letters

Appendix A

COMMENTS/ QUESTIONS NOTED AT PUBLIC INFORMATION CENTRE #2

The following questions/comments were noted at the PIC. Each number represents a different individual's comment.

1. Question about how to stop the designation of the area roads for truck haul routes. Concerns noted that the details being shown at the PIC are small and specific to individual properties and not addressing the much bigger issue of goods movements in Peel. Significant concern that the goods movement study when implemented or as it is implemented will impact the community in a harmful way. Strong objection to the possibility of an increase in trucks through the study area. Truck usage does not fit with the rural concept and would like to know how to stop this. No one seems to be listening and have heard that more trucks mean progress but what does this really mean? The development of the Goods Movement plan is top down and not bottom up. The Region of Peel needs to rethink the approach of designated these roads for a truck route and for an increase in truck usage. This would destroy the community and is not what the people want.
2. Very concerned about how the Project Team is picking and choosing how to address input particularly when voice of concern on specific properties is outweighed by overall general comment and feedback.

We don't believe that there is a drainage issue along Bush Street and yet drainage is a major piece of why the change is being recommended. We are very upset about the changes to the drainage ditch, the footpath that would go in front of our house on our property and the mountable curbs which would impact us. At present the walkway is very narrow and people drift by. The change being recommended will impact our privacy and enjoyment of our front yard and property. The hydro poles and pond and culvert on our property will be destroyed. We don't see why the section of the road needs to be widened and flattened out.

We don't want the urban streetscape. What is being shown is more of an urban look which doesn't fit with the rural character of the Village and area.

There is enough room there today for cyclists. Our experience is that the cyclists travel in herds. They travel five or six abreast and they can do this on the road the way that it is today.

The recommended changes are too close to our house.

The new designs show too much clutter.

If you want bike lanes then the regional road allowance is where this may be feasible but not along Bush Street in front of our homes.

For the section from Olde Base Line Road to King Street if it becomes wider, we will be seeing more asphalt. If paved over, we could see problems with floodwater. Instead of natural demarcation there will be six miles of paint.

Very upset that the recommendations if implemented will in our opinion destroy the heritage hamlet. It will look like any other roadway. We have provided our feedback citing concerns with the heritage fences and properties and this is being disregarded. Belfountain is a world biosphere area according to the Niagara Escarpment and needs to be maintained as a heritage village. Please back up and look at this again.

The following comments and questions were noted on the road plans and profiles displayed at the PIC. The station numbers correspond to the location identification on the plans. This is a summary of the comments. The Project Team is reviewing the detailed comments in the review of the input and finalization of the recommendations.

Table 1 - Summary of Comments Noted on Recommended Drawings	
Station	Comments noted
Mississauga Road	
20+300	Review warrants for all-way stop at Mississauga Road/Olde Base Line Road intersection
22+450	There is an additional culvert at driveway on east side
23+060	Please grade from heritage stone wall to mountable curb. Very important that wall has good drainage and will help re: maintenance. Move culvert south in line with stone arch in wall.
23+340	Consider reducing the slope on The Grange Side Road approach to the intersection (school bus has slid onto Mississauga Road).
23+500	Lay some fiber optic cable for high speed internet service
23+700	Allow for natural gas
24+520	Existing rock cut or fill on both sides
24+575	Save tree
24+600 to 25+000	Will there be passing lanes on this uphill section?
24+900	Land for potential acquisition is valued. Concern over property – consider curb
24+960	Like shoulder for bike lanes
25+680	Please do not impact the fence

Table 1 - Summary of Comments Noted on Recommended Drawings	
Station	Comments noted
25+800	Please do not widen the road – no sidewalk
26+100	Speeding issue – add sign warning of pedestrians
26+260 to 26+430	Prefer to retain narrow shoulders and no sidewalks
26+400	Property boundaries not accurate to title – off by 7+ft
26+430	Sidewalks? Place to park if walkways? Emergency?
Bush Street	
12+120	Retain parking
12+110	Investigate for sidewalk passage
12+010	Culvert replaced 7-8 years ago
11+360 to 12+100	Some residents support sidewalks, others strongly oppose them
11+300	Speeding problem
11+100	Please don't damage all the new evergreen trees (planted fall 2013 along old fence line)
11+100	Like that the road is being shifted to the south where it used to be – lots of room!
11+000	Replace our mailbox if moved back to the south
10+990	Don't feel this (driveway) culvert is required
10+240	Trim vegetation
10+100	Remove dangerous cement curb (at south jog of WCB intersection)
10+060	Dangerous intersection (north jog of WCB)
Winston Churchill Boulevard	
44+960	Consider stop sign at 10 th Side Road as speed control measure
44+480	Too close to pond
44+300	Protect trees; provide buffer
43+740	Beautiful trees

Table 1 - Summary of Comments Noted on Recommended Drawings	
Station	Comments noted
43+400	Resident noted drainage low area
43+140	Stop sign would slow traffic down
41+870	Consider stop sign at 5 th Side Road as a means to reduce speed
41+320	Ditch requirement through wetland?
40+000	What will happen to this area (OBL south of WCB)? – potholes, rough grading
Between 10 th Side Road and The Grange Side Road	Likely turtle overwintering pond (comment provided to NRSI; not noted on plan)
Olde Base Line Road	
30+600	Driveway goes up. If profile lowered, driveway more leveled – who is responsible for grading?
30+640 to 30+820	Noise concern
30+820	Suggest a deeper rock cut to lower top of knob to avoid (reduce) filling the bottom
31+000 to 31+160	Stone wall under wooden fence
32+280	Can hear trucks
General comments	
WCB and OBL	Is it worth investment to have wide shoulders when there are few cyclists?

Appendix B

COMMENTS FROM FEEDBACK FORMS

Feedback Forms were provided to obtain written responses. The following responses were received. These are verbatim comments transcribed from the individual forms.

1. Recommended Designs for each of the roads

The study team presented the recommended designs for each of the roads and other alternatives that were considered. Do you agree with the recommended designs? What comments or suggestions do you have about the recommended designs for each of the roads that concern you?

The following were noted on the Feedback Forms. Each number represents a different response:

Table 2 – Comments on the Recommended Designs

Overall

1. Please try to do something to slow traffic. I live in the south area of the study area (below the gravel portion) and the speed of vehicles is very high and if it is all paved it will exponentially increase the volume as well.
2. Recommended designs look acceptable. Would like to know which of the study area roads will have mountable curbs. Thank you for your hard work and patience.
3. Should place signs for cyclists reminding them to ride single file and not five abreast.
4. I agree that the roads system identified should be upgraded to the current E.A. design for pavement structure, lane widths and for posted speed. The new site line design should accommodate 90 km per hour. It does not make economic sense to build to 60 – 70 km per hour and then in a few years have to reconstruct. The present traffic flow between 6 a.m. and 9 a.m. and 3:30 p.m. and 6:30 p.m. demands that Olde Base Line to Winston Churchill and Winston Churchill accommodate high volumes of commuter traffic. A growing population in the Erin Area adds to the urgency of the maximum upgrade for commuter traffic.
5. Adding bike lanes would be great. We are supportive of lower speed limits and greater enforcement to improve safety. Smoothing out the hills may only encourage greater speeds and defeat the original purpose.
6. I support the design as put forward save for my comments about robust structure above. If you build these roads correctly the first time you may never have to do so again for many years. While the design put forward is structurally suitable to carry all traffic, one should consider the important role that these roads will play as hundreds of thousands of new residents and jobs are added to the area south of the Study Area.

Olde Base Line Road:

1. There is no need for Olde Base Line Road to be a heavy truck route. Gravel trucks from future James Dick Quarry located north of Bush Street on Winston Churchill Boulevard can use Wellington (#52) over to Trafalgar Road as there is an existing truck route from the Erin pit. Keep Olde Base Line as a rural road.

Mississauga Road:

1. We agree with the recommendations that you have proposed and wish that you could start as soon as possible.
2. Preserve heritage stone wall at 23+000 Mississauga Road. Relocate culvert in road to match natural drainage through stone wall (at the stone arch) from there it goes into an old concrete culvert for 300 feet. Grade away from stone wall and match grade with mountable curb.

Winston Churchill Road:

1. I reviewed the Winston Churchill Design and I like it – especially the recommended design – the paved shoulder is an important safety improvement.
2. Excessive speed on Winston Churchill south of Olde Base Line Road needs to be dealt with. Need to discuss increased coverage with Police. Concerned about heavy truck activity and gravel trucks.
3. Consider stop signs on Winston Churchill Boulevard at 5 sideroad and 10 sideroad as a way to enforce speed limit.

Belfountain Village:

1. Absolutely no sidewalk or road widening inside the Belfountain Hamlet as the lanes are too close to the road already. Do not dump project rubble on Belfountain as you did with the thousand plus truckloads from the Forks of the Credit Road project. Please do nothing. In widening Mississauga Road entering Belfountain you will prevent flying squirrels from crossing the road.

2. Alternative Design Concepts and the Evaluation Process

The study team evaluated alternative design concepts based on evaluation criteria developed with public input. The preferred designs aim to accommodate the existing mix of traffic while maintaining the rolling terrain, retaining the rural character of the area, and minimizing impact to adjacent properties and landscapes. What comments do you have about how the alternative design concepts were evaluated?

The following were noted on the Feedback Forms. Each number represents a different response:

Table 3 – Comments on Alternative Design Concepts and Evaluation

1. Two comment forms indicated that they responded this in question one.
2. I would support the use of rock cuts along Olde Base Line Road to eliminate the requirement to place extensive fill in the low spots. Pending geotechnical confirmation, I think you will find that the hills are constituted of Bedrock Outcroppings that can easily be handled with vertical rock cuts. I would also support the use of traffic circles in this area.
3. A further individual re-emphasized their view that the speed limits should not be reduced.

3. Other Comments noted on the Feedback Forms

The following are additional comments noted on the Feedback Forms. Each number represents a different individual's comment.

1. We are strongly opposed to any proposed changes to the weight restrictions on any of these roads. We cannot have gravel trucks going through these roads due to safety, heritage and quality of life reasons. We spent eleven years fighting against the proposed James Dick quarry and do not want a repeat fight!
2. Pleased to hear that Winston Churchill Boulevard and Olde Base Line Road will not be designated as priority truck haul routes.
3. We wish you could expedite the addition of paved shoulders to improve safety for walking and biking on Mississauga Road.
4. The speed limit on Bush Street at Winston Churchill is 80 km per hour. Crossing it is a risk! The curve before and after limits vision. Heading north on Winston Churchill at Charleston Sideroad, the signs in the ditch block vision.
5. I like the pavement/asset management approach. Pavement shaving seems to be a fiscally responsible method of maintenance.
6. This study has a serious flaw in that it does not indicate a major upgrade in Winston Churchill from Olde Base Line south to Balinfad Road. This would best accommodate westbound traffic from the study area and west to the Guelph area and south to Highway No. 401 via Trafalgar Road.
7. It is important to balance the use of these roads as major roads with the desire of some residents to keep these roads exclusively as a private driveway to country estates. These roads can serve both functions as illustrated by extensive study by the Region. Improving these roads to accommodate all road users, cars, busses, trucks, farm machinery, bicycles and motorcycles is not only in the public interest, but also fulfills the Regional Official Plan policy to achieve a safe and efficient network for the movement of people and goods.

Appendix C

COMMENTS RECEIVED BY LETTER AND EMAIL

The following responses were received. These are verbatim comments transcribed from the individual letters and emails. Each number represents a different individual's letter or email.

1. Thanks for setting up the PIC #2. Please see my comments below. As you know I live and work in the Study Area residing at the major intersection of Mississauga Road and Olde Base Line. I support the use of these roads for all road users, including goods movement and truck uses. There was a handout that made the following statement: "This area is not designated for significant future growth, therefore the volumes will remain relatively constant. There is no need to construct the pavement to handle significant volumes of truck traffic." Unfortunately this statement is not accurate for the following reasons:
 - The area immediately south of this area is designated for some of the highest growth of any area in North America.
 - The materials (aggregates) needed to construct this high growth area come from immediately north of the Study Area.
 - The only reason that there is not more truck traffic on these roads today is the poor condition of the roads and the fact that they are posted as No Truck Routes.
 - The current lack of availability of these roads for truck traffic leads to congestion on other longer routes rather than promoting a safe and efficient Road Network as stated in the Peel Official Plan.
 - My company would save at least a million kilometers per year of travel if these roads were permitted to carry truck traffic today. These savings would increase over time as the land between Mississauga Road and WCB north of Bovaird Drive begins to develop as is currently being planned.
 - These roads are designated Major Roads in the Peel Official Plan.
 - These roads are designated Medium Capacity Arterial Roads in the Caledon Official Plan.
 - These roads are identified as a Primary Truck Route in the recent Peel Goods Movement Network Study.
 - Now is the appropriate time to construct this route with a robust Pavement Design to accommodate both the current users of this road, but also future traffic as is planned for this road network.

2. Comments received by email from individual representing Norval pit-STOP Community Organization
 - Impacts on the broader area should be considered, with a special consideration given to potential aggregate haul routes (gravel trucks). Even though not part of the study area, these road reconstructions could lead the way to paving a truck route through all of Winston Churchill, south of the study area.
 - Winston Churchill Blvd south of the study area, between Hwy #7 & Wanless Rd will never be a suitable gravel haul route, even with proposed "improvements". The natural

heritage, fish habitat, steep topography, structural deficiencies, hidden drive ways, residential and educational institutional uses on WCB north of Norval make it unsuitable for hauling aggregate.

- WCB and Hwy#7 in Norval is a permanent bottle neck for truck traffic. As N.W. Brampton builds out and connects with Norval, walk-able and bike-able urban transit must be taken seriously. Bikes and pedestrians are not compatible with gravel trucks. There are better locations for a truck route in Brampton (for example; Mississauga Rd., and the planned N-S Parkway between Heritage & Mississauga Roads as found in the Halton Peel Boundary Area Study. There is also the future extension of the 410, connecting to the future Parkway in NW Brampton, should be considered in how future truck routes should flow.
- If the roads in this EA are to be used as a gravel haul route, the aggregate producer should pay for all of the road reconstruction. Stop passing costs down to the taxpayer.

3. Email comments from a member of the CWG. While I appreciate you have may have met your obligations regarding public consultations under the confines of your EA scope, I believe you are missing an golden opportunity to participate with the local community in a little more depth, while there is still the opportunity before your design becomes more rigid and has less chance of being modified. Talking to my neighbours, there is clearly community concern over the design. And I do not mean detail design, but rather broad stroke design. For instance, your design for Bush Street shows rollover curbs with grated drainage, which may not be in keeping with community wishes, and without alternatives ever presented during the consultation process. While you note that “To accommodate sidewalks through this constrained area and beyond would mean significant property and environmental impacts”, I for one believe it would not hurt to give this further exploration.

We care very much about the rural look and feel of our community, and the impact that a potential ‘cookie-cutter’ Regional standard design configuration may impose. There is a wonderful opportunity for the Region to take advantage of an engaged local community in the Region’s provision to the community with a design that the community is proud of and that does not potentially change our rural character, perhaps drastically. Once you have upgraded the roads, they will be permanently changed; there will be no opportunity for a ‘do-over’ here. The Region’s principle drivers for the upgraded roads are safety and low long-term maintenance costs. As community stakeholders, we must add the equally important drivers of functionality and aesthetics. It would be sad and disheartening to our community if we missed the opportunity to have them incorporated at this, the pre-design stage.

I for one would like you and your team (or a subset as appropriate) to meet with us again, even if it is under the auspices of good public relations with the ratepayers (rather than the ‘official’ EA process). As I mentioned, this is an opportunity for further local input of

Belfountain Hamlet people on what is not just a Regional road (Old Main Street and Bush Street), but what is more appropriately framed as 'our local town' road.

4. There is nothing new here in what you are putting forward; I know about the pinch point, I also thought we talked about using the term multi-use trail or even paved shoulder, not a Brampton Mississauga type sidewalk. Please listen - I thought you heard; very frustrating. Heed comments and try again. Documenting questions and concerns is not good enough. What I have been talking about has been on the table since the beginning of this process. Based on the quality of this iteration, the design for the Hamlet must be approved by the BCO; 'consultation' is not yet over folks. Let's get this right, or at least better.
5. It's a relief that the EA has concerns that Base Line not be reconstructed as a heavy truck route. I think it took a lot of "wind out of the sails" for many residents. It's a beautiful country road...what Caledon is all about. I don't mind the "weekend Caledon wannabees"...it's the noisy tri-axle gravel trucks with pup trailers that concern me, the horses and cyclists.

When James Dick gets his ducks in order and develops his gravel pit off Winston Churchill, north of Belfountain, the haul route can be established along Wellington roads #52/124 , Trafalgar Rd. south to Highways #7/ 401/ 407, and the future truck bi-pass. This route is already in use from many pits, including the Erin pit at the 10th line. The Belfountain pit would exit onto the section of Winston Churchill that is at Wellington road; so why not have the entire route in Wellington / Halton? This is a shrewd approach; Peel gets the gravel taxes...fewer trucks on our roads, and Wellington/ Halton has to maintain their haul route!

6. The CCC has said it one way.....although that applies to Belfountain too, what I will add is more specific to Belfountain...the design and look of the road is still yet to be determined as well as the much desired community connectivity and pedestrian and cyclist safety concerns addressed. Intersection configuration at Mississauga Road and Olde Baseline and at Bush Street and Winston Churchill Boulevard, governing safety and traffic volume allocation, is also still in question.
7. Very many thanks for your prompt reply. Obviously the objectives of your Department have merit in theory. As a retired geologist who likes walking I appreciate what you are trying to do here. However, with respect to our little property and hereabouts I believe that there is no immediate need for any more road work for several reasons based on the dominant importance of road side ditches and relatively porous overburden, our climate, and width of roadways. The most important aspects of drainage and safety were dealt with satisfactorily about 10 years ago by ditch deepening and installment of traffic lights at our intersection. Regrettably walking hereabouts is necessarily on the hard top roads everywhere except in the village where curbs occur. In fact no one walks on our side of the

roads north and east of our fences because there is no need to as we are located outside the "paths" required to access school, playground, library and post box, for example.

8. We have owned our property on Olde Baseline Road for 40 years. We realize many of those involved in this study were not born or were not in the regional government 40 years ago. Let us briefly share what we have seen in that time, related only to roads – 40 years ago, Olde Baseline Road was unpaved and had a natural curve around a large rock outcropping just west of Mississauga Road. That curve had the effect of slowing traffic. The gravel road at that time had been constructed by local government presumably meeting the required road-building standards. The gravel had the effect of keeping traffic low because outsiders, especially weekend visitors, did not like getting dust on their cars.

The local government decided to 'improve the road', make it safer, by removing the rock outcropping to reduce accidents. I recall my father saying, "This will increase traffic, increase speed, and increase accidents." He was right. People could drive faster, so they did drive faster, never mind the speed limit.

The local government then, without our support, decided it should 'improve' the road, make it safer, by paving it. Presumably they paved it in conformity with their road-building standards. Again my father said, "This will increase traffic, increase speed, and increase accidents." He was right. The local government turned Olde Baseline Road into a shortcut for out of area commuters, and they are hell-bent to get to work as fast as they can.

We have the same number of homes on this section of Olde Baseline Road as we did 40 years ago. Our needs have not changed.

Now, local government has decided that 'improving sight lines', a euphemism for removing the rolling hills natural to the area's roads, will make the road safer. I echo my late father's words and say that it will increase traffic, increase speed and increase accidents. All of the 'improvements' made so far have had that effect. Each time an 'improvement' has been made in the name of safety; traffic, speed and accidents have increased. With drivers being able to 'see farther ahead', they will feel comfortable stepping up their speed even more.

The local government changed its road standards and now find Olde Baseline Road does not meet its standards – not surprising in light of the fact they changed the standard. The local government staff has decided to widen the road, pave the shoulders and exercise its 30 meter right of way, in the name of safety. Thousands of mature trees, including some 50-100 year old maples, will be cut. Fences, wood and stone, that have been in place for decades will need to be moved at the owner's expense. Already traffic consistently exceeds the speed limit. We invite anyone to stand with us at our road between 6:30 and 8:30am, and again in the evening, to observe traffic. Instead of 60 kph, it averages in excess of 80 kph. An unobtrusive speed camera (not one with the large sign showing the speed in lights) left in the area for a month would tell the story clearly. We even have

crazy drivers passing on that stretch to gain an extra 30 to 45 seconds in their commute. Widening the road, and paving the shoulders will lead to higher speeds and ever more serious accidents.

Consider how we feel. Noise, visual, and diesel/gasoline pollution have increased with these 'improvements'. The quiet enjoyment of our property has been taken from us, and will worsen with these 'improvements'. The safety of ourselves, our children and grandchildren has deteriorated. These 'improvements' have consistently shown that they increase danger, not safety, as measured by the very statistics that staff uses to propose yet further improvements. No-one seriously thinks that making the roads more amenable to higher speeds and more traffic will make them safer. The value of our properties will decrease. This is essentially a 'taking' of our property without any compensation. Even a 30 meter right of way was imposed on the land-owners, never purchased.

We live in an area where we need three different approvals just to expand a deck on our house – the escarpment, green belt and regional government interests must be met. We are hesitant to clear a tree on our property for fear of disturbing the well-studied salamander. We have had government employees studying wetlands on our property. Yet, along comes the local government to excavate the natural rolling hills, to cut down thousands of trees, to move or blast natural rock of the escarpment, to force us to move our rock and wood fences, all in the name of 'improvements' that we do not want, have never asked for and with which we do not concur.

We fought for a decade to defeat the plans of our recently-moved-in neighbor to the west, Mr. Dick, to tear a massive hole in the escarpment. Now, with the support of him and his lobbyists, the roads coming past his recently acquired 'farm' are being classified as a 'heavy truck haul route'. When he revives his application for his gravel pit in the next 5-10 years, all the arguments against the traffic issues will be moot.

Outside users of our section of Olde Baseline Road have never observed the speed limits or even the signage. When trucks were prohibited, they still used the road. When they were limited by time of day, they did not adhere to the limits. Cars and trucks do not observe the speed limits. Local government has never enforced the limits. We do not think the government even has the resources to enforce the limits. Perhaps installing speed bumps similar to those used by the 'red hills' every quarter mile would regulate speed. We suspect that drivers would petition the government to remove them because it impedes their commute.

In any event, while we expect that our government has carefully used the regulations and laws that it proscribes itself, and is following this process to simply attain what it has, from the outset, designed to have, we desire to have our objection together with the rationale behind it, put on record.

**Community Working Group
Meeting Notes**



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File: 2.2
 Project # 6776

Orientation Session Notes

Project: Class EA for Regional Road Corridors bound by Bush Street, Mississauga Road, Olde Base Line Road and Winston Churchill Boulevard

Subject: **Community Working Group Meeting #1 – Orientation Session**

Meeting Date & Time: Tuesday, October 23, 2012, 6:30 p.m.

Location: Belfountain Community Centre
17204 Main Street, Belfountain

Prepared by: Nathalie Baudais

Attendees:
 Glenn McMichael – CWG member
 David Jobe – CWG member
 Bryan Bibby Smith – CWG member
 Carolin Spanetta – CWG representative for Sergio Panetta
 Steve Ganesh – Region of Peel
 Hitesh Topiwala – Region of Peel
 Asha Saddi – Region of Peel
 Sue Cumming – Facilitator, Cumming and Company
 Stephen Keen – HDR, Consultants
 Nathalie Baudais – HDR, Consultants
 Mrs. Jobe - Observer

Regrets: Sergio Panetta – CWG member
Sarah Morgenstern – CWG member

Distribution: All Attendees

	Item
1.0	Welcome, Introductions, Overview of Agenda
1.1	Roundtable introductions were made. CWG members commented on why they wanted to be part of the CWG together with preliminary observations/comments about transportation and community issues.

2.0	Opening Remarks
2.1	Steve Ganesh introduced the project and explained that the Region of Peel was looking to work with the community to preserve and maintain the community character while providing a safe road network.
2.2	Steve Ganesh provided a brief overview of some of the Region of Peel initiatives including the Active Transportation Plan which encourages alternative modes of travel, such as walking and cycling, and, the Road Characterization Study, which considers the design elements within the road right of way that reflect the character of communities adjacent to the road.
2.3	Steve Ganesh thanked the members for contributing their time and thoughts throughout the process.
3.0	Mandate, Roles, Responsibilities and Meeting Organization
3.1	<p>Sue Cumming reviewed the Terms of Reference with the CWG, confirming the non-voting stature of the committee and its important role in providing input and advice on study directions. She also provided the committee with Ground Rules for the facilitator's commitment to the CWG members:</p> <ul style="list-style-type: none"> • Treats everyone equally • Helps everyone feel comfortable participating • Ensures that everyone's voice is heard • Sets a tone of respect for different viewpoints • Stays neutral • Keeps the discussion on topic, organized and focused • Provides guidance and support for the CWG <p>She discussed expectations of the CWG members, seeking their commitment to the following:</p> <ul style="list-style-type: none"> • Everyone is equal • All members need to feel comfortable to participate • We don't have to agree, but will respect each other's viewpoints • Bring your ideas to the meetings - not your agenda • If you don't understand, just ask what is meant • One speaker at a time, do not interrupt, be respectful of time • Ensure opinions outside of the CWG meetings represent personal viewpoints versus those of the Group <p>Sue Cumming conveyed that significant agenda time at all meetings would be devoted to roundtable discussion. Presentations by the Project Team when warranted would be short, and, where feasible, information would be sent out in advance of meetings with the meeting agenda.</p> <p>An important role of the CWG was to liaise with members in the community. She</p>

	advised that in doing so, should members communicate their opinions, they need to do so in a manner that is reflective of their own views and not speak on behalf of other CWG members.
3.2	All correspondence between CWG members and the Project Team should be forwarded to Asha Saddi and copied to all members. Meeting notes will be taken at each future meeting to record the ideas and key messages. These will be provided in draft for the CWG's confirmation with a request for any changes within two weeks of distribution after which time the notes would be posted on the Region of Peel web site.
3.3	If a CWG member was not able to attend, an alternate could attend on their behalf and the name of the individual should be provided in advance to Asha Saddi.
3.4	The timing for future CWG meetings was agreed to be appropriate with a 6:30 p.m. start. A light supper will continue to be provided at future meetings.
4.0	Project Background
4.1	Hitesh Topiwala and Stephen Keen reviewed the PowerPoint presentation with the CWG.
4.2	An overview of the Environmental Assessment (EA) process and Schedule "C" projects was provided. It was explained that the creation of the Community Working Group and the scheduled Public Open House for October 30, 2012 were not mandated by the EA process. The Region of Peel had included these as an opportunity for community engagement in this project.
4.3	It was noted that no changes had been made to the drainage system since the 2009 study was initiated. It was explained that the Region of Peel could not proceed with any drainage recommendations until the EA study was completed and the necessary permit approvals were in place.
4.4	It was noted that the Rockfort Quarry application delayed the study expansion.
4.5	Winston Churchill Boulevard is part of the shared jurisdiction between Wellington County and Region of Peel. Wellington County is a participant through the Technical Advisory Committee.
5.0	Group discussion on transportation issues, the needs and vision for the roads
5.1	<i>Questions/comments about why the study is being undertaken (again)</i> Why is the study being undertaken? It was noted that the study had started twice before and the community will want to know why it was being started again. What will the outcome be? Discussion ensued on who makes decisions on the study,

	<p>the role of senior Regional staff and Council, and, the impact of the community is voice in influencing outcomes.</p>
5.2	<p><i>Important to convey to the community - what the study is not about:</i></p> <ul style="list-style-type: none"> ▪ Not about the widening of roads ▪ Not about improvements that do not respect the natural and rural character of the area and the roads ▪ Not about safety improvements at any cost
5.3	<p><i>Overriding theme of balance and respect for rural and village character</i></p> <ul style="list-style-type: none"> ▪ Opposed to having an urbanized treatment of roads in the Study Area. ▪ Want to see a “made in the community” solution respecting the rural character of the area and the roads. ▪ Do not want to see Mississauga Road and Winston Churchill urbanized. Do not want to see the roads become a “King Street”.
5.4	<p><i>Must have focus on improvements for pedestrian and cycling that enhance the community character</i></p> <ul style="list-style-type: none"> ▪ It was noted that 100% of school children (180) are bussed to the Belfountain School. This is determined by the District School Board’s policy respecting road classification. A Grade 4 class had written to the Mayor requesting bike lanes and sidewalks be built around the school and in the community. The Mayor had attended the school to talk with the school children. ▪ Would like to see safe pedestrian and cycling corridors to/from the school. Would like to improve safety for school children walking along Old Base Line Road to the Conservation Area for field trips where there was no wide shoulder or sidewalks. ▪ A question was raised about whether there was room on the shoulders for cycling infrastructure, given the soft shoulders, narrow space and steep grades. A reference was also made about concerns of driving in to a pond on an owner’s property. ▪ In some areas it may be practical to separate cars from people while in other areas the topography would preclude this and other improvements need to be explored. Rumble strips along the shoulder could be considered. ▪ It was acknowledged that safety considerations were a challenge given the existing grades. The issue of guard rails was discussed. ▪ Unsafe cycling practices were occurring on Old Base Line Road with cyclists travelling in the center of the road. ▪ Vehicle sight lines were an issue with cycling safety. CWG members were not supportive of flattening out the road. They felt focus should be on cycling behaviour. ▪ Members supported a sidewalk or pathway being built through the Village from Caledon Mountain Drive to Bush Street and to Belfountain School. It was noted that presently there was no safe way to walk to the Village.

5.5	<p><i>Not supportive of road improvements that would result in an increase in truck traffic</i></p> <ul style="list-style-type: none"> ▪ The Community was concerned about the potential for growth in truck traffic. Constraints on existing roads do not allow truck traffic. The CWG felt that road improvements should not accommodate truck traffic.
5.6	<p><i>Need to balance improvements so that traffic is not shifted from one road to another</i></p> <ul style="list-style-type: none"> ▪ It was acknowledged that there was more traffic. There were more people travelling to the stores in the Belfountain Village and Erin which brought in more commuter traffic. ▪ People will travel the path of least resistance. Members did not want to see one route improved over another in order to redirect traffic. Achieving a balance was important to the community members. ▪ Speeding was an issue along Winston Churchill despite the sight lines. The road improvements along Winston Churchill have resulted in more racing and speeding in the area. Noise is also an issue for those whose homes are closer to the road. There are marsh and pond areas near the edge of the road along Winston Churchill. ▪ The community members would like to see traffic volume and speed addressed, especially commuter traffic going south along Winston Churchill in the morning. ▪ It is important to working group members not to redirect the problem from one route to another. There was a lot of traffic moving through Belfountain Village. New planters and gardens were put in to help reduce cut through traffic. ▪ Community members underscored the importance of an overall solution that did not impact the Village, Winston Churchill or other roads in area. ▪ Volume was also a factor to be considered. There was a need to determine how to manage volume from motorcyclists, tourists and commuters in a way that maintained the character of the roads and the area.
5.7	<p><i>Ideas about community character</i></p> <ul style="list-style-type: none"> ▪ There was a strong environmental and natural character in the area which was why many chose to live in the area. All agreed that this was extremely important and that the outcomes of this study could not be safety at any cost. Improvements need to respect the environment. ▪ People in the community were very passionate about the natural and rural character of the area and supported preserving / enhancing the area. This would be a strong factor in assessing the benefits of any safety improvements that came forward in this Study. ▪ Not looking for a lot of change. Many in the community do not support the building of sidewalks and curbs along the roads. There is an interest in traffic calming / slowing people down. Roads as they are with some potholes are quite acceptable. Community members were not looking for a perfect road. They did not want to see a highway type of road condition in the area. People like Mississauga Road as it is.
5.8	<p><i>Other information communicated to the Project Team</i></p> <ul style="list-style-type: none"> ▪ Members have previously requested collision data and there is not much data

	<p>available.</p> <ul style="list-style-type: none"> ▪ It was noted that a proposal for 72 lots off Woodland Court, from Mississauga Road to Bush Street, was being considered again with a potential application for the preparation of a draft plan. The community members wanted to ensure the study team was aware of the pending application. It was noted that this application had been debated for at least 30 years. The Project Consultant advised that a development in this area would not justify road widening. ▪ Water issues in Belfountain. ▪ Winter ponding was not being experienced on the roadways.
5.9	<p><i>Suggestions for outreach</i></p> <ul style="list-style-type: none"> ▪ The community members were supportive of expanding outreach through networks. Notices could be forwarded to CWG members who would distribute them through the Belfountain Village Association, Schools and other networks. ▪ The community members confirmed that notices posted in the community hall, coffee shop and community space would be effective. Some community members felt that only a few residents may see an advertisement in the newspapers and supported a mail out (Canada Post mail drop) along the streets in the Study Area, including River Road and Caledon Mountain Drive.
6.0	<p>October 30 Open House Outreach</p>
6.1	<p><i>Input for Open House</i></p> <ul style="list-style-type: none"> ▪ A start time of 6:30 p.m. was felt to be early. A presentation start time after 7:00 p.m. was good and would provide residents with an informal opportunity to learn about the study and discuss what was important to them. ▪ Draft Workbook was good for use at the Open House. ▪ It was felt that a representative from the Credit Valley Conservation Authority should also be present at the Open House to answer any appropriate questions. ▪ The community members wanted to see key messages communicated: <ul style="list-style-type: none"> ○ The study was not about widening roads. ○ It was important to learn about the community character to ensure the right balance is achieved between making the road improvements and enhancing the community character. ○ Focus on engagement with community through CWG, Open House and other means.
7.0	<p>Next Steps</p>
7.1	<p>Open House – October 30, 2012 Technical Studies Community Working Group Meeting – Winter 2013 Public Information Centre – Winter 2013</p>
8.0	<p>Closing Remarks</p>

8.1 Steve Ganesh reiterated that the Region of Peel would like to actively engage the community and if the CWG members knew of other organizations (e.g. ski clubs, school students, etc.) that would appreciate additional outreach, to please advise the study team.

Meeting Notes

- Project:** Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road (MOBOW) EA
- Subject:** **Community Working Group (CWG) Meeting #2**
- Meeting Date & Time:** Thursday, April 4, 2013, 6:30 p.m.
- Location:** Belfountain Community Centre
17204 Main Street, Belfountain
- Prepared by:** Veronica Restrepo, HDR (Consultants)
Sue Cumming, Facilitator
- Attendees:** Steve Goyeche – CWG member
Rachel Ingram – CWG member
David Jobe – CWG member
Gord McArthur – CWG member
Glenn McMichael – CWG member
Neil Morris – CWG member
Sergio Panetta – CWG member
Ward Pitfield – CWG member
Penny Richardson – CWG member
Bryan Bibby Smith – CWG member
Greg Sweetnam – CWG member
Steve Ganesh – Region of Peel
Gino Dela Cruz – Region of Peel
Asha Saddi – Region of Peel
Sue Cumming – Facilitator, Cumming and Company
Tyrone Gan – HDR, Consultants
Veronica Restrepo – HDR, Consultants
Richard Paterak – Councillor, Town of Caledon Ward 1
Allan Thompson – Councillor, Town of Caledon Ward 2
Lori-Ann Thomsen – Region of Peel (Observer)
- Regrets:** Marion Miles – CWG member
Sarah Morgenstern – CWG member
Heather Wilkinson – CWG member
- Distribution:** All Attendees
-

	Item
1.0	Welcome, Introductions and Purpose of Meeting
1.1	<p>Sue Cumming introduced the new Project Managers for the Region of Peel and the Consultant and explained the purpose of the meeting. Roundtable introductions were made.</p> <p>Sue Cumming welcomed the new members to the CWG and asked everyone to provide a short introduction about what was important to them about being part of the CWG. CWG members commented on why they wanted to be part of the CWG together with preliminary observations/comments about transportation and community issues. The following points were noted:</p> <ul style="list-style-type: none"> • To convey the importance of respecting and maintaining the unique community and rural character. Road character is important to local context. • To provide a voice for the children in the area. • To better understand how the study is being carried out. • To address the needs of all road users including farm vehicles, sanders, snow ploughs and tractors in a safe way while maintaining the character of the community. • A CWG member discussed issues concerning traffic and truck movements including sand and gravel business north of the area and safe goods movement along these roads. The member wanted to ensure that efficient goods movement and planned function of the roads are taken into account given the large urban area to the south while preserving the unique community feel. Several other CWG members disagreed, citing that there are not a lot of trucks and trucks should not be able to use these roads. • To protect the area with conservation of its history, ecology and character. The development in Erin was of concern. Some CWG members did not want to see the roads turned into transportation corridors. • To maintain the Belfountain community as is. Safety in the Village was noted to be a local issue. • To preserve the environment that exists. Sustainability should be an important focus of this study. A CWG member noted living in Inglewood and traveling to the area by car, bicycle and horseback.

2.0	Opening Remarks
2.1	Steve Ganesh provided a brief overview of the project. He explained the role of the CWG and thanked the members for their time and input throughout the process.
2.2	Steve Ganesh explained the role of the Consultant. Although the Region of Peel conducts some studies in-house, the Consultant's expertise in context sensitive solutions was an asset to the uniqueness of this project.
3.0	Roles, Responsibilities and Meeting Organization
3.1	<p>Sue Cumming reviewed roles and responsibilities of CWG members, Region of Peel and the Consultant team.</p> <p>Sue Cumming provided the Committee with Ground Rules for the facilitator's commitment to the CWG members:</p> <ul style="list-style-type: none"> • Treats everyone equally • Helps everyone feel comfortable participating • Ensures that everyone's voice is heard • Sets a tone of respect for different viewpoints • Stays neutral • Keeps the discussion on topic, organized and focused • Provides guidance and support for the CWG <p>She discussed expectations of the CWG members, seeking their commitment to the following:</p> <ul style="list-style-type: none"> • Everyone is equal • All members need to feel comfortable to participate • We don't have to agree, but will respect each other's viewpoints • If you don't understand, just ask what is meant • One speaker at a time, do not interrupt, be respectful of time
4.0	Project Update
4.1	<p>Tyrone Gan provided a project update, including what had been heard through public consultation.</p> <p>CWG members agreed that the rural character and scenic quality of the area needs to be maintained. The hilly nature of the road should be preserved including the existing vertical alignment and cross-section. CWG members added that this should be balanced with providing a safe road network.</p>
5.0	Summary of Technical Work
5.1	<p>The Consultant presented the technical work completed so far to the CWG members.</p> <p><i>Traffic Trends</i></p> <p>Although over the last 15 years, traffic has increased, the overall traffic has been relatively stable. CWG members noted that trends in traffic shifting from Mississauga</p>

Road to Winston Churchill Boulevard and from Bush Street to Olde Base Line Road in 2008-2009 coincide with road closures due to reconstruction. It was also noted that origins and destinations were not traced so cars may not have been counted on both graphs as many drivers use a combination of Mississauga Road and Bush Street, or, Winston Churchill Boulevard and Olde Base Line Road.

5.2 *Road and Intersection Characteristics*

A summary of road and intersection characteristics was presented, including the existing features and geometry in the study area.

Visibility issues and sightline deficiencies were identified throughout the study area, based on Transportation Association of Canada (TAC) standards. CWG members asked if residents had been asked if this is a concern to them as the deficiencies did not seem to be a problem to some CWG members. It was noted that many of these were historical driveways. The team reiterated that although preserving the rural character of the area is one of the guiding principles for any solution that is developed, public safety will also be a priority and standards will need to be followed.

It was noted that better sightlines along Winston Churchill Boulevard were due to the reconstruction that took place between Bush Street and The Grange in 1969.

It was suggested to consider alternative options to address sightline issues, including roundabouts, rumble strips, etc. to reduce speeds, rather than changing the road profile. The Project Team is considering all these options, which were discussed later in the presentation.

The detailed design will start for Winston Churchill from Terra Cotta to Olde Base Line, including the intersection of Winston Churchill and Olde Base Line.

Construction for this project is expected to start in 2015-2016 and some property acquisition might be required. There are no parallel studies identified in the Region of Peel's Long Range Transportation Plan nor was there a future plan for a corridor in the area.

The Project Team explained that although the roads serve residents in the study area, they also provide connections to major destinations outside the study area, connecting to workplaces that support the Region of Peel's economy. Some CWG members understood that these are arterial roads for commuter traffic, but did not want to see the roads used by trucks.

A CWG member noted that there were gaps in north-south infrastructure and asked why the segment of Mississauga Road between Olde Base Line and King Street was not considered as part of this study. The Project Team reiterated there was no need for a change in the number of travel lanes and explained the difference between widening the road for more lanes (which is not being considered) versus increasing the shoulders to allow for safe movement.

5.3 *Collisions*

A summary of collisions in the study area between 2006 and 2010 was presented. The

	<p>data was provided by the OPP.</p> <p>Discussion ensued about whether driver error was considered in the collision analysis and if the collisions occurred on weekends when there were tourists and unfamiliar drivers.</p> <p>Trends showed that wildlife fatalities seemed to be increasing and it was noted that if the wildlife fatalities were removed then the collision data would be similar to that of any subdivision in Brampton.</p> <p>There was speeding on Olde Base Line Road notwithstanding the topography of the road.</p> <p>It was also noted that a cyclist fatality occurred on Mississauga Road south of The Grange in July 2012.</p> <p>The Performance Safety Index (PSI) ranks for the study area were discussed. Lower numbers represent road segments or intersections requiring the most improvements. None of the segments or intersections in the study area rank in the top 100.</p>
5.4	<p><i>Driveways</i></p> <p>A driveway analysis was presented, summarizing the number of driveways that meet the minimum TAC standards. It was suggested to consider lowering (and enforcing) posted speeds as a means of improving sightlines at driveways and vegetation could also be trimmed to improve sightlines.</p>
5.5	<p><i>Standards</i></p> <p>The process to identify design solutions (the needs assessment process) is driven by the Region of Peel’s mandate for standards on arterial roads. The Region of Peel follows, among other guidelines, the national TAC standards. Some CWG members asked to review the standards that were being followed in the development of options for this study. Steve Ganesh explained that the Project Team was not at that stage yet and this might lead to pre-determining an outcome to the problem statement before the public has had a chance to comment. He suggested HDR continue to follow the EA process and develop preliminary cross-sections, at which point the rationale for standards and the cross-section development process can be discussed.</p>
6.0	Draft Problem Statement
6.1	<p>The team presented the draft problem statement, which includes:</p> <ul style="list-style-type: none"> • Improving pavement conditions and drainage • Improving sightlines • Meeting the needs of all users • Enhancing safety for all road users • Reducing motor vehicle accidents involving wildlife and improving safety of wildlife

6.2	<p>The Community Working Group suggested that the problem statement be presented before the needs assessment and technical work summary at PIC #1 on May 9.</p> <p>Other suggestions include:</p> <ul style="list-style-type: none"> • Define the rural character • Revise the wording to state the problems clearly • Meeting the needs of all users should be listed under the guiding principles • Drainage to be discussed in the summary of technical work. A CWG member noted that having driven the roads he did not see any ponding of water or frost heaves. Another member advised that drainage had been covered in the first CWG meeting and should be added here to provide context for discussion and input.
<p>7.0 Guiding Principle for Solutions</p>	
7.1	<p>The draft guiding principles were presented. These included:</p> <ul style="list-style-type: none"> • Maintain the rural character and countryside scenic quality • Preserve historic fences and heritage / cultural / archaeological features • Preserve / enhance the natural environment • Balance interests of all road users • Provide a context sensitive design • Enhance local tourism and economic development of the area
7.2	<p>Suggestions from CWG members include:</p> <ul style="list-style-type: none"> • Add “protecting the Niagara Escarpment” • Remove wildlife (specifically deer) from road users category • Add a statement about these roads being treated differently from other Regional roads • Add specifics about the character of the area • Maintain the function of the road as existing • Provide solutions that can safely and efficiently move vehicles, including trucks
<p>8.0 Preliminary Alternative Solutions</p>	
8.1	<p>The preliminary alternative solutions were presented. These include operational and physical improvements.</p> <p>Operational improvement options for consideration included:</p> <ul style="list-style-type: none"> • Pavement markings –adding or repainting such as edge of travel lane and

SLOW markings

- Improving traffic signage e.g. larger street name signs, clearing sign clutter
- Adding animal crossing warning signs where needed
- Lowering speed limits in some locations
- Removing overgrown vegetation at Winston Churchill / Old Base Line intersection to improve visibility
- Bike racks at local businesses
- Landscaping to shelter pedestrians from vehicular traffic
- Seasonal communications and education regarding deer activity
- Enforcement

Physical improvement options for consideration included:

- Road rehabilitation or reconstruction where pavement condition is poor
- Widening shoulders in some locations
- Partially paving shoulders in some locations
- Sidewalks in parts of the Village of Belfountain
- Countermeasures for roadside hazards – removing hazards, installing guiderails
- Roundabouts at Mississauga Road / Olde Base Line Road and Winston Churchill Boulevard at Bush and Olde Base Line Road
- Potential changes in road profile to address sightline deficiencies
- Designated wildlife crossing areas

8.2 CWG members agreed that a good range of options is being considered.

Suggestions include:

- Removing overgrown vegetation – this should be considered at intersections and along the road side, where required and feasible (not just at the Winston Churchill Boulevard / Old Base Line Road intersection)
- Only consider options that respect the rural character and countryside scenic quality of the area – no cement walls
- Remove “potential changes to road profile”
- Consider adding “narrowing lanes” under physical improvements, as a traffic calming measure
- Consider internet installation through re-pavement – there isn’t a sufficient population to warrant this, and it is also part of living in the country
- Roundabouts should be designed with school buses, agricultural vehicles and trucks in mind
- Consider adding bike lanes with signage
- Consider treating Winston Churchill Boulevard/Olde Base Line Road different from Mississauga Road / Bush Street
- Consider signals (flashing lights when vehicles are entering the road) as mitigation measures for deficient sightlines
- Consider the addition of maple trees, for example, which would enhance the rural character of the area

There was concern that road rehabilitation/reconstruction, and widening shoulders and partially paving shoulders will result in increased traffic speeds.

One CWG member asked about historical records for road maintenance and improvements for the study area as well as the cost to reconstruct versus re-paving a road.

It was suggested that since cyclists like to bike in tandem, providing a paved shoulder would not work. Cyclists should be consulted on this issue, to see if they would use a paved shoulder. Steve Ganesh mentioned that the Caledon cycling group had been in contact with the Region of Peel.

Most CWG members expressed concerns about physical improvements and believed that these could drastically change the road profile and compromise the rural character of the area. Steve Ganesh suggested conducting a tour of nearby areas where road improvements have not taken away from the rural character of the area. This tour could take place after PIC #1. But before cross-sections are developed. Alternatively, pictures of before/after conditions can be provided for the CWG to review and discuss.

9.0	Preliminary Evaluation Criteria
9.1	<p>The preliminary Evaluation Criteria was presented. It included:</p> <p>Natural Environment</p> <ul style="list-style-type: none"> • Vegetation and wildlife habitat • Aquatic habitat • Terrestrial habitat • Species at Risk • Wildlife safety • Natural hazards <p>Socio-Economic Environment</p> <ul style="list-style-type: none"> • Residential Properties • Businesses • Archaeological Resources • Built and Cultural Heritage Resources • Air, noise, vibration impacts <p>Transportation</p> <ul style="list-style-type: none"> • Geometric Alignment • Traffic Operations • Driveway Operations • Accommodation of all road users • Safety • Stormwater quality and quantity <p>Capital Costs</p> <ul style="list-style-type: none"> • Property Acquisition
9.2	<p>CWG members asked where property acquisition would be required. Utility relocation, culverts and slopes could potentially result in property acquisition.</p> <p>It was explained that alternatives to property acquisition include easements. Details in the design could address/minimize potential property acquisition and expropriation would only be a worst-case scenario.</p>
10.0	Public Information Centre #1
10.1	<p>The PIC #1 is scheduled for Thursday, May 9 at the Belfountain Public School.</p> <p>The format will be similar to the October 30 Open House, with arrival and Open</p>

	<p>House from 6:30 p.m. – 7:00 p.m. and a presentation and discussion from 7:00 p.m. – 9:00 p.m. Workbooks will be provided for comments.</p> <p>CWG members noted that having stations around the room to provide comments, as at the October Open House was good and this could be done again at the PIC.</p>
11.0	Next Steps
11.1	<p>Public Information Centre #1 – May 9, 2013</p> <p>Completion of Technical Studies</p> <p>Development of Alternative Designs</p> <p>Community Working Group Meeting – Fall 2013/Winter 2013-2014</p> <p>Public Information Centre #2 – Fall 2013/Winter 2013-2014</p>
12.0	Closing Remarks
12.1	<p>Steve Ganesh reiterated that the Region of Peel would like to actively engage the community and once again thanked the CWG members for their participation and encouraged them to attend the upcoming PIC.</p>

Meeting Notes

- Project:** Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road EA
- Subject:** **Community Working Group (CWG) Meeting #3**
- Meeting Date & Time:** Wednesday, October 16, 2013, 6:30 p.m.
- Location:** Belfountain Community Centre
17204 Main Street, Belfountain
- Prepared by:** Veronica Restrepo and Sue Cumming
- Attendees:** Steve Goyeche – CWG Member
Rachel Ingram – CWG Member
David Jobe – CWG member
Gord McArthur – CWG Member
Glenn McMichael – CWG member
Neil Morris – CWG Member
Sergio Panetta – CWG member (represented by Caroline Panetta at the beginning of the meeting)
Ward Pitfield – CWG Member
Penny Richardson – CWG Member
Greg Sweetnam – CWG Member
Allan Thompson – Regional Councillor, Town of Caledon Ward 2
Doug Beffort – Area Councillor, Town of Caledon Ward 1
Lori-Ann Thomsen – Region of Peel (Observer)
Steve Ganesh – Region of Peel
Gino Dela Cruz – Region of Peel
Sue Cumming – Facilitator, Cumming and Company
Tyrone Gan – HDR, Consultants
Veronica Restrepo – HDR, Consultants
- Regrets:** Sarah Morgenstern – CWG member
Bryan Bibby Smith – CWG Member
Heather Wilkinson – CWG Member
Richard Paterak – Regional Councillor, Town of Caledon Ward 1
Asha Saddi – Region of Peel
- Distribution:** All
-
-

	Item
1.0	Welcome, Introductions and Purpose of Meeting
1.1	<p>Sue Cumming introduced the project team from the Region of Peel and the Consultant, and explained the purpose of the meeting. Brief roundtable introductions were made by all CWG members and observers.</p> <p>It was noted that all CWG members received the draft presentation ahead of the meeting, and although there was a lot of background information in the slides, the focus of the meeting would be the recommended designs. The project team plans to present the road profile, cross-sections and plans for each of the roads. The goal of the meeting is to receive feedback on the recommended designs along with alternatives considered.</p> <p>This information will be reviewed with the public at the Public Information Centre (PIC) scheduled for November 20, 2013. Sue Cumming advised that the format of this meeting has been designed to allow maximum opportunity for residents and stakeholders to look at large plans and understand the recommended design for each area. It will be an Open House format allowing people to drop in anytime between 4:30 p.m. - 8:30 p.m. to view the plans and discuss ideas and concerns with the project team. This is an excellent format for providing the level of detailed information that is being presented and ensuring that residents and stakeholders can have one on one review of the information. CWG members were encouraged to provide advice on any of the materials and large plans being reviewed today and other information that would be helpful to have available for the PIC.</p> <p>It was further noted that this would be the last CWG meeting for the study and the upcoming Public Information Centre (PIC) is the last scheduled.</p>
2.0	Truck Routes
2.1	<p>A continuing concern of CWG members is the implication of the Strategic Goods Movement Networks Study and use of area roads by trucks. Before presenting the recommended designs, truck routes were discussed. CWG members asked if the proposed design recommendations being presented today would bring the roads up to “truck route standards”. The project team clarified that the designs would bring the roads up to Regional standards and provide safe roads for all modes. It was reiterated that the Strategic Goods Movement Network Study proposed roads as potential future routes, and would require further investigation before any given road can be designated as a truck route and its truck restrictions are modified.</p>
3.0	Project Recap, Alternative Solutions, Evaluation Results and What is Being Recommended
3.1	<p>Key design principles to guide the design options were presented. The preferred designs aim to accommodate the existing mix of traffic while maintaining the rolling terrain, retaining the rural character of the area, and minimizing impact to adjacent properties and landscapes. The project team re-emphasized the importance of the</p>

	<p>design principles that have been developed with public input and that these contributed to the how the recommended designs were developed.</p>
<p>3.2</p>	<p>Speed Reductions proposed throughout the study area. Speed reductions are proposed throughout the study area to address deficient stopping sight distance and driveway sightlines while minimizing changes to the existing vertical profiles. Proposed posted speeds are as follows (design speeds are 10km/h higher than posted speeds):</p> <ul style="list-style-type: none"> • Village: keep at 40km/h with 50km/h transition towards Bush Street and Old Main Street • Mississauga Road: keep at 60km/h between Caledon Mountain Drive and The Grange, and lower from 70km/h to 60km h from The Grange to Olde Base Line Road • Bush Street: lower from 80km/h to 70km/h between Winston Churchill Boulevard and just west of Shaws Creek • Winston Churchill Boulevard: keep at 60km/h between Bush Street and Sideroad 10; lower from 70km/h to 60km/h from Sideroad 10 to Olde Base Line Road (also consistent with 60km/h posted speed for Winston Churchill Boulevard south of Olde Base Line Road as per approved Environmental Study Report) • Olde Base Line Road: lower from 60km/h to 50km/h from Winston Churchill Boulevard to Mississauga Road <p>These speed reductions would improve safety for all road users, including motorists, cyclists and pedestrians, and are expected to reduce the number and severity of collisions involving wildlife. In general, CWG members are supportive of the proposed speed reductions. Several members commented that although the speed reduction is good, enforcement will continue to be an issue and wondered whether stop signs at intersections are being considered. The project team advised that the traffic volumes do not warrant stop signs. CWG members would like to see more enforcement on the roads.</p> <p>It was suggested that the public information materials should show the changes in travel times associated with the proposed reduction of speed limits for different segments of the study area.</p>
<p>4.0</p>	<p>Recommended Designs for each of the roads</p> <p>The project team presented the recommended designs for each of the roads and other alternatives that were considered. The group discussed potential modifications to the preferred options.</p>
<p>4.1</p>	<p>Bush Street (Winston Churchill Boulevard to Shaws Creek)</p>
<p>4.1.1</p>	<p><u>Profile</u></p> <p>In general, there are no profile changes proposed along this segment of the study area.</p>

	A CWG member asked what this meant with respect to resurfacing. The project team advised that full reconstruction is recommended.
4.1.2	<p><u>Cross-section</u></p> <p>Cross-section options for Bush Street west of Shaws Creek were presented. In general, the wider right-of-way (ROW) at this location allows for a rural option. The preferred option is the 11.4m platform rural option, with two 3.5m wide lanes, 1.7m paved shoulders to accommodate active transportation and adequate ditches on both sides. The buffer between the travel lane and paved shoulder would be pavement line markings, and the shoulders would be signed as a cycling route through the use of signed posts (pavement would not be marked specifically for cyclists). Pavement markings would also be used at the edge of the shoulder. Various CWG opinions were shared about views on the effectiveness for cyclists of buffers vs. white strips. It was noted by some members that a buffer would be nice to have but not essential. A further question was noted about whether the road width could be reduced to 3.3 metres. The project team advised that the TAC standards are 3.5m. for a posted speed of 60k/hr.</p>
4.1.3	<p><u>Plan</u></p> <p>A plan showing the extent of the grading impacts corresponding to the recommended design was presented. In general, all impacts are within the existing ROW, and localized improvements will be considered at the locations where impacts extend beyond the existing ROW.</p>
4.2	Winston Churchill Boulevard (Bush Street to Olde Base Line Road)
4.2.1	<p><u>Profile</u></p> <p>The proposed profile for this segment includes subtle changes to the existing vertical profile in order to minimize impacts to the rolling terrain while maintaining a reasonable design speed. Proposed profile changes include raising the profile at The Grange. A CWG member asked if a retaining wall was considered with natural stone instead of a cut. The project team indicated that this will be considered and reemphasized the subtle change with a 60k/hr posted speed.</p>
4.2.2	<p><u>Cross-section</u></p> <p>Cross-section options for Winston Churchill Boulevard were presented. The preferred option is the 11.4m platform rural option, with two 3.5m wide lanes, 1.7m paved shoulders, and adequate ditches on both sides. However, the existing ROW along Winston Churchill Boulevard is relatively narrow and a rural cross-section would result in significant impacts to adjacent properties and natural/culturally significant landscapes. Where the landscape is constrained (by features such as trees and fences, for example), an 11.4m platform, semi-rural cross-section (with two 3.5m wide lanes, 1.7m mountable curbs and underground drainage infrastructure) would be preferred. Both options would accommodate active transportation.</p> <p>CWG members asked what percentage of the road would be rural and what</p>

	<p>percentage would be semi-rural with a mountable curb. The project team was still in the process of assessing the corridors and will present this level of detail at the upcoming PIC. It was suggested to show pictures of mountable curbs at the PIC for the public to visualize what the design will look like. Some CWG members are not in favour of mountable curbs and don't like the look of them. Most agree, however, that a semi-rural cross-section would be a good compromise between providing an adequate road design for all users and minimizing impacts.</p>
4.2.3	<p><u>Plan</u></p> <p>A plan showing the extent of the grading impacts corresponding to both cross-section options was presented. The project team will further evaluate both options and determine the location where each cross-section option is recommended, and the recommended design (likely a combination of rural and semi-rural) will be presented at the upcoming PIC. Localized improvements will also be considered where impacts extend beyond the existing ROW.</p>
4.3	<p>Mississauga Road (Olde Base Line Road to north of Caledon Mountain Drive)</p>
4.3.1	<p><u>Profile</u></p> <p>The proposed profile for this segment includes subtle changes to the existing vertical profile in order to minimize impacts to the rolling terrain while maintaining a reasonable design speed. Proposed profile changes include raising /lowering driveways at some locations, but these changes are not drastic.</p>
4.3.2	<p><u>Cross-section</u></p> <p>Cross-section options for Mississauga Road were presented. The preferred option is the 11.4m platform rural option, with two 3.5m wide lanes, 1.7m paved shoulders, and adequate ditches on both sides. However, the existing ROW along Mississauga Road is relatively narrow (around 20m, with a designated 30m ROW in the Official Plan), and a rural cross-section would result in significant impacts to adjacent properties and natural/culturally significant landscapes. Where the landscape is constrained (by features such as cemeteries, trees and fences, for example), an 11.4m platform semi-rural cross-section (with two 3.5m wide lanes, 1.7m mountable curbs and underground drainage infrastructure) would be preferred, similar to Winston Churchill Boulevard. Both options would accommodate active transportation.</p> <p>It was noted that due to the narrower, constrained ROW, Mississauga Road is more likely to have longer segments of semi-rural cross-section (and shorter rural cross-section segments) compared to Winston Churchill Boulevard. More details will be presented at the upcoming PIC.</p> <p>Some CWG members suggested designing rural cross-sections with narrower lanes (to reduce footprint and avoid the need for a semi-rural option) and post at lower speeds than currently being proposed.</p>
4.3.3	<p><u>Plan</u></p> <p>A plan showing the extent of the grading impacts corresponding to both cross-section</p>

options was presented. The project team will further evaluate both options and determine the location where each cross-section option is recommended, and the recommended design (likely a combination of rural and semi-rural) will be presented at the upcoming PIC. Localized improvements will also be considered where impacts extend beyond the existing ROW. For example, at the pond north of The Grange, special design considerations will be taken into account and a semi-rural cross-section at this location could allow the mountable curb to drain water away from the pond, while a rural cross-section on the other side of the street would provide a ditch. Concerns were noted about how the design would be developed to take in to account for ponds and other features close to the road. The project team confirmed that this would be the approach taken and that options would be reviewed with property owners.

4.4. Olde Base Line Road (Winston Churchill Boulevard to Mississauga Road)

4.4.1 Profile

The proposed profile for this segment includes some significant changes to the existing vertical profile. The project team has tried to minimize elevation changes at all driveways, while also minimizing impacts to adjacent landscapes and providing a safe road for all users. The proposed profile, in combination with a reduction in the posted speed limit, attempts to balance all impacts and trade-offs.

It was suggested to produce cross-sections for locations of deep cuts/fills. It was noted that a lot of the cuts occur in bedrock areas. If fills are minimized and cuts are increased, it would create an interesting landscape to drive through.

It was noted that the rolling profile along Olde Base Line Road results in conflicts with cyclists as they slowly move up the hill, creating a greater speed differential with motorists and making it dangerous to share the lane. It is therefore important to provide a bike lane or shoulder. This is reflected in the cross-section options presented.

CWG members asked if the Niagara Escarpment Commission had been consulted about cut and fill. The project team confirmed that the NEC was involved through the Technical Advisory Committee and their input is being sought.

A further question was noted as to how individual property owners would be able to provide input and whether the project team would be receptive to changes to accommodate fences and trees. It was noted that an exaggerated profile will be shown at the PIC on November 20 to illustrate what is proposed and to discuss fences and trees with property owners. The project team advised that meetings could also occur with property owners when the design is 60 to 90% complete to review the design, options to accommodate specific conditions and to discuss mitigation.

4.4.2 Cross-section

Cross-section options for Olde Base Line Road were presented. The preferred option is the 11.4m platform rural option, with two 3.5m wide lanes, 1.7m paved shoulders, and adequate ditches on both sides. However, the existing ROW along Winston

Churchill Boulevard is relatively narrow, and a rural cross-section would result in significant impacts to adjacent properties and natural/culturally significant landscapes. Where the landscape is constrained (by features such as trees and fences, for example), an 11.4m platform semi-rural cross-section (with two 3.5m wide lanes, 1.7m mountable curbs and underground drainage infrastructure) would be preferred. Both options would accommodate active transportation.

Similarly to Mississauga Road, it was noted that due to the narrower, constrained ROW, Olde Base Line Road is more likely to have longer segments of semi-rural cross-section (and shorter rural cross-section segments) compared to Winston Churchill Boulevard. More details will be presented at the upcoming PIC.

4.4.3 Plan

A plan showing the extent of the grading impacts corresponding to both cross-section options was presented. The plan also illustrates subtle shifts to the road centreline to centre the road within the existing ROW. This would maximize utilization of the existing ROW and minimize impacts on either side of the road.

The study team will further evaluate both cross-section options and determine the location where each cross-section option is recommended, and the recommended design (likely a combination of rural and semi-rural) will be presented at the upcoming PIC. Localized improvements will also be considered where impacts extend beyond the existing ROW.

Questions about roundabouts were noted and why these are not being recommended. This discussion is referenced in section 5.0 of the CWG meeting notes.

4.5 **Belfountain Village**

4.5.1 Profile

In general, there are no profile changes proposed along this segment.

4.5.2 Cross-section

Due to the uniqueness of the village area, cross-section options for various locations along Bush Street and Mississauga Road/Old Main Street through the village were presented. In general, the existing ROW is narrow and constrained compared to the rest of the study area. All options presented include 3.3m wide travel lanes. The project team welcomed suggestions for modifications to the cross-sections that were presented, that would further accommodate all road users through this area. The project team will look at these options more closely and revise the designs through this area.

Bush Street approx. 300 m east of Shaws Creek

The available ROW at this location is approx. 12 m. Three options were presented: sidewalks on either side; shoulders on either side; or narrow buffer on one side and parking on the other side. A sidewalk with a rollover curb was suggested, but the project team pointed out safety concerns. It was questioned whether sidewalks/shoulders are required on both sides of the street. In order to better

accommodate active transportation through the portion of the study area experiencing the highest volume of cyclists and pedestrians, it is recommended to have shoulders/sidewalks on both side of the street. The second option, with shoulders on either side, more closely matches existing conditions.

Bush Street approx. 200 m west of Mississauga Road /Old Main Street

The available ROW at this location is approx. 10.5 m. Two options were presented: sidewalk on one side and no Active Transportation (AT) facility on the other side; or narrower buffer/shoulder on both sides. The second option, with buffer/shoulders on either side, more closely matches existing conditions. It was pointed out that due to the school zone on the south side, it might make sense to have a full sidewalk on the south side of the street. CWG members asked whether input has been received from the community regarding their preference for sidewalks. Through consultation to date, there are mixed opinions. From a policy perspective, it makes sense to accommodate pedestrians through the village, where the highest pedestrian volumes occur. The project team noted that the sidewalk design, look, and material are open to suggestions.

Bush Street approx. 60 m west of Mississauga Road /Old Main Street

The available ROW at this location is approx. 9.0 m. Two options were presented: narrow buffer on one side and no AT facility on the other side; or narrower buffer/shoulder on both sides. The second option, with buffer/shoulders on either side, more closely matches existing conditions.

Mississauga Road /Old Main Street approx. 85 m south of Bush Street

The available ROW at this location is approx. 13.3 m. Only one option was presented: 2.25 m paved shoulder on either side. This shoulder would provide space for active transportation, and might accommodate parking. CWG members suggested modifying this design to provide adequate parking on one side, and a sidewalk or multi-use trail on the other. Having parking at this location is important, but so is having safe, separate space to accommodate pedestrians and cyclists through this segment of the village. Sidewalks in this area are noted to be very important. Multi-use pathways are also considered to be very attractive for all users and for connections to the village.

Mississauga Road /Old Main Street approx. 275 m south of Bush Street (east of the community centre)

The available ROW at this location is approx. 8.0 m. Only one option was presented: narrow paved shoulder on either side. In order to accommodate an adequate sidewalk, widening to the south (but still within the existing ROW) would be required. It was suggested to bury hydro lines along this segment to maximize the available space available, or move the hydro poles onto people's lawns through the pinch point area. It was also suggested to hang the sidewalk as a cantilever over the existing retaining wall, but this creates a safety issue as pedestrians would be on the inner curve and sightlines are not adequate. It was further suggested to connect the sidewalk on either side of the pinch point with a trail behind the existing buildings; this might be a

challenge because of the marsh area at that location, but could be further investigated. It was noted that there is a tree buffer between the road and hydro corridor north of Caledon Mountain Drive. CWG members recommended that these options be considered and noted the importance of sidewalks and multi-use trails along the road in this location. Conflicts with cyclists were also highlighted as needed more review in the development of designs.

5.0 Roundabouts

Although roundabouts are no longer being considered for the study area, roundabout concepts for Mississauga Road /Olde Base Line Road, Winston Churchill Boulevard/Olde Base Line Road, and Winston Churchill Boulevard /Bush Street were discussed.

CWG members asked why roundabouts are no longer being proposed, as they would slow down motorists and have the potential to reduce the number of collisions. The project team recognizes the safety benefits of roundabouts, but there are significant impacts associated with them. In addition to their large footprint and impact to adjacent properties/landscapes, they would require profile adjustments beyond those currently being proposed by the team. Roundabouts would also result in a safety concern for cyclists, as it is difficult to accommodate them through a roundabout in rural settings. An off-street cycle path was suggested, but cyclists in this area tend to stay on or closer to the street. CWG members suggested consulting with local cyclist groups and the project team noted that there has been consultation with several groups in the area.

There are other safety mitigation measures that can be implemented in the study area, such as reducing speed limits, and removing/relocating obstacles like overgrown vegetation and guiderail at some locations.

CWG members noted that roundabouts might encourage traffic to divert. This study is not looking to divert traffic or change traffic patterns in the area. Policies to encourage roundabouts as a means of creating a network of roundabouts throughout the Region of Peel are also not a guiding principle for this study.

For the Mississauga Road /Olde Base Line Road intersection, it was suggested to have the posted speed transition from 60 to 70km/h south of Olde Base Line Road, rather than at the intersection. It was also suggested to consider 4-way stops at this intersection, as this would force motorists to stop. It was pointed out that some motorists already stop at this intersection due to misinterpretation of the yellow beacon.

6.0 Other Design Details

6.1 Lane Widths

CWG members asked why the lanes outside the village are 3.5m wide instead of 3.2-3.3m, as narrower lanes would reduce impacts to adjacent properties at constrained locations, in addition to encourage lower speeds. The project team referred to Transportation Association of Canada (TAC) standards, which stipulate a minimum

	lane width for a specific posted speed and range of traffic volumes.
6.2	<p><u>Paved Shoulder Design</u></p> <p>It was asked if there was any evidence of the effectiveness of a wider buffer compared to pavement line markings. The Consultant explained that a buffer is nice to have, but it would not be required due to the proposed speed reductions.</p>
6.3	<p><u>Pavement Design</u></p> <p>CWG members asked if the proposed designs would require full depth reconstruction, or resurfacing only. The design recommends a combination of the two, based on the geotechnical assessment recommendations, proposed profile changes, and type of cross-section.</p>
6.4	<p><u>Drainage</u></p> <p>It was questioned whether a mountable curb or full ditch are really required. The team explained these are required to provide adequate drainage. Existing conditions do not allow to adequately drain the road. Although the drainage details are still being looked at, there is no proposed storm water management pond.</p>
6.5	<p><u>Cut and Fill</u></p> <p>CWG members asked if natural stone retaining walls were considered instead of cuts. The Consultant explained that where significant grading impacts exist as a result of the proposed cross-section and profile adjustments, retaining walls will be considered as an alternative.</p>
6.6	<p><u>Design Level of Detail</u></p> <p>CWG members asked what level of detail would be provided in the designs though the EA. The project project team explained that the EA would complete the design to 30%. Then, during the detailed design phase, a consultant would be retained to complete a peer review of the preliminary resign, including the geotechnical recommendations, and determine if additional boreholes and other studies are required to move forward with the detailed design.</p>
6.7	<p><u>Localized Improvements</u></p> <p>The proposed design will identify areas where localized improvements would be required to minimize/mitigate impacts. Designs would be developed through consultation with individual property owners. All property owners are therefore encouraged to attend the upcoming PIC for these discussions to start taking place.</p>
6.8	<p><u>Land Acquisition</u></p> <p>CWG members asked if the decision between rural and semi-rural cross-section where there is a constrained ROW would depend on how willing land owners are to negotiate property acquisition. The project team explained the decision would be based on a variety of constraints, including but not limited to, encroachment on private property. Lori-Ann Thomsen from the Region of Peel explained the property acquisition process and noted that there would be no negotiations at this stage.</p>

	<p>Consultation with land owners (such as at the upcoming PIC) would encourage the discussions to start taking place, but land owners would be contacted individually at a later stage once more of the design details are confirmed. Negotiations for land acquisition do not typically take place until the detail design stage (60-90% design). At that stage, there would be meetings with the individual land owners to discuss the impacts and options, and there are opportunities for design modifications and mitigation strategies before the design is finalized.</p>
6.7	<p><u>Cost</u></p> <p>CWG members asked about the construction cost for the project. Since some design elements need to be confirmed and finalized, costs cannot be reasonably estimated at this stage. More details will be available at the upcoming PIC.</p>
6.8	<p><u>Recent Construction in Close Proximity to the Study Area</u></p> <p>The group asked about the cross-section for the newly constructed portion of Mississauga Road between King Street and Olde Base Line Road. The project team agreed to have details of that design (cross-section elements, dimensions) at the PIC as the team did not have this information at hand.</p>
7.0	<p>Public Information Centre #2 – November 20, 2013</p> <p>PIC #2 is scheduled for Wednesday, November 20 at the Caledon Country Club.</p> <p>As noted, the format will be different from the May 9 PIC, with no formal presentation. There will be an Open House from 4:30 p.m. – 8:30 p.m. The public will be able to arrive anytime between those hours and talk to the different project team members about the specific concerns or interest. The material will be displayed by “themes”, and the recommended designs (cross-section, plan, and profile) will be organized by corridor. CWG members agreed that this was a good format for holding the meeting.</p> <p>Feedback forms will be provided for people to provide their comments.</p>
8.0	<p>Next Steps</p> <p>Public Information Centre #2 – November 20, 2013</p> <p>Confirmation of Preferred Designs – Winter 2013/2014</p> <p>Environmental Study Report – Spring 2014</p> <p>Study Completion and Filing with MOE – Spring/Summer 2014</p>
9.0	<p>Closing Remarks</p> <p>Gino reiterated that the Region of Peel would like to actively engage the community and once again thanked the CWG members for their participation and encouraged them to attend the upcoming PIC.</p>

Newsletters



Project Bulletin

Study Expansion

July 11, 2012

Study to now include Olde Base Line Rd & Winston Churchill Blvd

Issue No. 1

Study Limits Expanded planning a better project

We listened to your feedback from the Mississauga Rd / Bush St environmental assessment to look at the needs for the whole corridor and have expanded the study area to include Winston Churchill Blvd and Olde Base Line Rd.

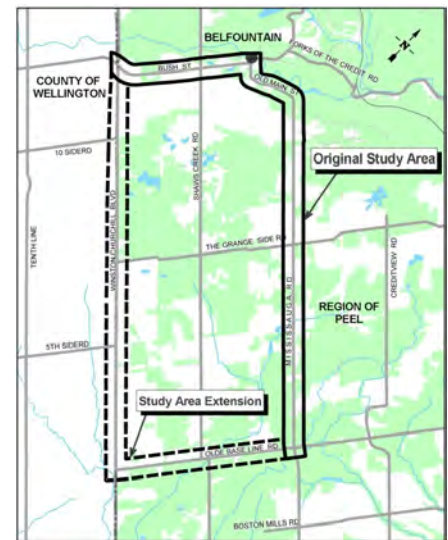
Some of the components that will be investigated in the expanded study will include:

- pavement condition
- sight lines
- pedestrian and cycling needs
- drainage

This study will not consider increasing the numbers of lanes. We will build on the previous study information to provide a solution for the study area that meets the needs of all road users and that also maintains the character of the community. The approach for the study will be to use context sensitive design principles.

Context sensitive design is responsive to local community values and:

- ✓ balances safety, mobility, community and environmental goals;
- ✓ involves the public and affected agencies early and often;
- ✓ uses a skilled team to tailor project needs;
- ✓ addresses all types of travel needs;
- ✓ uses flexible design standards, and;
- ✓ strives to make a space that is both functional and pleasing.

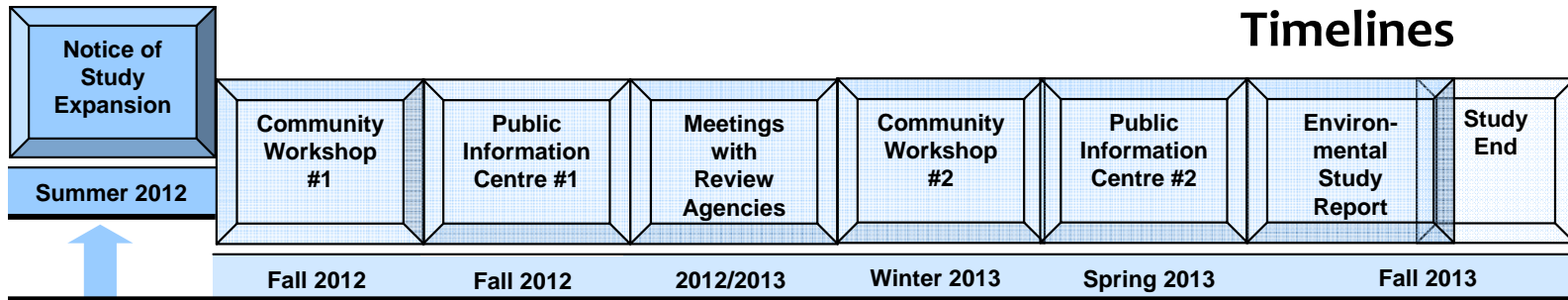


“An expanded study area will allow us to look at transportation needs holistically for all road users.”



Municipal Class Environmental Assessment

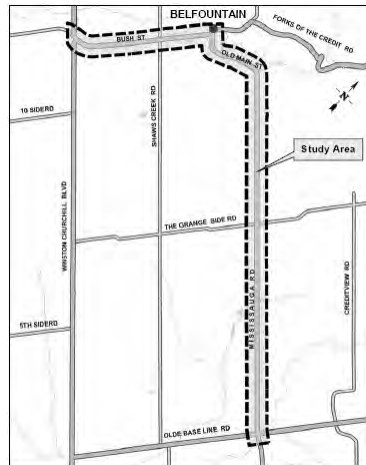
Timelines



Minimizing Impacts

previous study findings

A number of technical studies were undertaken for the Mississauga/Bush study. Study findings are summarized as follows:



A key aim for this study is to carry out consultation in a manner that allows for the sharing of ideas, education and testing of creative solutions.

Built & Cultural Heritage

Built heritage resources and cultural heritage landscapes will not be impacted.

Stage 1 Archaeological Assessment

The existing right of way does not retain archaeological site potential due to previous ground disturbances. If reconstruction extends beyond the existing right of way then a Stage 2 assessment is recommended.

Drainage and Hydraulics

Drainage deficiencies have been identified and consist of inadequate ditches and shoulder deficiencies which cause ponding and overgrowth of vegetation.

Natural Environment

Natural heritage features have been identified and species at risk have been identified. Mapping studies have determined areas of environmental sensitivity.

Similar studies will be completed for the expanded study area.

Community Involvement

Contact me if you would like more information on the expanded project or wish to be involved as a member of the community working group.

Hitesh Topiwala, RPP, PMP
Project Manager
905-791-7800 x7805
hitesh.topiwala@peelregion.ca

Stay updated with us!

Visit the project website:
peelregion.ca/pw/roads/envirom-assess/

Messages From Community Working Group Members



From left to right: Rachel Ingram, Glenn McMichael, Bryan Bibby Smith, Neil Morris, Greg Sweetnam, Steve Goyeche and David Jobe (absent: Sarah Morgenstern, Sergio Spanetta, Penny Richardson, Gord McArthur, Marion Miles, Ward Pitfield, Heather Wilkinson)

Having a CWG for this project has been a good idea. I can appreciate that this can be labour intensive for staff but CWG members and residents appreciate this extra step. By capturing feedback from residents in the area the project members are able to obtain a wider set of viewpoints which will lead to a better end result.

There has been a huge effort on the Team's part to ensure formal, informal, email and web based communication. There's been totally open communication that has been shared.

-- Sarah Morgenstern --

I wanted to become a member of the CWG to ensure that the community in the village and the roads surrounding it remain relatively the same and that we don't lose the country atmosphere.

Having a CWG for this project is absolutely a good idea; there is a strong feeling in the community that residents want the environment to remain the same.

-- Penny Richardson --

I live at a major intersection within the study area and I support the improvements. Today, cars, buses, trucks, farm equipment, motorcycles and bicycles use these roads on a regular basis. It is important that sight lines, road alignments and road structure be brought up to a standard where all road users can use these roads safely. I think that the Region is being responsible at this time in initiating these long overdue improvements.

-- Greg Sweetnam --

Caledon residents are passionate about the environment in which they live; they value it. Some people talk about wanting to "preserve" the environment. I prefer the word "enhance"; to me, it's more progressive. I would like the environment in the study area to be enhanced. This to me means that there will be an "improvement to move forward".

-- David Jobe --

Yes, I do feel that the process has been transparent so far. The Open House that was held last October helped with this transparency when a call for interest was made. The community has become engaged as a result of the Open House; residents have an understanding of the issues involved here and of the role of the CWG and all this has resulted in doubling the CWG membership.

Yes, we do have a balanced perspective on the CWG, especially now that we have further representation from local interest groups and businesses.

-- Glenn McMichael --

I was interested in becoming a member of the Community Working Group because I felt I could contribute to the area in which I owned real estate...with my knowledge of the area I feel that I'll be able to contribute positively to the work of the Community Working Group.

-- Ward Pitfield --

With divergent stakeholder concerns often associated with roads, it can be a challenge sometimes to get balanced and objective input. I am an environmentalist with an interest in sustainability and I hope to provide an objective viewpoint on the Community Working Group.

Yes I do think having a Community Working Group is a good idea for this project due to the relative unique and sensitive nature of the roadways in this area.

-- Neil Morris --

My interest in becoming a member of the Community Working Group stems from the stakeholders I come in contact with at Belfountain Public School. It's the school children who have an interest in the condition of these roads and their voices need to be reflected in the process.

I feel the meetings held so far carry a positive note, there's conciliation between competing interests. I'm enjoying being part of the Community Working Group.

-- Bryan Bibby Smith --

Contact Us: Gino Dela Cruz – Project Manager
Region of Peel
10 Peel Centre Dr., Suite B, 4th Floor
Brampton, ON L6T 4B9
gino.delacruz@peelregion.ca
Tel: 905-791-7800 ext. 7805

Asha Saddi – Technical Analyst
Region of Peel
10 Peel Centre Dr., Suite B, 4th Floor
Brampton, ON L6T 4B9
asha.saddi@peelregion.ca
Tel: 905-791-7800 ext. 7794

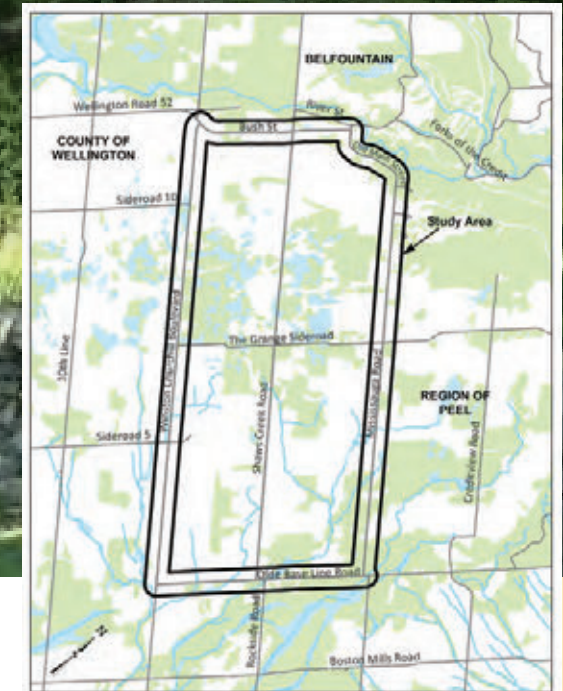
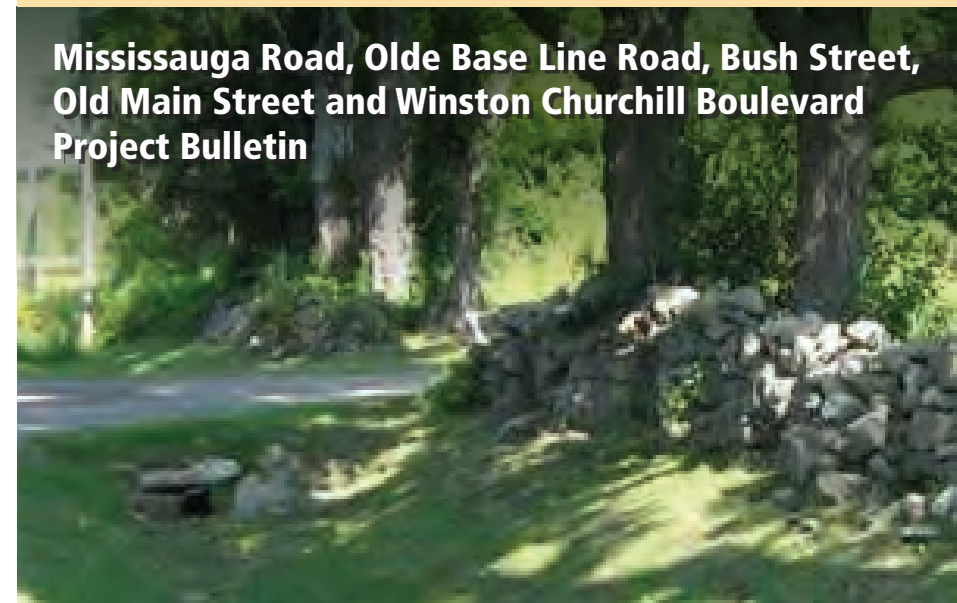
Tyrone Gan – Project Manager
HDR Corporation
100 York Boulevard, Suite 300
Richmond Hill, ON L4B 1J8
tyrone.gan@hdrinc.com
Tel: 289-695-4622

Municipal Class Environmental Assessment

APRIL 2013

Issue No. 2

Mississauga Road, Olde Base Line Road, Bush Street, Old Main Street and Winston Churchill Boulevard Project Bulletin



What is this project about?

- ✓ Rehabilitation of the roads
- ✓ Enhancing safety
- ✓ Supporting bicycling and walking

This project will not be considering road widening or increasing the number of lanes.

Existing problems on the study area road network consist of:

- Poor pavement conditions and drainage
- Deficient sightlines
- Safety for all road users, including safety of wildlife
- Motor vehicle accidents involving wildlife

Context Sensitive Design Principles

Alternative solutions will follow context sensitive design principles and maintain the rural character and countryside scenic quality of the study area.

Please join us for our Public Information Centre

When:
Thursday, May 9, 2013
6:30 p.m. – 9:00 p.m.

Where:
Belfountain Public School
17247 Shaws Creek Road, Caledon

Open House: 6:30 p.m. – 7:00 p.m.
Presentation and Discussion: 7:00 p.m. – 9:00 p.m.

How are we keeping you informed?

- ✓ Newsletters
- ✓ Public Information Centres
- ✓ Region of Peel study website:

<http://www.peelregion.ca/pw/transportation/environ-assess/mississauga-road-bush.htm>

Region of Peel
Working for you

Message from the Councillors



As a resident who lives directly south of this vicinity, as a lifelong Caledon resident and as the regional representative of our Ward 2 community, I can say confidently that the need for this EA is long overdue in my opinion.



The long overdue and postponed EA process is moving forward with many opportunities for public involvement.

Key to the EA is rationalizing the roads so that commuters make the right choices on the best route to take to work.

It is important that these roads be brought up to standard so that future investments in repaving etc will secure their full value.

Staff understand that the look and feel of the roads as they exist today is important to residents. The EA is not about building speedways, but building and maintaining roads in a cost effective manner while respecting the natural environment and values of the residents.

Allan

Councillor Allan Thompson

R

Councillor Richard Paterak

Traffic is only going to continue to increase over the next few years and we need to be proactive and develop and establish solutions that are workable for our residents and the community.

To date a lot of good work has been done and your involvement as residents is very important. I know many residents have already become engaged in the process and I want to encourage as many of you as possible to become involved and have your say.

Community Working Group (CWG)

The CWG is a non-voting group that makes recommendations for consideration.

The 14 members of our CWG

- ✓ share their personal knowledge of the study area, users of the roads and community character
- ✓ are informed and engaged
- ✓ help to identify community issues and potential solutions

CWG meetings were held on October 23, 2012 and April 4, 2013. Summary notes from these meetings are available on the Region of Peel study website.

Open House

A Public Open House held on October 30, 2012, was attended by approximately 100 community residents and stakeholders who spoke of the importance of maintaining the historic and much valued countryside and scenic character of the area.

The Open House provided the opportunity for the public to discuss context sensitive planning and design principles used for the study.

Community residents and stakeholders had many comments about the study scope and process, identified valued community characteristics and discussed transportation issues, condition of the roads and the natural environment.

The Facilitator's Feedback Report is available on the study website.

Technical Studies Undertaken

- Transportation Needs Assessment
- Geotechnical and Drainage Investigations
- Inventory of existing conditions:
 - ✓ Natural Environment
 - ✓ Archaeological Assessment
 - ✓ Built/Cultural Heritage Assessment

Existing Problems and Alternative Solutions

Safety

Existing problems in the study area include:

- Substandard sight distances for drivers due to the rolling terrain
- Limited sight distances at many of the driveways and intersections
- Steep grades in some sections
- High number of collisions at the intersection of Mississauga Road and Olde Base Line Road
- Roadside hazards
- Large number of wildlife-related collisions

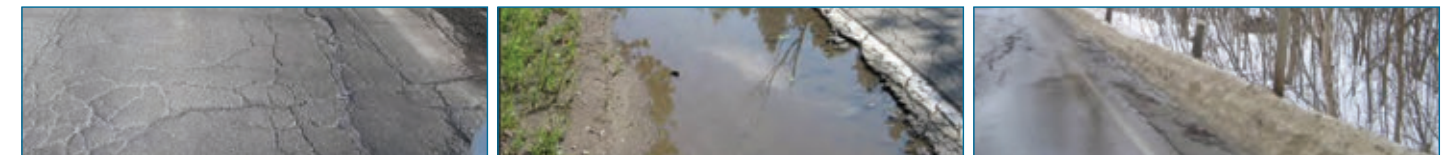
Alternative solutions include:

- Improving pavement markings
- Additional warning signs
- Reduced speed limits
- Changes in the road profile
- To address roadside hazards, countermeasures such as removing hazards or installing guiderails
- Designated wildlife crossing areas
- Consideration of roundabouts at the following intersections:
 - Mississauga Road / Olde Base Line Road
 - Winston Churchill Boulevard / Olde Base Line Road
 - Winston Churchill Boulevard / Bush Street

Pavement Condition

Existing problems in the study area:

- The preliminary findings of the geotechnical investigations reveal that structural capacity and strength of all roads are in poor condition and are expected to continuously deteriorate.
- The main cause to pavement distress is attributed to variable granular thickness along the roadways with a non-uniform base and sub-base materials.
- Shoulder granular is also thinner than the sub-base below the roadway which affects the drainage of the base leading to frost heave and rutting.
- Some of the pavement deficiencies identified throughout the study area include:
 - Wheel tracking and rutting
 - Transverse and longitudinal meander and mid-lane cracking
 - Alligator pavement edge cracking



Alternative solutions include:

- Roadway rehabilitation and full depth reconstruction

Traffic

Existing traffic related problems include:

- Excessive vehicular speeds
- Accommodation for all road users
- Conflicts between motorized vehicles, pedestrians, cyclists and wildlife
- Parking congestion in Belfountain

Alternative solutions include:

- Partially paving shoulders in some locations
- Widening shoulders in some locations
- Sidewalks or landscaping to shelter pedestrians from vehicular traffic in parts of the Village of Belfountain

Timeline



Notice of Study Completion

NOTICE OF STUDY COMPLETION MISSISSAUGA ROAD / OLD MAIN STREET, BUSH STREET, OLDE BASE LINE ROAD AND WINSTON CHURCHILL BOULEVARD

The Study

The Region of Peel has completed the Schedule 'C' Municipal Class Environmental Assessment (EA) to develop a road design that addresses safety, drainage, and pavement deficiencies on Mississauga Road / Old Main Street, Bush Street, Olde Base Line Road and Winston Churchill Boulevard. The approximate limits of the project area are illustrated on the map.

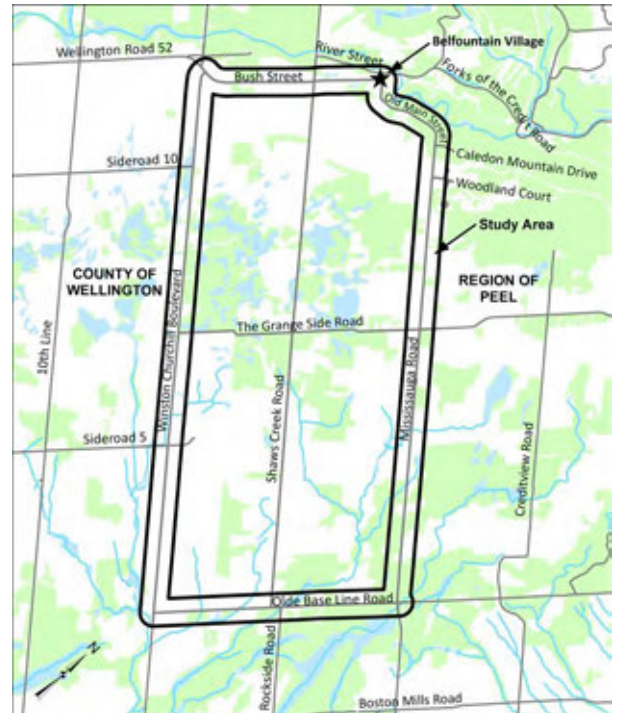
The Process

The project team received input from interested stakeholders, the public and agencies at an Open House, two Public Information Centres, three Community Working Group meetings, and numerous agency and stakeholder meetings. The team evaluated road improvement alternatives, assessed the potential environmental effects of the proposed improvements and developed reasonable means to mitigate any adverse impacts.

Key Elements of the Recommended Design

The study recommends:

- A rural cross-section with paved shoulders and ditches for parts of Bush Street and Olde Base Line Road
- A semi-rural cross section with paved shoulders, mountable curbs and underground storm sewers for the remainder of the study area
- Parking and a sidewalk through parts of Belfountain Village
- Modest profile adjustments to improve sightlines



Environmental Study Report

An Environmental Study Report (ESR) has been prepared to document the planning and decision making process undertaken for this study. The results of the study will be available for review for 30 calendar days starting June 30, 2014 and ending July 29, 2014. The ESR is available for review at the following locations:

<p>Region of Peel, Clerk's Office 10 Peel Centre Drive, Suite A Brampton, ON L6T 4B9</p>	<p>Town of Caledon, Clerk's Office 6311 Old Church Road Caledon, ON L7C 1J6</p>	<p>County of Wellington, Clerk's Office 74 Woolwich Street Guelph ON, N1H 3T9</p>	<p>Town of Caledon Library, Belfountain Branch 17247 Shaw's Creek Road Caledon, ON L7K 0E8</p>
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Comments

Please visit our website: peelregion.ca/pw/transportation/environ-assess/mississauga-road-bush.htm for more information. Please provide written comments to **Gino Dela Cruz** within the 30-day review period. If the concerns cannot be resolved, you may request that the Minister of the Environment make an Order for the project to comply with Part II of the Environmental Assessment Act, which addresses individual environmental assessments. The Minister must receive requests for Part II Orders at the address below by 4:30 pm on July 29, 2014.

The Honourable Jim Bradley, Minister of the Environment

Ministry of the Environment, 77 Wellesley Street West, 11th Floor, Toronto, ON M7A 2T5

A copy of the Part II Order request must also be sent to:

Mr. Gino Dela Cruz, Project Manager, Region of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, ON L6T 4B9
Tel: 905-791-7800 ext. 7805
Fax: 905-791-1442
E-mail: Gino.DelaCruz@peelregion.ca

Mr. Tyrone Gan, Project Manager, HDR
100 York Boulevard, Suite 300
Richmond Hill, ON L4B 1J8
Tel: 289-695-4622
Fax: 289-695-4601
E-mail: Tyrone.Gan@hdrinc.com

The Region of Peel is committed to ensure that all Regional services, programs and facilities are inclusive and accessible for persons with disabilities. Please contact the Project Manager if you need any disability accommodations to review the ESR.

Date posted: June 23, 2014

Email Correspondence

Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Thursday, February 28, 2013 2:58 PM
To: Gan, Tyrone; Baudais, Nathalie
Cc: Saddi, Asha
Subject: FW: File 11-4360 Mississauga Rd, Olde Base study area

FYI – see email below from local resident and response provided.

Gino

From: Dela Cruz, Gino
Sent: February 28, 2013 2:53 PM
To: [REDACTED]
Cc: Saddi, Asha
Subject: RE: File 11-4360 Mississauga Rd, Olde Base study area

Hi [REDACTED]

Thank you for providing your comments. Asha forwarded me your email below.

The open house on October 30, 2012 was conducted to meet the community residents and stakeholders, to discuss the study process, and to get a sense of the transportation issues and community characteristics in the study area. We received a lot of input and the key messages are summarized in the Public Open House Feedback Report (which can be accessed online at <http://www.peelregion.ca/pw/transportation/envIRON-assess/mississauga-road-bush.htm>).

The project is now progressing with the various technical studies which, along with the comments received from the public and stakeholders at the open house and thereafter, will lead to the development of the problem/opportunity statement and alternative solutions. This will be presented at the upcoming Public Information Centre (tentatively scheduled for May 2013).

With regards to your comments on providing parking at the Badlands to alleviate congestion from Belfountain, I note that the Badlands is approximately 8km from Belfountain and is outside our study area. We will review this with the Project Team, but improvements to parking congestion in the Belfountain area may be limited.

For your information, separate from this EA study, the Region is aware of parking and related traffic issues at the Badlands site and is currently planning some traffic calming measures at the site. One of the long-term options that is being reviewed is a parking lot at the site. The parking lot currently being considered would be within the property limits of the site.

Again, thank you for providing your comments. We will add you to the study mailing list and make sure you are informed of any study updates. If you have any additional questions or comments please do not hesitate to contact me.

Kind regards,

P. Gino Dela Cruz, P.Eng.
Project Manager, Transportation Program Planning
Region of Peel, Public Works
10 Peel Centre Dr., Suite B 4th Floor
Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: February 28, 2013 1:31 PM
To: Saddi, Asha
Subject: File 11-4360 Mississauga Rd, Olde Base study area

Hi Asha,

I was reading the reports from the open house and noted that there were suggestions that a permanent parking area be constructed across from the Badlands. This parking area would alleviate the congestion from the Belfountain area. We also own this property across from the Badlands. What was discussed?

When is the next public meeting?

Thanks in advance.

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Kong, Carol

Subject: FW: question

AMServiceURLStr: <https://Slingshot.hdrinc.com:443/CFSS/control?view=services/FTService>

From: Dela Cruz, Gino

Sent: April 8, 2013 1:29 PM

To: [REDACTED]

Cc: Saddi, Asha; [REDACTED] Ganesh, Steve

Subject: RE: question

Hi [REDACTED]

The information regarding the construction phases is what has been planned for and is subject to the annual capital budget review process. The Region needs to plan out its infrastructure improvement projects well in advance to ensure funding and other requirements such as EA approvals are in place when the infrastructure improvements are needed. The length of the construction phase is only an estimate of the timeline for construction based on the information we know today and may change as a result of new information or findings and recommendations from this current study.

As noted in this morning's email to CWG members, we are reviewing the costs associated with maintaining the roads in the study area and comparing that with the potential costs to reconstructing the road. Keep in mind that cost is only one of many factors being considered when determining the need to improve these roads. Other factors include addressing safety, pavement and drainage deficiencies to ensure Regional roads are up to standard and in a state of good repair for both residents and the travelling public.

We are pursuing this Municipal Class EA study to determine the problem statement, review alternative solutions and determine a preferred solution that addresses the identified problem(s). At this stage of the study, we have yet to determine a preferred solution. This will be analysed based on the findings from our technical studies, and in consultation with agencies, CWG members and the public.

Please give me a call if you have any additional questions/comments or if you would like, we can setup a meeting in the Belfountain area or at the Regional office to discuss your questions further.

Best Regards,
Gino

P. Gino Dela Cruz, P.Eng.
Project Manager, Transportation Program Planning
Region of Peel, Public Works
10 Peel Centre Dr., Suite B 4th Floor
Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]

Sent: April 8, 2013 11:27 AM

To: Dela Cruz, Gino

Cc: Saddi, Asha; [REDACTED]

Subject: question

how do you know the length of the construction phase for each of these roads if in fact a decision has not been made as to what is going to be done to the roads???

Perhaps we as taxpayers will decide it is far less expensive to just maintain Winston Churchill rather than paving it!!

Sounds to me that perhaps some decisions have already been made and if so the meetings are a waste of time.

perhaps I am misinterpreting



Kong, Carol

From: Sue Cumming <cumming1@total.net>
Sent: Wednesday, April 10, 2013 3:52 PM
To: 'Ganesh, Steve'; 'Dela Cruz, Gino'; 'Saddi, Asha'; Gan, Tyrone; Restrepo, Veronica; 'Susan Cumming'
Cc: 'Chiu, Ken'
Subject: RE: Roads Reconstruction Environmental Assessment (EA) Process: Moving Along; 9 May Next Public Session

Hello Everyone

I could participate in person on either of the following: April 22 or April 24 anytime between 11:00 a.m. and 4:00 p.m. My preference would be for a start time around 11:00 or 11:30 if that works out for everyone else.

Could you forward the chain of emails that [REDACTED] started (it would be helpful to see what they are saying) before we meet!

Best Regards
Sue Cumming, MCIP RPP
Cumming+Company
866 611-3715

From: Ganesh, Steve [mailto:Steve.Ganesh@peelregion.ca]
Sent: April-10-13 1:39 PM
To: Dela Cruz, Gino; Saddi, Asha; 'Gan, Tyrone'; Restrepo, Veronica; 'Susan Cumming'
Cc: Chiu, Ken
Subject: FW: Roads Reconstruction Environmental Assessment (EA) Process: Moving Along; 9 May Next Public Session

Hi Team:

See below the chain of emails that was forwarded to me from Councillor Paterak. This chain plus the ones started by [REDACTED] will cause a "stir" to say the least among the public prior to the May 9 PIC. Issues that keep arising include:

- EA process and the difference between 30% design, detailed design
- reconstruct VS annual maintenance – what is better in the long run
- trucks vs. no trucks

It is obvious that some CWG members have put their own spin on our material so I would like to propose a pre May 9 PIC meeting to talk about pre-emptive communication we may need to take so we are all on the same page and how we can deal with this at the PIC.

Gino, Asha and I are briefing our internal management team on April 16 and Councillors on April 25

I would like to suggest a meeting with all of us and Ken Chui from Peel's Communication Branch to brainstorm some options to handle the line of questions we will get on May 9. PIC No. 1 will set the stage for the pace of this project moving fwd so if we can get a few "dress rehearsals" in before hand it would be good. I would also like to meet before April 25 so Gino and I can let both Councillors' know how we plan to handle these issues.

Gino can you please work with Tyrone to coordinate a date & time say 2 hrs to meet. Sue we will need you for this session so perhaps teleconference if face to face can't work. I am available April 22 all day, 23 - 9 to 11 or 3 to 5, 24 - all day.

I am away April 18 and 19 on vacation but I can teleconference from Ottawa if required.

Thanks,
Steve

Steve Ganesh, MCIP, RPP
Manager, Transportation Program Planning
Region of Peel
Public Works
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: Ganesh, Steve
Sent: April 10, 2013 1:07 PM
To: [REDACTED]
Subject: RE: Roads Reconstruction Environmental Assessment (EA) Process: Moving Along; 9 May Next Public Session

Thanks very much [REDACTED] I will pass on to the team so we can prepare for May 9.

Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Transportation Program Planning
Region of Peel
Public Works
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: [REDACTED]
Sent: April 10, 2013 1:03 PM
To: Ganesh, Steve
Subject: FW: Roads Reconstruction Environmental Assessment (EA) Process: Moving Along; 9 May Next Public Session

FYI

From: [REDACTED]
Sent: Monday, April 08, 2013 11:04 AM
To: [REDACTED]
Cc: [REDACTED]
Subject: RE: Roads Reconstruction Environmental Assessment (EA) Process: Moving Along; 9 May Next Public Session

Thank you very much for this note [REDACTED] and for being part of the working group on behalf of all the WCB residents. I was not aware from the previous public meeting on October 30th that James Dick Construction was part of the Working Group and find it disturbing that they are given a voice during your meetings. Is this correct protocol? In addition, will Sweetnam's presentation be repeated for the public at the upcoming meeting on May 9th so that, once again, our residents will be made publically aware of what JDC's actual hidden agenda is?

Enforced, reduced speed limits and additional stop signs are all necessities to provide our residents with safe traffic flow, but in order to prevent gravel and large truck traffic WCB needs to be designated as a no truck traffic road. Who determines that status? If it is the Region of Peel, then strong efforts must be directed at them. Established truck routes are in place in the area now, north of Bush St. to service all of JDC's current and future needs, I see no acceptable argument as to why that should ever change.

Regards,

[REDACTED]
[REDACTED]

From: [REDACTED]
Sent: Sunday, April 07, 2013 6:14 PM
To: [REDACTED]
Cc: [REDACTED]
Subject: Re: Roads Reconstruction Environmental Assessment (EA) Process: Moving Along; 9 May Next Public Session

Hi [REDACTED]

Thank you for the up-date. The increased traffic is already very noticeable and our tranquil properties are polluted with the noise of speeding cars and increased traffic. The road improvements and as you said " efficient movement of goods " means that we will be soon subject to even more noise caused by heavy truck traffic and their breaks etc.

Do we have any voice left in this regard or all the issues have been discussed and already "agreed on " ? Do we need to consider moving out to some less noisy areas ?

[REDACTED]
[REDACTED]

On Apr 7, 2013, at 3:20 PM, [REDACTED] wrote:

Greetings neighbours;

There was a second working group session Thursday 4 April night with the Region of Peel (RoP) about the reconstruction of Mississauga Road, Bush Street, Winston Churchill Boulevard (WCB) and Olde Base Line (OBL).

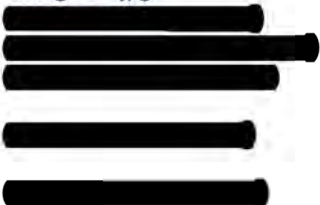
I have attached the .pdf presentation from Thursday evening. There is a lot of information contained in it. The Working Group made recommendations for changes; they are not reflected here. Slide 6 is one of the most interesting charts, as it shows since 2008-2009 that the traffic load has increased on WCB and OLB – the chart shows that it is now more balanced between Mississauga Road and WCB, and between Bush Street and OLB. The consensus being, this balancing of traffic volume coincided with the hard surfacing / tar & chipping of WCB, and commuters from the direction of East Wellington find the drive down and up via WCB and OLB less onerous than going thru Belfountain. It also likely has something to do with the closing of Mississauga Rd through Belfountain for reconstruction around that time.

It was clear from the meeting that the Region of Peel intends to reconstruct these roads - they will be dug up and repaved. This will be a monumental task on some portions of WCB as the base on some sections is a “corduroy” road – logs laid side by side. Improving sightlines also appears to be a priority of the Region. This could involve removing some curves, hummocks, etc. The working group reinforced the importance of preserving the rural nature of these roads and their scenic quality. As a point of interest to WCB residents, [REDACTED] attended the meeting and was vocal about the roads needing to be reconstructed to provide for the “efficient movement of goods” (his words) – meaning, of course, trucks from his gravel pit. I voiced the opinion, several times, that none of the WCB residents want our road to become a thorough-fare for truck traffic. In my opinion, one of the best ways to avoid this is to reduce speed limits, put in more stop signs so that the traffic flow is disrupted, etc. These are things that we will need to push for in order to ensure that WCB does not become a truck route.

You should also know that the section of WCB between OLB and Ballinafad Road will be paved this summer. The section from Ballinafad Rd to Terra Cotta will not be paved at the same time as the Region of Peel is still trying to negotiate a funding contribution arrangement with Halton Hills.

There is another, second Public Meeting scheduled for 9 May, Thursday evening at 6:30 at Belfountain Public School; I hope you can attend. I will be in France, but [REDACTED] will see you there!

<image003.jpg>



This email communication is CONFIDENTIAL AND LEGALLY PRIVILEGED. If you are not the intended recipient, please notify me at the telephone number shown above or by return email and delete this communication and any copy immediately. Thank you.

L'information paraissant dans ce message électronique est CONFIDENTIELLE. Si ce message vous est parvenu par erreur, veuillez immédiatement m'en aviser par téléphone ou par courriel et en détruire toute copie. Merci.

<WCB EA April 4 2013.presentation.pdf>

Kong, Carol

From: Saddi, Asha <Asha.Saddi@peelregion.ca>
Sent: Wednesday, April 17, 2013 12:26 PM
To: [REDACTED]
Cc: [REDACTED]
[REDACTED] Dela Cruz,
Gino; Ganesh, Steve; Gan, Tyrone; Restrepo, Veronica; 'Susan Cumming'
Subject: RE: meeting in May

Hello [REDACTED]

Thank you for your email.

The Public Information Centre (PIC) on May 9 is going ahead as scheduled and has not been cancelled. All residents in the study area will be receiving notification of the meeting next week and as indicated in my earlier email, advertisements for this meeting will also be appearing in the local newspapers.

I'm sorry to hear that you won't be able to attend the meeting. The PIC is a public meeting so anyone interested in this study is welcome to attend.

If you have any other questions please don't hesitate to contact me.

Thanks,

Asha Saddi
Technical Analyst
Transportation Program Planning
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
saddia@peelregion.ca

From: [REDACTED]
Sent: April 16, 2013 6:44 PM
To: Saddi, Asha
Subject: meeting in May

Rumour has it that this meeting might be cancelled!!!! true???? I will not be able to attend as I am attending a function for the Region of Peel that evening. Could I send someone in my place???

Please advise
[REDACTED]

Kong, Carol

From: Saddi, Asha <Asha.Saddi@peelregion.ca>
Sent: Monday, April 29, 2013 10:47 AM
To: [REDACTED]
Cc: [REDACTED];
[REDACTED];
[REDACTED];
Subject: Dela Cruz, Gino; Ganesh, Steve; Gan, Tyrone; Restrepo, Veronica; 'Susan Cumming'
RE: Roads Reconstruction Environmental Assessment (EA) Process: Moving Along; 9 May
Next Public Session

Good Morning [REDACTED]

Thank you for your email.

The CWG meetings are open to the public and anyone interested in this study is welcome to attend the meetings as an observer. The provisions of a Schedule "C" Class EA calls for two points of contact with the general public in the study area, known as Public Information Centres. These are published in the newspapers. The CWG is designed to involve a select group of people living and working in the area and who expressed interest (by way of application) in learning about this project.

We are no longer accepting additional members to the CWG; the deadline for submissions was November 14, 2012.

The presentation at the May 9 Public Information Centre will outline the role of the CWG as an additional point of public contact for the Municipal Class EA process. The role and mandate of the CWG will not be a formal part of the presentation. However, Sue will introduce the CWG members and point out that the CWG is an additional point of contact above and beyond the legislative requirements for a Schedule "C" Class EA.

I hope this helps. If you have any other questions, please let me know.

Thanks,

Asha Saddi
Technical Analyst
Transportation Program Planning
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
saddia@peelregion.ca

From: [REDACTED]
Sent: April 27, 2013 12:11 PM
To: Saddi, Asha
Cc: Ganesh, Steve
Subject: FW: Roads Reconstruction Environmental Assessment (EA) Process: Moving Along; 9 May Next Public Session

Hello Asha;

I have a concern from a resident....

They would like to attend the CWG sessions as an observer...and they are wondering why the CWG sessions are not advertised and open to the public.

Are additional participants still needed for the CWG?

Is the mandate of the CWG / description of the CWG part of the 9 May presentation?

Thanks,



Kong, Carol

From: Gan, Tyrone
Sent: Monday, May 13, 2013 5:46 PM
To: Chow, Tavia
Subject: FW: SGMNS and RCS Slides
Attachments: RCS-SGMN Slides.pdf; CWG#2 April 4.meeting notes.pdf

TYRONE GAN
P. Eng.

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Note: Effective March 17th, my phone number changed. The direct dial number is 289.695.4622

From: Ganesh, Steve [<mailto:Steve.Ganesh@peelregion.ca>]
Sent: Monday, May 13, 2013 4:21 PM
To: [REDACTED]
Cc: Dela Cruz, Gino; Saddi, Asha; Saiyed, Sabbir; 'cumming1@total.net'; Gan, Tyrone; Maraj, Ashwantie (Kelly); Saiyed, Sabbir
Subject: SGMNS and RCS Slides

Hello [REDACTED]

As discussed today please see attached a copy of the two slides I spoke of at last Thursday's meeting. The first being the Strategic Goods Movement Network – a long term plan for goods movement in Peel. The second is the Road Characterization Study which identifies WCB, Olde Base, Mississauga Road, and Bush Street as a "Rural Road" and Old Main Street as a "Rural Main Street". Regional Council endorsed these initiatives at the Council meeting of May 9.

Both initiatives were developed by members from local area municipalities, including staff from the Town of Caledon. I have copied Sabbir Saiyed on this email so he can answer specific questions you may have.

As indicated at the PIC on May 9, the EA process is one way the Region can use to further investigate the findings of the Strategic Goods Movement Network. With respect to previous CWG meetings we have always discussed this project as taking a balanced view to all users of the roads. For your reference I have attached notes from the latest CWG meeting whereby Section 7.2 states...."Suggestions from CWG members include..provide solutions that can safely and efficiently move vehicles, including trucks".

I welcome your offer to meet and discuss the asset management costs for these roads and I have copied Ms. Kelly Maraj on this email to organize a meeting with us. Please let her know when you are available in the coming week.

In the meantime should you have any questions please contact me.

Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Transportation Program Planning
Region of Peel
Public Works
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Thursday, August 29, 2013 11:30 AM
To: [REDACTED]
Cc: [REDACTED]
[REDACTED] Gan, Tyrone; Restrepo, Veronica;
'Susan Cumming'; [REDACTED] Saddi, Asha; [REDACTED] Ganesh, Steve
Subject: RE: EA Update: Tour of Roads

Hello [REDACTED]

The latest major correspondence with the CWG is attached and was sent about a month after PIC #1. The project team provided a link to the Feedback Report which summarizes the PIC and the comments we heard back from the public. Also provided in the email were minutes from the meeting with area residents, an information sheet on asset management and maintenance costs, and some examples of roadway improvements.

We have been busy during the summer months developing and evaluating alternative designs for the study area corridor and plan to present this at the upcoming PIC in mid-November (final date to be confirmed). We will also be scheduling a meeting with the CWG as well as our Technical Agencies Committee (TAC) before the PIC.

If you have any comments or questions regarding this study, please feel free to send me an email or give me a call.

Regards,

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: August 26, 2013 2:12 PM
To: [REDACTED] Saddi, Asha
Cc: [REDACTED]
[REDACTED] Dela Cruz, Gino; 'Gan, Tyrone';
'Restrepo, Veronica'; 'Susan Cumming'; [REDACTED]
Subject: Re: EA Update: Tour of Roads

Has anyone heard anything re roads???? I have not heard a peep since the meeting on WC. Was correspondence sent to the region????

[REDACTED]

From: [REDACTED]
Sent: Wednesday, May 15, 2013 11:25 AM
To: [REDACTED]; 'Saddi, Asha'
Cc: [REDACTED]
[REDACTED]; 'Dela Cruz, Gino'; 'Gan, Tyrone'; 'Restrepo, Veronica'; 'Susan Cumming'; [REDACTED]
Subject: Re: EA Update: Tour of Roads

Hello everyone,

I attended the meeting May 9th, and was stunned to learn, that council that very day, had endorsed the "good movement Initiative". Has the Region now aligned themselves with the aggregate companies????.... if so, shame, shame, shame!!!. Really, was there ever any consideration for the residents, at anytime during the process', prior to the meeting on May 9th? As the meeting progressed it was apparent the Region's representatives that night, could only continue to reiterate what was on the slides in the presentation, often not being able to fully answer questions posed by the public. The serenity, the green space shared by man, and wildlife, is what enticed me to relocate from a fast paced life in Halton Hills 21 years ago. Neighbors in this area, being here well before I, were of the impression from the Region then (80's/early 90's) that, WCB would not become a truck route. As with the application for the proposed pit (WCB/OBL), was valiantly fought against and won, by the residents. So to, this new agenda of "good movement initiative" will not go away quickly or quietly...it is strongly opposed by the residents.

██████████

----- Original Message -----

From: ██████████
To: 'Saddi, Asha'
Cc: ██████████; 'Dela Cruz, Gino'; 'Gan, Tyrone'; 'Restrepo, Veronica'; 'Susan Cumming'; ██████████
Sent: Wednesday, May 15, 2013 9:03 AM
Subject: RE: EA Update: Tour of Roads

Hello Asha,
I am also interested in the "tour".

I was out of the country last week and missed the public meeting, but suffice it to say, I was so very disappointed to hear that the Region's new "good movement initiative" involves turning WCB and OBL into truck haul routes. If the Region has been working on this "initiative" for some time, why were we not told about this at the Working Group meetings? You will recall that I and others repeatedly stated that we did not want WCB/OBL to become haul routes – but yet no one from the Region mentioned this "good movement initiative". I know that you have heard from others on the same issue, and can expect this to be something that will be loudly and actively opposed by the residents.

Thanks.

██████████

From: ██████████
Sent: May-15-13 8:39 AM
To: 'Saddi, Asha'
Cc: ██████████
'Dela Cruz, Gino'; 'Gan, Tyrone'; 'Restrepo, Veronica'; 'Susan Cumming'; ██████████
Subject: RE: EA Update: Tour of Roads

Like ██████████ I was not under the impression individual interest was to have been expressed to initiate what I will refer to as 'the tour of roads'....self-guided is fine for me....as long as we have an idea of what we are looking for / at.....depends on how many CWG members are interested I suppose.

Looking forward to the itinerary,

██████████

From: Saddi, Asha [<mailto:Asha.Saddi@peelregion.ca>]
Sent: May-14-13 15:08

To: [REDACTED]
Cc: [REDACTED];
Dela Cruz, Gino; 'Gan, Tyrone'; 'Restrepo, Veronica'; 'Susan Cumming'; [REDACTED]
Subject: RE: EA Update

Hello [REDACTED]

Thank you for your email.

We only had a response from one CWG member for the site visit. We would be happy to arrange a site visit for any of the CWG members that are interested.

If you are still interested, please let me know your availability and we can schedule accordingly.

Thanks,

Asha Saddi
Technical Analyst
Transportation Program Planning
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax: 905-791-1442
saddia@peelregion.ca

From: [REDACTED]
Sent: May 14, 2013 2:44 PM
To: [REDACTED] Ganesh, Steve
Cc: [REDACTED];
Dela Cruz, Gino; 'Gan, Tyrone'; 'Restrepo, Veronica'; 'Susan Cumming'; Saddi, Asha
Subject: RE: EA Update

Hello Steve;

The information [REDACTED] asking for will be useful in building your business case / benefits assessment. I too appreciate the approach you are taking. I feel an NPV and / or an IRR analysis using the RoP's cost of capital, will be additional useful knowledge given that the timeline of investment is considered. If you would like assistance with this, let me know

How is the 'Tour of Roads' coming together?

Thanks,

[REDACTED]

From: [REDACTED]
Sent: May-13-13 13:36
To: Ganesh, Steve
Cc: [REDACTED]

[REDACTED]
Dela Cruz, Gino; Gan, Tyrone; 'Restrepo, Veronica'; 'Susan Cumming'; Saddi, Asha; [REDACTED]

Subject: Re: EA Update

I respect your preference as to how you would like to handle my request and I do understand why you have handled them in this manner, however I would still like to know what the costs have been to maintain Winston Churchill Blvd and Olde Base line (within the boundaries to which we have been referring) for the last 15 years.

Thank you,

[REDACTED]
From: [Ganesh, Steve](#)

Sent: Monday, May 13, 2013 12:45 PM

To: [REDACTED]

Cc: [REDACTED]

[REDACTED]; [Dela Cruz, Gino](#) ; [Gan, Tyrone](#) ; ['Restrepo, Veronica'](#) ; ['Susan Cumming'](#) ; [Saddi, Asha](#) ; [REDACTED]

Subject: EA Update

Hello [REDACTED]

In speaking with Asha I understand you have some questions regarding the Region's maintenance and operational costs for the roads within the EA study limits. A similar request was made by [REDACTED] at last Thursday's meeting. To best handle this request I would prefer to discuss and present scenarios that show:

1. The condition of the roads in the study area today versus the Region's service level for all roads within Peel
2. The condition and cost if the Region continues to "patch" the roads (Do Nothing Approach)
3. The condition and cost if the Region undertakes reconstruction and rehabilitation

As a quick update our analysis shows that if the roads remain in their current condition (Option 2), the base structure will continue to deteriorate and the cost to maintain a state of good repair to the Regional standards will be approximately \$225/m² versus \$65/m² to undertake the proper works and bring the roads to standard (Option 3). Some of this information is shown on Slide 37 from the PIC material.

With respect to design standards for the roads, we are now moving into this stage of the EA and we will bring forward the standards we use to develop cross-sections to the roads. However Gino, Asha and I are open to meeting with you and [REDACTED] to review the typical standards that we would use in developing a rural cross section for a regional road.

Any information we provide to you will be shared with the rest of the CWG members. In the meantime should you wish to meet to discuss the Region's asset management approach and design standards please let me know and we can arrange.

Best Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Transportation Program Planning
Region of Peel
Public Works
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: [REDACTED]
Sent: May 14, 2013 6:46 PM
To: Dela Cruz, Gino
Cc: Tyrone.Gan@hdrinc.com
Subject: Mississauga Rd,Old Main,Bush Street,Winston Churchill &Olde Base Line 'EA'

Please include my comments in the study:

I travel Mississauga Road to Olde Base Line every day. I would like to see a report on the collisions at this intersection,

as I have never seen one in the 16 years I have travelled it.

I feel that installing any more roundabouts would be a waste of money, and I do not feel they are safe.

I would like to know the cost of the one that went in at Dixie & Olde Base Line.

You could reduce the speed on Mississauga Road, but drivers do not observe the current one.

Many cars pass me doing highway speeds.

This is a country road so I do not want to see paved shoulders or curbs, or guard rails were they are not warranted.

I also don't support putting sidewalks in Belfountain.

- Thank you

[REDACTED]
[REDACTED]
[REDACTED]

Kong, Carol

From: Gan, Tyrone
Sent: Thursday, May 23, 2013 6:58 AM
To: Chow, Tavia
Subject: FW: Environmental Assessment PIC #1
Attachments: Peel Environmental Assessment PIC#1..pdf

Follow Up Flag: Follow up
Flag Status: Completed

TYRONE GAN
P. Eng.

HDR Corporation
Senior Vice President
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From: Saddi, Asha [<mailto:Asha.Saddi@peelregion.ca>]
Sent: Wednesday, May 22, 2013 1:24 PM
To: [REDACTED]
Cc: Dela Cruz, Gino; Gan, Tyrone; 'Susan Cumming'
Subject: FW: Environmental Assessment PIC #1

Hello [REDACTED]

Thank you for your email and attachment.

Your comments have been forwarded to the project team for consideration and will be included in the Facilitator's Feedback Report for the May 9 PIC meeting.

Asha Saddi
Technical Analyst
Transportation Program Planning
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax: 905-791-1442
saddia@peelregion.ca

From: [REDACTED]
Sent: May 21, 2013 7:41 PM

To: Saddi, Asha

Cc: Dela Cruz, Gino; 'Tyrone.Gan@hdrinc.com'

Subject: Environmental Assessment PIC #1

Please consider my attached letter regarding the Environmental Assessment of Mississauga Road, Bush Street, Winston Churchill Blvd and Olde Base Line Road.

██████████
████████████████████

May 21, 2013

Ms. Asha Saddi
Peel Region Technical Analyst
Transportation Division/Public Works
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, Ontario L6T 4B9

**RE: Environmental Assessment PIC #1
Mississauga Road, Bush Street, Winston Churchill Blvd. and Olde Base Line**

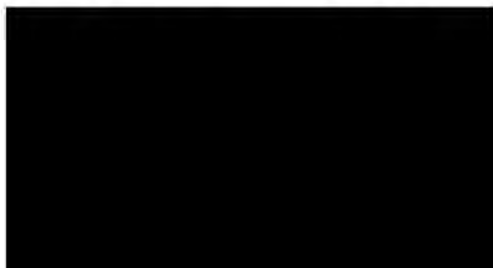
Dear Asha,

I live on Winston Churchill Blvd. within the study area. I would like to see the improvement of these roads. I firmly believe that the hummocky terrain along the area roads should be graded to allow for safe sight distances. I know of at least two major accidents that have occurred due to the sight distance issue along Olde Base Line Road.

One involved an off duty police officer coming over one of the hills and having to ditch his car due to children crossing the road to a school bus. This occurred at 262 Olde Base Line Road. Clearly this could have been a tragedy and can easily be avoided in future by properly reconstructing the road.

I believe that the area needs a higher level of service route, one that accommodates all vehicles including trucks. This should be Olde Base Line Road and Winston Churchill Road.

Thank you for considering my input,



cc.

Gino.DelaCruz@peelregion.ca

Asha.Saddi@peelregion.ca

Tyrone.Gan@hdrinc.com

Kong, Carol

From: Gan, Tyrone
Sent: Thursday, May 23, 2013 6:47 AM
To: [REDACTED]
Cc: [REDACTED]
Subject: RE: Belfountain EA

Follow Up Flag: Follow up
Flag Status: Completed

[REDACTED]

We are still responding to public comments from a recent open house.
We will be in touch with you when we start to develop alternative designs over the next several months.

Tyrone

TYRONE GAN
P. Eng.

HDR Corporation
Senior Vice President
Director of Transportation, Canada

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From: [REDACTED]
Sent: Wednesday, May 22, 2013 5:49 PM
To: Gan, Tyrone; Restrepo, Veronica
Cc: [REDACTED]
Subject: Belfountain EA

Tyrone or Veronica,
Could you update us on the project?
Thanks.

[REDACTED]

From: [REDACTED]
Sent: May-22-13 5:42 PM
To: 'Baudais, Nathalie'
Cc: [REDACTED]
Subject: Belfountain EA

Nathalie,

We have not heard anything concerning the Belfountain EA project in a while. Can you update us?

We are keen on providing our input.
Thanks.



From: Baudais, Nathalie [<mailto:Nathalie.Baudais@hdrinc.com>]

Sent: March-19-13 12:44 AM

To: [Redacted]

Cc: Gan, Tyrone; Restrepo, Veronica

Subject: RE: Belfountain EA

Hello Everyone,



We are busy preparing for a Technical Advisory Committee meeting, Community Working Group meeting and Public Information Centre and will be presenting many of your findings to these groups (existing conditions/problems/opportunities).

Please keep in close contact with Tyrone and Veronica during my absence.

Tyrone.gan@hdrinc.com

Veronica.restrepo@hdrinc.com

Regards,
Nathalie

NATHALIE BAUDAIS
P.ENG., P.E.

HDR Corporation
Project Engineer

PLEASE NOTE THE CHANGE IN MY CONTACT INFORMATION

4838 Richard Road SW Suite 140 | Calgary | AB | T3E 6L1
(647) 268-3771

Nathalie.Baudais@hdrinc.com | hdrinc.com

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Kong, Carol

From: Gan, Tyrone
Sent: Saturday, May 25, 2013 4:01 PM
To: cumming1@total.net; Chow, Tavia
Subject: FW: Peel Environmental Assesement near Belfountain

Follow Up Flag: Follow up
Flag Status: Flagged

TYRONE GAN
P. Eng.

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From: [REDACTED]
Sent: Thursday, May 23, 2013 10:03 PM
To: Gino.DelaCruz@peelregion.ca
Cc: Asha.Saddi@peelregion.ca; Gan, Tyrone
Subject: Re: Peel Environmental Assesement near Belfountain

May 22, 2013

Mr. Gino Dela Cruz
Peel Region Project Manager
Transportation Division
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, Ontario
L6T 4B9

Gino.DelaCruz@peelregion.ca
Asha.Saddi@peelregion.ca
Tyrone.Gan@hdrinc.com

RE: Enironmental Assessment of Mississauga Road, Bush Street, Winston Churchill Blvd. and Olde Base Line

Dear Mr. Dela Cruz,

I am a resident who lives in the area being studied. I use these roads on a daily basis. I have the following comments on the Environmental Assessment for the above roads:

- There are serious safety concerns regarding sight lines in the study area. Some of the hills should be smoothed out so that appropriate sight distances can be achieved.
- I am supportive of the utilization of Winston Churchill Blvd. and Olde Base Line Road as truck routes. This will keep trucks out of Belfountain and provide a suitable route through this area.
- Provisions should be made to keep cyclists off the traveled portions of the road perhaps by paving the shoulders.

Thanks,



Kong, Carol

From: Gan, Tyrone
Sent: Friday, May 24, 2013 11:04 AM
To: Chow, Tavia
Subject: FW: EA Assessment in Caledon

Follow Up Flag: Follow up
Flag Status: Flagged

TYRONE GAN
P. Eng.

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From: Dela Cruz, Gino [<mailto:Gino.DelaCruz@peelregion.ca>]
Sent: Friday, May 24, 2013 8:46 AM
To: [REDACTED]
Cc: Saddi, Asha; Gan, Tyrone
Subject: RE: EA Assessment in Caledon

Good morning [REDACTED]

Thank you for providing your comments from the PIC held on May 9th. Please note that these comments will be included in the Facilitators Feedback Report from the May 9th Public Meeting and considered by the Project Team.

If you would like to discuss your comments further, please give me a call at the number noted below.

Best Regards,

P. Gino Dela Cruz, P.Eng.
Project Manager, Transportation Program Planning
Region of Peel, Public Works
10 Peel Centre Dr., Suite B 4th Floor
Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: May 23, 2013 9:58 PM
To: Dela Cruz, Gino
Cc: Saddi, Asha; Tyrone.Gan@hdrinc.com
Subject: Re: EA Assessment in Caledon

May 22, 2013

Mr. Gino Dela Cruz
Peel Region Project Manager/ Public Works
Transportation Division
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, Ontario L6T 4B9

RE: Environmental Assessment of Mississauga Road, Bush Street, Winston Churchill Blvd. and Olde Base Line

Dear Mr. Dela Cruz:

I live in the area being studied. I am glad that the road network is finally being upgraded to proper standards. I have several suggestions that I would appreciate being taken into account as you plan these road upgrades.

1. Please fix the hilly areas where sight lines are cut down to an unsafe distance, and thrill seekers are more likely to drive above the set speed limits.
2. Please upgrade Olde Base Line, Winston Churchill Blvd and Mississauga Road south of Belfountain, so they are safe, smooth, and wide enough to be used as the designated Primary Truck Route.
3. Please consider intersection improvements to The Grange Sideroad and Mississauga Road where a hill on The Grange Sideroad slopes steeply toward Mississauga Road. Attempting to stop before sliding onto Mississauga Road during winter driving conditions is rather treacherous.

Yours Truly,

██████████

Gino.DelaCruz@peelregion.ca
Asha.Saddi@peelregion.ca
Tyrone.Gan@hdrinc.com

Kong, Carol

From: Gan, Tyrone
Sent: Saturday, May 25, 2013 4:00 PM
To: [REDACTED]
Cc: Gino.DelaCruz@peelregion.ca; Asha.Saddi@peelregion.ca; cumming1@total.net; Chow, Tavia
Subject: RE: Belfountain area EA Assessment

Follow Up Flag: Follow up
Flag Status: Flagged

Dear [REDACTED]

Thank you for your comments and bringing up your concerns. We will consider your them as we proceed with the study.

Please feel free to contact our team at any time.

Regards,

TYRONE GAN
P. Eng.

HDR Corporation
Senior Vice President
Director of Transportation, Canada

100 York Boulevard, Suite 300 | Richmond Hill, ON L4B 1J8
t: **289.695.4622** | f: 905.882.1557
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From: [REDACTED]
Sent: Thursday, May 23, 2013 10:07 PM
To: Gan, Tyrone
Cc: Gino.DelaCruz@peelregion.ca; Asha.Saddi@peelregion.ca
Subject: Re: Belfountain area EA Assessment

May 21, 2013

Mr. Tyrone Gan
HDR Project Manger
100 York Boulevard, Suite 300
Richmond Hill, ON
L4B 1J8
Tyrone.Gan@hdrinc.com

RE:PIC #1 Mississauga Road, Bush Street, Winston Churchill Blvd. and Olde Base Line

Dear Mr. Gan,

I am a new driver who uses these roads on a daily basis. My grandparents also live in the area. There are some hills in the area that are too steep to see over. I sometimes ride my bike on these roads and it is dangerous to ride on those sections.

I think that it would be a good idea to make a truck route on roads that are away from Belfountain Village. Winston Churchill Blvd and Olde Base Line make sense to me for this purpose.

████████████████████
██

Cc Gino.DelaCruz@peelregion.ca
Asha.Saddi@peelregion.ca

Kong, Carol

From: Gan, Tyrone
Sent: Thursday, May 23, 2013 11:26 AM
To: Chow, Tavia
Subject: FW: Feedback - Public Info Session May 9 2013

Follow Up Flag: Follow up
Flag Status: Completed

TYRONE GAN
P. Eng.

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From: Dela Cruz, Gino [<mailto:Gino.DelaCruz@peelregion.ca>]
Sent: Thursday, May 23, 2013 11:25 AM
To: [REDACTED] Saddi, Asha; Gan, Tyrone
Cc: [REDACTED] Ganesh, Steve;
cumming1@total.net
Subject: RE: Feedback - Public Info Session May 9 2013

Good morning [REDACTED]

Thank you for providing your comments from the PIC held on May 9th. Please note that these comments will be included in the Facilitators Feedback Report from the May 9th Public Meeting and considered by the Project Team.

If you would like to discuss your comments further, please give me a call at the number noted below.

Best Regards,

P. Gino Dela Cruz, P.Eng.
Project Manager, Transportation Program Planning
Region of Peel, Public Works
10 Peel Centre Dr., Suite B 4th Floor
Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: May 22, 2013 5:21 PM
To: Dela Cruz, Gino; Saddi, Asha; Tyrone.Gan@hdrinc.com

Subject: Feedback - Public Info Session May 9 2013

Re: Environmental Assessment Study Public Information May 9th 2013 – Mississauga Rd, Olde Base Line Rd, Bush St, Old Main St, Winston Churchill Blvd.

Overall

I was not at all happy with the responses provided by the presenters to the comments and questions from the public. It appeared that no matter what issues were raised; many of the responses were justifications as to why the so called “experts” were right and the public view point was flawed. I got the feeling that, although the public was heard, no one was listening to the point of understanding or caring about what the public opinion was. Is this just an exercise in placating the residents with an “opportunity” to speak and some small gestures of minor amendments to what the so called ‘experts’ will decide anyway?

Specifically

The presenters indicated that traffic volume had remained steady however, some of the volume had transferred from Mississauga to Winston Churchill; this is true, I live on Winston Churchill and I have witnessed this. The reason for the shift is because Winston Churchill was paved over the gravel.

I understand the need to upgrade the road, it’s in poor condition and will get worse.

I am in favour of paving the shoulder for cycle and pedestrian use.

This was the first time I recall any mention of Winston Churchill Boulevard and Olde Base Line being developed as a truck route; this answers a lot of questions about why there is all the fuss about sight lines and flattening parts of the road!

I have no objection to increased commuter traffic, but object strongly to creating a truck route.

An Old Base Line/Winston Churchill truck route makes absolutely no sense at all when you consider the “Principles for Guiding Solutions” outlined in the presentations including:

- Maintain the rural character and countryside scenic quality
- Preserve historic fences and heritage/culture/archaeological features
- Preserve /enhance the natural environment
- Protect the Niagara Escarpment
- Balance interests and meet the needs of all road users
- Provide a context sensitive design
- Enhance local tourism and economic development

Good East/West truck routes already exist at King Street and Charleston Side Road (124) both leading to Hwy 10.

I saw no evidence of the need to create additional truck routes through this supposedly protected area and, I thought we had been through all the reasons this area needs protecting from trucks during the James Dick quarry battle

There are already too many trucks using Winston Churchill illegally, creating noise, vibration and air pollution. In truth we were better off when the road was gravel and quiet!

Please do all in your power to avoid an unnecessary truck route.

[REDACTED]
[REDACTED]
[REDACTED]

Kong, Carol

From: Gan, Tyrone
Sent: Saturday, May 25, 2013 4:37 PM
To: [REDACTED]
Cc: Gino.DelaCruz@peelregion.ca; Asha.Saddi@peelregion.ca; cumming1@total.net; Chow, Tavia
Subject: RE: Environmental Assessment Study -comments from [REDACTED] Mississauga Rd Resident

Follow Up Flag: Follow up
Flag Status: Flagged

Dear [REDACTED]

Thank you for your comments. We understand and appreciate your concerns, and they will be considered as we proceed with the study.

Please feel free to contact us if you have any questions.

Regards,

Tyrone

TYRONE GAN
P. Eng.

HDR Corporation
Senior Vice President
Director of Transportation, Canada

100 York Boulevard, Suite 300 | Richmond Hill, ON L4B 1J8
t: **289.695.4622** | f: 905.882.1557
Tyrone.Gan@hdrinc.com | hdrinc.com

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Note: Effective March 17th, my phone number changed. The direct dial number is 289.695.4622

From: [REDACTED]
Sent: Thursday, May 23, 2013 12:09 PM
To: Gan, Tyrone
Cc: Gino.DelaCruz@peelregion.ca; Asha.Saddi@peelregion.ca
Subject: Environmental Assessment Study -comments from [REDACTED] Mississauga Rd Resident

May 19, 2013

Mr. Tyrone Gan
HDR Project Manger
100 York Boulevard, Suite 300
Richmond Hill, ON
L4B 1J8
Tyrone.Gan@hdrinc.com

RE: PIC #1 Mississauga Road, Bush Street, Winston Churchill Blvd. and Olde Base Line

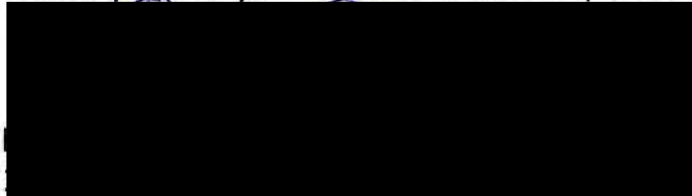
Dear Tyrone,

Thanks for your presentation the other night. My message is simple, do the responsible thing and fix the roads.

Having an efficient route for all road users pays dividends for our society. The true environmental costs come when commuters and local goods are diverted miles out of their way resulting in millions of tons of unnecessary gas emissions. More important to me is the cost in lives. Every extraneous mile travelled will statistically translate into loss of life. It is also fact that unsafe road conditions inevitably result in loss of life. As such I whole heartedly support the improvements you are making to the road network.

These roads are not the private driveways for a few select residents. They are part of a network that serves, really, all of Ontario. Having a strong economy allows us to enjoy the lives we do. With the coming population to the south it is important to put in place a safe and efficient road network that serves everyone.

Thank you for your efforts in this important issue,



Cc Gino.DelaCruz@peelregion.ca
Asha.Saddi@peelregion.ca

Kong, Carol

From: Gan, Tyrone
Sent: Thursday, May 30, 2013 4:04 AM
To: Restrepo, Veronica
Subject: FW: May 9: Public Information Centre #1

From: [REDACTED]
Sent: Wednesday, May 29, 2013 8:05 PM
To: [REDACTED] Ganesh, Steve; [REDACTED]
Cc: Gan, Tyrone; Dela Cruz, Gino; cumming1@total.net; Saddi, Asha; [REDACTED]
Subject: Re: May 9: Public Information Centre #1

One more thing. I would choose my words a but more carefully. If I were to interpret a lack of response as non interest

Sent from my BlackBerry 10 smartphone on the TELUS network.

From: [REDACTED]
Sent: Wednesday, May 29, 2013 7:44 PM
To: [REDACTED] Ganesh, Steve; [REDACTED]
Cc: tyrone.gan@hdrinc.com; Dela Cruz, Gino; cumming1@total.net; Saddi, Asha; [REDACTED]
Subject: Re: May 9: Public Information Centre #1

Please do not interpret my lack of time as lack of interest. Believe me, if I were not interested I would not be emailing you with questions and emailing you again for the answers.

You have consistently invited me to come to your office to discuss the numbers that I question. Which must mean that I have misinterpreted them. This may be very well the case. If so, it would be very easy to explain it in an email.

[REDACTED]
[REDACTED]
[REDACTED] I simply can't find the time to come and see you.

I believe the questions I asked regarding traffic flow were valid concerns. As confirmed by Tyrone both sets if numbers were accurate. This obviously means there is a decrease in traffic during peak hours.

All this means is that when I have time I will look at your material. It does not mean I am not interested.

Sent from my BlackBerry 10 smartphone on the TELUS network.

From: [REDACTED]
Sent: Wednesday, May 29, 2013 4:21 PM
To: 'Ganesh, Steve'; [REDACTED]
Cc: Gan, Tyrone; 'Dela Cruz, Gino'; cumming1@total.net; 'Saddi, Asha'; [REDACTED]
Subject: RE: May 9: Public Information Centre #1

I do not know if you are or not, but please do not suggest the lack of a face-to-face meeting as any indication of diminished interest in what has been asked for. Not my case. It's an indication of busy people who have a lot on the go. You know what they say, give a job to a busy person, and it'll get done. In this situation, it's receiving the information that's more critical, not necessarily the way it's conveyed.

It was the original intent that all CWG members see the output of the requests I made. I cannot speak for [REDACTED] but suspect same intent.

[REDACTED]

From: Ganesh, Steve [<mailto:Steve.Ganesh@peelregion.ca>]
Sent: May-29-13 15:02
To: 'Penny Richardson'; 'David Jobe'
Cc: tyrone.gan@hdrinc.com; Dela Cruz, Gino; cumming1@total.net; Saddi, Asha
Subject: FW: May 9: Public Information Centre #1

Hi [REDACTED]

It appears that a meeting to present and discuss the material is no longer of interest. Accordingly the material that we are able to pull together will be sent as part of the facilitators notes so all CWG members get the same information. I am not sure how far back our records for Operations and Maintenance go in terms of years, but we will distribute what figures we can get. In addition we will present the asset management/life cycle costing analysis for all four roads in comparison to the Regional Roads and we will suggest some areas that you may wish to visit.

Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Transportation Program Planning
Region of Peel
Public Works
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: Ganesh, Steve [<mailto:Steve.Ganesh@peelregion.ca>]

Sent: Wednesday, May 29, 2013 9:51 AM

To: [REDACTED]

Cc: Saddi, Asha; Dela Cruz, Gino; cumming1@total.net; Gan, Tyrone; [REDACTED]

Subject: RE: May 9: Public Information Centre #1

Good Morning [REDACTED]

Not a problem. I will send you the same material I provide [REDACTED] I will send via email and if for some reason the file sizes are large I will arrange for it to be couriered to your home.

Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Transportation Program Planning
Region of Peel
Public Works
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: [REDACTED]

Sent: May 29, 2013 9:47 AM

To: Ganesh, Steve; [REDACTED]

Cc: Saddi, Asha; Dela Cruz, Gino; cumming1@total.net; tyrone.gan@hdrinc.com; [REDACTED]

Subject: RE: May 9: Public Information Centre #1

Hello Steve;

Thank you for your offer to meet in person. Although generally very effective as a mechanism to share information and knowledge, I don't think a face-to-face is necessary in this case. The tour of roads and business case ought to be small enough to send over e-mail even to my slow account. An excerpt from the road standard document ought to suffice as well. If I have questions, I hope I can call you instead.

I am not to Brampton often during daytime working hours.

Thanks,

[REDACTED]

From: [REDACTED]
Sent: May 29, 2013 11:23 AM
To: Ganesh, Steve; [REDACTED]
Cc: [REDACTED] Nesbitt, Richard; Dela Cruz, Gino
Subject: Re: May 9: Public Information Centre #1

Sorry [REDACTED] I think I may have misread the message. I do not need to meet with Steve. I would like to still receive the maintenance figures for Winston Churchill over the last 15 years if possible. This would include the hard topping of WC in the past 10 years.
Many thks
[REDACTED]

From: [Ganesh, Steve](#)
Sent: Wednesday, May 29, 2013 9:36 AM
To: [REDACTED]
Cc: [REDACTED] ; [Nesbitt, Richard](#) ; [Dela Cruz, Gino](#)
Subject: FW: May 9: Public Information Centre #1

Hi [REDACTED]

Could you please work with [REDACTED] to find a time in the next week or so to meet with me, Gino and Rick Nesbitt.

Perhaps 1 hour at our office.

Thanks,
Steve

Steve Ganesh, MCIP, RPP
Manager, Transportation Program Planning
Region of Peel
Public Works
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: [REDACTED]
Sent: May 29, 2013 9:04 AM
To: Ganesh, Steve
Subject: Re: May 9: Public Information Centre #1

Yes very interested! Monday and Thurs evening are usually the best, however since I work at home I can juggle "stuff" and get out thru the day....however I doubt others can
[REDACTED]

From: [Ganesh, Steve](#)
Sent: Tuesday, May 28, 2013 3:36 PM
To: [REDACTED]
Cc: [Saddi, Asha](#) ; [Dela Cruz, Gino](#) ; <mailto:cumming1@total.net> ; tyrone.gan@hdrinc.com
Subject: RE: May 9: Public Information Centre #1

Hello [REDACTED]

I hope all is well with you. I am just following-up to our previous discussions and emails regarding your interest in the asset management/life cycle information we have developed for the roads in the EA. Are you still interested in meeting with the team to discuss this information. If so please let me know some dates and I will arrange.

Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Transportation Program Planning
Region of Peel
Public Works
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: [REDACTED]
Sent: May 28, 2013 3:31 PM
To: [REDACTED] cumming1@total.net; Saddi, Asha; [REDACTED]
[REDACTED]
[REDACTED]
Cc: Ganesh, Steve; Dela Cruz, Gino; 'Susan Cumming'; 'Gan, Tyrone'; 'Restrepo, Veronica'
Subject: RE: May 9: Public Information Centre #1

Good reminder [REDACTED] ...thank you

Sue, if you would please add

- the tour of roads
- the business case
- maintenance costs

to your request list, that would be great.

[REDACTED]

From: [REDACTED]
Sent: May-28-13 12:01
To: cumming1@total.net; Saddi, Asha; [REDACTED]
[REDACTED]
[REDACTED]
Cc: Ganesh, Steve; Dela Cruz, Gino; Susan Cumming; Gan, Tyrone; Restrepo, Veronica
Subject: RE: May 9: Public Information Centre #1

Thank you Sue. Can you give me a time frame of when these questions might be addressed?

[REDACTED]



From: Sue Cumming [<mailto:cumming1@total.net>]

Sent: May-14-13 12:39 PM

To: [REDACTED]; 'Saddi, Asha'; [REDACTED]

Cc: 'Ganesh, Steve'; 'Dela Cruz, Gino'; 'Susan Cumming'; 'Gan, Tyrone'; 'Restrepo, Veronica'

Subject: RE: May 9: Public Information Centre #1

Dear [REDACTED]

Thank you for forwarding your additional comments from the PIC held on May 9th. Please note that these comments will be included in the Facilitators Feedback Report from the May 9th Public Meeting and considered by the Project Team.

Sincerely,

Sue Cumming, MCIP RPP

Facilitator

Cumming+Company

866 611-3715

From: [REDACTED]

Sent: May-11-13 10:09 AM

To: Saddi, Asha; [REDACTED]

Cc: Ganesh, Steve; Dela Cruz, Gino; Susan Cumming; Gan, Tyrone; Restrepo, Veronica

Subject: RE: May 9: Public Information Centre #1

I would like to congratulate all of you for an excellent meeting on May 9th, 2013. I believe there was good discussion and a lot of points were cleared up. I did not have an opportunity to complete my statements so I thought I would share them with you now.

I would personally like to thank Tyrone for the answers to some of my questions regarding the data for the traffic studies. I was concerned that 1 study indicated that the AADT was close to 726 vehicles and the other indicated 3450 vehicles. It was pointed out that I was incorrect in interpreting these numbers. Thank you again for showing me that the 726 vehicles was peak AM and PM only and not daily traffic.

By your confirmation that both of these studies are in fact correct, it brings up more questions and concerns. If we assume that peak AM and PM traffic is 6-9 AM and 4-7 PM respectively, that is 6 hours in total. This means that there is an average of 726/6 or 121 vehicles/hr. This seems reasonable. This leaves ~2700 vehicles for the rest of the day. Now those of us that live in the area will recognize that there is virtually no traffic overnight, but for the benefit of doubt let's assume that the rest of the traffic is distributed throughout the rest of the day. This would mean that there are 2750 vehicles in 18 hours or 151 vehicles/hour. Is it plausible that this humble area is the only area in the entire world that has a higher hourly traffic rate during non peak hours than peak hours?

Tyrone, you also mentioned that you have an obligation to hold public Safety Paramount. I can certainly sympathize with you. You are in a difficult position as you are getting the numbers from ROP, but as a traffic

engineer you surely cannot believe these numbers to be accurate. As a traffic engineer, you certainly cannot believe that turning Winston Churchill and Olde Base Line Rd into truck routes will improve the safety of the road. I felt you were sincere when you spoke about how you would feel if you did nothing and someone in the future was hurt or killed in an accident in this area. The reciprocal is also true, what if you completed your task here and there are more accidents?

I have made a few assumptions in my calculations. I apologize if they are incorrect. If my numbers are flawed, I would welcome any input that can rectify them. It seems the ROP is transparent in this Assessment, as none of my questions are being answered.

It is apparent that the facts and figures provided by ROP are flawed, incomplete, and misleading. It is also apparent that these facts and figures are endorsed by HDR with little to no validation or verification. One can only conclude that there is a hidden agenda that we are not aware of yet. I for one would appreciate a direct and honest approach. ROP has to stop being transparent and state what they want and why they want it and stop pretending they are not influencing the process.

[REDACTED]

From: Saddi, Asha [<mailto:Asha.Saddi@peelregion.ca>]

Sent: April-09-13 1:46 PM

To: [REDACTED]

Cc: Ganesh, Steve; Dela Cruz, Gino; 'Susan Cumming'; 'Gan, Tyrone'; 'Restrepo, Veronica'

Subject: May 9: Public Information Centre #1

Hello CWG Members,

This is to confirm that the Public Information Centre (PIC) will be held on May 9 at the Belfountain Public School (17247 Shaws Creek Road, Caledon) from 6:30 p.m. – 9:00 p.m.

There will be:

- ? Arrival and Open House from 6:30 p.m. – 7:00 p.m.
- ? Presentation and Discussion from 7:00 p.m. – 9:00 p.m.

You are encouraged to attend the PIC to hear the discussion and people's perspective and opinions relating to the problem statement and preliminary alternative solutions. During the Open House, you may want to assist

members of the public in reviewing information and direct people to specific Project Team members who will be available to respond to questions.

Attached for your information is a one page overview for the PIC which includes the timing, format, set-up and communications tools. If you are able to attend, you may want to arrive just before or at 6:30 p.m.

We look forward to meeting you all at the PIC. In the meantime if you have any questions please don't hesitate to contact me.

Thanks,

***Asha Saddi
Technical Analyst
Transportation Program Planning
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
saddia@peelregion.ca***

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Kong, Carol

From: Gan, Tyrone
Sent: Saturday, June 08, 2013 8:17 AM
To: Restrepo, Veronica
Subject: FW: Feedback Report

From: [REDACTED]
Sent: Friday, June 07, 2013 1:08 PM
To: 'Saddi, Asha'; [REDACTED]
[REDACTED]
Cc: 'Ganesh, Steve'; 'Dela Cruz, Gino'; Gan, Tyrone; 'Susan Cumming'; Paterak, Richard
Subject: RE: Feedback Report

Thank you for this. Please ensure our Regional Councillor Richard Paterak, receives this, attachments and all. Do Not copy me on the e-mail; one 6 MG e-mail is enough!

From: Saddi, Asha [<mailto:Asha.Saddi@peelregion.ca>]
Sent: June-07-13 12:03
To: [REDACTED]
[REDACTED]
Cc: Ganesh, Steve; Dela Cruz, Gino; 'Gan, Tyrone'; 'Susan Cumming'
Subject: Feedback Report

Hello CWG Members,

Thank you to many of you who took the time to attend the first Public Information Centre (PIC) on May 9, 2013. Attached for your information is the Feedback Report to the meeting which includes the details of the open discussion and the input received through the completed Feedback Forms, letters and emails.

Area residents and additional stakeholders who attended the PIC have been notified of the availability of the report on the project study website at: <http://www.peelregion.ca/pw/transportation/enviro-assess/mississauga-road-bush.htm> and copies of the Report are also available for viewing at the Belfountain Public Library.

In addition to the Feedback Report, this email contains five additional pieces of information for your information:

- 1. Meeting with [REDACTED] to discuss the EA and the SGMNS**
[REDACTED] expressed an interest in discussing the Strategic Goods Movement Network Study (SGMNS) and its impact on this EA. They were invited to meet and discuss their concerns with Sabbir Saiyed, Manager, Transportation System Planning, Steve and Gino. To keep you all informed and aware of the information exchanged, attached are the notes of the meeting held last Friday, May 31, 2013.
- 2. Asset Management**

Also attached is an explanation of our road conditions and asset management approach. A graph provided shows that the existing and projected conditions of the roadways are below the network average and in the next 3-5 years are likely to fall below the standard level of service. Allowing roads to fall to poor conditions will mean the cost of rehabilitation could be at least 5-8 times more expensive.

Another graph provided shows an average of the projected condition of the roadways and a comparison of two scenarios: "Do Nothing" but patch, and, "Maintain Good Repair".

3. Maintenance Records

We have received the available maintenance records from our Operations group which are attached for your review.

As previously mentioned, cost is only one of several factors being considered when determining the need to improve the roadways in the study area. Other factors include addressing safety, pavement and drainage deficiencies to ensure Regional roads are up to standard and in a state of good repair for both residents and the travelling public.

4. TAC Guidelines

The road standards that the Region of Peel uses for the roadways are based on the design from the Transportation Association of Canada (TAC) manual "Geometric Design Guide for Canadian Roads". The TAC guidelines provide standards for the design of the roadway, including horizontal and vertical alignments, lane and shoulder widths, and other cross-sectional elements to ensure the roadways function safely. The link to the TAC website is attached:

<https://onlineservices.tac-atc.ca/English/bookstore/products.cfm?catid=9&subcatid=18&prodid=54>

5. Site Visits

Some of you have expressed an interest in reviewing some road improvement projects that you can visit on your own. Attached are two examples that you may want to review:

1. Olde Base Line (McLaughlin to Hurontario) – shows paved shoulders and urban cross-sections where the right-of-way is constrained, channelized right-turn intersection treatment with landscape features at McLaughlin Road, and,
2. Winston Churchill Blvd (south of Olde Base Line to 1.2km south of Ballinafad Road) – detailed design is just starting up but we do have the preliminary plan and profile from the approved EA study. The plans show the proposed changes to the profile, rural cross-section with sections of urban to avoid constrained areas, accommodation of natural, heritage and terrestrial environment, and minimal property requirements.

If you have any questions on the information provided please don't hesitate to contact our Project Manager, Gino Dela Cruz, at gino.delacruz@peelregion.ca or myself.

Thanks,

***Asha Saddi
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
asha.saddi@peelregion.ca***

INFORMATION SHEET

Mississauga Road, Bush Street, Old Main Street, Winston Churchill Boulevard and Olde Base Line Road Environmental Assessment

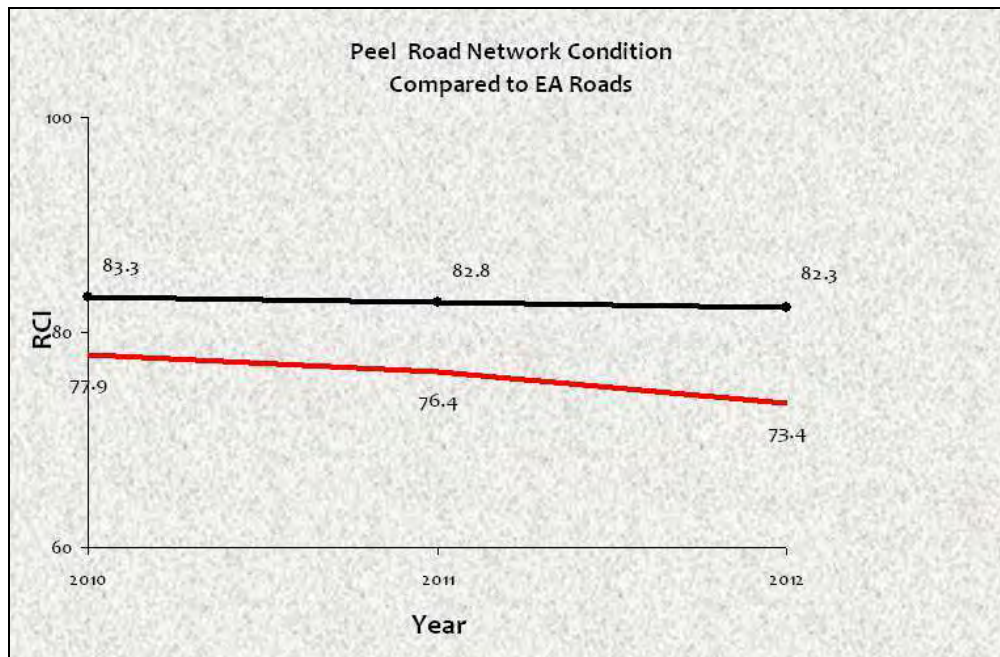
Road Conditions and Asset Management Approach

The Region of Peel undertakes pavement condition surveys every three years to evaluate the conditions based on surface distresses.

The existing and projected conditions of the roads in the study area are shown in the graph below. The timeframe shown is from 2010 to 2012 and the measure used to capture the road condition is the Ride Condition Index (RCI).

The RCI is a quantitative number that represents the overall condition and quality of the Regional road network. For example, the RCI combines the rating of many different types of roads defects including cracking, rutting, potholes and surface quality into one measurable number.

The graph shows that the study area roads are far below the network average, deteriorating faster than the network average and will likely fall below the standard level of service in the next 3-5 years.



Legend: Black line represents projected network RCI for all Regional Roads.
Red line represents projected RCI for the roads in the study area

Source: Region of Peel Data

A RCI rating of 70-100 on a road section represents a road in a state of good repair.

An RCI rating below 70 represents a road that is getting closer to the rehabilitation zone. The Regional strategy is not to let roads fall into disrepair as they become more expensive to maintain and rehabilitate.

In the asset management industry a well known phrase is “good roads cost less to maintain”. The Region of Peel has completed a detailed analysis of different budget scenarios and has been able to quantify this fact, showing that by letting roads slip into poor condition, where moisture enters the base material, the cost of rehabilitation can be 5 - 8 times as expensive. The result is that more money is spent for equivalent or lesser results in terms of road condition and safety.

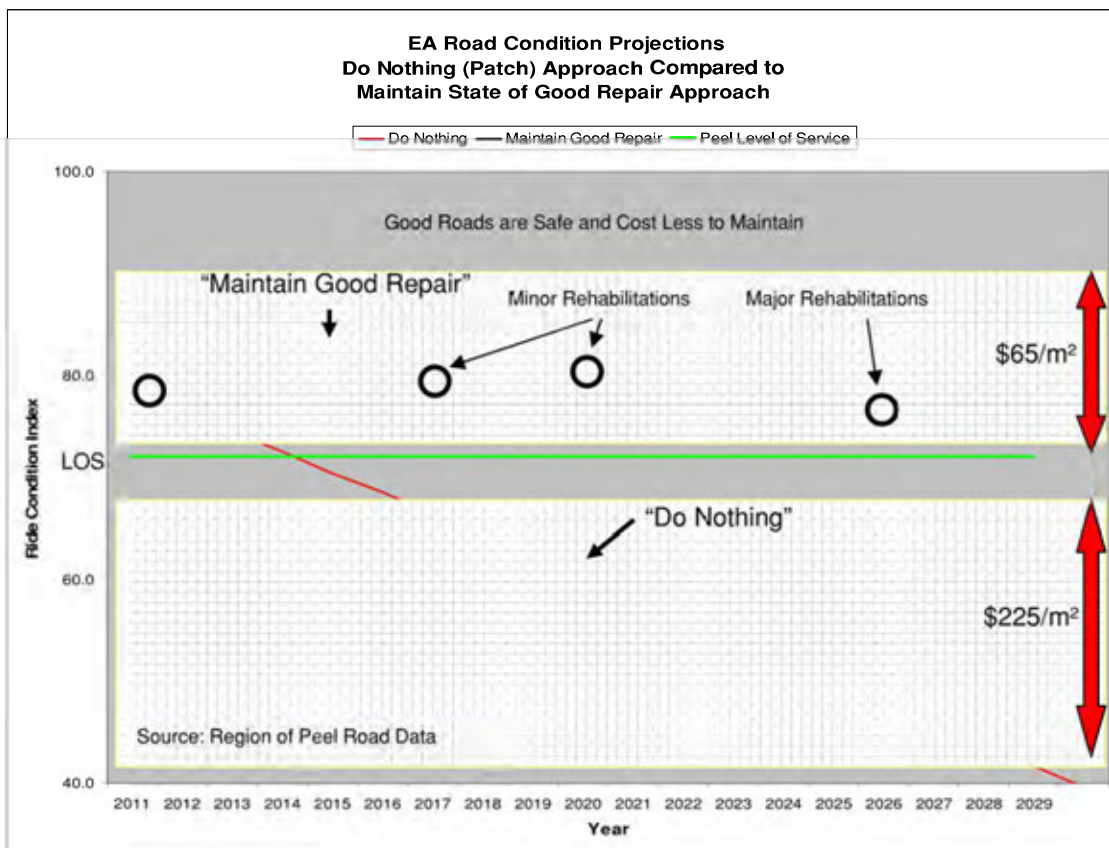
The Study Area Roads: Do Nothing versus Maintain Good Repair

The graph below illustrates an average of the projected condition of the study area roads and a comparison of two scenarios:

1. “Do Nothing” but patch
2. “Maintain Good Repair”

The “Do Nothing” scenario represents the existing maintenance schedule where “patch” jobs are completed by the Region of Peel on the road as problems arise. These “patch” jobs are band-aid solutions that while not requiring extensive capital investment now, will lead to a scenario where the road conditions deteriorate quickly (since the underlying issues with the road base, sub-base and drainage conditions are not being addressed) and fall below the standard level of service. As noted above, by letting roads slip into poor condition, where moisture enters the base material, the cost of rehabilitation can be 5-8 times as expensive.

The “Maintain Good Repair” scenario represents an initial investment in the roadway to address pavement deficiencies such as the road base, sub-base and drainage functions. This ensures that future investments into the roadway are cost-effective. The “Maintain Good Repair” approach will provide roadway users with safe roads, in good condition, that cost less to maintain and rehabilitate. This asset management approach, when applied across a road network, is key to an agency being able to maintain roads in good condition.



The analysis completed to date for the EA verifies the base and pavement conditions of the roads have cracking, heaving and drainage deficiencies. These conditions do not lend themselves to simple band aid patch solutions and reconstruction will be the more cost effective approach in the long term.

Maintenance Costs

The summary of maintenance costs shown below include records specific to the road surface and base drainage for the study area roads from 2009 to 2012 with the exception of Winston Churchill Boulevard, which includes the extensive work in 2004 and 2005 to pave the road, and, Mississauga Road/Old Main Street, which includes the works in 2008 associated with preventing 50 metres of the road section from sliding down the valley.

Note that the reporting of yearly maintenance records goes back to 2009 when the Region began recording location measurements with the records for data entry.

Before 2009, locations of the maintenance work was hand written on work orders and may not be accurate.

Roadway	Total Maintenance Costs
Mississauga Road	\$1,253,453 ¹
Winston Churchill Boulevard	\$690,307 ²
Olde Base Line Road	\$162,891
Bush Street	\$6,792
TOTAL	\$2,213,443

Notes: 1- Includes costs associated with anchoring a section of roadway in 2008

2- Includes costs associated with surface treatment in 2004/2005

Again, over \$2.2 million was spent over the years to patch the roadway as issues arose and the condition of the roads has not improved. This reinforces the preliminary recommendation to reconstruct for long term asset management and cost savings.

MEETING NOTES

**Mississauga Road, Olde Base Line Road, Bush Street, Old Main Street, Winston Churchill Boulevard
Environmental Assessment and the Strategic Goods Movement Network Study**

1:30 p.m. – 2:30 p.m.

Friday, May 31, 2013

Region of Peel, 10 Peel Centre Drive, Suite B Cafeteria

**Present: Steve Ganesh (Manager, Infrastructure Programming & Studies), Sabbir Saiyed (Manager, Transportation System Planning),
Gino Dela Cruz (Project Manager), Gabriella Klein and Scott Dunlop (Wellington residents)**

Absent: Rachel Ingram (Wellington resident)

ITEM	DETAILS
<p>1. Region of Peel's Planning Process</p>	<ul style="list-style-type: none"> • Steve began the meeting with a discussion of the Region of Peel's long range planning process which sets the stage for Environmental Assessments. After the Environmental Assessment (EA) has been completed the project advances to Detailed Design. • Steve explained that the EA goes to 30 percent design and the Roads Capital section in Public Works will take the project from 30 percent to construction. • Steve pointed out that the OMB decision on the Rockfort Quarry was based on the Quarry application not the viability of roads for trucks and the decision does not limit the Region's authority to study or assess the roads for improvement.
<p>2. Strategic Goods Movement Study</p>	<ul style="list-style-type: none"> • Sabbir explained the background to the Region's Strategic Goods Movement Strategy file and indicated that the Region of Peel will be undertaking public education on goods movement, one of that being the Region's Official Plan review process, currently underway. • There was agreement by all that the Strategic Goods Movement Study was not final; the EA will be used to further assess Olde Base Line Road and Winston Churchill Boulevard as truck priority routes. • Steve explained that trucks currently use Olde Base Line Road and Winston Churchill Boulevard and that these roads provide network connections to destinations within the Region of Peel and the Greater Toronto and Hamilton Area. • Steve pointed out that Wellington County is a funding partner to the EA and Mark Ebby is the County's representative. • There was agreement by all that the Strategic Goods Movement Study, like all planning studies, serves as input to the EA and is a separate process from the EA.
<p>3. Existing Conditions in the Study Area</p>	<ul style="list-style-type: none"> • Scott acknowledged that the analysis of the EA demonstrated poor pavement conditions and deficient sightlines, and, therefore, the EA should proceed on these merits.

Olde Base Line Road – McLaughlin Road to Hurontario Street





SERVICE DATA					
SERVICE	DATE	INIT.	SERVICE	DATE	INIT.
SAN SEWERS			GAS MAINS		
STORM SEWERS			BELL U/G CABLE		
WATERMANS			HYDRO U/G CABLE		
TRANSIT			HYDRO ONE		
PARKS & REC.			CTV		
ONT. CLEAN WATER			COMMUNIC. CABLES		

REVISIONS		
DATE	DETAILS	INIT.

HALTON HILLS / COUNTY OF WELLINGTON

KEY PLAN (N.T.S.)

LEGEND:

- PROPOSED GRADING LIMIT
- - - PROPOSED PROPERTY LINE
- MESH FENCE
- - - PROPOSED CENTER LINE
- - - EXISTING CENTER LINE
- PROPOSED EDGE OF PAVEMENT
- EXISTING RIGHT-OF-WAY
- PROPOSED STORM SEWER
- PROPOSED CATCHBASIN
- DIRECTION OF DRAINAGE
- PROPOSED OVERHEAD HYDRO DUCT
- H EXISTING HYDRO POLE
- ▼ PROPOSED TRAFFIC SIGN
- ▼ EXISTING TRAFFIC SIGN
- LS EXISTING LIGHT POLE
- B EXISTING BELL POLE

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Designed by: *Chkd.* Approved by: _____

NOTICE TO CONTRACTOR

48 HOURS PRIOR TO COMMENCING WORK NOTIFY THE FOLLOWING

THE REGIONAL MUNICIPALITY OF PEEL	CABLE TELEVISION/FIBROPTIC PROVIDERS:
CITY OF MISSISSAUGA WORKS DEPT.	BELL CANADA
CITY OF BRAMPTON WORKS DEPT.	ENERSOURCE TELECOM
TOWN OF CALEDON WORKS DEPT.	HYDRO ONE TELECOM
BELL CANADA	ROGERS CABLE
ENBRIDGE INCORPORATED-GAS DISTRIBUTION	ALLSTREAM
ONTARIO MINISTRY OF TRANSPORTATION	PSN (PUBLIC SECTOR NETWORK)
ONTARIO CLEAN WATER AGENCY	FUTUREWAY (FIBRO/BROADBAND)
HYDRO ONE NETWORKS	
ENERSOURCE, HYDRO MISSISSAUGA	
HYDRO ONE BRAMPTON	

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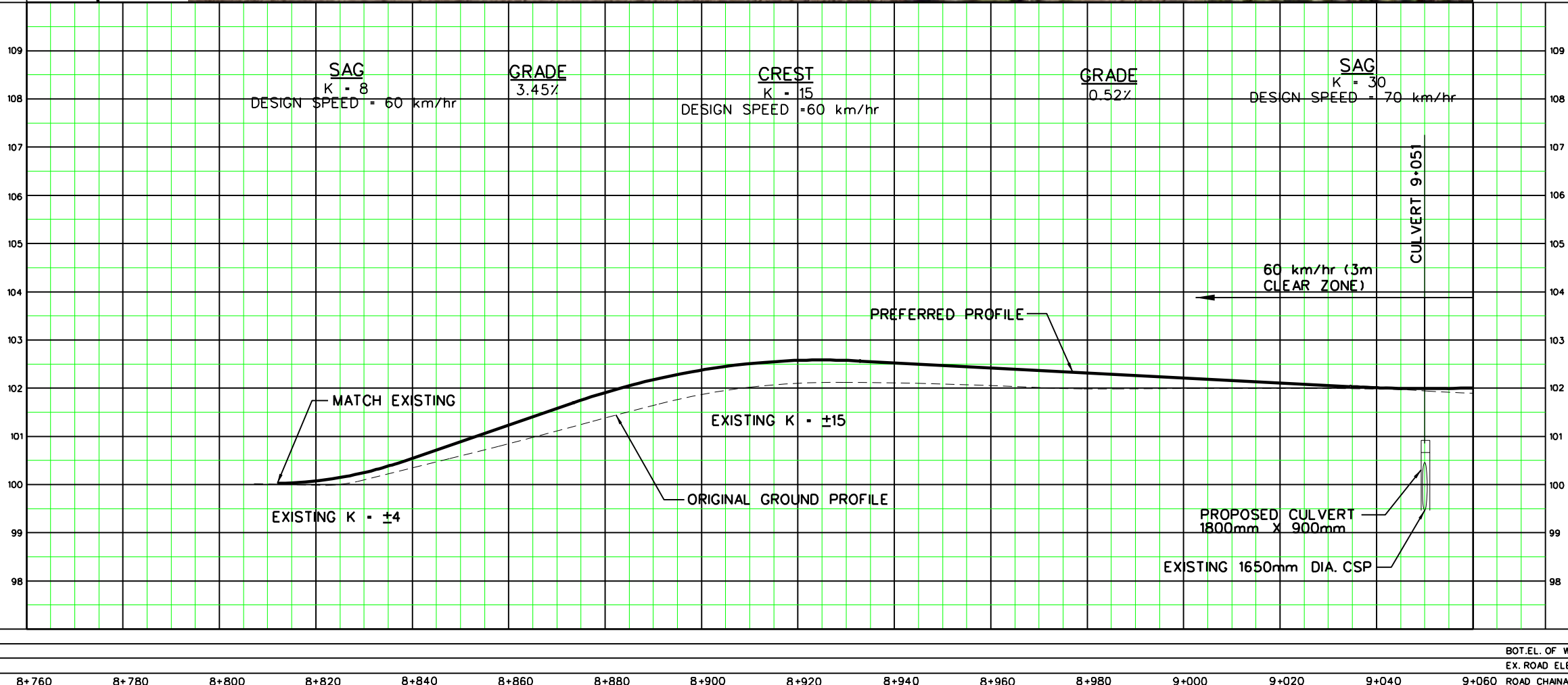
1m 0 1 2 3m VERTICAL SCALE

Region of Peel
Working for you

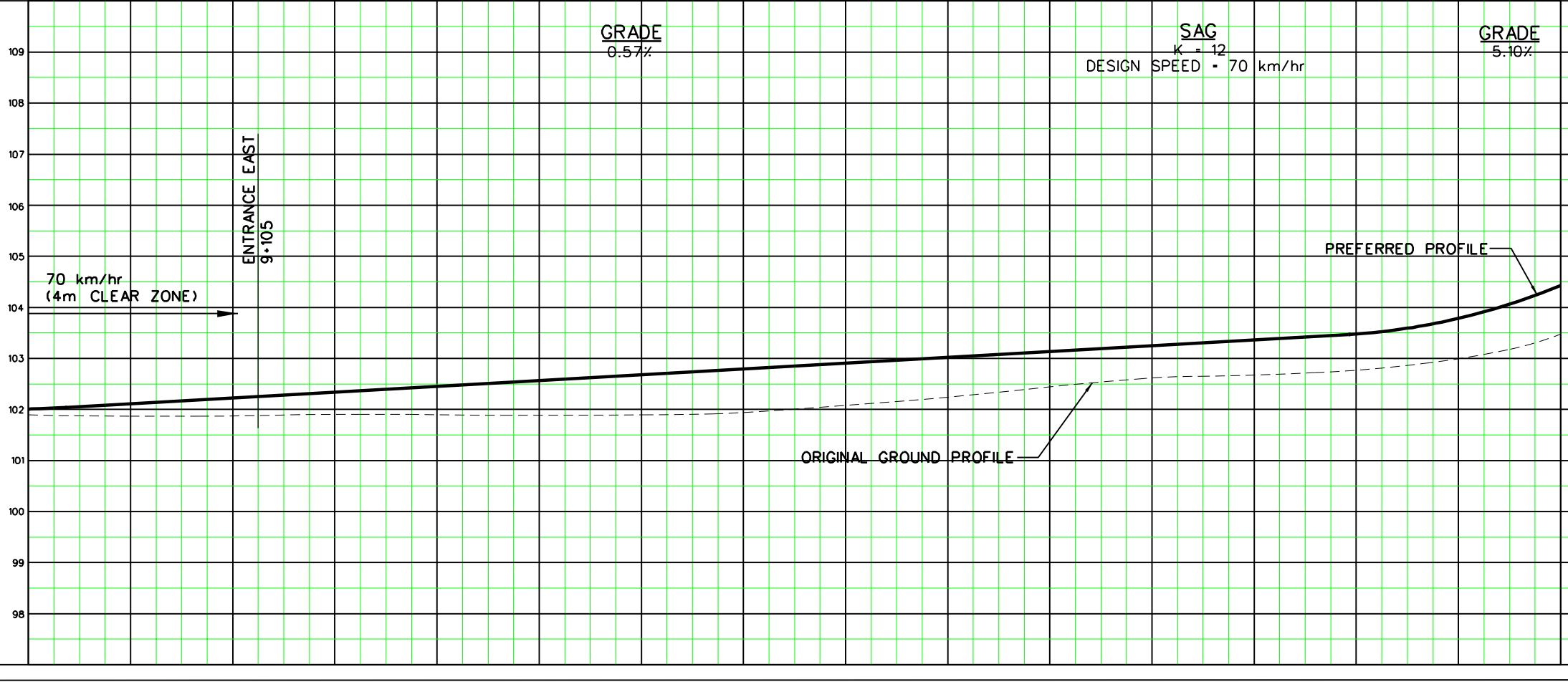
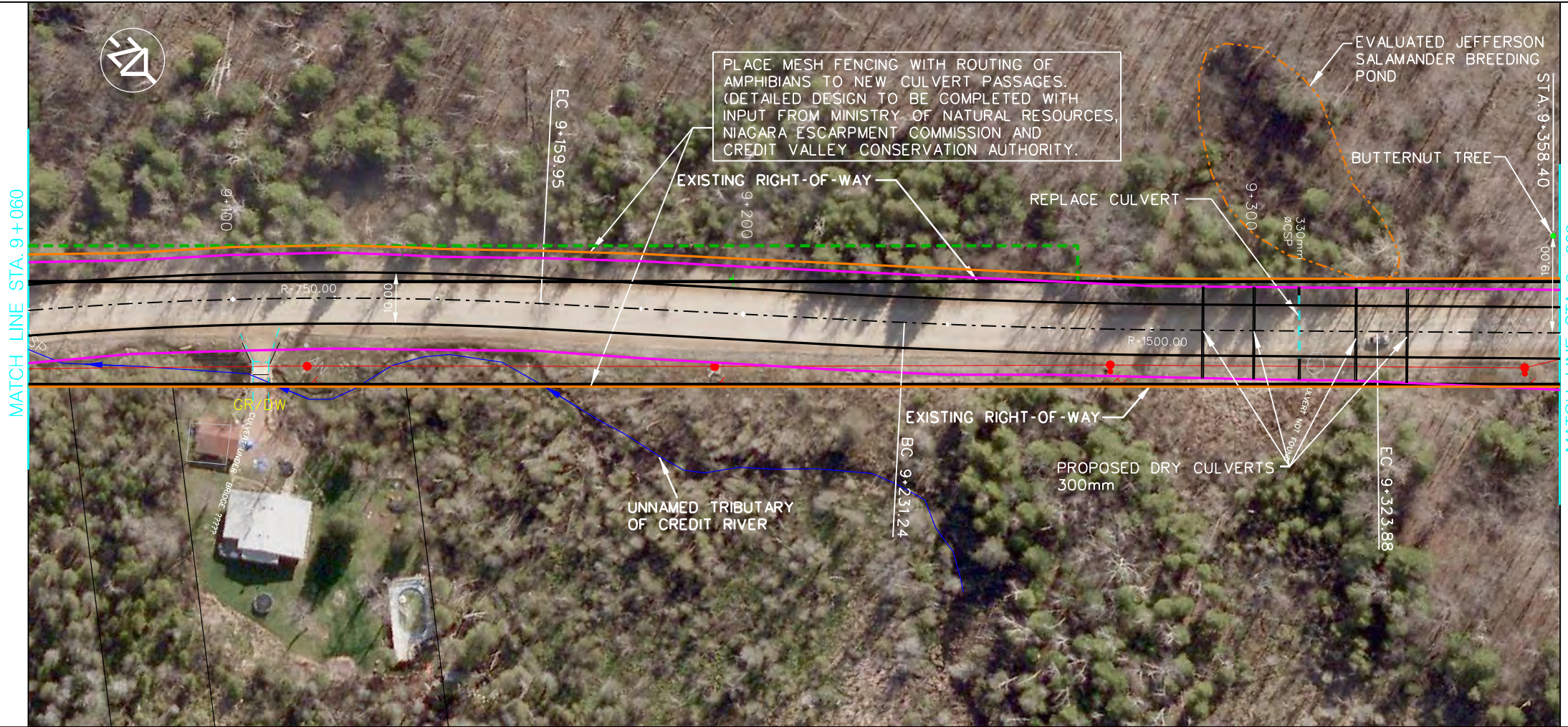
WINSTON CHURCHILL BLVD.
(FROM 1200m SOUTH OF BALLINAFAD ROAD TO OLDE BASE LINE ROAD)

NEW CONSTRUCTION

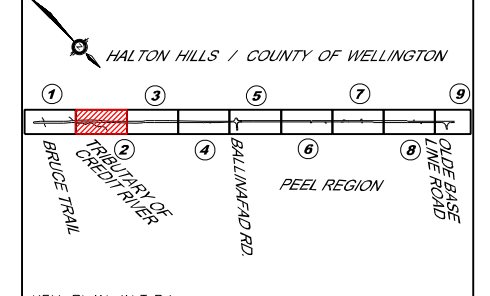
BOT. EL. OF WM	CAD Area	Area Z-B	Project No. XX-XXXX
EX. ROAD ELEV.	Checked by	Drawn by J.N.	Plan No. -D
8+760	Date MAR. 03, 2010	Sheet 1 of 9	



8+760	8+780	8+800	8+820	8+840	8+860	8+880	8+900	8+920	8+940	8+960	8+980	9+000	9+020	9+040	9+060
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SERVICE DATA					
SERVICE	DATE	INIT.	SERVICE	DATE	INIT.
SAN SEWERS			GAS MAINS		
STORM SEWERS			BELL U/G CABLE		
WATERMAINS			HYDRO U/G CABLE		
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ONT. CLEAN WATER			COMMUNIC. CABLES		



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HYDRO ONE NETWORKS	
ENERSOURCE, HYDRO MISSISSAUGA	
HYDRO ONE BRAMPTON	

10m 0 10 20 30m HORIZONTAL SCALE

1m 0 1 2 3m VERTICAL SCALE

Region of Peel
Working for you

WINSTON CHURCHILL BLVD.
(FROM 1200m SOUTH OF BALLINAFAD ROAD TO OLDE BASE LINE ROAD)

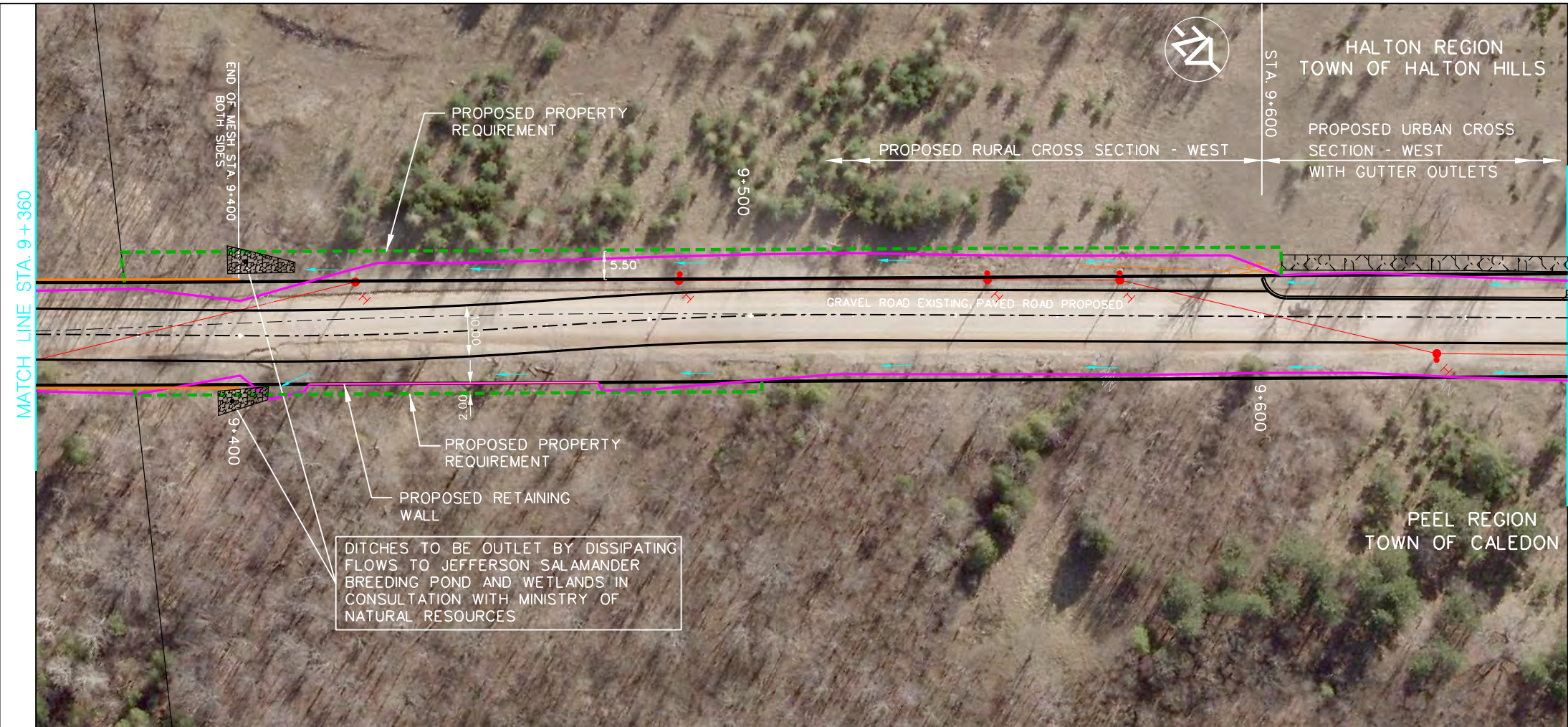
NEW CONSTRUCTION

STATION	BOT. EL. OF WM.	CAD Area	Area Z-B	Project No.
9+060				XX-XXXX
9+080				
9+100				
9+120				
9+140				
9+160				
9+180				
9+200				
9+220				
9+240				
9+260				
9+280				
9+300				
9+320				
9+340				
9+360				

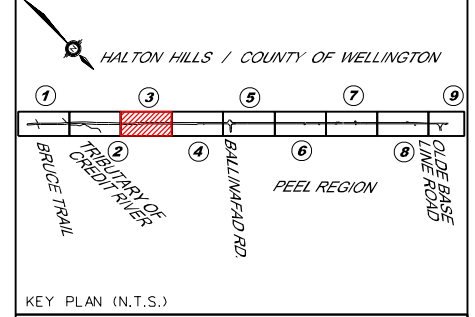
EX. ROAD ELEV. Checked by: _____ Date: MAR. 03, 2010

ROAD CHAINAGE Sheet 2 of 9

Plan No. -D



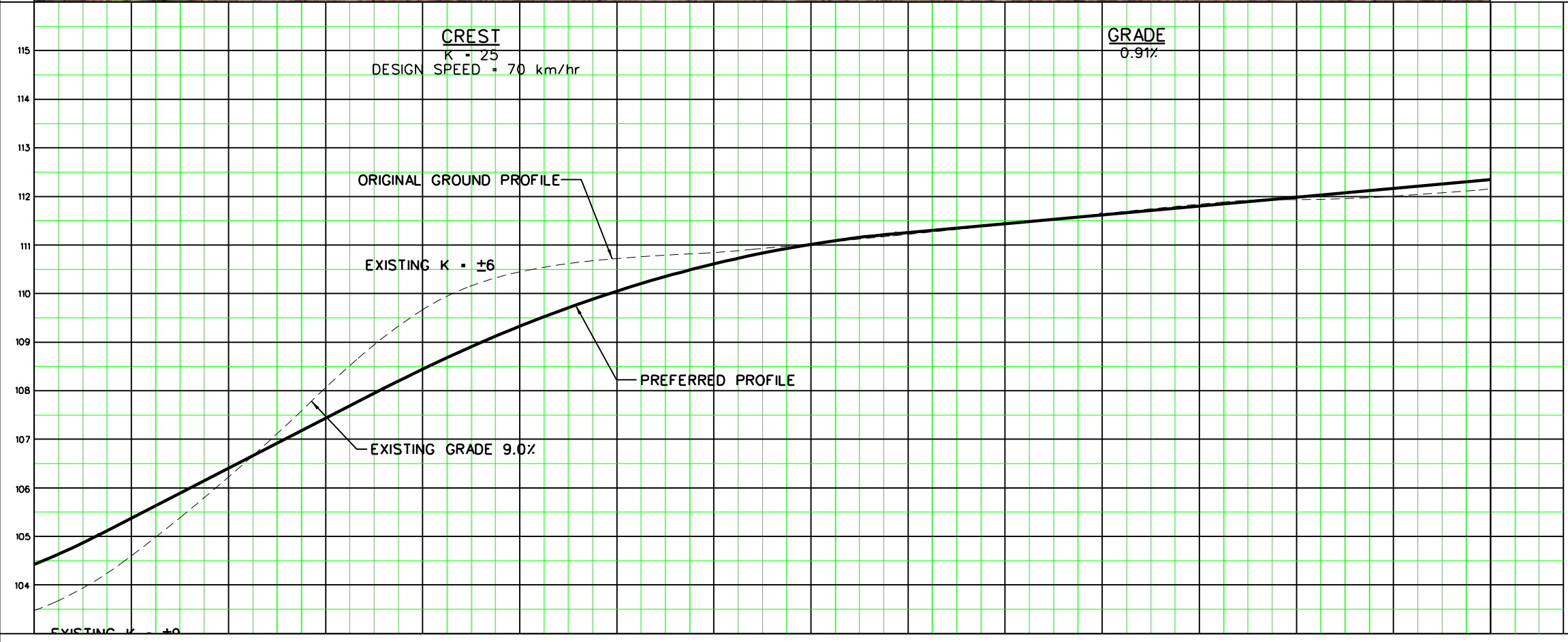
SERVICE DATA					
SERVICE	DATE	INIT.	SERVICE	DATE	INIT.
SAN SEWERS			GAS MAINS		
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WATERMANS			HYDRO UFG CABLE		
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Region of Peel
Working for you

WINSTON CHURCHILL BLVD.
(FROM 1200m SOUTH OF BALLINAFAD ROAD TO OLDE BASE LINE ROAD)

NEW CONSTRUCTION

BOT. EL. OF WM.		CAD Area	Area Z-B	Project No.																		
9+360	9+380	9+400	9+420	9+440	9+460	9+480	9+500	9+520	9+540	9+560	9+580	9+600	9+620	9+640	9+660	ROAD CHAINAGE	Checked by	Drawn by J.N.	Date MAR. 03, 2010	Sheet 3 of 9	Plan No.	XX-XXXX



SERVICE DATA					
SERVICE	DATE	INIT.	SERVICE	DATE	INIT.
SAN SEWERS			GAS MAINS		
STORM SEWERS			BELL U/G CABLE		
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TRANSIT			HYDRO ONE		
PARKS & REC.			CTV		
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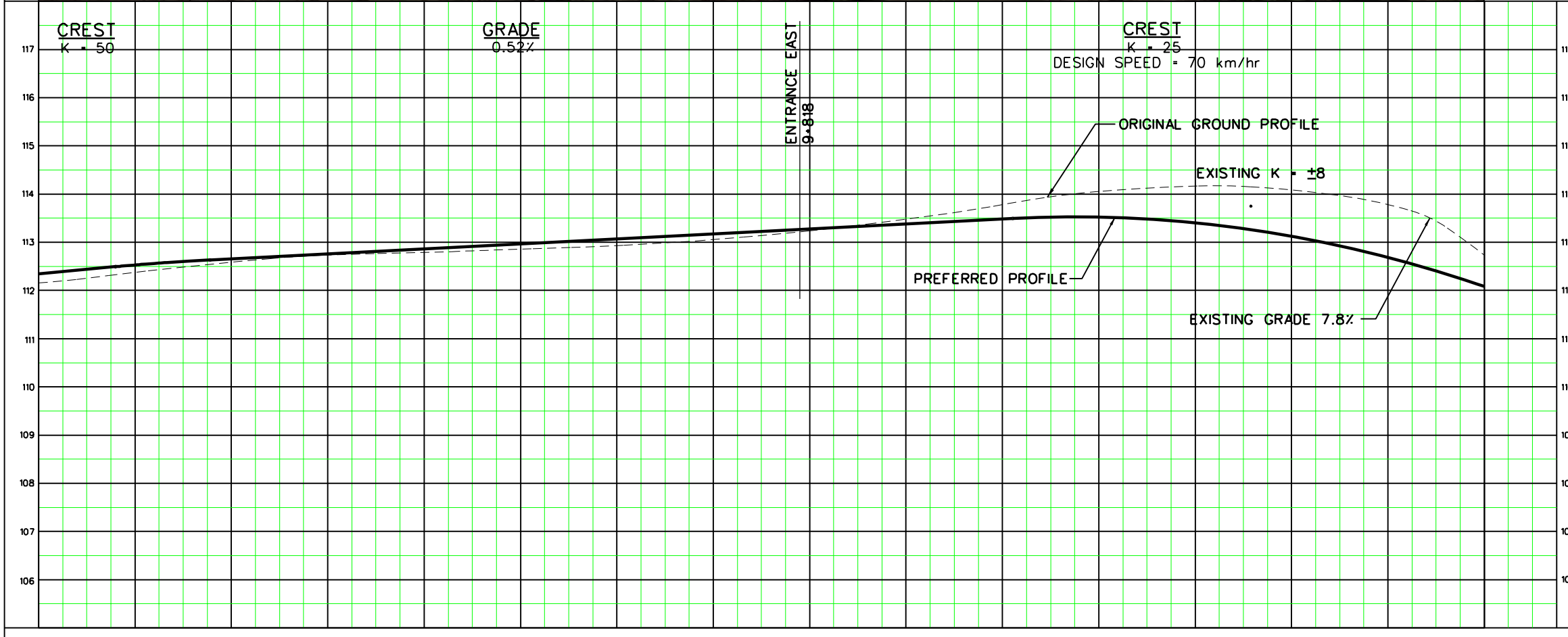
REVISIONS		
DATE	DETAILS	INIT.

HALTON HILLS / COUNTY OF WELLINGTON

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10m 0 10 20 30m HORIZONTAL SCALE

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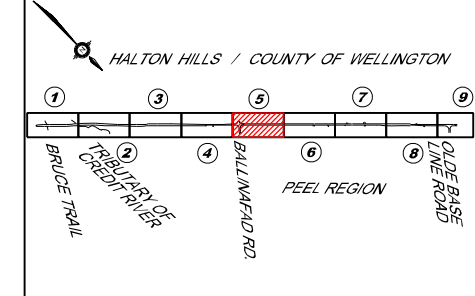
Region of Peel
Working for you

WINSTON CHURCHILL BLVD.
(FROM 1200m SOUTH OF BALLINAFAD ROAD TO OLDE BASE LINE ROAD)

NEW CONSTRUCTION

9+660	9+680	9+700	9+720	9+740	9+760	9+780	9+800	9+820	9+840	9+860	9+880	9+900	9+920	9+940	9+960
BOT. EL. OF WM. CAD Area															
EX. ROAD ELEV. Checked by: _____ Area Z-8 Drawn by: J.N. Project No. XX-XXXX															
ROAD CHAINAGE Date: MAR. 03, 2010 Sheet 4 of 9 Plan No. -D															

SERVICE DATA					
SERVICE	DATE	INIT.	SERVICE	DATE	INIT.
SAN SEWERS			GAS MAINS		
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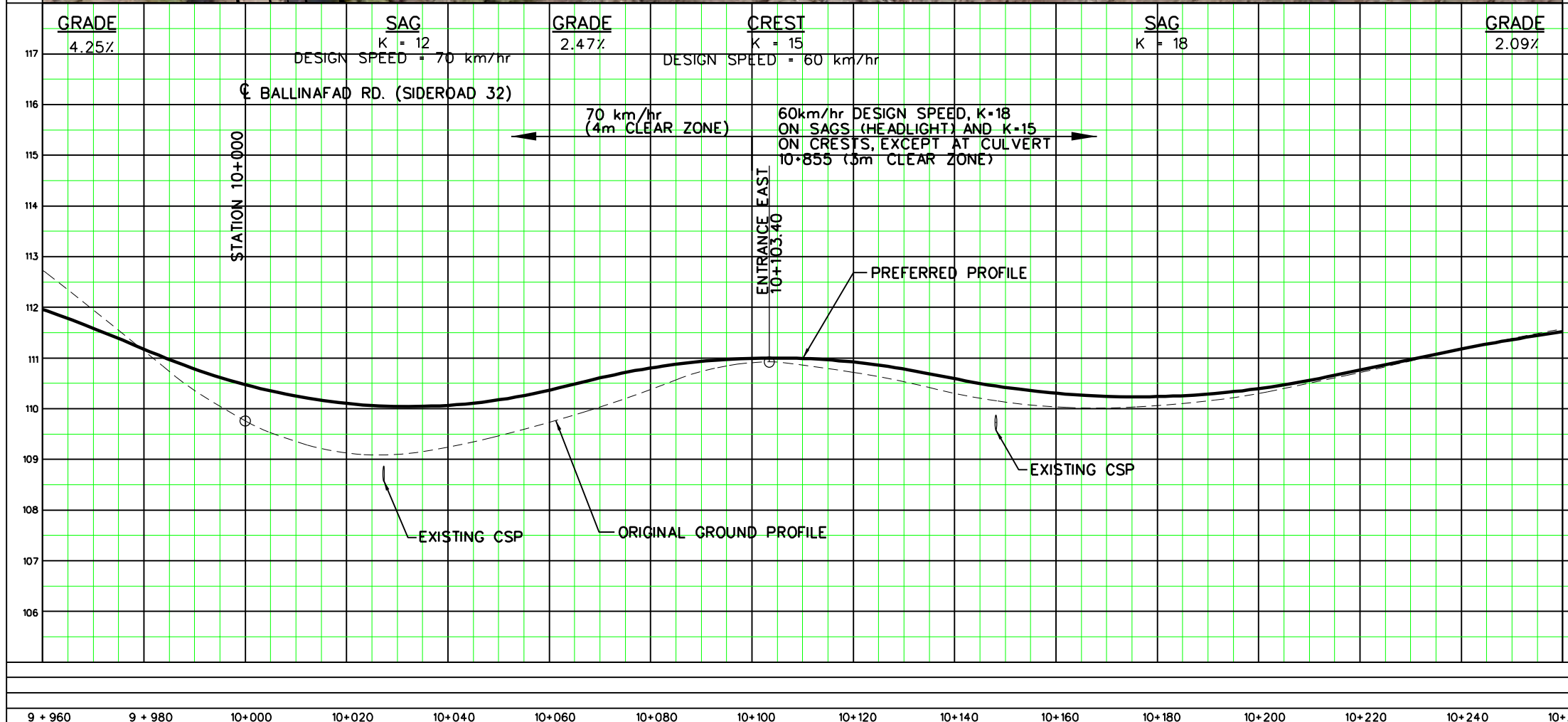
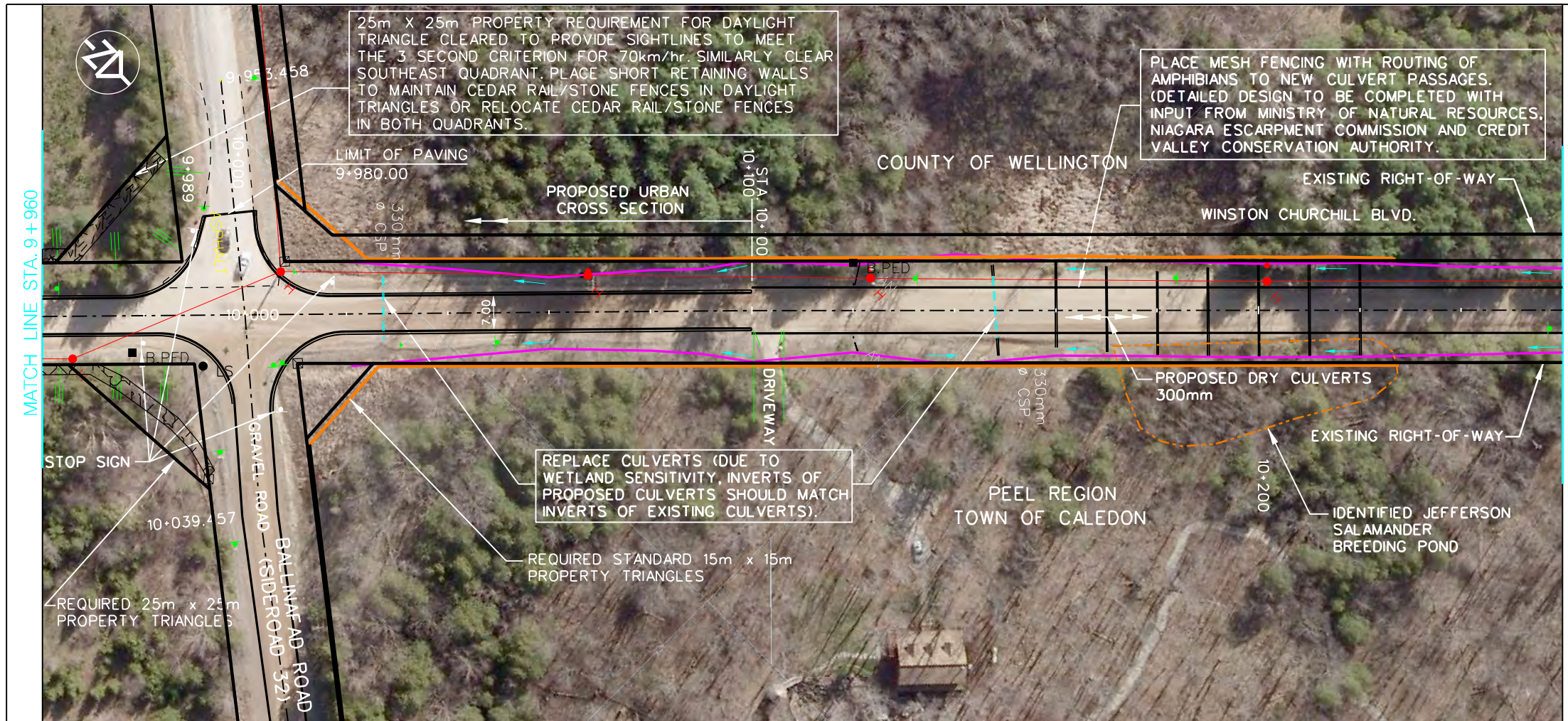
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HYDRO ONE BRAMPTON	



WINSTON CHURCHILL BLVD.
 (FROM 1200m SOUTH OF BALLINAFAD ROAD TO OLDE BASE LINE ROAD)
NEW CONSTRUCTION

CAD Area	Area Z-8	Project No. XX-XXXX
Checked by	Drawn by J.N.	Date MAR. 03, 2010
Date	Sheet 5 of 9	Plan No. -D



9 + 960	9 + 980	10 + 000	10 + 020	10 + 040	10 + 060	10 + 080	10 + 100	10 + 120	10 + 140	10 + 160	10 + 180	10 + 200	10 + 220	10 + 240	10 + 260
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MATCH LINE STA. 9+960

MATCH LINE STA. 10+260

25m X 25m PROPERTY REQUIREMENT FOR DAYLIGHT TRIANGLE CLEARED TO PROVIDE SIGHTLINES TO MEET THE 3 SECOND CRITERION FOR 70km/hr. SIMILARLY CLEAR SOUTHEAST QUADRANT. PLACE SHORT RETAINING WALLS TO MAINTAIN CEDAR RAIL/STONE FENCES IN DAYLIGHT TRIANGLES OR RELOCATE CEDAR RAIL/STONE FENCES IN BOTH QUADRANTS.

PLACE MESH FENCING WITH ROUTING OF AMPHIBIANS TO NEW CULVERT PASSAGES. (DETAILED DESIGN TO BE COMPLETED WITH INPUT FROM MINISTRY OF NATURAL RESOURCES, NIAGARA ESCARPMENT COMMISSION AND CREDIT VALLEY CONSERVATION AUTHORITY.)

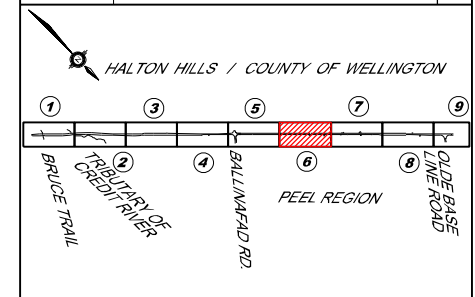
REPLACE CULVERTS (DUE TO WETLAND SENSITIVITY, INVERTS OF PROPOSED CULVERTS SHOULD MATCH INVERTS OF EXISTING CULVERTS).

REQUIRED STANDARD 15m x 15m PROPERTY TRIANGLES

REQUIRED 25m x 25m PROPERTY TRIANGLES



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PARKS & REC.			CTV		
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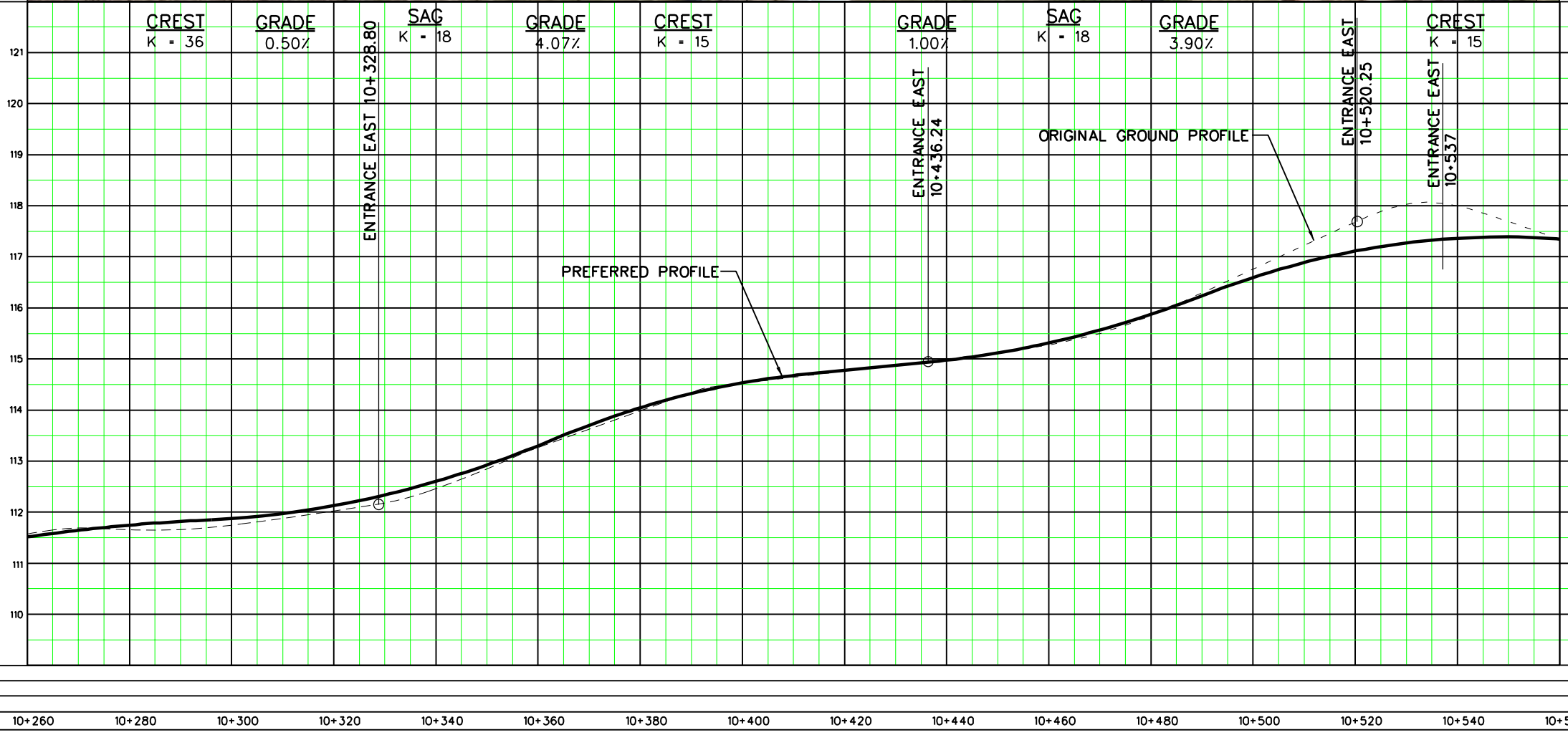
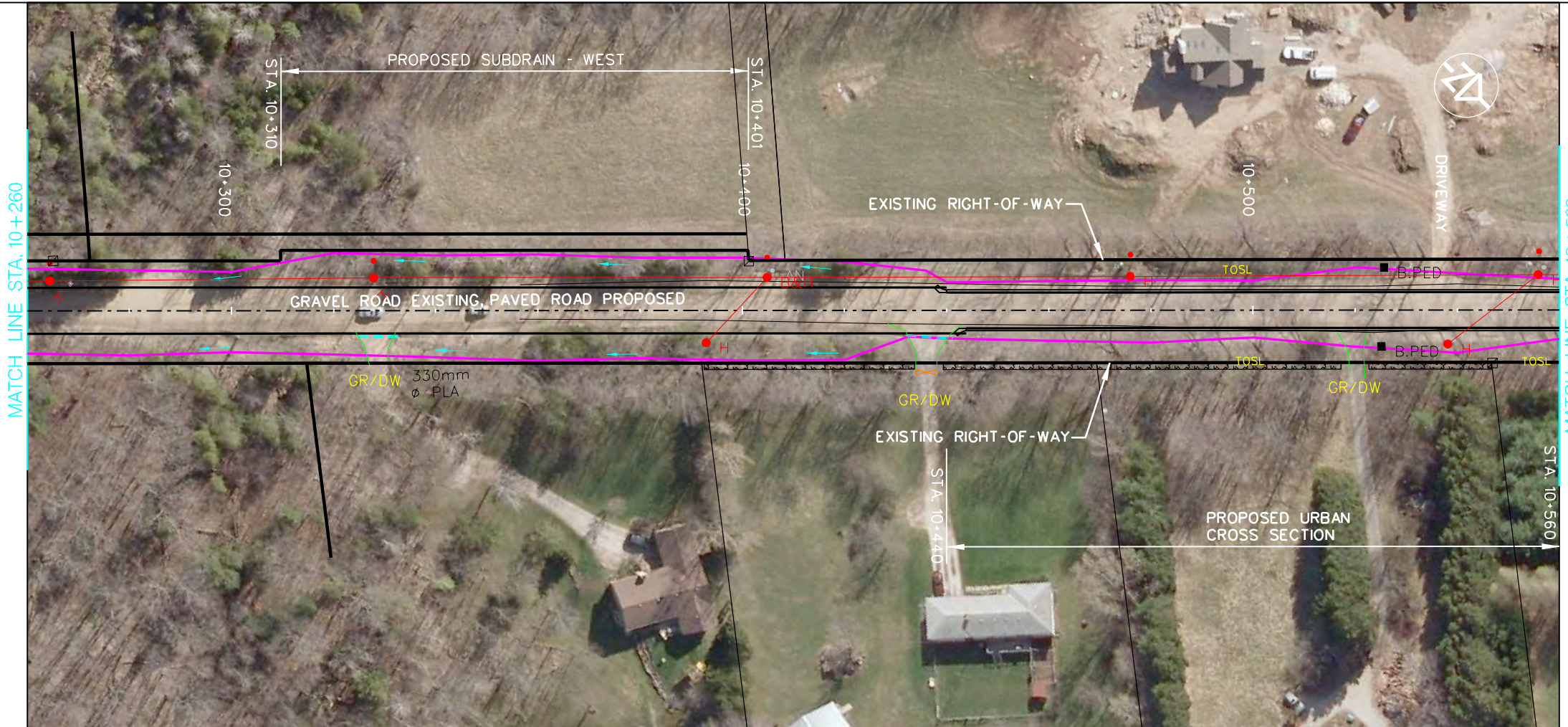
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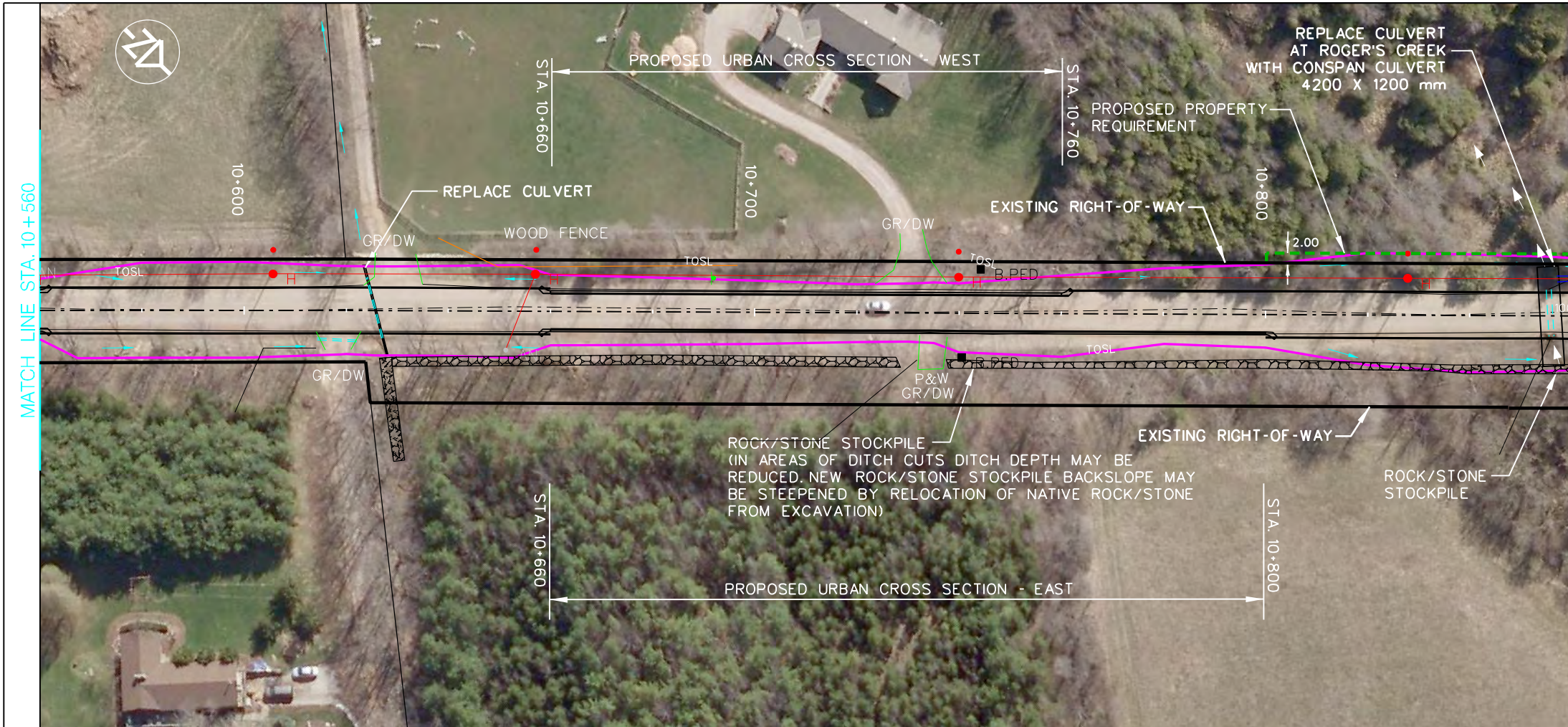
1m 0 1 2 3m VERTICAL SCALE

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NEW CONSTRUCTION

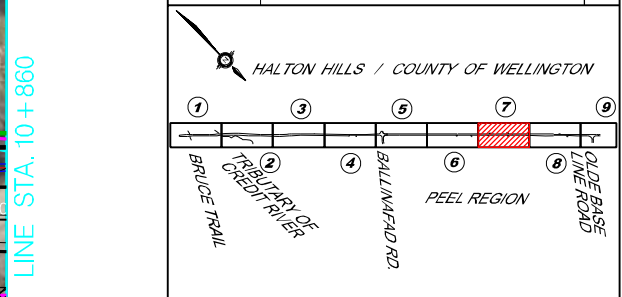
BOT.E.L. OF WM	CAD Area	Area Z-B	Project No. XX-XXXX
EX. ROAD ELEV.	Checked by	Drawn by J.N.	Plan No. -D
Date MAR. 03, 2010	Sheet 6 of 9		



10+260	10+280	10+300	10+320	10+340	10+360	10+380	10+400	10+420	10+440	10+460	10+480	10+500	10+520	10+540	10+560	ROAD CHAINAGE
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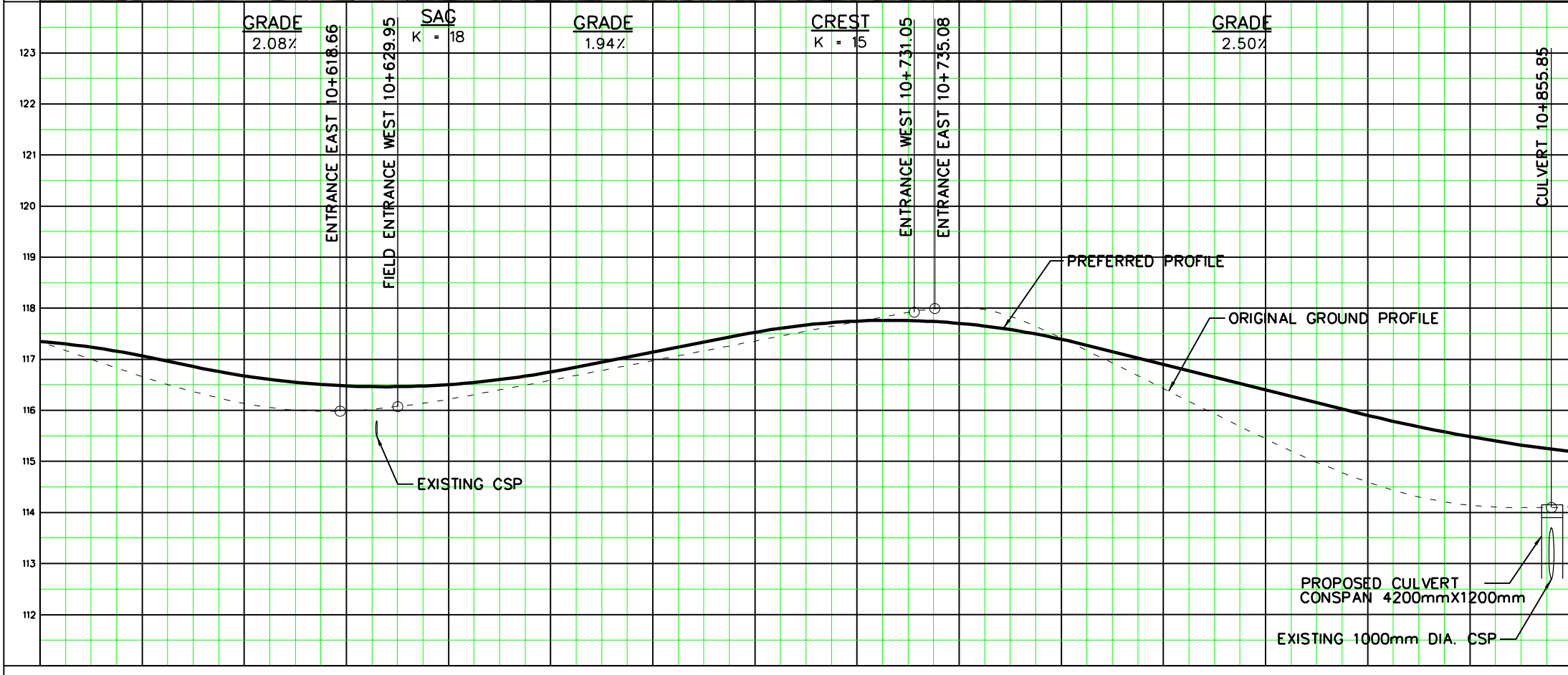
SERVICE DATA					
SERVICE	DATE	INIT.	SERVICE	DATE	INIT.
SAN SEWERS			GAS MAINS		
STORM SEWERS			BELL U/G CABLE		
WATERMANS			HYDRO U/G CABLE		
TRANSIT			HYDRO ONE		
PARKS & REC.			CTV		
ONT. CLEAN WATER			COMMUNIC. CABLES		



KEY PLAN (N.T.S.)

LEGEND:

- PROPOSED GRADING LIMIT
- PROPOSED PROPERTY LINE
- MESH FENCE
- PROPOSED CENTER LINE
- EXISTING CENTER LINE
- PROPOSED EDGE OF PAVEMENT
- EXISTING RIGHT-OF-WAY
- PROPOSED STORM SEWER
- PROPOSED CATCHBASIN
- DIRECTION OF DRAINAGE
- PROPOSED OVERHEAD HYDRO DUCT
- EXISTING HYDRO POLE
- PROPOSED TRAFFIC SIGN
- EXISTING TRAFFIC SIGN
- EXISTING LIGHT POLE
- EXISTING BELL POLE



General Notes

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- Type 'B' Bedding Unless Otherwise Noted (SAN)

B.M. No. Elev.

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Designed by: Chid. Approved by: _____

NOTICE TO CONTRACTOR

48 HOURS PRIOR TO COMMENCING WORK NOTIFY THE FOLLOWING:

THE REGIONAL MUNICIPALITY OF PEEL	CABLE TELEVISION/FIBROPTIC PROVIDERS:
CITY OF MISSISSAUGA WORKS DEPT.	BELL CANADA
CITY OF BRAMPTON WORKS DEPT.	ENERSOURCE TELECOM
TOWN OF CALEDON WORKS DEPT.	HYDRO ONE TELECOM
BELL CANADA	ROGERS CABLE
ENBRIDGE INCORPORATED-GAS DISTRIBUTION	ALLSTREAM
ONTARIO MINISTRY OF TRANSPORTATION	PSN (PUBLIC SECTOR NETWORK)
ONTARIO CLEAN WATER AGENCY	FUTUREWAY (FOI BROADBAND)
HYDRO ONE NETWORKS	
ENERSOURCE, HYDRO MISSISSAUGA	
HYDRO ONE BRAMPTON	

10m 0 10 20 30m HORIZONTAL SCALE

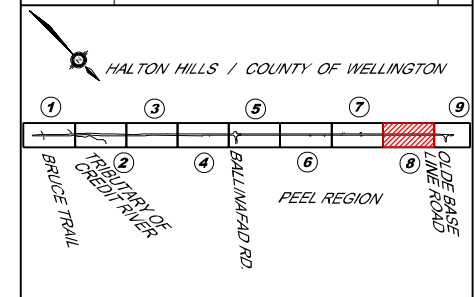
1m 0 1 2 3m VERTICAL SCALE

Region of Peel
Working for you

WINSTON CHURCHILL BLVD.
(FROM 1200m SOUTH OF BALLINAFAD ROAD TO OLDE BASE LINE ROAD)
NEW CONSTRUCTION

STATION	EX. ROAD ELEV.	ROAD CHAINAGE	BOT. EL. OF WM.	CAD Area	Area Z-8	Project No.
10+560						XX-XXXX
10+580						
10+600						
10+620						
10+640						
10+660						
10+680						
10+700						
10+720						
10+740						
10+760						
10+780						
10+800						
10+820						
10+840						
10+860						

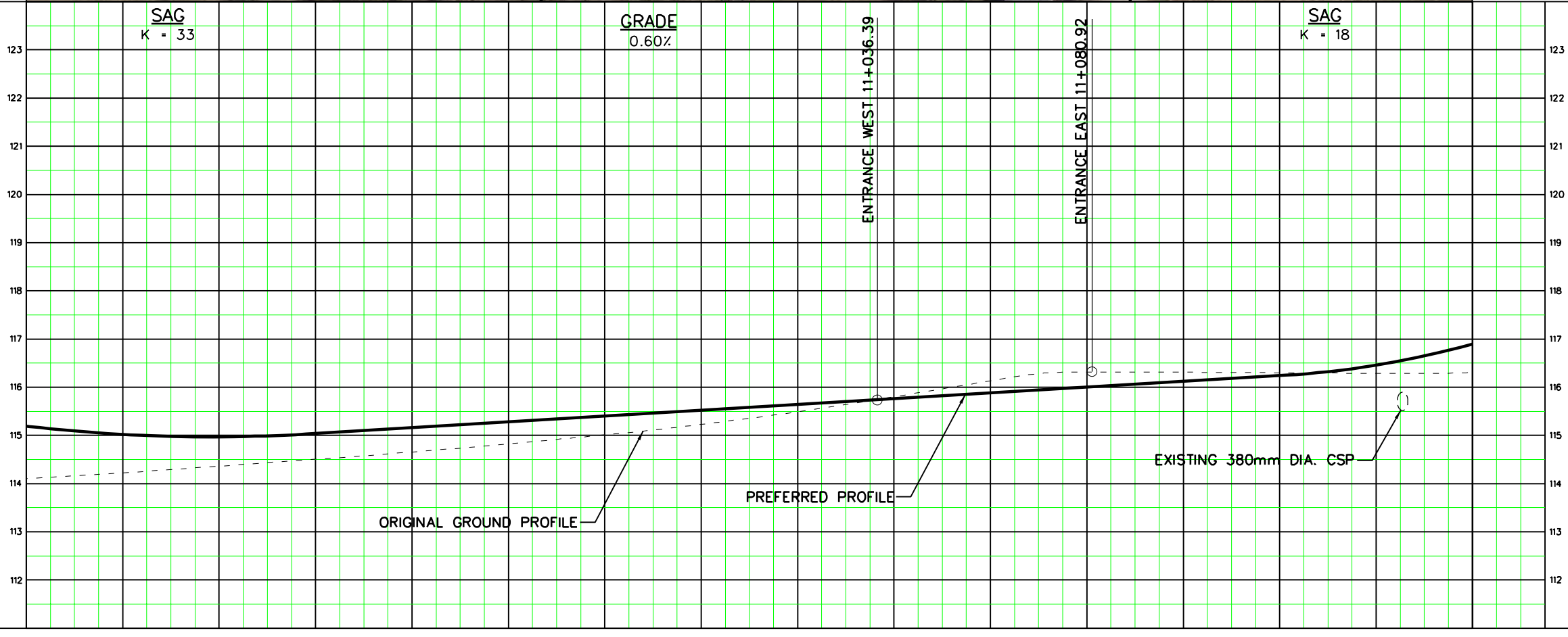
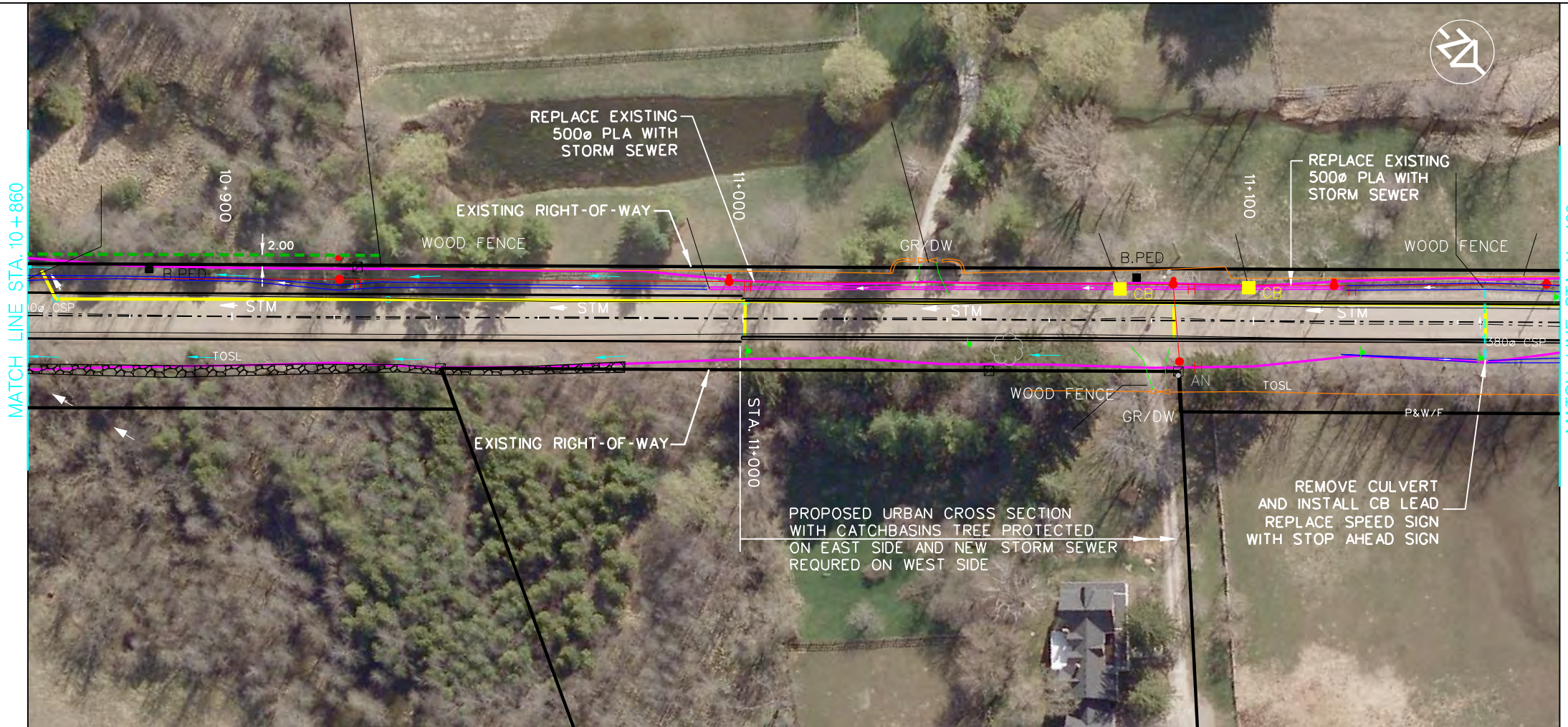
SERVICE DATA					
SERVICE	DATE	INIT.	SERVICE	DATE	INIT.
SAN SEWERS			GAS MAINS		
STORM SEWERS			BELL U/G CABLE		
WATERMANS			HYDRO U/G CABLE		
TRANSIT			HYDRO ONE		
PARKS & REC.			CTV		
ONT. CLEAN WATER			COMMUNIC. CABLES		



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Designed by: Chkd. Approved by

NOTICE TO CONTRACTOR

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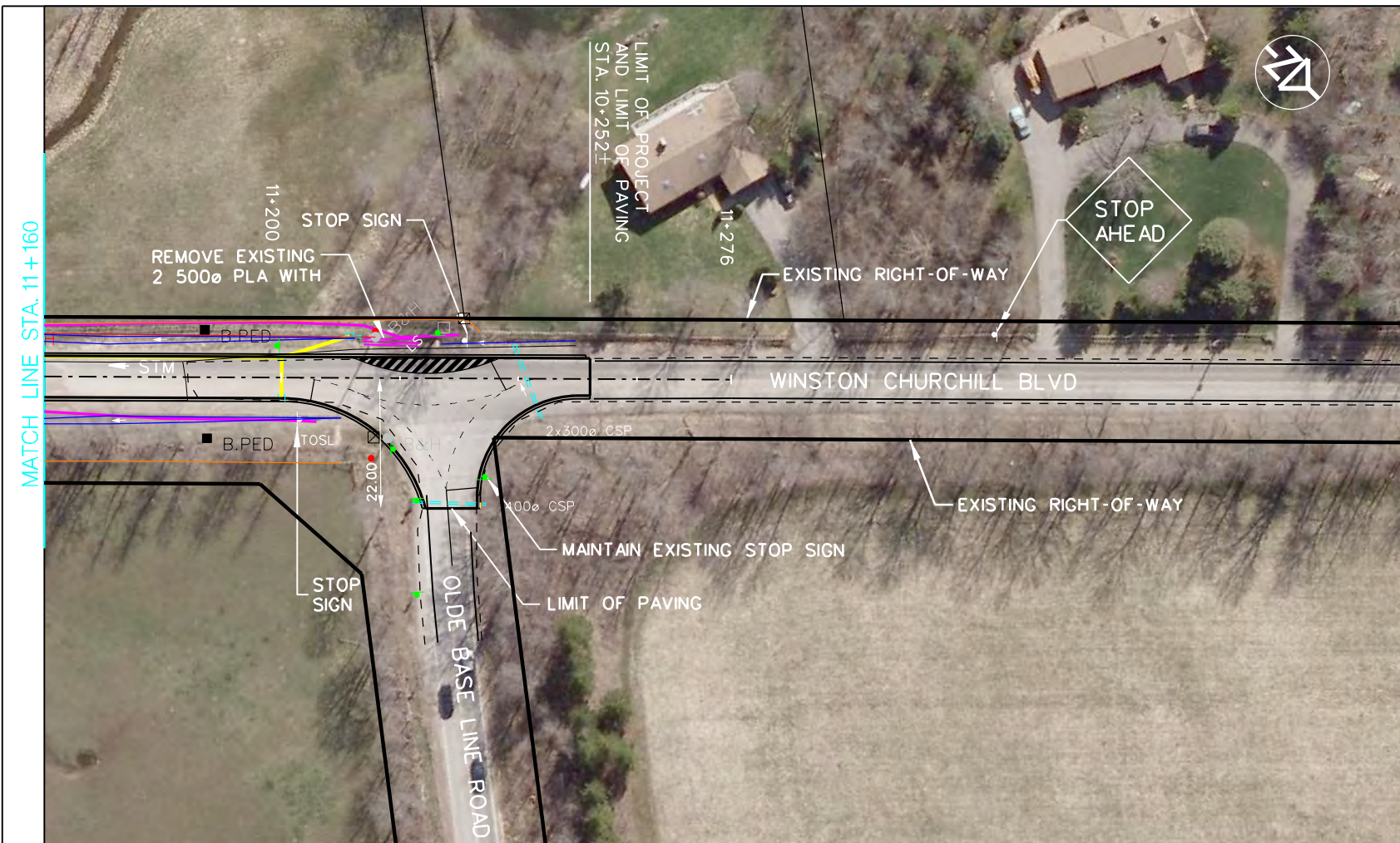
THE REGIONAL MUNICIPALITY OF PEEL	CABLE TELEVISION/FIBROPTIC PROVIDERS:
CITY OF MISSISSAUGA WORKS DEPT.	BELL CANADA
CITY OF BRAMPTON WORKS DEPT.	ENERSOURCE TELECOM
TOWN OF CALEDON WORKS DEPT.	HYDRO ONE TELECOM
BELL CANADA	ROGERS CABLE
ENBRIDGE INCORPORATED-GAS DISTRIBUTION	ALLSTREAM
ONTARIO MINISTRY OF TRANSPORTATION	PSN (PUBLIC SECTOR NETWORK)
ONTARIO CLEAN WATER AGENCY	FUTUREWAY (FCI/BROADBAND)
HYDRO ONE NETWORKS	
ENERSOURCE, HYDRO MISSISSAUGA	
HYDRO ONE BRAMPTON	

10m 0 10 20 30m HORIZONTAL SCALE
1m 0 1 2 3m VERTICAL SCALE

Region of Peel
Working for you

WINSTON CHURCHILL BLVD.
(FROM 1200m SOUTH OF BALLINAFAD ROAD TO OLDE BASE LINE ROAD)
NEW CONSTRUCTION

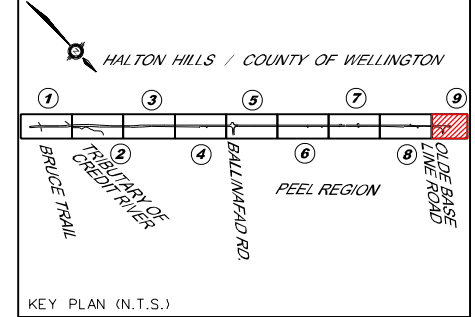
10+860	10+880	10+900	10+920	10+940	10+960	10+980	11+000	11+020	11+040	11+060	11+080	11+100	11+120	11+140	11+160	ROAD CHAINAGE
BOT.E.L. OF WM. CAD Area																
EX. ROAD ELEV. Checked by																
Date MAR. 03, 2010																
Area Z-8 Drawn by J.J.N. Sheet 8 of 9																
Project No. XX-XXXX Plan No. -D																



MATCH LINE STA. 11+160

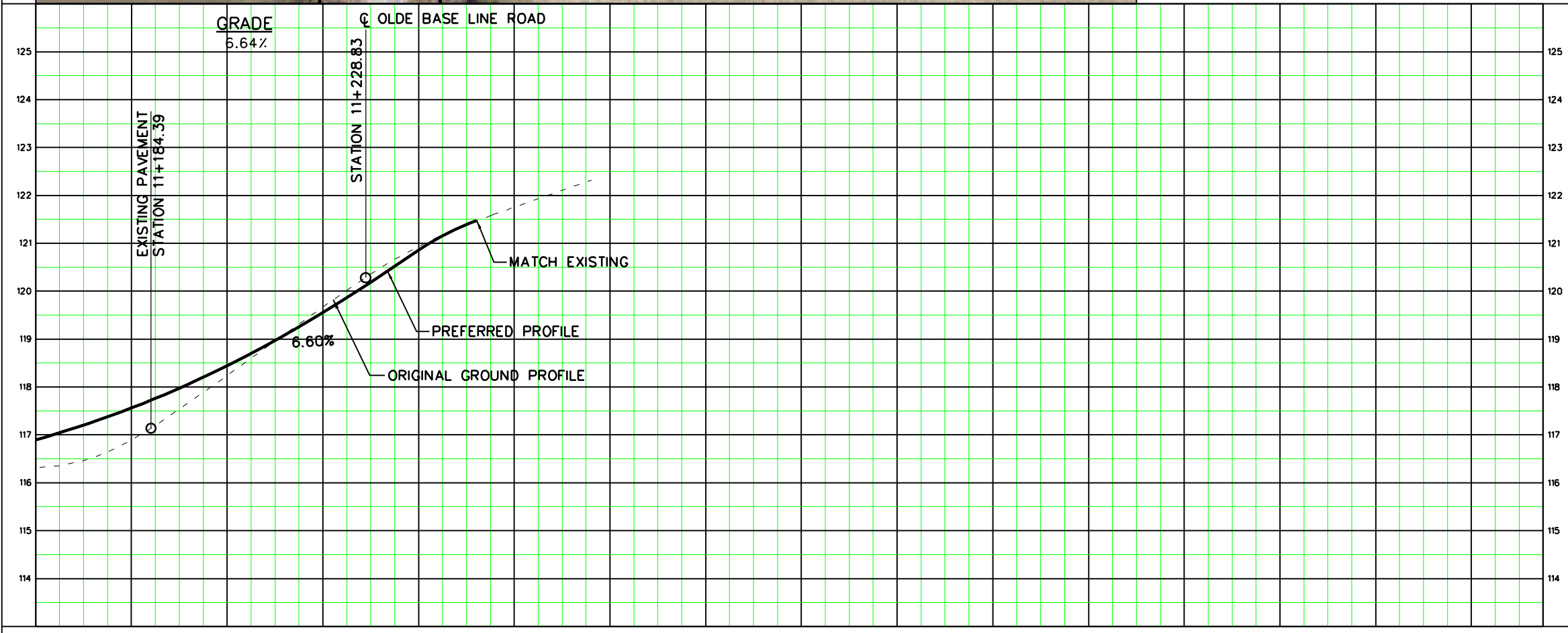
SERVICE DATA					
SERVICE	DATE	INIT.	SERVICE	DATE	INIT.
SAN SEWERS			GAS MAINS		
STORM SEWERS			BELL U/G CABLE		
WATERMANS			HYDRO U/G CABLE		
TRANSIT			HYDRO ONE		
PARKS & REC.			CTV		
ONT. CLEAN WATER			COMMUNIC. CABLES		

REVISIONS		
DATE	DETAILS	INIT.



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- DIRECTION OF DRAINAGE
- PROPOSED OVERHEAD HYDRO DUCT
- H EXISTING HYDRO POLE
- PROPOSED TRAFFIC SIGN
- ▼ EXISTING TRAFFIC SIGN
- LS EXISTING LIGHT POLE
- B EXISTING BELL POLE



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HYDRO ONE NETWORKS	
ENERSOURCE, HYDRO MISSISSAUGA	
HYDRO ONE BRAMPTON	

10m 0 10 20 30m HORIZONTAL SCALE

1m 0 1 2 3m VERTICAL SCALE

Region of Peel
Working for you

WINSTON CHURCHILL BLVD.
(FROM 1200m SOUTH OF BALLINAFAD ROAD TO OLDE BASE LINE ROAD)

NEW CONSTRUCTION

11+160	11+180	11+200	11+220	11+240	11+260	11+280	11+300	11+320	11+340	11+360	11+380	11+400	BOT. EL. OF WM.	CAD Area	Area Z-B	Project No. XX-XXXX
													EX. ROAD ELEV.	Checked by	Drawn by J.N.	
													ROAD CHAINAGE	Date MAR. 03, 2010	Sheet 9 of 9	Plan No. -D

Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Friday, June 28, 2013 2:59 PM
To: [REDACTED]
Cc: Ganesh, Steve; Gan, Tyrone; cumming@total.net; Restrepo, Veronica; Saddi, Asha
Subject: RE: Winston Churchill/Olde Baseline Road/etc. project

Hello [REDACTED]

Thank you again for providing your comments, input and insight to the study corridor from the perspective of someone that uses the roadway on a daily basis. They have been shared with the project team and will be considered in the upcoming phases of the study as we develop alternative designs.

If you have any further comments or questions at this time, please do not hesitate to contact me.

Have a great long weekend,
Gino

P. Gino Dela Cruz, P.Eng.
Project Manager, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor
Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: June 27, 2013 1:50 PM
To: Saddi, Asha
Cc: Ganesh, Steve; Dela Cruz, Gino; Tyrone.Gan@hdrinc.com; cumming@total.net; Veronica.Restrepo@hdrinc.com
Subject: Re: Winston Churchill/Olde Baseline Road/etc. project

Thank you for your response.

While staff works through establishing standards, then determining that roads of a sudden do not meet those standards, we who live in the area have to live with the outcomes - more traffic, more noise, more speeding at ever increasing speeds, and more truck traffic (trucks are presently prohibited, but any kind of survey will show you what we see every day - they ignore the posting).

We would be happy to have the road returned to a gravel surface. We are also sure that if the posted speeds were observed, the sight lines and stopping distances would be acceptable if not, a simple reduction of the authorized speed limit should do the trick.

As for the number of deer accidents, a careful review would likely find that most of them are due to speeds exceeding the limits. Some deer accidents are to be expected in a rural area - more when the population of deer increases, less when it decreases.

A speed camera on the relevant sections of Winston Churchill and Olde Baseline Roads would give you some idea of how common excessive speeds are on those roads. This data should be gathered before coming to the inevitable conclusion that the roads need to be torn up, widened, and resurfaced, as is outlined in the documents you forwarded. It is so clearly evident that the intent is to go forward with these improvements, and increase truck traffic regardless of the feedback from those of us who live on the roads. The documents are put in place, then made reference to with a reverence we do not endorse. 'Improving' these roads, as has been repeatedly pointed out, will result in more speeding and at higher rates of speed as motorists feel 'safer' with the 'improved' roads.

While we see the direction you intend to go, we passionately oppose it, and will take all steps we can legally to do so. The process is already in place and the steps you have planned will result, in effect, of a 'taking' of our property without any compensation. The land values will decline from what they would be without these 'improvements', and we will not only suffer the consequences of increased traffic, increased truck traffic, increased noise and ever increasing speeds, but we will not be compensated for the resulting loss in value of our properties. I don't think that is how government 'for the people' is supposed to work.

Sincerely,

[Redacted Signature]

In a message dated 6/27/2013 9:52:56 A.M. Pacific Daylight Time, Asha.Saddi@peelregion.ca writes:

Good afternoon [Redacted]

Thank you for your email. I have forwarded your comments to the Project Team and they will take your comments in to consideration as we move into the design phase of this study and assess the various design alternatives.

In response to the points highlighted in your email, the Project Team ask that you consider the following:

Current Environmental Assessment (EA) Study

The current EA study for the Mississauga Road, Olde Base Line Road, Bush Street, Old Main Street and Winston Churchill Boulevard Regional roads is being undertaken to address fundamental deficiencies in pavement and safety conditions that apply whether or not the roadways are designated for truck traffic.

The pavement conditions are summarized by structural deficiencies that include:

- inadequacies in the roadway surface
- the roadway base, and,
- subsurface drainage characteristics.

The roads in the study area were initially constructed over 100 years ago and have never been reconstructed to provide an adequate road base to support modern vehicles. The roadside drainage too is limited. Without a proper road base and drainage, the subgrade will get saturated and the road surface will continue to deteriorate.

Many sections of the roads in the study area do not meet the Transportation Association of Canada (TAC) design standards for grades and/or minimum stopping sight distance, resulting in poor visibility and unsafe driving conditions. The roads generally have a straight horizontal alignment and a rolling profile with many steep vertical curves.

As part of this EA, there are opportunities to enhance the road experience in the community for all road users including pedestrians and cyclists. The study is also investigating ways to minimize and mitigate the high proportion of collisions involving wildlife, particularly white-tailed deer in the study area.

Rockfort Quarry History

On November 12, 2010 the OMB issued its decision on the Rockfort Quarry applications and turned down the proposed quarry. The OMB concluded that James Dick Construction Limited had not provided sufficient information to show that the proposed quarry would not result in unacceptable impacts on the surrounding natural environment and that the proposed quarry would result in unacceptable negative impacts on cultural heritage as well as visual, noise and social impacts.

The Region of Peel was involved throughout the OMB process. Regional Council considered the application in September 2003 and again in March 2009, and on both occasions, Regional Council directed staff and legal counsel to oppose the approval on the proposed Rockfort Quarry.

Strategic Goods Movement Network Study (SGMNS)

The SGMNS is a long term plan for goods movement in the Region of Peel. The SGMNS was undertaken from the Council approved Goods Movement Strategic Plan (April 2012) which recommended, under Action Item 20, the development of a strategic goods movement network that will preserve and protect existing and potential truck priority routes for goods movement throughout the Region of Peel. This will help support the economy by providing a transportation system that moves goods and delivers services quickly, efficiently and safely, while reducing environmental costs and community impacts. The network is made up of strategic roads that connect key goods-generators, destinations and highway.

You can view and download the SGMNS from the Region of Peel website at:

<http://www.peelregion.ca/pw/transportation/goodsmovement/pdf/peel-final-technical-report.pdf>

Road Characterization Study (RCS)

The RCS is an implementation measure of the Long Range Transportation Plan (LRTP). The outcomes of the RCS include six new road typologies for Regional roads, cross-sections, and associated access control measures. The study identifies Winston Churchill Boulevard, Olde Base Line Road, Mississauga Road and Bush Street as a "Rural Road" and Old Main Street as a "Rural Main Street". Regional Council endorsed these initiatives at the Council meeting on May 9, 2013.

You can view and download the RCS from the Region of Peel website at:

<http://www.peelregion.ca/pw/transportation/business/pdfs/rcs-full-report.pdf>

Both the Strategic Goods Movement Network and Road Characterization studies were developed by members from local area municipalities, including staff from the Town of Caledon.

The SGMNS and the Current EA

The current EA would have been undertaken irrespective of the SGMNS. This EA addresses the drainage, sightlines, pavement and safety deficiencies identified in the study area and the recommended solution must be

linked to these elements as opposed to broader economic goals put forth by the SGMN; broadening the scope to a high level would undermine the EA.

The identification or designation of Winston Churchill Boulevard and Olde Base Line Road will be determined through the implementation process outlined in the SGMNS which will include an infrastructure analysis and/or restriction suitability assessment. The findings of the current EA study will be used as input to the SGMNS implementation strategy and not have any bearing on the preferred solution.

If you have any questions on the information provided please don't hesitate to contact our Project Manager, Gino Dela Cruz, at gino.delacruz@peelregion.ca or myself.

Asha Saddi

Technical Analyst, Infrastructure Programming & Studies

Transportation Division

Public Works, Region of Peel

Tel: 905-791-7800 x7794

Fax:905-791-1442

asha.saddi@peelregion.ca

From: [REDACTED]
Sent: June 26, 2013 12:54 PM
To: Saddi, Asha
Cc: [REDACTED]
Subject: Winston Churchill/Olde Baseline Road/etc. project

We have read the PIC #1 Feedback Report in detail. The overwhelming sentiment is that the road not be 'improved', that 'improvement' is a euphemism for turning the route into a truck route.

We realize that most of the changes in the area (road straightening, road paving, road widening) occurred before any of you were in your positions, indeed before some of you were even born. Nevertheless, it is important to recognize that Olde Baseline Road

between Mississauga and Winston Churchill, and Winston Churchill Road between Olde Baseline Road and Belfountain were paved without the support of the majority of those of us who live along those roads. We knew it would bring increased speeds and truck traffic, albeit illegal.

Now the region seems intent on making that route a designated 'truck route'. There are truck routes along 124 and 10 that exist and can be used. The notion that anyone ever conceived of these rural roads becoming designated regional heavy truck routes is, as evidenced by the comments at the hearing, a complete surprise.

We are unalterably opposed to making this route a truck route. We would support going back to a gravel surface. We do not support further destruction of the historical rural character of the roads through elimination of hills, widening of the roads, elimination of vegetation, etc.. We DO strongly support some enforcement of the speed limits along these roads. A visit on any weekday morning or evening will provide ample evidence of complete disregard for the speed limits. 'Improving' the roads will make this worse, not better.

Please consider the input of the residents. We have owned the property at [REDACTED] Olde baseline Road for 40 years. We fought the quarry at Olde Baseline and Winston Churchill. We fear the road 'improvements' are nothing more than the implementation of the improvements contemplated and presented by Mr. Dick, setting the stage for another effort on his part to develop the quarry in a few years.

If the Region has already made its mind up, and is simply going through the steps to satisfy a requirement to communicate to local residents, be honest and declare it so. That will permit those in opposition to know what the plans are and how best to deal with the decision.

sincerely,

[REDACTED]

Kong, Carol

From: Saddi, Asha <Asha.Saddi@peelregion.ca>
Sent: Thursday, July 25, 2013 4:01 PM
To: [REDACTED]
Cc: Dela Cruz, Gino; Ganesh, Steve; Gan, Tyrone; Restrepo, Veronica; 'Susan Cumming'
Subject: RE: Mississauga Road / Bush Street EA

Hello [REDACTED]

Thank you for your email.

The second PIC for this study is anticipated to be held in the Fall when we will mail out a notification of the meeting to all residents in the area. Advertisements will also be posted in the local newspapers: Erin Advocate, Caledon Enterprise / Citizen, Georgetown Independent / Acton free Press, Bolton Caledon and the Wellington Advertiser.

For your information, you can view and download information on this study from the Region of Peel's website at: <http://www.peelregion.ca/pw/transportation/envIRON-assess/mississauga-road-bush.htm>

If you have any other questions please do not hesitate to contact me.

Thanks,

Asha Saddi
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax: 905-791-1442
asha.saddi@peelregion.ca

From: [REDACTED]
Sent: July 25, 2013 3:30 PM
To: Saddi, Asha
Subject: Mississauga Road / Bush Street EA

Hello,

I have been following this EA process and I attended on May 9th (PIC #1). I have the feedback report, too, thank you. I'm just wondering if another PIC has been scheduled, or when you think it might be scheduled. I recall on May 9th, the public asked for another PIC sometime this summer. Thanks.

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Kong, Carol

From: Gan, Tyrone
Sent: Saturday, August 10, 2013 4:58 PM
To: cumming1@total.net; Dela Cruz, Gino
Cc: Saddi, Asha; Restrepo, Veronica
Subject: RE: traffic and transporation numbers questions and comments

The existing traffic data is consistent.

From the 2010 study, Exhibit 6 shows that existing traffic (2010) on Mississauga Road is about 320 vehicles per hour in the AM peak hour, and about 300-320 vehicles per hour in the PM peak hour, depending on exactly where you take the count.

Since the peak hour is approximately 10% of the daily traffic, one would expect daily traffic to be in the 3,000-3,200 vehicles per day range (from the 2010 study). This matches well with Slide 13 from the May PIC which shows approximately 3,000 vehicles per day in 2010.

The data is consistent.

It is not clear to me where the 726 vehicles figure comes from. If he can clarify that, I can address it.

TYRONE GAN
P. Eng.

HDR Corporation

Senior Vice President
Director of Transportation, Canada

100 York Boulevard, Suite 300 | Richmond Hill, ON L4B 1J8


t: 289.695.4622 | f: 289.695.4601

Tyrone.Gan@hdrinc.com | hdrinc.com

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From: Sue Cumming [mailto:cumming1@total.net]
Sent: Tuesday, August 06, 2013 1:50 PM
To: Gan, Tyrone; Dela Cruz, Gino
Cc: Saddi, Asha; Restrepo, Veronica
Subject: FW: traffic and transporation numbers questions and comments

Additional information 

From: 
Sent: August-06-13 1:45 PM
To: cumming1@total.net
Subject: RE: traffic and transporation numbers questions and comments

These 2 seem to be contradicting.

<http://www.peelregion.ca/pw/transportation/enviro-assess/pdf-mississauga-road/PIC1-MississaugaRoad.pdf>

<http://www.peelregion.ca/pw/transportation/enviro-assess/pdf-mississauga-road/pic1-display-boards.pdf>



From: Sue Cumming [<mailto:cumming1@total.net>]
Sent: August-06-13 1:42 PM
To: [REDACTED]
Subject: RE: traffic and transportation numbers questions and comments

Thanks [REDACTED]
I will forward this to Tyrone and Gino. Are there any other studies that you wish to reference for the call?
Sue Cumming, MCIP RPP
Cumming+Company

From: [REDACTED]
Sent: August-06-13 1:30 PM
To: cumming1@total.net
Subject: RE: traffic and transportation numbers questions and comments

It is too large a file for me to send. You can find it here.

Kong, Carol

From: Gan, Tyrone
Sent: Thursday, September 05, 2013 6:53 PM
To: Restrepo, Veronica
Subject: FW: traffic and transporation numbers questions and comments

TYRONE GAN
P. Eng.

HDR Corporation
Senior Vice President
Director of Transportation, Canada

100 York Boulevard, Suite 300 | Richmond Hill, ON L4B 1J8
t: [289.695.4622](tel:289.695.4622) | f: 289.695.4601
Tyrone.Gan@hdrinc.com | hdrinc.com

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From: Dela Cruz, Gino [<mailto:Gino.DelaCruz@peelregion.ca>]
Sent: Thursday, September 05, 2013 3:19 PM
To: [REDACTED] cumming1@total.net; Gan, Tyrone
Subject: RE: traffic and transporation numbers questions and comments

Hi [REDACTED]

Yes, peak periods are usually 3 hour duration. It is when demand for travel is at its highest. Note that AM or PM peak may also refer to the peak hour. I believe peak hour is what is referenced in the previous study's PIC #1 material.

We can discuss more tomorrow.

Regards,

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: September 5, 2013 12:59 PM
To: cumming1@total.net; tyrone.gan@hdrinc.com; Dela Cruz, Gino
Subject: RE: traffic and transporation numbers questions and comments

Can you let me know what the typical Peak AM/PM duration is? Typically it is 3 hours in the morning and 3 hours in the evening.

[REDACTED]

From: Sue Cumming [<mailto:cumming1@total.net>]
Sent: September-05-13 11:10 AM
To: [REDACTED]; tyrone.gan@hdrinc.com; gino.delacruz@peelregion.ca
Subject: RE: traffic and transporation numbers questions and comments

Dear [REDACTED]
Gino and Tyrone would be available to talk with you tomorrow morning at either 9:00 or 9:45 a.m. Are either of these times convenient for you. Please advise and then I will provide a call in number for you.

Thank you
Sue Cumming, MCIP RPP
Cumming+Company
866 611-3715

From: [REDACTED]
Sent: September-04-13 11:38 AM
To: cumming1@total.net; tyrone.gan@hdrinc.com; gino.delacruz@peelregion.ca
Subject: RE: traffic and transporation numbers questions and comments

I can have this call any time this week. Please pick 2 times that are convenient for you.n
I do have 1 question, what is the length of time the Region of Peel considers peak time in Am and PM?

[REDACTED]

From: Sue Cumming [<mailto:cumming1@total.net>]
Sent: August-06-13 1:22 PM
To: [REDACTED]
Subject: traffic and transporation numbers questions and comments

Hi [REDACTED]
Further to my voicemail. Thank you for your patience. I spoke with Tryone Gan and he suggested that a telephone conference be set up between the two of you and Gino DeLa Cruz at your convenience to review the questions that you have relative to the transportation numbers and other concerns. To assist in having the discussion, Tyrone asked if you could advise on what studies or reports you are /would be referencing so

Kong, Carol

From: Gan, Tyrone
Sent: Friday, September 13, 2013 4:03 PM
To: [REDACTED]
Cc: cumming1@total.net; gino.delacruz@peelregion.ca; Saddi, Asha (Asha.Saddi@peelregion.ca); Restrepo, Veronica
Subject: Summary of Conference Call on September 6, 2013
Attachments: Summary of Conference Call with [REDACTED] Sept 6 2013 v2.pdf

Hello [REDACTED]

We appreciated the opportunity to discuss the Mississauga Road, Olde Base Line Road, Bush Street, Old Main Street and Winston Churchill Boulevard EA project with you during our Friday, September 6, 2013 conference call. Thank you for setting up the call number.

Attached is a summary of our discussion.

TYRONE GAN

P. Eng.

HDR Corporation

Senior Vice President

Director of Transportation, Canada

100 York Boulevard, Suite 300 | Richmond Hill, ON L4B 1J8

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File: 2.2
Project # 6776

Conference Call Summary

Project: Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road EA

Subject: **Conference Call with** [REDACTED]

Conference Call Date: Friday, September 6, 2013

Participants:

[REDACTED] – CWG Member
Gino Dela Cruz – Project Manager, Region of Peel
Tyrone Gan – Project Manager, HDR Corporation

Distribution:

Participants
All CWG Members

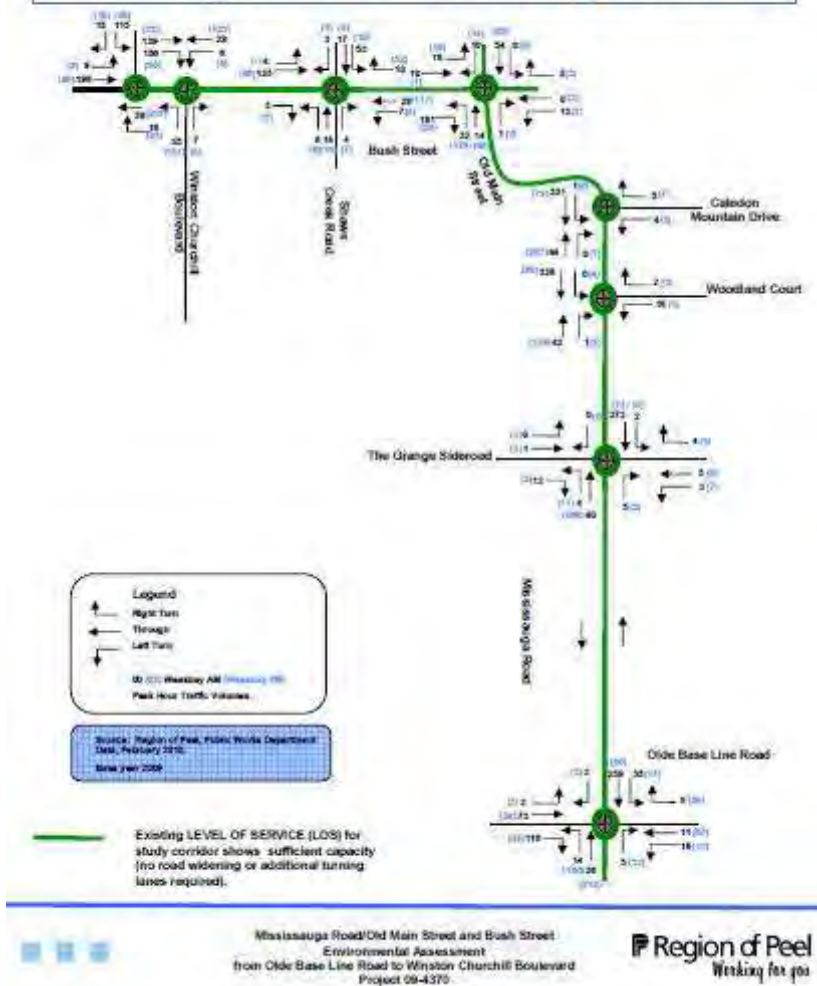
During our discussion reference was made to the following documents which all participants had access to:

<http://www.peelregion.ca/pw/transportation/enviro-assess/pdf-mississauga-road/PIC1-MississaugaRoad.pdf>

<http://www.peelregion.ca/pw/transportation/enviro-assess/pdf-mississauga-road/pic1-display-boards.pdf>

In particular, reference was made to the following exhibits:

6 Existing AM/PM Traffic & Turning Movements



From the 2010 Public Information Centre for Bush Street and Mississauga Road, display #6 shows:

1. traffic counts conducted by the Region of Peel in and around 2009, and,
2. the highest AM peak hour and highest PM peak hour for individual intersections on Bush Street and Mississauga Road.

It was noted that, since the traffic counts are typically carried out on different days, there were some differences in the traffic volumes when total traffic counts on Bush Street or Mississauga Road are compared with adjacent intersections.

Display #6 shows traffic for the single highest peak hour in the AM and the single highest peak hour in the PM. The remaining 2 hours in the 3-hour AM peak period will be lower than the single highest hour. This means that multiplying the highest AM peak hour traffic by 3 overestimates the 3-hour AM peak period traffic. In other words, traffic in the 3-hour AM peak

period will be less than 3 times the single highest hour traffic. The same applies to the PM peak.

During discussions, we went through the exercise of taking the traffic counts in Display #6 and determining the AM peak and PM peak hour traffic volumes. Below, is a summary of the calculations undertaken together during our conference call.

Bush Street (west of the north leg of Winston Churchill)

The AM peak hour volume is the sum of all traffic on Bush Street west of the north leg of Winston Churchill Boulevard. During discussions, you noted that this leg of Bush Street is outside the jurisdiction of the Region of Peel.

9 eastbound to northbound left turns
196 eastbound through
38 westbound through
15 southbound to westbound right turns
258 total vehicles on Bush in the AM peak hour

Bush Street (between the north and south legs of Winston Churchill)

There are two sources for traffic counts for this section of Bush Street: the count undertaken for Bush Street at the north leg of Winston Churchill Boulevard, and the count conducted for Bush Street at the south leg of Winston Churchill Boulevard. Using the traffic counts for Bush Street at the south leg of Winston Churchill Boulevard, the AM peak hour volume for this segment of Bush Street is:

35 northbound to westbound left turns
29 westbound through
139 eastbound through
156 eastbound to southbound right turns
359 total vehicles on Bush Street in the AM peak hour

Using the traffic counts for Bush at the north leg of Winston Churchill Boulevard, the AM peak hour volume for Bush Street (between the north and south legs of Winston Churchill Boulevard) is:

18 westbound to northbound right turns
38 westbound through
196 eastbound through
115 southbound to eastbound left turns
367 total vehicles on Bush Street in the AM peak hour

During discussions we noted that the traffic on this segment of Bush Street (between the north and south legs of Winston Churchill Boulevard) includes north-south traffic on Winston Churchill Boulevard that is using this short section of Bush Street to travel north-south, because of the jog on Winston Churchill Boulevard at Bush Street. That is why the traffic volumes on this short segment are higher than the rest of Bush Street to the east and west.

Bush Street (between Winston Churchill and Shaws Creek Road)

There are two sources of traffic counts for this section of Bush Street: the count taken at the intersection of Bush Street at the south leg of Winston Churchill Boulevard, and the count taken for Bush Street at the Shaws Creek Road intersection. Using the traffic count for Bush Street at the south leg of Winston Churchill Boulevard, the AM peak hour volume for Bush Street between Winston Churchill Boulevard and Shaws Creek Road is:

139 eastbound through
7 northbound to eastbound right turns
29 westbound through
6 westbound to southbound left turns
181 total vehicles on Bush in the AM peak hour

Using the traffic count for Bush Street at Shaws Creek Road, the AM peak hour volume for Bush Street between Winston Churchill Boulevard and Shaws Creek Road is:

4 eastbound to northbound left turns
135 eastbound through
3 eastbound to southbound right turns
28 westbound through
6 northbound to westbound left turns
3 southbound to westbound right turns
159 total vehicles on Bush in the AM peak hour

The traffic volumes on Mississauga Road were also reviewed during the conference call, using the traffic count for Mississauga Road at the intersection of The Grange Sideroad, from Display #6.

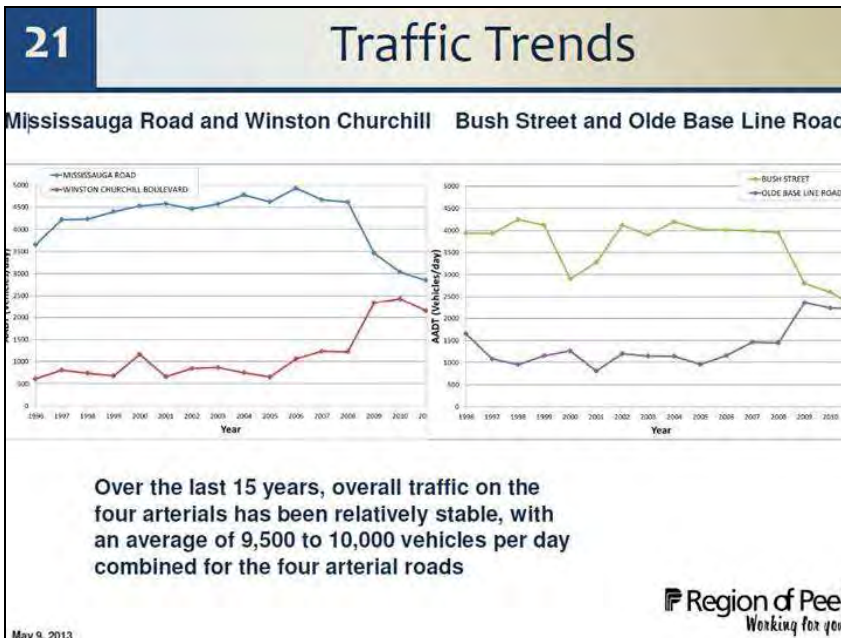
Mississauga Road (north of The Grange Sideroad):

272 southbound through
2 southbound to eastbound left turns
0 southbound to westbound right turns
4 westbound to northbound right turns
40 northbound through
0 eastbound to northbound left turns
318 total vehicles on Mississauga Road in the AM peak hour

Mississauga Road (south of The Grange Sideroad):

272 southbound through
12 eastbound to southbound right turns
3 westbound to southbound left turns
4 northbound to westbound left turns
40 northbound through
5 northbound to eastbound right turns
336 total vehicles on Mississauga Road in the AM peak hour

Display #21 from the May 2013 Public Information Centre was also referred to.



You had expressed the opinion that the traffic counts “don’t make sense”. I had disagreed, especially after going through the traffic calculations together.

We noted that the AM peak hour is approximately 10% of the daily traffic. As an example, we referred to Mississauga Road. The AM peak hour volume calculations of 318 and 336 vehicles per hour on Mississauga Road at The Grange Sideroad compare well with the daily traffic of 3,500 vehicles per hour in 2009 from Display #21. The traffic counts are generally consistent.

During our discussion, we also noted the daily traffic of 2,800 vehicles on Bush Street in 2009, from Display #21. For clarification, this count was conducted just west of Old Main Street. If this count is compared with the traffic volumes shown in Display #6, the following results:

- 16 eastbound to northbound left turns
- 10 eastbound through
- 181 eastbound to southbound right turns
- 32 northbound to westbound left turns
- 10 southbound to westbound right turns
- 249 total traffic on Bush in the AM peak hour

Although we did not go through this calculation during our conference call, we are relaying this information to you to help you compare the 2,800 daily vehicles with the AM peak hour traffic counts. With AM peak hour traffic representing approximately 10% of daily traffic, the 249 AM peak hour volumes in Display #6 times 10 compare well with the 2,800 vehicles shown in Display #21.

You expressed concern that the Region is “not showing all the data”. The Region has always been cooperative in providing information as long as it’s clear what specific information is

being requested. In our conversation, we again offered to provide traffic data that you request, provided that the request is reasonable and that the data is available.

In our discussion, we agreed that the Region will provide the following traffic data requests:

- Intersection traffic counts for Display #6 from the 2010 Public Information Centre
- Traffic counts that show hourly volumes over the day
- Accident summaries – the Region is unable to provide specific collision records from the OPP due to privacy concerns

This information will be mailed to you separately as the documents are too large to send electronically.

As always, we are pleased to work with you to help clarify information.

Kong, Carol

From: Saddi, Asha <Asha.Saddi@peelregion.ca>
Sent: Wednesday, October 02, 2013 4:20 PM
To: [REDACTED]
[REDACTED]
[REDACTED]
Cc: Ganesh, Steve; Dela Cruz, Gino; Gan, Tyrone; 'Susan Cumming'; Restrepo, Veronica
Subject: FW: Accident Summaries
Attachments: Collision Summary.pdf; Traffic Data for CWG.zip; Traffic Data for CWG.doc

Hello CWG Members,

The Project Team has summarised the data requested.

As you are aware public safety on these roads is a priority for the Region of Peel. Irrespective of the number of collisions, the roads are below the TAC standards, for minimum sightlines, horizontal and vertical alignments and other cross-sectional elements that ensure the roads function safely.

Regarding the accident summaries and collision records from the OPP, the question asked was for raw collision data. The Region of Peel is not able to provide specific collision records from the OPP due to privacy concerns. The Project Team has, however, summarized the data into the format requested by [REDACTED] and as such, please find attached a summary of the collision data within the study area from January 1, 2006 to December 31, 2010.

Also attached are the traffic counts available for the current as well as the 2010 studies that were requested from the teleconference with [REDACTED]

If you have any questions on the information provided please don't hesitate to contact our Project Manager, Gino Dela Cruz, at gino.delacruz@peelregion.ca or myself.

Thanks,

Asha Saddi
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax: 905-791-1442
asha.saddi@peelregion.ca

From: Saddi, Asha
Sent: September 19, 2013 10:09 AM
To: [REDACTED]
Cc: Ganesh, Steve; Dela Cruz, Gino; 'Gan, Tyrone'; 'Susan Cumming'; 'Restrepo, Veronica'; [REDACTED]
[REDACTED]
Subject: RE: Accident Summaries

Good Morning [REDACTED]

Thank you for your email.

In order to keep you informed, I wanted to let you know that the Project Team is presently summarising the data requested and I will forward this information to everyone as soon as it is completed.

Thanks,

Asha Saddi
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
asha.saddi@peelregion.ca

From: [REDACTED]

Sent: September 16, 2013 2:00 PM

To: [REDACTED]

Cc: Ganesh, Steve; Dela Cruz, Gino; 'Gan, Tyrone'; 'Susan Cumming'; 'Restrepo, Veronica'; [REDACTED]

Subject: Accident Summaries

Asha

In the PDF document you noted “Accident summaries – the Region is unable to provide specific collision records from the OPP due to privacy concerns”. This statement was not given any context. Could you please give this statement context by detailing the question that would have prompted this response?

Notwithstanding your response to the question above, since safety is one of the Region of Peel’s paramount concerns in the design of the four regional roads in question, the CWG would like to see a breakdown of accidents that have occurred over a period of time (minimum two year period, to a maximum 5 year period). To be useful it should include detail on the number of accidents over that period broken out for each of the individual 4 road segments of the study area, and further sub-broken down in the following categories:

1. Vehicle collisions involving 2 or more motor vehicles, and how many of these reported collisions involved a) no injury or death, b) injury(s) but not death(s) and c) one or more death(s);
2. Vehicle collisions involving a single motor vehicle and a bicyclist(s), and how many of these reported collisions involved a) no injury or death, b) injury(s) but not death(s) and c) one or more death(s);
3. Vehicle collisions involving a single motor vehicle and a pedestrian(s), and how many of these reported collisions involved a) no injury or death, b) injury(s) but not death(s) and c) one or more death(s);
4. Vehicle collisions involving a single motor vehicle and an animal, and how many of these reported collisions involved a) no injury or death, b) injury(s) but not death(s) and c) one or more death(s);
5. Vehicle collisions involving a single motor vehicle that ended up in the ditch (*not involving* another car, bicyclist, pedestrian or animal as counted in the first four statistics above), and how many of these reported collisions involved a) no injury or death, b) injury(s) but not death(s) and c) one or more death(s);

The above categories are meant to cover all collision circumstances without overlapping categories.

In anticipation of privacy concerns, we understand that the Ontario legislation “Freedom of Information and Protection of Privacy Act (FIPPA)” comes into play with respect to what specific information can be released to the public. However, in detailing the request as we have noted above, only aggregated data over a multiple-year period is requested; this

aggregated data requires no personal identifying information that could breach anyone's individual privacy and therefore contravene FIPPA.

Please find attached an MS Word file that contains a simple tabular template that illustrates the request above and that could be used for collating this aggregate collision data.

Thank you for your kind attention to this matter. I am available for further clarification of this request as required.

Best regards,

[Redacted signature]

[Redacted text]

[Redacted text]

This communication is intended only for the use of the individual or entity to whom/which it is addressed, and information contained in this communication is privileged and confidential. If the receiver of this message is not the intended recipient, you are hereby notified that any dissemination, distribution or copying of this communication is strictly prohibited. If you have received this communication in error, please notify the sender of this email (so that we may correct our internal records) and delete this communication without making a copy of it.

From: Saddi, Asha [mailto:Asha.Saddi@peelregion.ca]

Sent: Monday, September 16, 2013 10:26 AM

To: [Redacted]

Cc: Ganesh, Steve; Dela Cruz, Gino; 'Gan, Tyrone'; 'Susan Cumming'; 'Restrepo, Veronica'; [Redacted]

Subject: Conference call with [Redacted]

Good Morning CWG Members,

In the interest of open communication, the Project Team would like to share the information exchanged with [Redacted] during a conference call about the traffic counts and volumes along Bush Street and Mississauga Road.

You will see in the attached summary that reference is also made to additional traffic data requests. These documents are too large to forward to everyone electronically but if you are interested in receiving this information please let me know and I will forward it to you under separate cover.

Thanks,

Asha Saddi

Technical Analyst, Infrastructure Programming & Studies

Transportation Division

Public Works, Region of Peel

Tel: 905-791-7800 x7794

Fax:905-791-1442

asha.saddi@peelregion.ca

Collisions/Accidents on Mississauga Road between Old Base Line and Bush Street, for the period 20xx to 20xx

Motor Vehicle		Single motor vehicle and a bicyclist(s)			Single motor vehicle and a pedestrian(s)			Single motor vehicle and an animal			Single motor vehicle ended up in the ditch <i>involving</i> another vehicle, bicyclist, pedestrian or animal as counted in the first four columns	
How many with injury(s) but no death(s)	How many with death(s)	How many without injury or death	How many with injury(s) but no death(s)	How many with death(s)	How many without injury or death	How many with injury(s) but no death(s)	How many with death(s)	How many without injury or death	How many with injury(s) but no death(s)	How many with death(s)	How many without injury or death	How many with injury(s) but no death(s)

above table for each of the other 3 road segments

Collision Summary

Number of Collisions by causal factor from January 1, 2006 to December 31, 2010 within Study Area

Location	Multiple Motor Vehicles ¹			Single Motor Vehicle and Cyclist(s) ²			Single Motor Vehicle and Pedestrian(s) ²			Single Motor Vehicle and Animal(s) ²			Single Motor Vehicle Only, Involving Off-Road Objects ³		
	Property Damage Only	Non-Fatal Injury	Fatal	Property Damage Only	Non-Fatal Injury	Fatal	Property Damage Only	Non-Fatal Injury	Fatal	Property Damage Only	Non-Fatal Injury	Fatal	Property Damage Only	Non-Fatal Injury	Fatal
Intersection of Olde Base Line Rd and Mississauga Rd	6	1	-	-	-	-	-	-	-	2	-	-	1	-	-
Mississauga Rd Between Olde Base Line Rd And Bush St	7	-	-	-	1	-	-	-	-	8	-	-	5	1	-
Intersection of Mississauga Rd and Bush St	2	-	-	-	-	-	-	-	-	2	-	-	-	-	-
Bush St Between Mississauga Rd And Winston Churchill Blvd	1	-	-	-	-	-	-	-	-	2	-	-	-	-	-
Intersection of Bush St and Winston Churchill Blvd	1	-	-	-	-	-	-	-	-	1	-	-	2	1	-
Winston Churchill Blvd Between Bush St And Olde Base Line Rd	1	1	-	-	-	-	-	-	-	5	-	-	2	-	-
Intersection of Winston Churchill Blvd and Olde Base Line Rd	1	-	-	-	-	-	-	-	-	2	-	-	-	1	-
Olde Base Line Rd Between Winston Churchill Blvd And Mississauga Rd	1	-	-	-	-	-	-	-	-	8	-	-	2	-	-
Total Collisions	20	2	-	-	1	-	-	-	-	30	-	-	12	3	-
	22			1			0			30			15		

Notes:

- 1) 'Multiple Motor Vehicles' collisions include collisions cause by, but did not necessarily collide with multiple motor vehicles.
- 2) 'Single Motor Vehicle' collisions involving cyclists, pedestrians, or animals, include collisions caused by, but did not necessarily collide with the external factor. (Ex. A collision in which a vehicle swerved to avoid an animal and thus collided with the guardrail, was considered a 'Single Motor Vehicle and Animal' collision).
- 3) 'Single Motor Vehicles Only, Involving Off-Road Objects' collisions include collisions in which vehicles lost control due to external factors (not including motor vehicles, cyclists, pedestrians, or animals), and the single motor vehicles ended up in the ditch, or collided with stationary objects such as guiderails or posts.

Table 1: 2009 Turning Movement Count Data, Source: MG8 ENG

Location	Count Date
Mississauga Road and Olde Base Line Road	November 11, 2009
Mississauga Road and The Grange Sideroad	November 11, 2009
Mississauga Road and Woodland Court	June 17, 2009
Mississauga Road and Caledon Mountain Drive	June 17, 2009
Bush Street and Shaws Creek Road	November 17, 2009
Bush Street and Winston Churchill Boulevard NB	November 11, 2009

Note: Traffic data used for the 2010 Mississauga Road / Old Main Street and Bush Street EA

Table 2: 2012 Turning Movement Count Data, Source: MG8 ENG

Location	Count Date
Winston Churchill Boulevard and Olde Base Line Road	July 18, 2012
Winston Churchill Boulevard and 5th Sideroad	July 18, 2012
Winston Churchill Boulevard and The Grange Sideroad	July 17, 2012
Winston Churchill Boulevard and Bush Street	July 17, 2012
Olde Base Line Road and Shaws Creek Road	July 18, 2012
Olde Base Line Road and Rockside Road	July 19, 2012
Mississauga Road and Olde Base Line Road	July 19, 2012
Mississauga Road and The Grange Sideroad	July 24, 2012
Mississauga Road and Woodland Court	July 24, 2012
Mississauga Road and Caledon Mountain Drive	July 25, 2012
Mississauga Road and Bush Street	July 26, 2012 and July 28, 2012 (Saturday)

Note: Traffic data used for the ongoing Belfountain Area Class EA (Mississauga Road / Old Main Street, Bush Street, Winston Churchill Boulevard, and Olde Base Line Road)

Kong, Carol

Subject: FW: Accident Summaries

AMServiceURLStr: https://Slingshot.hdrinc.com:443/CFSS/control?view=services/FTService

From: Saddi, Asha
Sent: October 4, 2013 12:59 PM
To: [REDACTED]
Cc: Dela Cruz, Gino
Subject: RE: Accident Summaries

Hello [REDACTED]

Thank you for your email. Your neighbour, [REDACTED] is welcome to attend the meeting as an observer, this will also give her an opportunity to meet the other CWG members and project team.

As you know we will be serving a light supper before the meeting so please let me know if [REDACTED] has any dietary requirements.

Thanks.

Asha Saddi
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
asha.saddi@peelregion.ca

From: [REDACTED]
Sent: October 2, 2013 9:25 PM
To: Saddi, Asha
Subject: Re: Accident Summaries

Thank you for the info Asha. I wanted to let you and your team know that my neighbour, [REDACTED] would like to attend the working group meeting on Oct 16 as an observer.

Thanks!

[REDACTED]
[REDACTED]
[REDACTED]

Kong, Carol

From: Saddi, Asha <Asha.Saddi@peelregion.ca>
Sent: Tuesday, October 15, 2013 9:14 AM
To: [REDACTED]
Cc: Dela Cruz, Gino; Ganesh, Steve; Gan, Tyrone; Restrepo, Veronica; 'Susan Cumming'
Subject: RE: CWG Meeting #3: Wednesday, October 16

Hello [REDACTED]

Thank you for your email.

Sorry you won't be able to attend the meeting on Wednesday; I will forward your comments to the Project Team.

Asha Saddi
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
asha.saddi@peelregion.ca

From: [REDACTED]
Sent: October 15, 2013 7:32 AM
To: Saddi, Asha
Subject: Re: CWG Meeting #3: Wednesday, October 16

Hi Asha – once again the meeting conflicts with the parent night at **my daughters'** school so I am unable to attend but I have reviewed the informative materials and am pleased to see the general proposal for speed reduction. Everything looks very sensible in the study so far. Thank you – [REDACTED]

[REDACTED]
[REDACTED]

From: "Saddi, Asha" <Asha.Saddi@peelregion.ca>

Date: Friday, October 11, 2013 3:53 PM

To: [REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Cc: "Ganesh, Steve" <Steve.Ganesh@peelregion.ca>, "Dela Cruz, Gino" <Gino.DelaCruz@peelregion.ca>, "'Gan, Tyrone'" <Tyrone.Gan@hdrinc.com>, 'Susan Cumming' <cumming@total.net>, "'Restrepo, Veronica'" <Veronica.Restrepo@hdrinc.com>

Subject: CWG Meeting #3: Wednesday, October 16

Hello CWG Members,

Please find attached the presentation slides that we will be reviewing at next Wednesday's meeting.

We look forward to seeing you all at the meeting.

Thanks,

*Asha Saddi
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
asha.saddi@peelregion.ca*

Kong, Carol

From: [REDACTED]
Sent: Friday, October 18, 2013 3:47 PM
To: Ganesh, Steve
Cc: [REDACTED] Dela Cruz, Gino; Gan, Tyrone; Saddi, Asha; Restrepo, Veronica; cumming@total.net
Subject: Re: Updated: Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Baseline Road Class EA - CWG Meeting #3

Thk u I will wait for your response and will add another question to the mix.....Who is responsible for the designation TRUCK HAUL ROUTE...what governance deems a road a truck haul route?

Many thks
[REDACTED]

Sent from my iPad

On Oct 17, 2013, at 18:11, "Ganesh, Steve" <Steve.Ganesh@peelregion.ca> wrote:

Hello [REDACTED]

I have copied the study team so we can prepare a response to your question. As you may recall the common practice for the team is to share all inquiries and responses with CWG members. You may also note that [REDACTED] asked a similar question of the team after most CWG members left last night.

To this end, my preference will be to prepare one consolidated response to all CWG members that addresses this issue.

From an over arching perspective, I offer the initial comments, which will be shared with others:

The four roads subject to the EA are Regional Roads and will be constructed to Regional standards that adhere to sound principles of safety and state of good repair. This message has been consistent throughout the study process. While cost is a factor in the EA process, the legislation is designed to ensure the proponent (in the case, Peel) look at all factors...ranging from natural, heritage, and socio-economic. This EA is a process to take the project to 30% preliminary design and therefore cost estimates of construction that we provide can be fine tuned as the project advances beyond the EA to detailed design..which takes the project from 30% to final design, tender and construction.

Beyond the 30% stage this project will have two major check points to verify cost and constructability...what is known as the 60% and 90% design stages.

In light of the above, by nature of the scope of this EA... the construction cost estimates for the recommend profiles, etc can be subject to change in the detailed design stages.

Regards,

Steve

Sent from my BlackBerry 10 smartphone on the Bell network.

From: [REDACTED]
Sent: Thursday, October 17, 2013 5:04 PM
To: Ganesh, Steve
Cc: [REDACTED]
Subject: Re: Updated: Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Baseline Road Class EA - CWG Meeting #3

Hi Steve, good meeting last evening. Please ask whoever knows the answer! If the roads are built according to how they have been mapped out to us.... Will they be up to truck haul standards ie aggregate trucks ???AND if so can they be constructed as mapped out but at less cost that is suitable for the traffic patterns and local deliver trucks that we are experiencing now?
thank you

[REDACTED]

Sent from my iPad

On Oct 11, 2013, at 20:51, "Ganesh, Steve" <Steve.Ganesh@peelregion.ca> wrote:

Hello [REDACTED]

Yes, those slides are most likely title slides. The plans and profiles are complete for the entire study area and we will present on Wednesday.

I suspect because of their scale they are hard to fit in a format suitable for PowerPoint or Pdf.

We look forward to showing you the proposed profile changes that minimize property acquisition...except only where necessary. The roll plan will show the grade changes required in certain areas to bring site lines, etc up to standard...such Olde Base and demonstrate that our design will maintain the rural character of the study area. For the PIC we will plot at a scale of 1:1000 to give people a nice visual.

If you would like to view the plans prior to Wed evening I can arrange for you to come by my office and walk you through. Please let me know.

Regards
Steve

Sent from my BlackBerry 10 smartphone on the Bell network.

From: [REDACTED]
Sent: Friday, October 11, 2013 8:38 PM
To: Saddi, Asha; [REDACTED]
[REDACTED] Dela Cruz, Gino; 'Gan, Tyrone'; 'Susan Cumming'; Lee, Thomas; 'Sue Cumming'; [REDACTED] Restrepo, Veronica'; [REDACTED]
Cc: [REDACTED] Paterak, Richard
Subject: RE: Updated: Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Baseline Road Class EA - CWG Meeting #3

Thanks for the presentation pack. [REDACTED] in the presentation pack, we see blank spaces under the headers for each of the four road 'plan and profile' on pages 16,17, 18, and 19.

Curious about what happened there.

[REDACTED]

From: Saddi, Asha [<mailto:Asha.Saddi@peelregion.ca>]
Sent: October-09-13 10:55
To: [REDACTED]
[REDACTED] Ganesh, Steve; Dela Cruz, Gino; 'Gan, Tyrone'; 'Susan Cumming'; [REDACTED] Sue Cumming'; Thomser [REDACTED] Restrepo, Veronica'; [REDACTED]
Cc: [REDACTED] Richard
Subject: RE: Updated: Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Baseline Road Class EA - CWG Meeting #3

Good [REDACTED]

Thank you for your email. We will be forwarding a presentation to CWG members by the end of the week to review in advance of next week's meeting.

Thanks,

Asha Saddi
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
asha.saddi@peelregion.ca

From: [REDACTED]
Sent: October 7, 2013 9:57 AM
To: Saddi, Asha; [REDACTED]
[REDACTED] Dela Cruz, Gino; 'Gan, Tyrone'; 'Susan Cumming'; [REDACTED] Sue Cumming'; [REDACTED] Restrepo, Veronica'; [REDACTED]
Cc: [REDACTED] Richard

Subject: RE: Updated: Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Baseline Road Class EA - CWG Meeting #3

Hello Asha;

In a prior note, you mentioned the roads in the study area are not up to TAC standards in terms of minimum sightlines, and horizontal and vertical alignments. For next Wednesday's meeting, please have the team prepared to show and tell us how that standard will specifically affect the roads in the study area. I realize folks have suggested roads I go and see for a reference, but I'm interested in how the standard could be applied to the roads in this study area.

Thank you,



-----Original Appointment-----

From: Saddi, Asha [<mailto:Asha.Saddi@peelregion.ca>]

Sent: October-04-13 12:12

To: [Redacted]

[Redacted]

[Redacted] Dela Cruz, Gino; 'Gan, Tyrone'; 'Susan Cumming'; [Redacted]; 'Sue Cumming'; Thomser [Redacted]

[Redacted] Restrepo, Veronica'; [Redacted]

Cc: [Redacted]

Subject: Updated: Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Baseline Road Class EA - CWG Meeting #3

When: October-16-13 18:30-21:00 (UTC-05:00) Eastern Time (US & Canada).

Where: Belfountain Community Centre - 17204 Main Street, Caledon ON

When: October 16, 2013 6:30 PM-9:00 PM (GMT-05:00) Eastern Time (US & Canada).

Where: Belfountain Community Centre - 17204 Main Street, Caledon ON

Note: The GMT offset above does not reflect daylight saving time adjustments.

~~*~*~*~*~*~*~*~*

Agenda is attached.

Arrival time is from 6:00 p.m. and a light supper will be served.

<< File: Agenda October 16 2013.docx >>

Kong, Carol

From: Ganesh, Steve <Steve.Ganesh@peelregion.ca>
Sent: Wednesday, October 23, 2013 12:08 PM
To: [REDACTED]
[REDACTED]
[REDACTED] Dela Cruz,
Gino; Gan, Tyrone; 'Susan Cumming'; [REDACTED] 'Sue Cumming'; [REDACTED]
Restrepo, Veronica; [REDACTED]
Cc: [REDACTED] Paterak, Richard; [REDACTED]
[REDACTED]
Subject: Axel Load Restrictions and Truck Haul Routes
Attachments: AASHTO Pavement Design Guide for Ontario Conditions.pdf
Importance: High

Hello CWG Members:

Following the CWG meeting last Wednesday the Project Team received two inquiries; the first was from David [REDACTED] about axel weights and the second from [REDACTED] about truck haul routes. Please see below the project team response to both inquiries

Purpose of this Environmental Assessment (EA)

The four roads subject to the EA are Regional Roads and will be constructed to Regional standards that adhere to sound principles of safety and state of good repair. This message has been consistent throughout the study process. Our goals for this EA have been to address the deficiencies that we have identified pertaining to the pavement, roadside drainage and safety conditions. We have aimed to address these deficiencies while minimizing impacts to the rural character, natural and heritage environments, wildlife and habitats, and resident's properties. At the same time, we are also enhancing the utilization of the right-of-way space to include active transportation for the community. The design we are proposing follows Regional standards which references various technical standards that are used regionally, provincially and nationally (e.g. TAC standards, AASHTO Guide for the Design of Pavement, etc.).

Response to October 17, 2013 email from [REDACTED] and October 20, 2013 email from [REDACTED]

In terms of pavement structure, our proposed 30% design follows the standard process for designing pavements in Ontario. Generally, the procedure is to assess the existing deficiencies of the pavement and base, and forecast the future traffic that will use the roadways and design a pavement structure that accommodates these conditions. The forecasted traffic is based on existing traffic and existing truck volumes, plus a nominal growth rate (2%) is applied to get the future traffic volumes. The existing traffic volumes and existing truck volumes are reflective of the current truck restrictions on the study area roads (see Region of Peel By-law 15-2013: <http://www.peelregion.ca/council/bylaws/2010s/2013/by15-2013.htm#r>). ***The EA is not proposing to change any of the existing truck restrictions.*** Any future changes to the By-law would be subject to a decision by Regional Council.

A summary of the current axle load weight restrictions according to By-law 15-2013 is provided below. Axle load weight restrictions are set to protect Peel Regional roads from excessive damage. Each year from March

1 to April 30, any axle on a commercial motor vehicle or trailer weighing more than 5,000 kg (5 tonnes per axle) is NOT allowed on any Regional road listed below.

Regional Road	From	To	Prohibited Months or Time
Bush Street / Forks of the Credit Road	Highway 10	The boundary between the Region of Peel and the County of Wellington	March and April
Mississauga Road	Mayfield Road	Bush Street / Forks of the Credit Road	March and April
Olde Base Line Road	McLaughlin Road	Winston Churchill Boulevard	March and April
Winston Churchill Boulevard	Bovaird Drive West	Bush Street / Forks of the Credit	Anytime

Source: SCHEDULE A, PART 22 OF BY-LAW NUMBER 15-2013, AXLE LOAD WEIGHT RESTRICTIONS

Conclusion

As noted above, our design goal has been focused on addressing the pavement, drainage and safety deficiencies while minimizing impacts to the rural character and natural environment. The currently preferred design reflects these goals and study principles **and does not make any recommendations to designate any of the roads as a ‘truck haul route’ or remove or alter any of the existing truck restrictions (referenced above)**. Specifically, we are proposing 3.5m lane widths (typical lane widths for Regional Roads with higher volumes of trucks are 3.65-3.75m); 1.7m shoulder for active transportation (typical shoulder widths for Regional Roads with higher volume of trucks are greater than 2.0m); and 60km/h and 50 km/h posted speed on Winston Churchill Boulevard and Olde Base Line Road, respectively, to address sight line deficiencies while minimizing impacts to the vertical profile (typical posted speeds for Regional Roads with higher volumes of trucks are 70-80 km/h).

In terms of moving forward and next steps, it is important that we keep in mind that this EA will be submitted to the Ministry of the Environment and reviewed on the merits of its scope as per provisions of a Municipal Class Schedule B EA. As for related issues that have come up during this EA, such as the Region’s Strategic Goods Movement Network (SGMN), the Region will embark to undertake further feasibility studies for the Strategic Goods Movement Network Study to determine which segments should remain in the network. In addition to this feasibility work, the Region may at a future date consider revising the Right-of-Way for Winston Churchill Blvd to less than 30m as currently allowed in the Official Plan. Merits of this reduction would be based on:

1. The findings of this EA which has demonstrated the proposed design can fit within the 20m ROW (for the most part)
2. The Region’s forthcoming update to the Long Range Transportation Plan may verify that the growth projected from 2031 to 2041 in this section of Peel does not warrant additional lanes.

On behalf of the project team I would like to take this opportunity to thank you for your time in participation of the CWG. As you know, we are moving forward with PIC No. 2, scheduled for November 20, 2013. In the meantime, should you require any additional information please contact me. For those of you who may be interested, please see attached the pavement design for Ontario conditions.

Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Infrastructure Programming & Studies

Public Works, Transportation Division
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Thursday, December 05, 2013 2:22 PM
To: Restrepo, Veronica; Gan, Tyrone; 'Susan Cumming'
Subject: FW: REMINDER: Important Local Roads Meeting: Nov 20th, 4:30 pm - 8:30 pm Caledon Country Club

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: Dela Cruz, Gino
Sent: November 25, 2013 3:31 PM
To: [REDACTED]
Subject: RE: REMINDER: Important Local Roads Meeting: Nov 20th, 4:30 pm - 8:30 pm Caledon Country Club

Good afternoon [REDACTED] – thanks for copying me on the email. I will be sure to share this with the project team and include it on the project record of public correspondence.

Let me know if you have any additional comments or questions on this study.

Best regards,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: November 23, 2013 12:09 PM
To: Dela Cruz, Gino
Subject: Fwd: REMINDER: Important Local Roads Meeting: Nov 20th, 4:30 pm - 8:30 pm Caledon Country Club

Hello Gino....Thanks for taking the time to discuss this issue with me. I thought you might want me to copy you in on the following E mail sent to these councillors.

Regards,
[REDACTED]

----- Forwarded message -----

From: [REDACTED]
Date: Sat, Nov 23, 2013 at 11:54 AM
Subject: Re: REMINDER: Important Local Roads Meeting: Nov 20th, 4:30 pm - 8:30 pm Caledon Country Club
To: [REDACTED]

Cc: [REDACTED]

Hi ALL.....

It's a relief that the EA has concerns that Base Line not be reconstructed as a heavy truck route. I think it took a lot of "wind out of the sails" for many residents. It's a beautiful country road...what Caledon is all about. I don't mind the "weekend Caledon wannabees"...it's the noisy tri-axle gravel trucks with pup trailers that concerns me, the horses and cyclists.

When [REDACTED] gets his ducks in order and develops his gravel pit off Winston Churchill, north of Belfountain, the haul route can be established along Wellington roads #52/124 , Trafalgar Rd. south to Hwys #7/ 401/ 407, and the future truck bi-pass. This route is already in use from many pits, including the Erin pit at the 10th line. The Belfountain pit would exit onto the section of Winston Churchill that is a Wellington road; so why not have the entire route in Wellington / Halton? This is a shrewd approach; Peel gets the gravel taxes...fewer trucks on our roads, and Wellington/ Halton has to maintain their haul route !!

Cheers !

[REDACTED]

On Thu, Nov 14, 2013 at 10:01 PM, [REDACTED] wrote:

REMINDER REMINDER REMINDER REMINDER

From: [REDACTED]
Sent: November-04-13 07:19
Cc: [REDACTED]
Subject: Important Local Roads Meeting: Nov 20th, 4:30 pm - 8:30 pm Caledon Country Club

The CCC has said it one way.....although that applies to Belfountain too, what I will add is more specific to Belfountain...the design and look of the road is still yet to be determined as well as the much desired community connectivity and pedestrian and cyclist safety concerns addressed. Intersection configuration at Mississauga Road and Olde Baseline and at Bush Street and Winston Churchill Boulevard, governing safety and traffic volume allocation, is also still in question.

Please consider attending, and express your views.

[REDACTED]

From: The Coalition of Concerned Citizens [<mailto:info@coalitioncaledon.com>]

Sent: October-30-13 23:12

To: [REDACTED]

Subject: Important local roads meeting: Nov 20th, 4:30 pm



**IMPORTANT MEETING REGARDING YOUR LOCAL ROADS
NOVEMBER 20th, 2013 - 4:30-PM-8:30 PM
Caledon Country Club**

Dear Resident/Neighbour,

As you may know, the Transportation Division of the Department of Public Works at the Region of Peel has been completing a Municipal Class Environmental Assessment Study of the Winston Churchill Blvd, Old Baseline Road, Mississauga Road and Bush Street rectangle to identify transportation issues and needs in the corridor. This EA is nearly complete. A final public meeting (Public Information Centre # 2) to present the proposed road reconstruction will be held on Wednesday, November 20th from 4.30 to 8.30 pm at the Caledon Country Club, 2121 Olde Base Line Road, Caledon L7C 0K7

All residents in this area will be affected by the proposed road reconstruction as it will involve the widening of the roads in certain areas to provide for paved shoulders and wider travelling lanes, and the creation of drainage ditches where the 30 metre right of way claimed by the Region is available. Where the 30 metre right of way is not available, the Region is proposing the construction of mountable curbs to facilitate drainage. The plans also involve the partial leveling of certain portions of the roads to improve sightlines. You will have the opportunity at the Public Information Centre to see how your property may be affected and to speak directly to the Region's staff about it.

In addition, the Peel Region Strategic Goods Movement Network Study, (otherwise known as the Truck Haul Route Study) endorsed by Regional Council on May 9th, 2013, proposes that the truck weight restrictions currently in place on the Winston Churchill Boulevard to Old Baseline Road to Mississauga Road corridor be removed to allow the route to become a **heavy truck haul route**. The upcoming Public Information Centre presents an excellent opportunity for local residents concerned about this to express their views to Regional staff and Councillors who will be in attendance.

We hope that you will be able to attend. Please feel free to contact any one of us if you have any questions.

The Roads Committee of the Coalition of Concerned Citizens

[Redacted]

Coalition of Concerned Citizens
Tel: [\(905\) 838-4333](tel:9058384333) | Fax: [\(905\) 702-1640](tel:9057021640)
info@coalitioncaledon.com | www.coalitioncaledon.com
To unsubscribe from future emails, Please [click here](#)

Kong, Carol

From: Saddi, Asha <Asha.Saddi@peelregion.ca>
Sent: Monday, November 11, 2013 12:38 PM
To: [REDACTED]
[REDACTED]
[REDACTED]
Cc: Ganesh, Steve; Dela Cruz, Gino; Gan, Tyrone; 'Susan Cumming'; Restrepo, Veronica
Subject: Factsheet
Attachments: Factsheet.pdf

Hello CWG Members,

Please find attached for your information a Factsheet which outlines the design differences between the Olde Base Line Road and Winston Churchill Boulevard roads and typical high truck volume roads in the Region of Peel.

The Factsheet will be uploaded to the study website and will also be available at the PIC next week.

Thanks,

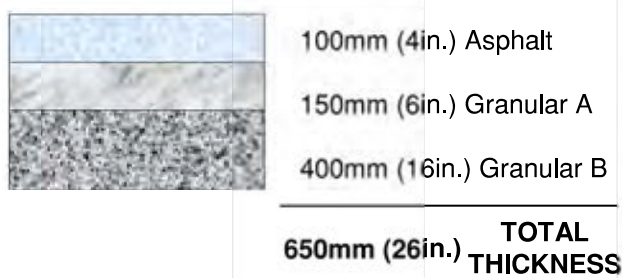
Asha Saddi
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Tel: 905-791-7800 x7794
Fax:905-791-1442
asha.saddi@peelregion.ca

What is the difference between the design for the Olde Base Line Road and Winston Churchill Boulevard roads vs typical high truck volume roads in the Region of Peel?

This area is not designated for significant future growth, therefore the volumes will remain relatively constant. There is no need to construct the pavement to handle significant volumes of truck traffic.

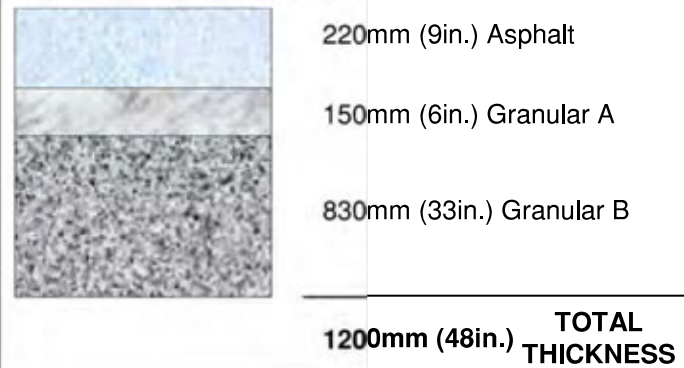
Current EA Design

1. Pavement Structure



Typical Pavement Design for High Truck Volume Regional Roads

e.g. Mayfield Road from Chinguacousy Road to Heart Lake Road



Current EA Design

2. Lane Widths



The paved shoulders will allow for safe movement of cyclists, pull over for emergency parking, and oversized agricultural equipment.

Typical Lane Widths for High Truck Volume Regional Roads



The 3.75m lane widths permit wide truck dimensions.

This EA is not recommending a pavement structure for high volume trucks

Current EA Design

3. Speed

Proposed posted speeds



Proposed 60 km/h for
Winston Churchill Boulevard



Proposed 50 km/h for
Olde Base Line Road

**Typical Posted Speed for High
Truck Volume Regional Roads**

e.g. Mayfield Road, Airport Road, and,
Highway 50



Contact Us

If you have any questions or concerns about the project, please contact **Gino Dela Cruz, Project Manager, Region of Peel** at 905-791-7800 x7805 or email at Gino.Dela.Cruz@peelregion.ca.

Further Information

To obtain more information about the study please visit our study website at:

<http://www.peelregion.ca/pw/transportation/envirom-assess/mississauga-road-bush.htm>

Kong, Carol

From: Gan, Tyrone
Sent: Wednesday, November 13, 2013 5:08 AM
To: Restrepo, Veronica
Subject: FW: 2011 Roads Report: Old Baseline and Mississauga Road Intersection

TYRONE GAN
P. Eng.

HDR Corporation

Senior Vice President
Director of Transportation, Canada

100 York Boulevard, Suite 300 | Richmond Hill, ON L4B 1J8

t: [289.695.4622](tel:289.695.4622) | f: 289.695.4601

Tyrone.Gan@hdrinc.com | hdrinc.com

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From: [REDACTED]
Sent: Tuesday, November 12, 2013 2:40 PM
To: [REDACTED]
Cc: Ganesh, Steve; Gan, Tyrone; Saddi, Asha
Subject: 2011 Roads Report: Old Baseline and Mississauga Road Intersection

Hello RP;

Thanks for forwarding the report.

I see that about two-thirds of collisions in the Region occur at intersections...this is consistent for Caledon...esp. if one removes collisions with wildlife (i.e. with deer) from the equation....I think Caledon would hold the majority of the stats on that aspect of the greater stats....regardless, there seems to be some concern about the stats for the intersection of Old Base Line and Mississauga Road. This was mentioned in the April PIC material...there was also mention at the October session of the sightline issue, e.g., travelling westbound and looking southbound. **Is it still the opinion to do nothing about this collision site?**

I suppose 2011 would be too early for any stats re. the traffic circle's effectiveness at reducing speed and increasing safety, along with energy savings...

■

Kong, Carol

Subject: RE: Roads Terminology

AMServiceURLStr: <https://Slingshot.hdrinc.com:443/CFSS/control?view=services/FTService>

From: [REDACTED]

Sent: Tuesday, November 19, 2013 9:19 PM

To: 'Saddi, Asha'

Cc: [REDACTED]

[REDACTED] Gan, Tyrone; 'Dela Cruz, Gino'; Restrepo, Veronica; 'Susan Cumming'

Subject: RE: Roads Terminology

Strategic Goods --> no RoP formal definition

High Truck Volume Roads ---> no RoP formal definition

In the future, perhaps these terms should not be used in assumedly official ways in official fora, such as CWG sessions and the like. I also remember back in the spring public meeting, an announcement was made by [REDACTED] about a strategic goods motion that was passed by Regional Council just hours earlier - quite the **bombshell at the time**. if there is no official definition of strategic goods, what was the vote all about?

Really puzzled now,

From: Richard Paterak [mailto:richard.paterak@caledon.ca]

Sent: Monday, November 18, 2013 2:34 PM

To: 'Saddi, Asha'; [REDACTED]

Cc: Ganesh, Steve; [REDACTED]

[REDACTED] Gan, Tyrone; Dela Cruz, Gino; Restrepo, Veronica; 'Susan Cumming'

Subject: RE: Roads Terminology

Asha,

In that there is an elevated public consciousness about gravel trucks within this study area, it might be useful to understand that the Town of Caledon Official Plan does refer to the route which will be used by a given aggregate operation as "the haul route." Further I believe that "the haul route" is identified on the Site Plan of an aggregate license. In that there are no licensed aggregate operations on any of the road sections in question none of these sections has been identified as a haul route. With this in mind Regional staff should apply appropriate side comments and footnotes on what a haul route is and how it is assigned according to the Town OP as the opportunity arises.

Regards,

Richard

From: Saddi, Asha [mailto:Asha.Saddi@peelregion.ca]

Sent: November-18-13 14:22

To: [REDACTED]

Cc: [REDACTED]

[REDACTED] 'Gan, Tyrone'; Dela Cruz, Gino; 'Veronica.Restrepo@hdrinc.com'; 'Susan Cumming'

Subject: RE: Roads Terminology

Hello [REDACTED]

Thank you for your email. In terms of a response, please see below.

With respect to "strategic goods", there is no formal definition, however the Region's Strategic Goods Movement Network Study (SGMNS) can be found on our website at:

<http://www.peelregion.ca/pw/transportation/goodsmovement/peel/study.htm> . You can view and download the full report of the SGMNS at: <http://www.peelregion.ca/pw/transportation/goodsmovement/peel/pdf/goods-movement-final-report.pdf> and a summary of this report is also available at:

<http://www.peelregion.ca/pw/transportation/goodsmovement/peel/pdf/goods-movement-executive-summary.pdf> in particular, pages 5 of the summary outlines the definitions of a Strategic Truck Route, Primary Truck Route and a Truck Route Connector.

The Region of Peel does not have a formal definition of high truck volume roads or haul routes roads. As outlined in the Factsheet (see attached), Regional roads such as Highway 50, Airport Road and Mayfield Road are designed for high truck volumes. The example of Mayfield Road shown in the Factsheet was designed for a truck percentage of approximately 15% of the regular traffic. In comparison, the truck percentage used for the roads in this EA is approximately 5% of the regular traffic. Additionally, the EA design recommends lower posted speeds, narrower lane widths and a thinner pavement structure that is not designed for high truck volumes.

Thanks,

Asha Saddi

Technical Analyst, Infrastructure Programming & Studies

Transportation Division

Public Works, Region of Peel

Tel: 905-791-7800 x7794

Fax:905-791-1442

asha.saddi@peelregion.ca

From: [REDACTED]

Sent: November 14, 2013 9:17 AM

To: Saddi, Asha; 'Gan, Tyrone'; Ganesh, Steve

Cc: Paterak, Richard

Subject: Roads Terminology

Folks;

I trust you can clear up something I find a bit confusing....what's the difference in road construction terms for 'strategic goods' versus 'high truck volume' versus 'haul route' versus what the current plan is for the EA roads...it's all very confusing. What does it all mean from a practical real life perspective?

Thanks,

[REDACTED]

Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Monday, December 09, 2013 12:43 PM
To: Restrepo, Veronica; 'Susan Cumming'; Gan, Tyrone
Cc: Saddi, Asha
Subject: FW: Roads EA - Belfountain Specifics

FYI and documentation – latest correspondence with [REDACTED]

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: December 7, 2013 1:14 PM
To: Ganesh, Steve
Cc: Saddi, Asha; [REDACTED]; Dela Cruz, Gino; [REDACTED]
Subject: RE: Roads EA - Belfountain Specifics

Steve

Thank you for your response. I could mention a dozen elements worth further discussion. That was the point of previous email. Not just the two that I mentioned in my email, which you refer to below. So while I appreciate your response, there are other issues/concerns that should be looked at further. I am disappointed the finality of your response, that is, that consultation is complete. As a community stakeholder in this project, let me reiterate, I am disappointed in the finality expressed in your response.

Best regards,

From: Ganesh, Steve [mailto:Steve.Ganesh@peelregion.ca]
Sent: Friday, December 6, 2013 4:17 PM
To: [REDACTED]
Cc: Saddi, Asha; Albanese, Damian; Dela Cruz, Gino; [REDACTED]
Subject: RE: Roads EA - Belfountain Specifics

Hello [REDACTED]:

Thank you for your comments. Peel recognizes that Belfountain and surrounding area is a unique and special place and the residents want to be engaged. This is why we chose to undertake this Environmental Assessment (EA) with an enhanced consultation process involving open houses, PIC's and a Community Working Group (CWG). We appreciated the participation of you and all members of the CWG.

Recommended Cross-Section for the Village - Bush Street

The project team developed a cross-section to maintain the character of the village, in particular Bush Street. The semi-rural cross-section provides the balance between environmental impacts, safety and functionality within a very constrained narrow right-of-way. This is achieved through design elements (i.e. 3.3m lane widths,

1.7m sidewalks) to ensure the property impacts to residents are minimized. I have attached a copy of the Village Cross-Section along Bush Street for your reference.

Orb Development

It is important to note that the 5m allowance through the Orb Development (as referenced in the December 3, 2013 email from David Jobe) for trail connectivity can be examined and obtained gratuitously through the Planning Act, a process separate from this EA. Once the Draft Plan of Subdivision is filed with the Town of Caledon, the Town will administer a public consultation process to review the proposal. Through this process, Peel can work with the Town on examining property acquisition for the trail.

Key Messages from Stakeholders/Community - And how the design addresses them

1. Maintain the rural character

- Minimized profile changes and kept the rolling profile by adopting lower design speeds than typical Regional roads
- Minimized impacts to property, existing driveways and natural/heritage/cultural features by adopting semi-rural cross-section where the right-of-way is constrained
- Lane widths are narrower than typical Regional roads to help lower traffic speeds and minimize property impacts

2. Promote active transportation

- Provided paved shoulders throughout the road corridors to accommodate cyclists and improved safety for all road users
- Paved shoulders also serve to accommodate larger farm vehicles that occasionally use the study area roads

3. Context-sensitive / Multi-modal Village Design

- Provided pedestrian facility on one side of the road for walking in the Village and connecting to points of interest such as the 'downtown businesses' (Old Main St / Bush St), the Belfountain elementary school (Shaw's Creek Rd/ Bush St), and the Belfountain Community Centre.
- Provided for parking along Old Main St from north of the Belfountain Community Centre to Bush St where we have space in the right-of-way
- Lane widths are narrower than typical Regional roads to help lower traffic speeds and minimize property impacts

4. Address safety and excessive traffic speeds

- Lowered design speeds throughout the study area
- Improved sightlines at driveways and throughout the road corridors by adjusting vertical profile up to safe road standards
- Lane widths are narrower than typical Regional roads to help lower traffic speeds and minimize property impacts
- More consistent posted speeds along Mississauga Rd. and Winston Churchill Blvd.
- Improved conditions for potential animal related collisions by adopting lower design speeds, improving sight lines, and deer/other wildlife crossing mitigation measures (such as enhanced signage, fencing, crossings, etc. this will be reviewed in detailed design in consultation with MNR).

5. Address deficient pavement and drainage conditions

- Provided proper road base and pavement structure
- Provided proper drainage with both semi-rural and rural cross-sections to take water away from the road base and pavement structure

Next Steps

The Environmental Study Report (ESR) for this EA is scheduled to be released in May 2014. At that time the public will have opportunities to submit comments. The ESR will note the following text that speaks to the issues you have raised.

- If the Orb development application is submitted during the detailed design, the Region in conjunction with the Town of Caledon can examine property options for a possible trail connection.

- Review the feasibility of using alternative materials for curbs and other roadway elements to maintain the rural character and 'look' of the study area (e.g. using dark coloured curbs to blend in with the asphalt to be less noticeable)

Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Infrastructure Programming & Studies
Public Works, Transportation Division
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: [REDACTED]
Sent: December 4, 2013 7:43 PM
To: Ganesh, Steve
Cc: Saddi, Asha; [REDACTED]; Dela Cruz, Gino; [REDACTED] Paterak, Richard
Subject: RE: Roads EA - Belfountain Specifics

Steve

While I appreciate you have may have met your obligations regarding public consultations under the confines of your EA scope, I believe you are missing an golden opportunity participate with the local community in a little more depth, while there is still the opportunity before your design becomes more rigid and has less chance of being modified.

Talking to my neighbours, there is clearly community concern over the design. And I do not mean detail design, but rather broad stroke design. For instance, your design for Bush Street shows rollover curbs with grated drainage, which may not be in keeping with community wishes, and without alternatives ever presented during the consultation process. David Jobe's point below is another example of local concern; while you note that "To accommodate sidewalks through this constrained area and beyond would mean significant property and environmental impacts", I for one believe it would not hurt to give this further exploration.

We care very much about the rural look and feel of our community, and the impact that a potential 'cookie-cutter' regional standard design configuration may impose. There is a wonderful opportunity for the Region to take advantage of an engaged local community in the Region's provision to the community with a design that the community is proud of and that does not potentially change our rural character, perhaps drastically. Once you have upgraded the roads, they will be permanently changed; there will be no opportunity for a 'do-over' here. The Region's principle drivers for the upgraded roads are safety and low long-term maintenance costs. As community stakeholders, we must add the equally important drivers of functionality and aesthetics. It would be sad and disheartening to our community if we missed the opportunity to have them incorporated at this, the pre-design stage.

I for one would like you and your team (or a subset as appropriate) to meet with us again, even if it is under the auspices of good public relations with the ratepayers (rather than the 'official' EA process). As I mentioned, this is an opportunity for further local input of Belfountain Hamlet people on what is not just a Regional road (Old Main Street and Bush Street), but what is more appropriately framed as 'our local town' road.

[REDACTED]

From: Ganesh, Steve [<mailto:Steve.Ganesh@peelregion.ca>]
Sent: Wednesday, December 4, 2013 3:01 PM
To: ██████████ Paterak, Richard
Cc: Saddi, Asha; ██████████ Dela Cruz, Gino
Subject: RE: Roads EA - Belfountain Specifics

Hello Councillor Paterak ██████████:

With respect to the proposed development by Orb, the opportunity to acquire the 5m allowance gratuitously would be via the development process under provisions of the Planning Act. To address this matter at a “high level” in the EA, we could document this proposed Draft Plan in the Environmental Study Report (ESR) and examine at the appropriate time once the Region is in receipt of the Draft Plan.

██████████ – To your point about consultation “not being over” and the Hamlet design requiring approval through the Belfountain Community Group....

The Ministry of the Environment is the ultimate approval body for this EA. You may recall that Peel is undertaking this EA through the provisions of a Schedule C process. Section A.3.4.1(b) of Municipal Class Environmental Assessment requires a mandatory two points of contact in the way of Public Information Centres (PICs). In this EA Peel has undertaken following public points of contact:

- Project Open House – Oct 30, 2012
- PIC No. 1 – May 9, 2013
- PIC No. 2 – Nov 20, 2013

In addition we have developed a Community Working Group (CWG) which you were a member of, and Peel undertook three CWG meetings on:

- Oct 23, 2012
- April 4, 2013
- Oct 16, 2013

The project team also took the time to prepare several alternatives before presenting the preferred alternatives/designs on Nov 20, 2013. According to the CWG meeting notes of Oct 16, 2013 (which you attended), item 4.5.2 indicates that the project team would consider sidewalks and multi-use trails in areas without existing constraints. The material as provided by Gino dated Dec 3, 2013 adequately addresses this matter and has identified the constraints/pinch points.

In light of the above, the Region is not planning any further meetings with the public or special interest groups (i.e. BCO). Once the EA is filed, the ESR will provide you an opportunity to review how the project team as documented your comments.

Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Infrastructure Programming & Studies
Public Works, Transportation Division
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: [REDACTED]
Sent: December 3, 2013 6:33 PM
To: Dela Cruz, Gino; Ganesh, Steve; Paterak, Richard
Cc: Saddi, Asha; [REDACTED]
Subject: RE: Roads EA - Belfountain Specifics

There is nothing new here in what you are putting forward; I know about the pinch point, I also thought we talked about using the term multi-use trail or even paved shoulder, not a Brampton Mississauga type sidewalk. Please listen - I thought you heard; very frustrating. Heed Richards comments and try again.

Documenting questions and concerns is not good enough. What I have been talking about has been on the table since the beginning of this process. Based on the quality of this iteration, the design for the Hamlet must be approved by the BCO; 'consultation' is not yet over folks. Let's get this right, or at least better.

Thank you Richard for listening and hearing,

[REDACTED]

From: Dela Cruz, Gino [<mailto:Gino.DelaCruz@peelregion.ca>]
Sent: December-03-13 14:53
To: Ganesh, Steve; [REDACTED]; Paterak, Richard
Cc: Saddi, Asha; [REDACTED]
Subject: RE: Roads EA - Belfountain Specifics

Hi [REDACTED] – based on your comments from the PIC, we have prepared the attached document which reviews the extension of the sidewalk network beyond the Community Centre to the southeast. As you can see from the existing conditions and proposed plan, there is pinch point beyond the Community Centre where the roadway is constrained by houses in the Regional right-of-way on one side and the drop off in topography on the environmentally sensitive lands on the other. As such, we feel this section serves as a good transition point into the semi-rural cross-section further to the south to Mississauga Rd. To accommodate sidewalks through this constrained area and beyond would mean significant property and environmental impacts.

Thanks for your input – we will document your questions and concerns in the Environmental Study Report for this study.

P. Gino Dela Cruz, P.Eng.
Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: Ganesh, Steve
Sent: November 26, 2013 1:39 PM
To: [REDACTED]; Paterak, Richard
Cc: Dela Cruz, Gino; Saddi, Asha; [REDACTED]
Subject: Re: Roads EA - Belfountain Specifics

[REDACTED]

We are looking into it and will prepare a response for you within a week. As mentioned at the PIC we are not planning any future meetings with the CWG or residents. We will email you the response.

We will also include the response as part of the documentation that accompanies the ESR for filing the EA.

Regards,
Steve

Sent from my BlackBerry 10 smartphone on the Bell network.

From: [REDACTED]
Sent: Tuesday, November 26, 2013 1:13 PM
To: Ganesh, Steve; Paterak, Richard
Cc: Dela Cruz, Gino; Saddi, Asha; [REDACTED]
Subject: RE: Roads EA - Belfountain Specifics

Hello Steve;

When do you think you'll have something to show me based on the ideas we discussed at the meeting last Wednesday?

Thanks,

[REDACTED]

From: Ganesh, Steve [<mailto:Steve.Ganesh@peelregion.ca>]
Sent: November-19-13 15:33
To: Paterak, Richard
Cc: [REDACTED]; Dela Cruz, Gino; Saddi, Asha
Subject: RE: Roads EA - Belfountain Specifics

Hello Councillor Paterak:

For the Belfountain Area we will be showing 3 preferred cross-sections at the PIC.

1. Bush between Shaw's Creek and Old Main
2. Old Main Street east of the Community Centre
3. Old Main between Bush and the Community Centre

We developed the site specific cross-sections because of the existing right-of-way and property constraints. They will be posted tomorrow on the Region's website, but I have attached the slide for you and David to take a look at.

Regards,
Steve

Steve Ganesh, MCIP, RPP
Manager, Infrastructure Programming & Studies
Public Works, Transportation Division
10 Peel Centre Dr., Suite B 4th Floor
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Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: Richard Paterak [<mailto:richard.paterak@caledon.ca>]
Sent: November 19, 2013 3:17 PM
To: Ganesh, Steve
Subject: Fw: Roads EA - Belfountain Specifics

Steve

Please read [REDACTED] email below. I thought you had story boards indicating a trail platform on the south side of Bush with curb and parking on the north side of Bush. I expect something like that as space allows on Old Main. Please comment or correct.

Richard

From: [REDACTED]
Sent: Tuesday, November 19, 2013 03:12 PM Eastern Standard Time
To: Richard Paterak
Cc: [REDACTED]
Subject: Roads EA - Belfountain Specifics

Hello RP;

Detail about what the plans are for the Hamlet area of the Roads EA is not clear. Are there to be sidewalks? if so, where will they be, what will they look like....what's the parking situation to be? connecting trails? etc...

Let's see what Wednesday evening brings, but I suspect a session relating to hamlet specifics may be required.

Thanks,

[REDACTED]

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Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Thursday, December 05, 2013 2:09 PM
To: Restrepo, Veronica; 'Susan Cumming'; Gan, Tyrone
Subject: FW: My feedback comment EAS Belfountain

Please file as required

P. Gino Dela Cruz, P.Eng.
Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: November 26, 2013 9:47 AM
To: Dela Cruz, Gino
Subject: RE: My feedback comment EAS Belfountain

Dear Mr. Cruz,

Very many thanks for your prompt reply. Obviously the objectives of your Department have merit in theory. As a retired geologist who likes walking I appreciate what you are trying to do here. However, with respect to our little property and hereabouts I believe that there is no immediate need for any more road work for several reasons based on the dominant importance of road side ditches and relatively porous overburden, our climate, and width of roadways. The most important aspects of drainage and safety were dealt with satisfactorily about 10 years ago by ditch deepening and installment of traffic lights at our intersection. Regrettably walking hereabouts is necessarily on the hard top roads everywhere except in the village where curbs occur. In fact no one walks on our side of the roads north and east of our fences because there is no need to as we are located outside the "paths" required to access school, play ground, library and post box, for example.

Regards,

[REDACTED]

From: Gino.DelaCruz@peelregion.ca
To: [REDACTED]
Date: Mon, 25 Nov 2013 15:31:35 -0500
Subject: RE: My feedback comment EAS Belfountain

Good afternoon [REDACTED]

Thank you for providing your feedback/comment for this study. I will be sure to share this with the project team and include it on the project record of public correspondence. The points you make will be considered by the project team as we finalize our study and plan in the coming months.

The work we are proposing stems from the need to address the geotechnical deficiencies in the pavement structure and sightline / safety deficiencies along the area roadways in our study. We are also looking to enhance the experience and safety for other modes of transportations in the community including for pedestrians and cyclists.

Specific to the roadway adjacent to your property, the deficiency on Bush Street is mostly related to pavement deterioration associated with poor drainage. We are trying to address the Region having to continually maintain and repair damage to the roadway caused by water seeping into the pavement structure by rehabilitating the road to ensure the base is adequate and that drainage is provided to take water away from the roadway. This will ensure the Regional Roads are in a state of good repair and is safe to use by local residents as well as the greater community of Peel.

I appreciate your support of lowering the speed limits in the study area – this in combination with the other works we are proposing should achieve our goal in providing safe roadways that are in a state of good repair that also respects the local context and features.

With regards to your specific inquiry about impacts to certain trees, we will note your concerns as we advance our studies. The Environmental Assessment study will take the design up to a 30% stage. Subsequent to this process, it will advance to 'detailed design' (100% design stage) where more details are reviewed and assessed, and specific impacts can be identified with more certainty. We will document your comments at this stage of the design and note that you be consulted at the detailed design stage.

Thank you again for providing your feedback. If you have any further questions or concerns, please forward them to myself or the project team.

Best Regards,
Gino

P. Gino Dela Cruz, P.Eng.
Project Manager, Infrastructure Programming & Studies
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Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: November 23, 2013 4:29 PM
To: Dela Cruz, Gino; [REDACTED]
Subject: My feedback comment EAS Belfountain

Dear Mr. Cruz,

This concerns our property at the [REDACTED] corner of the intersection of Shaw's Creek Rd. and Bush St.

At the Public Information Centre #2 Study on Nov 20 we learned of your intention to alter our north boundary fence area which we object to for several reasons.

One is that this work would be a waste of money. Why should tax payers be obliged to support this unnecessary work? We are happy to be living in a rural area and do not want our place to look like a suburb of Mississauga.

There would be no significant improvement in the safety of the road at this point so close to the stop sign. During the past 26 years we have lived here no traffic safety problem has been seen by us here.

Your intention of cutting down the old maple tree is a great shame because it still has many years of beautiful life for everyone to enjoy. We would not mind your cutting the young ash tree at the corner because, like our other ash trees, it may soon die anyway.

Of course, we do support your proposal to lower the speed limit on this section of the road which is one of the noisiest anywhere.

██████████ and I would be pleased to expand on these and other points, e.g., by telephone at your convenience. Our number is ██████████

Yours sincerely,

████████████████████

Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Thursday, December 05, 2013 2:18 PM
To: Restrepo, Veronica; Gan, Tyrone; 'Susan Cumming'
Subject: FW: Olde Baseline Road study
Attachments: FW: letter; FW: Scan-to-Me from 172.21.0.33 2013-11-25 141819

Correspondence with the [REDACTED] below and attached – including the letter which both Councillor Paterak and I received and a response from Councillor Paterak

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: November 25, 2013 3:35 PM
To: Dela Cruz, Gino
Subject: Re: Olde Baseline Road study

Thank you, kindly.

In a message dated 11/25/2013 12:30:21 P.M. Pacific Standard Time,
Gino.DelaCruz@peelregion.ca writes:

Good afternoon [REDACTED]

Thank you for your email and the letter I received from Rod at our Public Information Centre last week. The points you make will be considered by the project team as we finalize our study and plan in the coming months.

With regards to your specific inquiry about impacts to your fence and trees, we will note your concerns as we advance our studies. We have and will continue to be very aware and sensitive to the rural context, environmental and heritage features in the study area. The Environmental Assessment study will take the design up to a 30% stage. Subsequent to this process, it will advance to 'detailed design' (100% design stage) where more details are reviewed and assessed, and specific impacts can be identified with more certainty. We will document your comments at this stage of the design and note that you be consulted at the detailed design stage. This is usually the stage when the Region will discuss potential impacts and mitigation options with property owners.

Thank you again for providing your feedback. If you have any further questions or concerns, please forward them to myself or the project team.

Best regards,
Gino

P. Gino Dela Cruz, P.Eng.
Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
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-----Original Message-----

From: [REDACTED]
Sent: November 21, 2013 8:58 PM

To: Dela Cruz, Gino
Subject: Olde Baseline Road study

Good evening, Mr. DelaCruz,

We were unable to attend the hearings last evening. We had one question which our neighbour, [REDACTED], was unable to answer.

With the proposed cutting of the hill in front of our property, [REDACTED] Olde Baseline Road, and the widening of the road shoulders and ditching, will that require the moving of our fence and the cutting of some of the great maples?

If so, is that something the region would do at its expense?

Thank you,

[REDACTED]

Sent from my iPhone

Kong, Carol

From: Ganesh, Steve <Steve.Ganesh@peelregion.ca>
Sent: Monday, November 25, 2013 3:44 PM
To: Dela Cruz, Gino
Cc: Paterak, Richard; Saddi, Asha
Subject: FW: Scan-to-Me from 172.21.0.33 2013-11-25 141819
Attachments: Scan-to-Me from 172.21.0.33 2013-11-25 141819.pdf

Hello Gino:

Please see below correspondence to Councillor Paterak for our inclusion in the ESR documentation. Could you please send Councillor Paterak the final version of the fact sheet that shows the difference in pavement structure from the MBOW roads and other roads in Peel.

Councillor Paterak – Thanks for the kind words. It was a team effort...thanks for your support.

Best,
Steve

Steve Ganesh, MCIP, RPP
Manager, Infrastructure Programming & Studies
Public Works, Transportation Division
10 Peel Centre Dr., Suite B 4th Floor
L6T 4B9
Tel: (905)791-7800, ext. 7824
Fax: (905)791-1442

From: Richard Paterak [<mailto:richard.paterak@caledon.ca>]
Sent: November 25, 2013 2:19 PM
To: Ganesh, Steve
Subject: FW: Scan-to-Me from 172.21.0.33 2013-11-25 141819

Steve,

Attached is a statement from a couple who live on Olde Base. I send it to you to put with other comments. If I hear back from these people I may arrange a meeting with your staff and the residents.

Regards,

Richard

PS The meeting was highly successful in my opinion. You folks did a great job. Please send me a pdf of the handout on the difference between what we are doing and a truck route. Thanks.

Richard

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Roads Study – A viewpoint from a long time property owner

We have owned the property at 690 Olde Baseline Road for 40 years. We realize many of those involved in this study were not born or were not in the regional government 40 years ago. Let us briefly share what we have seen in that time, related only to the roads –

- 40 years ago, Olde Baseline Road was unpaved and had a natural curve around a large rock outcropping just west of Mississauga Road. That curve had the effect of slowing traffic. The gravel road at that time had been constructed by the local government presumably meeting the required road-building standards. The gravel had the effect of keeping traffic low because outsiders, especially weekend visitors, did not like getting dust on their cars.
- The local government decided to 'improve the road', make it safer, by removing the rock outcropping to reduce accidents. I recall my father saying, "This will increase traffic, increase speed, and increase accidents." He was right. People could drive faster, so they did drive faster, never mind the speed limit.
- The local government then, without our support, decided it should 'improve' the road, make it safer, by paving it. Presumably they paved it in conformity with their road-building standards. Again my father said, "This will increase traffic, increase speed, and increase accidents." He was right. The local government turned Olde Baseline Road into a shortcut for out of area commuters, and they are hell-bent to get to work as fast as they can.
- We have the same number of homes on this section of Olde Baseline Road as we did 40 years ago. Our needs have not changed.
- Now, the local government has decided that 'improving sight lines', a euphemism for removing the rolling hills natural to the area's roads, will make the road safer. I echo my late father's words and say that it will increase traffic, increase speed and increase accidents. All of the 'improvements' made so far have had that effect. Each time an 'improvement' has been made in the name of safety; traffic, speed and accidents have increased. With drivers being able to 'see farther ahead', they will feel comfortable stepping up their speed even more.

- The local government changed its road standards and now finds Olde Baseline Road does not meet its standards – not surprising in light of the fact they changed the standard. The local government staff has decided to widen the road, pave the shoulders and exercise its 30 meter right of way, in the name of safety. Thousands of mature trees, including some 50-100 year old maples, will be cut. Fences, wood and stone, that have been in place for decades will need to be moved at the owner's expense. Already traffic consistently exceeds the speed limit. We invite anyone to stand with us at our road between 6:30 and 8:30 am, and again in the evening, to observe traffic. Instead of 60 kph, it averages in excess of 80 kph. An unobtrusive speed camera (not one with the large sign showing your speed in lights) left in the area for a month would tell the story clearly. We even have crazy drivers passing each other on that stretch of road to gain an extra 30 or 45 seconds in their commute. Widening the road, and paving the shoulders will lead to higher speeds and ever more serious accidents.

Consider how we feel. Noise, visual, and diesel/gasoline pollution have increased with these 'improvements'. The quiet enjoyment of our property has been taken from us, and will worsen with these 'improvements'. The safety of ourselves, our children and grandchildren has deteriorated. **These 'improvements' have consistently shown that they increase danger, not safety, as measured by the very statistics the staff uses to propose yet further improvements. No-one seriously thinks that making the roads more amenable to higher speeds and more traffic will make them safer.** The value of our properties will decrease. This is essentially a 'taking' of our property without any compensation. Even the 30 meter right of way was imposed on the land-owners, never purchased.

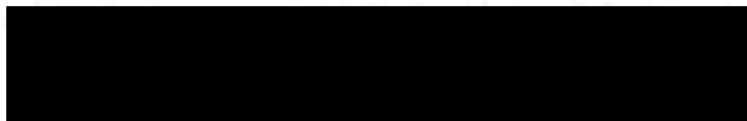
We live in an area where we need three different approvals just to expand a deck on our house – the escarpment, green belt and regional government interests must be met. We are hesitant to clear a tree on our property for fear of disturbing the well-studied salamander. We have had government employees studying wetlands on our property. **Yet, along comes the local government to excavate the natural rolling hills, to cut down thousands of trees, to move or blast natural rock of the escarpment, to force us to move our rock and wood fences, all in the name of 'improvements' that we do not want, have never asked for and with which we do not concur.**

We fought for a decade to defeat the plans of our recently-moved-in neighbor to the west, [REDACTED] to tear a massive hole in the escarpment. Now, with the support of him and his lobbyists, the roads coming past his recently acquired 'farm' are being classified as a 'heavy

truck haul route'. When he revives his application for his gravel pit in the next 5-10 years, all the arguments against the traffic issues will be moot.

Outside users of our section of Olde Baseline Road have never observed the speed limits or even the signage. When trucks were prohibited, they still used the road. When they were limited by time of day, they did not adhere to the limits. Cars and trucks do not observe the speed limits. Local government has never enforced the limits. We do not think the government even has the resources to enforce the limits. Perhaps installing speed bumps similar to those used by the 'red hills' every quarter mile would regulate speed. We suspect that drivers would petition the government to remove them because it impedes their commute.

In any event, while we expect that our government has carefully used the regulations and laws that it proscribes itself, and is following this process to simply attain what it has, from the outset, designed to have, we desire to have our objection together with the rationale behind it, put on record.





Albion, 1840

Alton, 1820

Belfountain, 1825

Bolton, 1823

Caledon East, 1821

Caledon Village, 1826

Campbell's Cross, c. 1820

Cataract, 1858

Cheltenham, 1827

Claude, c. 1832

Inglewood, 1883

Mayfield West, 2006

Melville, 1831

Mono Road, 1871

Mono Mills, 1819

Palgrave, 1846

Sandhill, 1839

Terra Cotta, 1855

Tullamore, c. 1820

Victoria, c. 1850

Wildfield, 1833

November 25, 2013

Dear [REDACTED]

[REDACTED] was kind enough to deliver your letter of objection to me in regard to the current EA on Olde Base and other Regional roads in the vicinity. I will forward your letter to Regional staff so that it becomes part of the formal record of the Environmental Assessment.

I would like to assure you that none of the roads within the EA terms of reference are being considered as Truck Routes. The current restrictions will remain in force and only local deliveries will be legally allowed. In addition the road will not be built to truck route standards, it will have narrower lanes than a truck route and it will not have full shoulders.

The south side of Olde Base to the west of you and both sides at your location are within the Niagara Escarpment Plan. The NEC has been consulted throughout the process and I can assure you that if stone walls have to be moved it will be at the Regional expense. In addition thousands of trees will not be taken down.

It was unfortunate that you were unable to attend the recent PIC. At the PIC were full profile and overhead maps demonstrating what the changes to each meter of the road will be. I will ask Regional staff to contact you so that a meeting can be arranged for you to see what changes are currently proposed along your frontage. I would be pleased to attend that meeting.

In the meantime if you have any additional questions or comments please be in touch by phone at 519.927.5034 or by email at richard.paterak@caledon.ca

Regards,

Richard Paterak
Regional Councillor, Ward 1

Richard Paterak
Regional Councillor
Ward 1

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Caledon, ON L7C 1J6
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Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Thursday, December 12, 2013 2:17 PM
To: Restrepo, Veronica; 'Susan Cumming'
Subject: FW: EA Study for Mississauga Road, OBL, WCB and Bush Street

FYI and documentation..

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: Dela Cruz, Gino
Sent: December 12, 2013 2:16 PM
To: [REDACTED]
Cc: Ganesh, Steve; Saddi, Asha; Tyrone.Gan@hdrinc.com; [REDACTED] Paterak, Richard
Subject: RE: EA Study for Mississauga Road, OBL, WCB and Bush Street

Good afternoon [REDACTED]

Thank you for your comments during the PIC and throughout the study. We are pleased to respond to your comments below:

The proposed designs for the study area roads take into account planned future growth and development in the Region of Peel (Caledon, Brampton and Mississauga) as well as overall growth in the Greater Toronto Area. The resulting traffic forecasts based on the Region of Peel Model incorporate this growth, and show a relatively small growth rate for the study area roads. In addition, the EA study was based on the retention of existing truck restrictions in the study area.

At the same time, the development of alternative road designs was based on guiding principles that were adopted for the study. One of the important principles for the community was retaining the existing rural character of the area – which means generally retaining the rolling vertical profiles of the existing roads and lowering traffic speeds to improve safety for all road users. The resulting recommended road designs balance the needs of existing road users.

We will document your specific comments on the design in the Environmental Study Report for this EA. Further work will be undertaken in detailed design to confirm the findings of the EA (including geotechnical) and advance the design beyond 30%.

For more information on the EA for Mississauga Road, Bush Street, Winston Churchill Boulevard and Olde Base Line Road, please visit our study website at: (<http://www.peelregion.ca/pw/transportation/enviro-assess/mississauga-road-bush.htm>).

Please do not hesitate to contact me if you have any additional comments or questions.

Regards,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]

Sent: December 5, 2013 1:50 PM

To: Saddi, Asha; Tyrone.Gan@hdrinc.com; Dela Cruz, Gino [REDACTED]; Paterak, Richard

Subject: EA Study for Mississauga Road, OBL, WCB and Bush Street

Hello,

Thanks for setting up the PIC #2. Please see my comments below. As you know I live and work in the Study Area residing at the major intersection of Mississauga Road and Olde Base Line. Incredibly, mine is the only house on this corner. I support the use of these roads for all road users, including goods movement and truck uses.

There was a handout that made the following statement:

"This area is not designated for significant future growth, therefore the volumes will remain relatively constant. There is no need to construct the pavement to handle significant volumes of truck traffic."

Unfortunately this statement is not accurate for the following reasons:

- the area immediately south of this area is designated for some of the highest growth of any area in North America.
- The materials (aggregates) needed to construct this high growth area come from immediately north of the Study Area.
- The only reason that there is not more truck traffic on these roads today is the poor condition of the roads and the fact that they are posted as No Truck Routes.
- The current lack of availability of these roads for truck traffic leads to congestion on other longer routes rather than promoting a safe and efficient Road Network as stated in the Peel Official Plan.
- My company would save at least a million kilometers per year of travel if these roads were permitted to carry truck traffic today. These savings would increase over time as the land between Mississauga Road and WCB north of Bovaird Drive begins to develop as is currently being planned.
- These roads are designated Major Roads in the Peel Official Plan.
- These roads are designated Medium Capacity Arterial Roads in the Caledon Official Plan.
- These roads are identified as a Primary Truck Route in the recent Peel Goods Movement Network Study.
- Now is the appropriate time to construct this route with a robust Pavement Design to accommodate both the current users of this road, but also future traffic as is planned for this road network.

From the Feedback Form I have the following comments.

1. Recommended Designs. I support the design as put forward save for my comments about robust structure above. If you build these roads correctly the first time you may never have to do so again for many years. While the design put forward is structurally suitable to carry all traffic, one should consider the important role that these roads will play as hundreds of thousands of new residents and jobs are added to the area south of the Study Area.

2. Alternative Design Concepts. I would support the use of rock cuts along Olde Base Line Road to eliminate the requirement to place extensive fill in the low spots. Pending geotechnical confirmation, I think you will find

that the hills are constituted of Bedrock Outcroppings that can easily be handled with vertical rock cuts. I would also support the use of traffic circles in this area.

3. Please see my comments above. It is important to balance to use of these roads as Major Roads with the desire of some residents to keep these roads exclusively as a private driveway to country estates. These roads can serve both functions as has been illustrated by extensive study by the Region. Improving these roads to accommodate all road users, cars, busses, trucks, farm machinery, bicycles and motorcycles is not only in the Public interest, but also fulfills the Regional Official Plan policy to achieve a safe and efficient network for the movement of people and goods.

Thanks,

[Redacted signature block]

Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Thursday, December 12, 2013 2:14 PM
To: Restrepo, Veronica; 'Susan Cumming'; Gan, Tyrone
Subject: FW: Comment for EAS, WCB-Miiss Rd

FYI and documentation

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
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From: Dela Cruz, Gino
Sent: December 12, 2013 2:14 PM
To: [REDACTED]
Cc: Saddi, Asha
Subject: RE: Comment for EAS, WCB-Miiss Rd

Good afternoon [REDACTED]

Thank you for your comments on the Environmental Assessment for Winston Churchill Boulevard, Mississauga Road, Olde Base Line, and Bush Street. We are pleased to respond to each of your comments below:

- Impacts on the broader area should be considered, with a special consideration given to potential aggregate haul routes (gravel trucks). Even though not part of the study area,
 - The Environmental Assessment (EA) is based on current traffic and truck patterns, and existing truck restrictions on the four roads being studied, and this is reflected in the recommended pavement design.
 - Our overall design goal has been focused on addressing the pavement, drainage and safety deficiencies in the study Regional Roads while minimizing impacts to the rural character and natural environment. The currently preferred design, which is highlighted by reduced posted speeds and narrower lane / shoulder widths, reflects these goals and study principles and does not make any recommendations to designate any of the roads as a 'truck haul route' or remove or alter any of the existing truck restrictions.
 - Broader area network considerations for goods movement (including aggregate trucks) in the Region of Peel have been examined in a separate study as part of the Region's Strategic Goods Movement Network Study (SGMNS). A link to this report and more information is provided below:
<http://www.peelregion.ca/council/agendas/pdf/rc-20130509/report-pw-a1.pdf>
<http://www.peelregion.ca/pw/transportation/goodsmovement/peel/study.htm>
- These road reconstructions could lead the way to paving a truck route through all of Winston Churchill, south of the study area. Winston Churchill Blvd south of the study area, between Hwy #7 & Wanless Rd will never be a suitable gravel haul route, even with proposed "improvements". The natural heritage, fish habitat, steep topography, structural deficiencies, hidden drive ways, residential and educational institutional uses on WCB north Norval make it unsuitable for hauling aggregate.
 - As noted above, the Environmental Assessment for the study area roads is based on current truck patterns and existing truck restrictions. South of the Study Area, an Environmental Assessment was completed for Winston Churchill Boulevard, from 1200 m south of Ballinafad Road to Olde Base Line Road, in 2010. That study recommended that existing truck restrictions on Winston Churchill Boulevard be retained.

- WCB and Hwy#7 in Norval is a permanent bottle neck for truck traffic. As N.W. Brampton builds out and connects with Norval, walk-able and bike-able urban transit must be taken seriously. Bikes and pedestrians are not compatible with gravel trucks. There better locations for a truck route in Brampton (for example; Mississauga Rd., and the planned N-S Parkway between Heritage & Mississauga Rds as found in the HHBATS). There is also the future extension of the 410, connecting to the future Parkway in NW Brampton, should be considered in how future truck routes should flow.
 - The Halton-Peel Boundary Area Transportation Study (HPBATS) recognized the existing “bottle neck” for auto and truck traffic through Norval. HPBATS recommended a long-term multimodal network that would serve all road users – trucks, autos and public transit – a network that includes a new East-West arterial connection that by-passes Norval and a new North-South transportation corridor. You may refer to the link below for more information on HPBATS: <http://www.peelregion.ca/planning/residents/transportation/projects/hpbats.htm>
- If the roads in this EAS are to be used as a gravel haul route, the aggregate producer should pay for all of the road reconstruction. Stop passing costs down to the taxpayer.
 - As noted above, the Environmental Assessment for the study area roads is based on current truck patterns and existing truck restrictions.

We appreciate your comments and trust that we have been able to address them. For more information on this EA for Mississauga Road, Bush Street, Winston Churchill Boulevard and Olde Base Line Road, please visit our study website at: (<http://www.peelregion.ca/pw/transportation/enviro-assess/mississauga-road-bush.htm>). As part of the documentation for this EA, we will include your correspondence in the Environmental Study Report.

Please do not hesitate to contact me if you have any additional comments or questions.

Regards,
Gino

P. Gino Dela Cruz, P.Eng.

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Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: December 7, 2013 8:21 PM
To: Dela Cruz, Gino; Saddi, Asha
Subject: Comment for EAS, WCB-Miiss Rd

Hi
Re. EAS for Mississauga Rd-Old Main St-Bush St-Winston Churchill Blvd

Please consider my comments, even though they are a couple days past the deadline.

- impacts on the broader area should be considered, with a special consideration given to potential aggregate haul routes (gravel trucks). Even though not part of the study area,
- these road reconstructions could lead the way to paving a truck route through all of Winston Churchill, south of the study area.
- Winston Churchill Blvd south of the study area, between Hwy #7 & Wanless Rd will never be a suitable gravel haul route, even with proposed “improvements”. The natural heritage, fish habitat, steep topography, structural deficiencies, hidden drive ways, residential and educational institutional uses on WCB north Norval make it unsuitable for hauling aggregate.
- WCB and Hwy#7 in Norval is a permanent bottle neck for truck traffic. As N.W. Brampton builds out and connects with Norval, walk-able and bike-able urban transit must be taken seriously. Bikes and pedestrians are not compatible with gravel trucks. There better locations for a truck route in Brampton (for example; Mississauga Rd., and the planned N-S Parkway between Heritage & Mississauga Rds as found in the HHBATS). There is also the future extension

of the 410, connecting to the future Parkway in NW Brampton, should be considered in how future truck routes should flow.

- If the roads in this EAS are to be used as a gravel haul route, the aggregate producer should pay for all of the road reconstruction. Stop passing costs down to the taxpayer.

Thanks,

[Redacted signature block]

Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Monday, January 06, 2014 11:37 AM
To: Restrepo, Veronica
Cc: Saddi, Asha; Gan, Tyrone; 'Susan Cumming'
Subject: FW: Response to EA Long Fax received Nov 27-13

Hi Veronica – please see below correspondence FYI and documentation.

Thanks,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: Dela Cruz, Gino
Sent: January 6, 2014 11:23 AM
To: [REDACTED]
Cc: Saddi, Asha
Subject: RE: Response to EA [REDACTED] Fax received Nov 27-13

Hello [REDACTED]

It was a pleasure speaking with you this morning regarding the Belfountain area and the proposed Regional works. As discussed, after the Public Information Centre on November 20, 2013, the Project Team is refining the proposed design and will continue to update the preliminary cost estimate for the proposed works for Mississauga Rd, Bush St, Winston Churchill Blvd, and Olde Base Line Rd. The 30% preliminary design cost estimate will be finalized in the coming months; documented in the Environmental Study report for this Environmental Assessment study; and refined with more detailed information in the subsequent detailed design stage prior to construction.

If you have any other questions, please do not hesitate to contact me.

Regards,
Gino

P. Gino Dela Cruz, P.Eng.

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Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: [REDACTED]
Sent: January 6, 2014 7:10 AM
To: Dela Cruz, Gino
Cc: Saddi, Asha
Subject: Re: Response to EA Long Fax received Nov 27-13

This was not my request at all. I requested the budget for work to be down on WCB/OBL/Miss/Rd..as proposed in the EA you are doing. I sent you a proposed budget that came from the JDCL consultant which was proposed by the Region. Would you telephone me [REDACTED] to confirm my the information I requested

Sent from my iPad

On Dec 5, 2013, at 12:53 PM, "Dela Cruz, Gino" <Gino.DelaCruz@peelregion.ca> wrote:

Good afternoon [REDACTED]

We are in receipt of the faxed excerpt from the July 2008 Addendum to the Rockfort Quarry Haul Traffic Impact and Haul Route Evaluation Study Report.

Based on the comments you noted on the excerpt, we have assumed that you are interested in the Winston Churchill Boulevard reconstruction project south of Olde Base Line to Balinafad Road which is a separate undertaking from the Environmental Assessment for Mississauga Rd, Bush St, Winston Churchill Blvd and Olde Base Line Rd.

The design of Winston Churchill Blvd. from 1200m South of Ballinafad Rd. to Olde Base Line Rd is at the 30% design stage (the Environmental Assessment was completed in March 2010) as per the Council Report (<http://www.peelregion.ca/council/agendas/pdf/rc-20130613/report-pw-a1.pdf>). The schedule is as follows:

Detailed Design: 2013 to 2014
Property Acquisition/Utility Relocation: 2014 to 2015
Construction Start: 2016, subject to property acquisition.

In the meantime, Roads-Operation continues with the regular maintenance of the road consisting of activities such as gravel resurfacing, grading of the gravel and dust control.

For more information on the EA for Mississauga Rd, Bush St, Winston Churchill Blvd and Olde Base Line Rd., please visit the study website (<http://www.peelregion.ca/pw/transportation/enviro-assess/mississauga-road-bush.htm>). As part of the documentation for this EA, we will include your correspondence in the Environmental Study Report.

If you have any other questions, please do not hesitate to contact me.

Regards,

P. Gino Dela Cruz, P.Eng.
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To Steve Ginesh.

URS Canada Inc.



Rockfort Quarry OMB Hearing
Traffic Impact & Haul Route Evaluation Study Report
Addendum



URS has prepared this Addendum to update the May 2005 Traffic Impact and Haul Route Evaluation Study Report ("the Report") for the proposed Rockfort Quarry in the Town of Caledon. Where appropriate, new base information has been collected and reviewed to identify whether this more recent information has materially impacted the analysis/conclusions reached in the Report.

When any section of the Report has new information, that information and any resulting review/analysis are provided as shown below.

Volume 1

SECTION 3 FUTURE TRENDS/PROJECTIONS

3A Existing Traffic Conditions

Existing traffic volumes (2006/2007) for the recommended haul route were obtained from the Region of Peel and the MTO. In addition URS undertook traffic counts for the recommended route in June 2008. These counts are summarized in Appendix A of this Addendum.

3B Traffic Growth

The traffic growth rates used to establish background traffic projections for the recommended haul route were revisited in the light of the more recent traffic counts. These rates and the projected background traffic volumes for the new horizon years are estimated and summarized in Appendix B of this Addendum.

SECTION 4 MAJOR ROAD NETWORK IMPROVEMENTS

4A Ontario Ministry of Transportation (MTO)

The MTO is currently constructing the following previously proposed works in the Study Area:

- Highway 410 from its present end at Mayfield Road north-westerly to connect with Highway 10 north of Valleywood Drive in the Town of Caledon. This work is expected to be complete in the Fall of 2009.
- Highway 10 is being widened to 4/5 lanes from just south of Caledon Village to Highway 9. This work is expected to be complete in the Fall of 2009.

4B Region of Peel

The Region of Peel 2008 Capital Budget and Forecast¹ includes the following anticipated expenditures on Regional roads in the study area during the 2008-2017 timeframe:

Handwritten scribbles

- Winston Churchill Boulevard from Balinafad Road to Olde Base Line Road-Class EA. This is scheduled to take place in 2008 at a cost of \$250,000.
- Winston Churchill Boulevard from Balinafad Road to Olde Base Line Road-two-lane reconstruction and/or pavement rehabilitation. The design of this project is expected to be initiated in 2009 with construction taking place in 2012. The total project cost is estimated at \$8,726,000.
- Winston Churchill Boulevard from approximately 560 metres north of the Terra Cotta Park entrance to Balinafad Road-two-lane reconstruction. This work is scheduled to take place in 2009 at a total cost of \$1,660,000.

CANT BE RIGHT

¹ Region of Peel. "2008 Capital Budget and Forecast", 2008

URS Canada Inc.

- Mississauga Road from Olde Base Line Road to Caledon Mountain Road-Class EA. This is scheduled to take place in 2009 at a cost of \$250,000.
- Mississauga Road from Olde Base Line Road to Caledon Mountain Road-two-lane reconstruction and/or pavement rehabilitation. The design is scheduled to be initiated in 2010 with construction in 2012. The total project cost is estimated at \$6,425,000.
- Winston Churchill Boulevard from Olde Base Line Road to Bush Street-Class EA. This is scheduled to take place in 2010 at a total cost of \$250,000.
- Winston Churchill Boulevard from Olde Base Line Road to Bush Street- two-lane reconstruction and/or pavement rehabilitation. The design is scheduled to be initiated in 2011 with construction in the 2013-2017 timeframe. The total project cost is estimated at \$7,096,000.
- Olde Base Line Road from Winston Churchill Boulevard to Mississauga Road-Class EA. This is scheduled to take place in 2010 at a total cost of \$200,000.
- Olde Base Line Road from Winston Churchill Boulevard to Mississauga Road-two-lane reconstruction and/or pavement rehabilitation. The design is scheduled to be initiated in 2011 with construction in 2012-2017 timeframe. The total project cost is estimated at \$3,841,000.
- Mississauga Road/King Street intersection-implementation of northbound right turn lane. This work is scheduled to take place in 2012 at a cost of \$54,000.

SECTION 5 ROCKFORT QUARRY

5B Phasing

Phase 1 operations are now assumed to commence in 2011

Phase 2 operations are now assumed to commence in 2014

Phase 3 operations are now assumed to commence in 2017

5D Site Generated Traffic

5.D.2 Daily/Hourly Truck Traffic Scenarios

Hourly Variation: Hourly shipping data was obtained for the Caledon Sand & Gravel (CS&G) operation in Caledon for October 2007 to determine whether there had been any significant pattern change since October 1997. The average percentage of daily truck shipments for the months of October 1997 and 2007 are shown graphically in Figure 5.3A herein. This shows that the hourly distribution pattern used in the earlier report has remained generally consistent through time, particularly during the critical AM Peak Period, and no changes to any subsequent analysis using this material was considered necessary.



Kong, Carol

From: Dela Cruz, Gino <Gino.DelaCruz@peelregion.ca>
Sent: Tuesday, February 04, 2014 2:28 PM
To: Restrepo, Veronica; Gan, Tyrone
Cc: Saddi, Asha
Subject: FW: EA for Mississauga Road - Old Main - Bush Winston Churchill - Olde Base Line

FYI and documentation in the ESR.

P. Gino Dela Cruz, P.Eng.

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From: Dela Cruz, Gino
Sent: February 4, 2014 2:05 PM
To: [REDACTED]
Cc: [REDACTED]; Paterak, Richard
Subject: RE: EA for Mississauga Road - Old Main - Bush Winston Churchill - Olde Base Line

Good afternoon [REDACTED]

Thank you for your email. Although there is no set deadline to send your comments in to the project team, we are moving towards completing the Environmental Assessment (EA) study by drafting and filing the Environmental Study Report (ESR) for public review in May of this year. The public, agency and any other stakeholders will be notified through mail and ads in the local newspaper.

The comment regarding the potential installation of high-speed internet infrastructure is one we have heard from a few residents throughout the study. We will note your comment in the ESR and pass along the comment to our detailed design group for consideration when the project proceeds towards construction.

If you have any additional comments or questions, please do not hesitate to call or email me.

Regards,
Gino

P. Gino Dela Cruz, P.Eng.

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From: [REDACTED]
Sent: February 4, 2014 12:17 PM
To: Dela Cruz, Gino
Cc: [REDACTED]; Paterak, Richard
Subject: EA for Mississauga Road - Old Main - Bush Winston Churchill - Olde Base Line

Hello Gino.

Thank you for the January 23 letter about the feedback report being open for comment. Was there a deadline for comment?

One suggestion I'd like to reinforce is to take any possible opportunity, if construction proceeds, to help facilitate installation of high speed internet cable, especially if utility relocations are involved.

I appreciate this is not the Region's mandate, but hope the suggestion helps to increase profile and priority of this issue in areas not served by the major carriers.

[REDACTED]

Appendix A.2

Agency Consultation

The study team received comments from MOE, NEC, CVC and MNR on the Draft Environmental Study Report, which was distributed to agencies (TAC members) for review on March 28, 2014. A summary of the comments and responses is provided in the enclosed table.

#	Draft ESR Section	Comment	Response / Action
1	Comments from Amanda Graham, MOE (April 25, 2014)		
1.1	General Comments	On page 27 of the Draft ESR Main Text, please change “Indian and Northern Affairs Canada” to “Aboriginal Affairs and Northern Development Canada”.	Text was be revised as suggested.
1.2	General Comments	The First Nations Correspondence section in Appendix A only contains responses from three groups. Please clarify whether follow-up was conducted with the other First Nations groups who did not provide a response to initial outreach.	All First Nations groups were contacted via notices throughout the study (notice of study commencement and notice of Open House/ PICs and notice of study completion).
1.3	Surface Water Comments	<p>We recommend that a statement be added to explain that Enhanced Level Protection may not be met for all areas where the proposed stormwater management facilities of OGS and/or grassed swales are utilized independently. MOE has determined that ditches/enhanced grassed swales or OGS cannot meet ‘Enhanced Water Quality Protection’ unless used as part of a treatment train (Stormwater Management Planning and Design Manual, MOE 2003).</p> <p>OGSs are typically used for small drainage areas (<2 ha). If runoff is over the capacity of the OGS, the potential for by-pass conditions with no treatment occurring during storm events increases. OGS sizing requirements will need to be considered in order to capture and treat at least 90% of the runoff volume that occurs at a site and to achieve a long-term average basis for water quality objectives of ‘enhanced protection’. This should be taken into account considering the ditches will continue to drain lands external to the roadway(s).</p>	A detailed evaluation of alternative BMP’s was carried out, resulting in the selection of two feasible quality control practices, OGS units and grassed swales. It is noted that runoff from existing roadways do not provide any quality control. The incorporation of OGS and grassed swale systems will provide a net improvement to the quality of storm runoff within the project limits.
1.4	Surface Water Comments	MOE defers any comments on the assessment, impacts, and mitigation of natural features and fish habitat to the Ministry of Natural Resources (MNR). Of particular concern are the effects that changes in water quality and water quantity may have on species identified in the study area classified as Endangered, Threatened or of Special Concern. Please ensure that MNR is consulted and any comments from MNR are incorporated into the final ESR.	Comment noted. MNR has been consulted throughout the study.
1.5	Surface Water Comments	Hydraulic analysis and proposals for sizing of stream culverts and bridge crossings are not reviewed here and should be sent to the local conservation authority for review by their engineering staff. Please ensure the local conservation authority is consulted and any comments are incorporated into the final ESR.	Comment noted. CVC has been consulted throughout the study.

#	Draft ESR Section	Comment	Response / Action
1.6	Surface Water Comments	While several sections of the ESR and Appendices acknowledge the need for a PTTW for groundwater pumping in exceedance of 50,000 LPD, it should be noted that a PTTW is also needed for surface water extraction and the active diversion of surface water flows by pumping in exceedance of 50,000 litres/day. A monitoring program for discharge water quality and quantity, as well as a mitigation program, may need to be developed. Please ensure that you consult with the MOE Central Region Permit to Take Water (PTTW) Coordinator prior to detailed design to confirm any approval requirements for water takings during construction or operation.	The ESR includes text to state that surface water takings will be required where culvert replacement/upgrades are proposed. The water quantity/quality monitoring program will be developed at the time the PTTW application is submitted.
1.7	Air Quality Review Comments	<p>During construction, please apply best management practices to mitigate any air quality impacts caused by construction dust. Please note that the ministry recommends that non-chloride dust suppressants be applied.</p> <p>For a comprehensive list of fugitive dust prevention and control measures, please refer to Cheminfo Services Inc. Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities. Report prepared for Environment Canada. March 2005.</p> <p>http://www.bieapfrempp.org/Toolbox%20pdfs/EC%20-20Final%20Code%20of%20Practice%20-%20Construction%20%20Demolition.pdf</p>	Comment noted. Text has been added to the ESR.
1.8	Contaminated Soil Comments	If soil removed during construction is determined to be contaminated, please ensure that the disposal of contaminated soil is consistent with Part XV.1 of the Environmental Protection Act and Ontario Regulation 153/04, Records of Site Condition, which detail the new requirements related to site assessment and clean up.	Comment noted. Text has been added to the ESR.
1.9	Groundwater Review Comments	There are over two hundred water supply wells identified within 500 m of the study area. It is conceivable that these wells may be affected by road construction, either because of construction activities or, later, due to additional or more proximate road salt application. We recommend confirming which wells are used domestically prior to construction, to ensure that affected well owners will continue to have water supplies of appropriate quality and in adequate quantities, and to ensure that any work done on affected wells or any replacement wells is done pursuant to O. Reg. 903, Wells (pursuant to the Ontario Water Resources Act).	Comment noted. Text has been added to the ESR. Wells to be confirmed during detailed design.
2	Questions from Nancy Mott, NEC (April 28, 2014)		
2.1	Niagara Escarpment Impacts	I have a question regarding Table 26. On page 136 there is an Evaluation Criteria called “Niagara Escarpment impacts”. Is this meant to indicate no impact to the physical Escarpment feature? If it is meant to indicate that there is no conflict with policies in the Niagara	Based on Figure 4 of NRSI’s report, the study area portion of the Winston Churchill Blvd ROW doesn’t occur within Niagara Escarpment

#	Draft ESR Section	Comment	Response / Action
		Escarpment Plan, I am not sure how it can be argued that there are no impacts when there are potential impacts to terrestrial habitat and species at risk both of which are the subject of policies in the NEP. Perhaps you can clarify.	Plan designated areas. Therefore, NRSI determined that improvements to that road corridor do not to impact Niagara Escarpment natural features.
2.2	NEP amendment	Secondly, we have just informed the Region of Peel that they will require an amendment to the NEP for proposed works on Winston Churchill which involve the regulated habitat of the Jefferson Salamander. Our policy is <u>no development in the habitat of an endangered species</u> . Are we going to encounter the same situation in this project?	It is our expectation that this project will require an amendment to the NEP based on potential for impact to Jefferson Salamander regulated habitat. This was discussed with NEC at a meeting on May 30, 2014 and additional text has been included in the ESR to clarify the plan amendment need.
3 Comments from Nancy Mott, NEC (May 1, 2014)			
3.1	General	We appreciate the need to consider road improvements in the Study Area from a safety perspective. In our view, of significant importance to the evaluation of the options for the road improvements within the Niagara Escarpment Plan Area (NEP) is whether the recommended options would be in conflict with the NEP. NEC staff has made it clear throughout the project that the NEP policy is that development shall locate outside wetlands (NEP Part 2.6.10). In addition, we have advised that with respect to wildlife habitat, NEP policy is that new development will not be permitted in identified habitat of endangered (regulated) plant or animal species (NEP Part 2.8.1). There is acknowledgement in the Natural Heritage Report (Appendix B) that an amendment to the NEP would be required with respect to wetlands, depending on the chosen option for road improvement, but there is not a similar statement with respect to proposed development in endangered species habitat.	NEP impacts are one component of the overall evaluation (high level comparison of the options, which included natural heritage evaluation carried out by NRSI), and additional details re: specific NEP conflicts would not change the outcome of the recommended options overall. The Natural Heritage report and ESR were revised to add reference to amendment to NEP with respect to proposed development in endangered species habitat. Detailed impacts/ mitigation for the proposed design are documented as part of NRSI's final natural heritage report, and reflected in the ESR text.
3.2	General	To recommend an option for a road improvement without noting in the ESR that there is a policy conflict with a Provincial Plan is misleading. Furthermore, it should be noted that even if a Plan Amendment were sought, there is no guarantee that the Amendment would be approved. Recommending the road improvement options in the ESR and deferring the issue of dealing with the policy conflict to the detailed design stage could result in significant delays. Understanding both the policy conflict	As with previous comment, the evaluation consisted of a high level comparison of the options, which included natural heritage evaluation carried out by NRSI. Although the Do-Nothing option is preferred in terms of the natural environment

#	Draft ESR Section	Comment	Response / Action
		and possible solutions, such as the “do nothing” alternative in some cases, is preferable before final decisions are made in the EA process about which alternatives are preferred.	and policy conflicts, it is not preferred overall as it does not address the safety, drainage, and state of good repair deficiencies. The report was revised to include the need for plan amendment. Detailed impacts/ mitigation for the proposed design are documented as part of NRSI’s final natural heritage report, and reflected in the ESR text.
3.3	General	While we recognise that some road improvements may be necessary, not all road improvements are inherently “essential” as defined in the NEP. Within the Escarpment Natural Area, “essential transportation and utility facilities” are permitted uses. Essential is defined as that which is deemed necessary to the public interest after all alternatives have been considered. We do not believe that sufficient justification has been provided for road improvements within the Escarpment Natural Area.	The evaluation consisted of a high level comparison of the options, which included natural heritage evaluation carried out by NRSI. The proposed designs address the safety, drainage, and state of good repair deficiencies, while minimizing impacts to the natural environment and other adjacent lands and features. Detailed impacts/ mitigation for the proposed design are documented as part of NRSI’s final natural heritage report, and reflected in the ESR text.
3.4	General	The drawings for the road sections indicate vegetation removal in certain areas. There is no discussion in the ESR about considerations for planting or how visual impact would be addressed in these locations. Part 2.7 of the NEP has an objective that new development should preserve as much as possible of wooded areas. Perhaps additional information could be included as to the approach that will be taken in the project to achieve that preservation and any commitment to undertaking visual impact assessment, where necessary, to evaluate the impact of vegetation removal.	Plantings are identified as part of impacts/mitigation section of NRSI’s final report, and included in ESR text. Impacts to wooded areas have been minimized in proposed design and tree removals are generally individual trees adjacent to the road. As discussed with NEC, Visual Impact Assessments should be carried out during detailed design, if required. This commitment has been identified in the ESR.

#	Draft ESR Section	Comment	Response / Action
3.5	General	In addition to protecting unique ecologic and historic areas, it is an objective of the NEP in Part 2.12 to inventory, interpret, evaluate, maintain and conserve the cultural heritage features of the Niagara Escarpment Plan Area. We appreciate that you have considered the protection of cemeteries and stone walls in the ESR. A greater commitment to maintaining and conserving the stone walls rather than deferring this consideration to detailed design would have been preferred.	Impacts to heritage features including stone walls were minimized through the proposed design. More detailed impacts/ mitigation text was included in the ESR, based on ASI's recommendations, to prioritize how the impact of the wall is to be reviewed during detailed design (modify the proposed design to avoid features, relocate features, salvage features, etc.).
3.6	General	We recommend further discussion between the NEC, the Region and the study team prior to the finalization of the ESR.	A meeting was held on May 30, 2014 with representatives from NEC, the Region, HDR and NRSI to discuss NEP policy issues and outstanding concerns. It was agreed to update the ESR text to clarify the need for a plan amendment at the detailed design stage, and to identify potential opportunities to further reduce impacts.
3.7	Page 5	Since Mr. Baran and Mr. Whitbread did not participate in the study you could leave their names out. Alternatively, you could put the names of all NEC staff together.	Names were removed.
3.8	Page 8	As part of Phase 5, the ESR should indicate the need for Plan Amendments and Development Permits from the NEC.	Text was revised to include.
3.9	Page 9	The discussion regarding the PPS should be updated to reference the PPS 2014 as it has now been released. This should also be done in the Natural Heritage report.	Natural heritage report and ESR text was revised to include.
3.10	Page 12	Given NEC staff comments above, it would be appropriate to state the need for a Plan Amendment and Development Permit (for any road works that are not exempt) from the NEC permit process.	Text was revised to include.
3.11	Page 15	Is it appropriate to discuss transit orientation in the context of this project given that no transit is available, as far as I am aware, to the Study Area?	Text was revised to remove references to transit.
3.12	Page 29	This section is called Existing Land Use but the discussion focusses more on land use designations in policy documents. It might be more appropriately titled Existing Land Use designations and should include reference to the specific NEP designations within the Study Area, not just the NEP in general.	Section heading was revised to "land use designations" and more detailed references to NEP designations were added.

Mississauga Rd. / Old Main St., Bush St., Olde Base Line Rd., and Winston Churchill Blvd. EA
 Draft ESR – Agency Comments and Responses

#	Draft ESR Section	Comment	Response / Action
3.13	Page 36	There is reference here to an “equalization culvert”. I think that the term should be clarified as the term may not be known to members of the public who will be reading the report.	NRSI clarified term, and definition was included in Natural Heritage report and ESR text.
3.14	Page 37	It is not clear what the presence of the Escarpment has to do with limiting archaeological potential. The NEP also protects cultural heritage resources. Also on the same page, Item 1, the third sentence is not clear.	This is a general description of the study area characteristics. Text was revised to clarify.
3.15	Page 101	Given that NEC staff has expressed concern about the possible impact of culvert changes on natural heritage I would have expected some comment in this section as to whether the proposed culverts are in the regulated habitat of an endangered species.	Text added as part of species at risk discussion.
3.16	Page 122	There is no discussion here about the timing of construction and de-watering with respect to the impact on wildlife or fish habitat.	Text included as part of impacts/ mitigation section of NRSI’s final report, and included in ESR text.
3.17	Page 184	Escarpment Protected Area should read Escarpment Protection Area. Using this chart as an example, the do nothing approach is the recommended option due to conflict with the NEP but another option with impact on the NEP Area is offered. The connection is not made between the policy conflicts identified under the Niagara Escarpment section heading to the evaluation of the options earlier in the chart as it relates to impact on regulated habitat. The overall choices are options that would involve impact to regulated habitat which is misleading given the need for a Niagara Escarpment Plan amendment which might not be successful. There should be consideration for revising the charts for the road sections in the NEP to identify the policy issue and how the overall choice can be justified in light of it.	Text was revised to read “Escarpment Protection Area”. NEC criteria is one component of the overall evaluation (high level comparison of the options, which included natural heritage evaluation carried out by NRSI). Although the Do-Nothing option is preferred in terms of the natural environment and policy conflicts, it is not preferred overall as it does not address the safety, drainage, and state of good repair deficiencies. Detailed impacts/ mitigation for the proposed design were documented as part of NRSI’s final natural heritage report, and reflected in the ESR text. Need for plan amendment was added to evaluation tables, where applicable.
3.18	Evaluation Tables	For the tables evaluating the preferred road improvement options for lands within the NEP Area, we reiterate our concern that road improvements are proposed within or adjacent to the habitat of endangered species. An option would be to combine Figure 4 (Environmental Policy Areas) with Figure 5b (Fish and Wildlife Habitat) to fully appreciate the policy conflicts. This was undertaken in the detailed design for the Winston Churchill Road project	Suggest not combining the figures, as so much information together would be confusing.

#	Draft ESR Section	Comment	Response / Action
		presently ongoing and it was helpful to the understanding of the potential conflicts between policy and the preferred alternatives from an engineering perspective.	
3.19	General (public communications)	Please note that the NEC has been contacted by the Belfountain Community Association. They have expressed concern about proposed road changes in Belfountain. We provided information about the EA process in general and encouraged them to review the ESR when it is posted before reaching any conclusions about what might be proposed.	Comment noted.
4	Comments from Jakub Kilis, CVC (May 2, 2014)		
4.1	Section 1.8.1	2005 PPS – This section should be updated to reflect the 2014 PPS. This should also be the case in any other sections or technical studies which make reference to the PPS	Text was updated to include in ESR and NRSI report.
4.2	Section 3.2.4.2	Surface water drainage & Aquatic Habitat Characterization – number of watercourse crossings should reflect comments CVC provided as part of our review of the drainage reports. Further, it is important to distinguish which equalization culverts link two wetlands together and which do not as the implications to function and importance can change and impact the design.	The following culverts have been identified as watercourse crossings within our study area and correspond to the eleven (11) watercourses CVC has identified: (OBL-02, OBL-04, OBL-08, WCB-06, WCB-09, WCB-14, and WCB-16, Culvert 10 on Bush Street, Culvert 14 on Old Main Street, Culverts 44 and 48 on Mississauga Rd.) HDR & NRSI have identified 1 culvert within the study area that functions as an equalization culvert (WCB-13).
4.3	Section 4.6	Drainage – Again the number of watercourse crossings in the study area should reflect our comments on the drainage reports. Again, it is important to distinguish between culverts which link wetlands on both side of the road and other equalization culverts. Please comment on when the field geomorphic assessment will be completed. CVC typically requires this to be done at the EA stage as it will influence culvert design of the crossings and could have impacts on grading etc.	The Draft Geomorphic Assessment Report was completed by the week ending May 16, 2014. The drainage report was updated to include the findings from the geomorphic assessment, and the ESR was revised to reflect the latest recommendations from all technical reports.
4.4	Section 6.2.6	Drainage – End of point 1. Please note that CVC does not support closed bottom crossings for watercourses. Wording in your ESR should reflect this. Further, importance of having open bottom culverts goes beyond fish passage and includes, sediment transports, provision of natural substrates, geomorphic consideration etc.	All recommended culverts at watercourse crossings are proposed to be open bottom. Revisions to both the Drainage Report and ESR reflect this.

#	Draft ESR Section	Comment	Response / Action
4.5	Section 6.2.6	Drainage – drainage also includes wetland flow across the road. Impacts/changes/mitigation of this should be discussed.	Included in NRSI's impact/mitigation section of final report, and ESR text.
4.6	Section 6.2.8	Plates – these have not been reviewed at this time as comments related to the drainage studies and SWM report may require changes to these. CVC will review these once our comments are addressed. Due to the sensitive nature of this study area CVC staff also plan to visit the study area to review the proposed preferred alternative and grading impacts in relation to watercourse crossings and wetlands. Further discussion may be required regarding outlets and impacts to wetlands.	The preliminary design plates were revised to reflect the latest technical recommendations.
4.7	Section 6.3.1	Summary of Concerns and Mitigation Measures – semi-rural cross sections along wetlands need to be evaluated for impacts if there are reductions/changes to water inputs.	Included in NRSI's impact/mitigation section of final report and ESR text.
4.8	Chapters 6-10	The comments above for Chapter 6 are specific to the Winston Churchill segment; however, they apply equally to the other road segments covered in Chapters 7, 8, 9 and 10.	Comment noted. All comments for WCB were addressed in a similar manner for all other roads.
5	Comments from Mark Heaton, MNR (May 13, 2014)		
5.1	Section 1.8	<p>Section 1.8 describes the planning context of the study. Please include a section on the Endangered Species Act that outlines the Act, its regulations, impacts to regulated habitat and species. As part of the EA process, MNR encourages impact avoidance to the maximum extent possible. In some cases, permits or other forms of authorization are required for work within regulated habitat areas. The primary species at risk for this study area is the Jefferson Salamander. The following URLs provide more information</p> <p>http://www.mnr.gov.on.ca/en/Business/Species/2ColumnSubPage/MNR_SAR_HBTT_PRTCTN_EN.html</p> <p>http://www.mnr.gov.on.ca/en/Business/Species/2ColumnSubpage/MNR_SAR_ENDNGR_SPC_TBSCS_EN.html</p> <p>http://www.mnr.gov.on.ca/en/Business/Species/2ColumnSubPage/MNR_SAR_JFRSN_SLMNDR_EN.html</p>	<p>Text was revised to include a section on the Endangered Species Act.</p> <p>Impacts have been minimized through proposed designs. Permits will be addressed during detailed design.</p>
5.2	Section 1.8	<p>Section 1.8 includes the PPS 2005 when describing the PPS, it is important that the report includes the natural heritage provisions of this policy. Site alteration and development are not permitted in endangered and threatened species habitat, ANSIs and provincially significant wetlands (south of the Canadian Sheild).</p> <p>Should the document be updated to the 2014 PPS, the wording of the natural heritage provisions has been</p>	<p>Text was revised to include the natural heritage provisions of this policy, and updated to include the 2014 PPS.</p> <p>The evaluation consisted of a high level comparison of the options, which included natural heritage evaluation</p>

#	Draft ESR Section	Comment	Response / Action
		adjusted to reflect that site alteration and development may be permitted in threatened and endangered habitat through authorizations issued under the Endangered Species Act. Please ensure that the analysis of the alternatives reflects impact avoidance for these provincial policy interests	carried out by NRSI. Detailed impacts/ mitigation for the proposed design were documented as part of NRSI's final natural heritage report, and reflected in the ESR text.
5.3	Section 3.2	add a section on Species At Risk highlighting the species in the study area, their designations and whether they have regulated habitat. It is advisable to include how many linear meters of the road study coincides with regulated habitat for Jefferson Salamander	NRSI natural environment report and ESR text were revised to include.
5.4	Sections 6.3.1, 7.3.1, 8.3.1, 9.3.1, 10.3.1	identify anticipated residual impacts of the preferred alternative to ANSIs, PSWs and regulated habitat for species at risk, recommended detailed design recommendations for further impact avoidance and recommended impact mitigation recommendations	Detailed impacts/ mitigation for the proposed design were documented as part of NRSI's final natural heritage report, and reflected in the ESR text.
5.5	Endangered Species Act Approvals	Wildlife road mortality mitigation approaches will be further discussed at the detailed design stage in consultation with this Ministry. MNR expects that the Region of Peel will undertake a more detailed analysis of area of impact within the regulated habitat for Jefferson Salamander at the detailed design stage. This information will be used to complete an Avoidance Alternatives Form (http://www.forms.ssb.gov.on.ca/mbs/ssb/forms/ssbforms_nsf/GetFileAttach/018-0178E~1/\$File/0178E_guide.pdf) as part of seeking project approval under the Endangered Species Act.	A statement was added to the ESR text to identify this commitment to be carried out during detailed design.

July 17, 2012

Mr. Hitesh Topiwala
Project Manager
Regional Municipality of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, ON L6T 4B9

Dear Ms Topiwala

**RE: Class Environmental Assessment – Notice of Study Commencement
Expanded Study Area for Regional Road Corridor bounded by Winston
Churchill Boulevard, Bush Street, Old Main Street, Mississauga Road and
Olde Baseline Road
Region of Peel**

Thank you for your letter dated July 9, 2012 informing the Board of the commencement of the above noted project. Please keep the Board informed of the status of this project and provide the Board with any information you have available so the Board may monitor its progress and provide comments as necessary.

If you require any further information please contact me at 905-890-1010, ext. 2217.

Yours truly,



Paul Mountford, MCIP RPP
Intermediate Planning Officer
Planning and Accommodation Department

c. S. Hare, Peel District School Board
S. Keen, HDR Corporation

Miss Bush Expand Comm.doc

Trustees

Janet McDougald, Chair
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Beryl Ford
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Meredith Johnson

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Jeff White
Rick Williams

Director of Education and Secretary
Tony Pontes

Associate Director,
Instructional Support Services
Pam Tomasevic

Associate Director,
Operational Support Services
Carla Kisko

From: Keen, Stephen
Sent: Monday, August 13, 2012 4:30 PM
To: Baudais, Nathalie
Subject: Fw: CLASS EA REGIONAL ROAD CORRIDOR (No. 11-4360) NEATS 34342
Attachments: RDIMS-#6077714-v2-NWP_APP_GUIDE_EN.PDF; RDIMS-#6077727-v2-FORM-_TC_APPLICATION_FORM.PDF

From: Craigs, Jeremy [<mailto:jeremy.craigs@tc.gc.ca>]
Sent: Monday, August 13, 2012 03:28 PM
To: 'hitesh.topiwala@peelregion.ca' <hitesh.topiwala@peelregion.ca>
Cc: Keen, Stephen
Subject: CLASS EA REGIONAL ROAD CORRIDOR (No. 11-4360) NEATS 34342

Thank you for the information regarding the above referenced project. We have reviewed the information, and note the following:

Transport Canada is responsible for the administration of the *Navigable Waters Protection Act* (NWP), which prohibits the construction or placement of any “works” in navigable waters without first obtaining approval. If any of the related project undertakings cross or affect a potentially navigable waterway, the proponent should prepare and submit an application in accordance with the requirements as outlined in the attached Application Guide and Form. Any questions about the NWP application process should be directed to the Navigable Waters Protection Program at **(519) 383-1863** or NWPontario-PENontario@tc.gc.ca.

Please review the [Minor Works and Waters \(Navigable Waters Protection Act\) Order](#), established to outline the specific standards and criteria under which Transport Canada considers a work as a minor and does not require an application under the NWP. It is the responsibility of the applicant, prior to submitting an application to the Navigable Waters Protection Program for review, to assess whether their work meets the criteria, as described, and, therefore, falls within one of the excluded classes. An application will only be required if it is determined that the work cannot meet the criteria established for that particular “class” of excluded work.

Transport Canada is also responsible for inspecting and auditing federally regulated railway companies that are subject to the *Railway Safety Act*. Transport Canada also regulates some provincial shortlines from the Province of Ontario that are part of an Agreement between the Federal Government and the Province of Ontario. The *Railway Safety Act*, with related regulations and rules, provides the legislative and regulatory framework for safe railway operations in Canada. The rail safety program develops, implements and promotes safety policy, regulations, standards and research, and in the case of railway grade crossings, subsidizes safety improvements. A list of all the Rail Safety legislations (the *Act*, Regulations, Rules, Guidelines, Policies and Standards) that applies to the federally regulated railways, can be found here:

<http://www.tc.gc.ca/eng/railsafety/legislation.htm>

The *Act* also addresses the construction and alteration of railway works, the operation and maintenance of railway equipment and certain non-railway operations that may affect the safety of federally regulated railways. If a proposed railway work is of a prescribed kind, pursuant to the *Notice of Railway Works Regulations*, the proponent shall not undertake the work unless it has first given notice of the work in accordance with the regulation. More information related to railway works is available at the following internet sites:

· *Railway Safety Act*: <http://www.tc.gc.ca/acts-regulations/acts/1985s4-32/menu.htm>

- *Notice of Railway Works Regulations*: <http://laws.justice.gc.ca/en/SOR-91-103/>
- *Standards Respecting Pipeline Crossings Under Railways*: <http://www.tc.gc.ca/eng/railsafety/standards-tce10-236.htm>
- *Guideline on Requesting Approval to Undertake Certain Railway Works*: <http://www.tc.gc.ca/eng/railsafety/guideline-283.htm>

General inquiries about the Rail Safety Program can be directed to RailSafety@tc.gc.ca or by calling 613-998-2985.

Thank you,

Jeremy Craigs

Environmental Officer, Transport Canada - Ontario Region (PHE)

4900 Yonge Street, North York, ON M2N 6A5 Phone: 416-952-0502

September 6, 2012

Thank you for circulating Infrastructure Ontario (formerly the Ontario Realty Corporation) on your Notice. Infrastructure Ontario (IO) is the strategic manager of the provincial government's real property with a mandate of maintaining and optimizing value of the portfolio, while ensuring real estate decisions reflect public policy objectives of the government.

As you may be aware, *IO is responsible for managing real property that is owned by Her Majesty the Queen in Right of Ontario as represented by the Minister of Infrastructure (MOI)*. There is a potential that IO manages lands that fall within your study area. As a result, your proposal may impact IO managed properties and/or the activities of tenants present on IO-managed lands. In order to determine if IO property is within your study area, IO requires that the proponent of the project conduct a title search by reviewing parcel register(s) for adjoining lands, to determine the extent of ownership by MOI or it's predecessors (listed below) ownership. Please contact IO if any ownership of provincial government lands are known to occur within your study area and are proposed to be impacted. IO is obligated to complete due diligence for any realty activity on IO managed lands and this should be incorporated into all project timelines. IO managed lands can ***include within the title but is not limited to*** variations of the following: Her Majesty the Queen/King, OLC, ORC, Public Works, Hydro One, PIR, MGS, MBS, MOI, MTO, MNR and MEI*. Please ensure that a copy of your notice is also sent to the ministry/agency on title. As an example, if the study area includes a Provincial Park, then MNR is to also to be circulated notices related to your project.

Potential Negative Impacts to IO Tenants and Lands

General Impacts

Negative environmental impacts associated with the project design and construction, such as the potential for dewatering, dust, noise and vibration impacts, and impacts to natural heritage features/habitat and functions, should be avoided and/or appropriately mitigated in accordance with applicable regulations best practices and Ministry of Natural Resources (MNR) and Ministry of the Environment (MOE) standards. Avoidance and mitigation options that characterize baseline conditions and quantify the potential impacts should be present as part of the EA project file. Details of appropriate mitigation, contingency plans and triggers for implementing contingency plans should also be present.

Impacts to Land holdings

Negative impacts to land holdings, such as the taking of developable parcels of IO managed land or fragmentation of utility or transportation corridors, should be avoided. If the potential for such impacts is present as part of this undertaking, you should contact the undersigned to discuss these issues at the earliest possible stage of your study.

If takings are suggested as part of any alternative these should be appropriately mapped and quantified within EA report documentation. In addition, details of appropriate mitigation and or next steps related to compensation for any required takings should be present. IO requests circulation of the draft EA report prior to finalization if potential impacts to IO-managed lands are present as part of this study.

Heritage Management Process & Class Environmental Assessment (EA) Process

Should the proposed activities impact cultural heritage features on IO managed lands, a request to examine cultural heritage issues which can include the cultural landscape, archaeology and places of sacred and secular value could be required. The IO (formerly Ontario Realty Corporation) Heritage Management Process should be used for identifying and conserving heritage properties in the provincial portfolio (this document can be downloaded from the Heritage section of our website: <http://www.ontariorealty.ca/What-We-Do/Heritage.htm>). Through this process, IO identifies, communicates and conserves the values of its heritage places. In addition, the Class EA ensures that IO considers the potential effects of proposed undertakings on the environment, including cultural heritage.

Potential Triggers Related to MOI's Class EA

*The IO is required to follow the MOI Class Environmental Assessment Process for **Realty Activities** Not Related to Electricity Projects (MOI Class EA). The MOI Class EA applies to a wide range of realty and planning activities including leasing or letting, planning approvals, disposition, granting of easements, demolition and property maintenance/repair. For details on the IO Class EA please visit the Environment and Heritage page of our website found at <http://www.ontariorealty.ca/AssetFactory.aspx?did=2240>. Please note that completion of any EA process does not necessarily provide an approval for IO's EA processes unless the process incorporates IO's applicable Class EA requirements.*

If the MOI Class EA is triggered, and deferral to another ministry's or agency's Class EA or individual EA is requested, the alternative EA will be subject to a critical review prior to approval for any signoff of a deferral by the proponent. The alternative EA needs to fulfill the minimum criteria of the MOI Class EA. When evaluating an alternative EA there must be explicit reference to the corresponding undertaking in the MOI Class EA (e.g., if the proponent identifies the need to acquire land owned by MOI, then "acquisition of MOI-owned land", or similar statement, must be referenced in the EA document). Furthermore, sufficient levels of consultation with MOI's/IO's specific stakeholders, such as the MNR, must be documented with the relevant information corresponding to MOI's/IO's undertaking and the associated maps. In addition to *archaeological and heritage reports*, a *Phase I Environmental Site Assessment (ESA)*, on IO lands should also be incorporated into the alternative EA study. Deficiencies in any of these requirements could result in an inability to defer to the alternative EA study and require completing MOI's Class EA prior to commencement of the proposed undertaking.

In summary, the purchase of MOI-owned/IO-managed lands or disposal of rights and responsibilities (e.g. easement) for IO-managed lands triggers the application of the MOI Class EA. If any of these realty activities affecting IO-managed lands are being proposed as part of any alternative, please contact the Sales and Marketing Group through IO's main line (Phone: 416-327-3937, Toll Free: 1-877-863-9672), and contact the undersigned at your earliest convenience to discuss next steps.

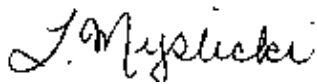
Specific Comments

If an EA for this project is currently being undertaken and **only if** the undertaking directly affects all or in part any IO-managed property, please send the undersigned a copy of the DRAFT EA report and allow sufficient time (minimum of 30 calendar days) for comments and discussion prior to finalizing the report to ensure that all MOI Class EA requirements can be met through the EA study.

Please remove IO from your circulation list, with respect to this project, if there are no IO managed lands in the study area. In addition, in the future, please send only **electronic copies of notices** for any projects impacting IO managed lands to:
Keith.Noronha@infrastructureontario.ca

Thank you for the opportunity to provide initial comments on this undertaking. If you have any questions on the above I can be reached at the contacts below.

Sincerely,



Lisa Myslicki
Environmental Advisor, Environmental Management
Infrastructure Ontario
1 Dundas Street West,
Suite 2000, Toronto, Ontario
M5G 2L5
(416) 212-3768
lisa.myslicki@infrastructureontario.ca

* Below are the acronyms for agencies/ministries listed in the above letter

OLC: Ontario Lands Corporation
ORC: Ontario Realty Corporation
PIR: Public Infrastructure and Renewal
MGS: Ministry of Government Services
MBS: Management Board and Secretariat
MOI: Ministry of Infrastructure
MTO: Ministry of Transportation
MNR: Ministry of Natural Resources
MEI: Ministry of Energy and Infrastructure

Ministry of Tourism, Culture and Sport

Culture Programs Unit
Programs and Services Branch
Culture Division
401 Bay Street, Suite 1700
Toronto ON M7A 0A7
ArchaeologyReports@ontario.ca

Ministre du Tourisme, de la Culture et du Sport

Unit des programmes culturels
Direction des programmes et des services
Division de culture
401, rue Bay, bureau 1700
Toronto ON M7A 0A7
ArchaeologyReports@ontario.ca



Aug 19, 2013

Robert Pihl (P057)
Archaeological Services Inc. - Bathurst
528 Bathurst Toronto ON M5S 2P9

**RE: RE: Entry into the Ontario Public Register of Archaeological Reports:
Archaeological Assessment Report Entitled, "Archaeological Assessment: Stage 1
Background Study and Property Inspection Olde Base Line Road and Winston
Churchill Boulevard Class Environmental Assessment Study Former Townships of
Caledon and Chinguacousy (Peel County) And Former Township of Erin
(Wellington County) Regional Municipality of Peel, Ontario", Dated Aug 19, 2013,
Filed with MTCS Toronto Office on N/A, MTCS Project Information Form Number
P057-719-2012, MTCS File Number 0000123**

Dear Mr. Pihl:

The above-mentioned report, which has been submitted to this ministry as a condition of licensing in accordance with Part VI of the Ontario Heritage Act, R.S.O. 1990, c 0.18 has been entered into the Ontario Public Register of Archaeological Reports without technical review.¹

Please note that the ministry makes no representation or warranty as to the completeness, accuracy or quality of reports in the register.

Should you require further information, please do not hesitate to send your inquiry to ArchaeologyReports@Ontario.ca.

cc. Archaeology Licensing Officer
Tyrone Gan, HDR Corporation
Hitesh Topiwala, Region of Peel

¹In no way will the ministry be liable for any harm, damages, costs, expenses, losses, claims or actions that may result: (a) if the Report(s) or its recommendations are discovered to be inaccurate, incomplete, misleading or fraudulent; or (b) from the issuance of this letter. Further measures may need to be taken in the event that additional artifacts or archaeological sites are identified or the Report(s) is otherwise found to be inaccurate, incomplete, misleading or fraudulent.

Niagara Escarpment Commission

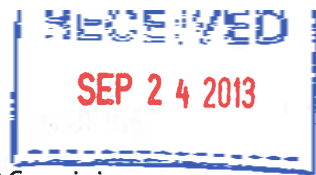
232 Guelph St.
Georgetown, ON L7G 4B1
Tel: 905-877-5191
Fax: 905-873-7452
www.escarpment.org

Commission de l'escarpement du Niagara

232, rue Guelph
Georgetown ON L7G 4B1
No de tél. 905-877-5191
Télécopieur 905-873-7452
www.escarpment.org



Niagara Escarpment Commission
An agency of the Government of Ontario



September 17, 2013

Ms. Veronica Restrepo, B.A.SC., EIT
Transportation Planner
HDR Corporation
100 York Boulevard, Suite 300
Richmond Hill ON L4B 1J8

Dear Ms. Restrepo:

**Re: Belfountain Transportation Corridor Class EA Study
Draft NRSI Natural Heritage Report**

Staff of the Niagara Escarpment Commission (NEC) has reviewed the draft Natural Heritage Report prepared in relation to the Environmental Assessment for proposed Regional road improvements in the Town of Caledon. We offer the following comments.

Species at Risk

We note at page 13 that the detailed information regarding Species at Risk (SAR) has only been provided to the Ministry of Natural Resources (MNR) and Credit Valley Conservation (CVC). In order for the NEC to determine conformity of proposed road improvements with the Niagara Escarpment Plan (NEP) Part 2.8, the NEC will also need to be provided with this information. It is common practice to provide such information to our agency in the context of Plan Amendment and Development Permit applications. We are bound by rules of confidentiality and understand that any SAR information received must not be distributed.

Until we receive the SAR information, we cannot comment on whether the report has adequately addressed wildlife habitat for areas within the NEP.

Niagara Escarpment Plan

Part 2.7 of the draft report contains some of the relevant NEP policies. It is correct to indicate that only essential transportation and utility facilities are allowed in the Escarpment Natural Area. "Essential" is defined in the NEP as "that which is deemed necessary to the public interest after all alternatives have been considered. In the Escarpment Protection and Rural Areas transportation and utility facilities do not have meet the test of being essential. In all designations, the policies and relevant development criteria of the NEP must be satisfied for a permitted use to be an approved use.

We recommend that this policy distinction be noted in the Natural Heritage Report.

Our further concern with the consideration of the NEP in the draft report is that after this initial section, all other natural heritage impacts seem to be addressed only in the context of the PPS, Regional and Local Official Plan policy.

Significant Wildlife Habitat

It is noted at page 25 and page 69 that the PPS contains policies to protect significant wildlife habitat. The NEP also contains policies to protect wildlife habitat, which can include significant wildlife habitat as defined in the PPS (e.g. special concern species). This should be noted in the report.

Provincially Significant Wetlands

On page 20 of the report it is noted that there are two Provincially Significant Wetland (PSW) complexes in the study area and that a permit would be required from the CVC to "proceed with any alteration or development within these wetland features, or within their adjacent lands". PSW's are a Designation Criterion for inclusion in the Escarpment Natural Area designation of the NEP where the Objective is to "maintain the most natural Escarpment features, stream valleys and wetlands". Furthermore, Part 2.6 of the NEP subsections 10 – 13 state that development shall locate outside wetlands and that development adjacent to wetlands must not result in the loss of wetland function or loss of contiguous wetland area. Section 5.1.3.1 does not reference the Objective and policies for wetlands found in the NEP, only the PPS policy is noted. This should be corrected.

Any consideration of development in PSW's would not be consistent with the NEP and other alternatives would have to be considered.

Vegetation Communities

On page 40 of the report in reference to tree species, there is a statement that because the trees were within a plantation they were "not considered significant". While staff understands that certain tree species may not be identified as significant from a SAR perspective, NEP Part 2.7 New Development in Wooded Areas contains the objective to "preserve as much as possible of wooded areas". The report should address consideration of the NEP objective and policies as related to tree preservation on lands within the NEP Area. There is no reference to the NEP provisions in Section 5.1.4.

Significant Natural Heritage Features and Habitats

In the discussion in Part 5 of the report with respect to lands within the NEP, the report suggests that in certain instances, avoiding impact to significant natural heritage features may not be feasible and that mitigation and monitoring would be utilised. If a policy in the NEP cannot be satisfied through the proposed road project, mitigation is not necessarily a satisfactory response depending on the policy tests. Mitigation can be used to minimize impacts, but may not be sufficient in demonstrating no negative impacts. In these cases, a Niagara Escarpment Plan amendment could be required, in which a justification for not meeting these tests would have to be made, along with demonstration that the overall Purpose and Objectives of the Plan and Act could be met.

Section 5.1.1 of the report should be corrected because it suggests that the test is "essential" infrastructure in the Escarpment Protection and Rural Areas whereas that test only applies in the Escarpment Natural Area. A full analysis of whether any of the

proposed infrastructure is essential will have to be taken as part of the EA process if the works are proposed in the Escarpment Natural Area.

ANSI's

Areas of Natural and Scientific Interest are discussed in Section 5.1.7 of the report. Part 2.14 of the NEP respecting Provincial and Regionally Significant Life Science ANSIs, which states that development shall be directed outside of Provincially Significant and Regionally Significant Life Science ANSI's, is not evaluated. . The report should undertake an evaluation of whether this policy test has been satisfied.

Table 7

Although the NEP is mentioned under general policy considerations at the start of the Table, there is no further analysis in the chart of NEP policy and the related policy considerations. The chart should be revised to include all the relevant NEP policy considerations as discussed in this letter.

"Opportunities"

Section 5.3 contemplates that some works may have to take place outside the road right of way. If any works are proposed on private land and a Development Permit is required, the private land owner would have to consent to the permit application.

Mitigation

We acknowledge the assessment of possible mitigation alternatives to reduce mortality due to road crossings. Further work would have to be done to demonstrate that this has been or could be a successful option to address the issue in consideration of the other options (road closings, speed reductions).

Monitoring and Adaptive Management

Monitoring is an important component of the implementation of a project and is an important component of the NEC program as outlined in the Purpose and Objectives section of the NEP. Actions and responsibilities developed as part of an "adaptive management" approach are also a necessary component, such that if an impact is measured, the follow-up actions are identified, the responsibilities have been assigned and the steps outlined to address that impact have been set out in agreement between the agencies. Further work would need to be undertaken in this regard.

Appendix 1

The NEC is not listed as being in attendance at the meeting on July 9, 2013. As NEC Staff was in attendance at the meeting, the meeting notes should be corrected.

We appreciate the opportunity to review the draft report and look forward to the meeting on October 8, 2013 to discuss our initial comments and those of other agencies. We note that the report references a "future impact assessment" as part of the analysis of the preferred design. It will be important for the NEC to be consulted as part of this process to determine how the NEP policies have been satisfied.

If you have any questions regarding our comments, you may contact me at 905-877-8363.

Yours truly,

N. Mott-Allen

Nancy Mott-Allen, MCIP, RPP
Senior Strategic Advisor

- cc. **Steve Ganesh, Peel Region**
Mark Heaton, MNR
Liam Marray, CVC
Mark Eby, County of Wellington
Ryan Grodecki, Town of Caledon



Memo

Project No. 1337

To: Nancy Mott-Allen, Niagara Escarpment Commission

From: Ryan Archer

CC: Veronica Restrepo, HDR Corp.

Date: October 3, 2013

Re: Response to NEC Comments on Belfountain Transportation Corridor Class EA Study, Draft NRSI Natural Heritage Report

Thank-you for your review and comments pertaining to NRSI's Draft Natural Heritage Report for the Belfountain Transportation Corridor Class EA study. I have reviewed your comments and have provided responses to each below.

1. Species at Risk

We note at page 13 that the detailed information regarding Species at Risk (SAR) has only been provided to the Ministry of Natural Resources (MNR) and Credit Valley Conservation (CVC). In order for NEC to determine conformity of proposed road improvements with the Niagara Escarpment Plan (NEP) Part 2.8, the NEC will also need to be provided with this information. It is common practice to provide such information to our agency in the context of Plan Amendment and Development Permit applications. We are bound by rules of confidentiality and understand that any SAR information received must not be distributed.

Until we receive the SAR information, we cannot comment on whether the report has adequately addressed wildlife habitat for areas within the NEP.

Response: NRSI has not yet prepared separate detailed reporting of SAR observations within the Class EA study area for the MNR or CVC. As directed by the MNR, SAR observations were not described in detail within the Natural Heritage Report due to data sensitivity. Once NRSI receives direction to prepare a detailed SAR observation report under separate cover, the NEC will be circulated the draft report along with the MNR and CVC.

2. Niagara Escarpment Plan

Part 2.7 of the draft report contains some of the relevant NEP policies. It is correct to indicate that only essential transportation and utility facilities are allowed in the Escarpment Natural Area. "Essential" is defined in the NEP as "that which is deemed necessary to the public interest after all alternatives have been considered". In the

Escarpment Protection and Rural Areas transportation and utility facilities do not have to meet the test of being essential. In all designations, the policies and relevant development criteria of the NEP must be satisfied for a permitted use to be an approved use.

We recommend that this policy distinction be noted in the Natural Heritage Report.

Our further concern with the consideration of the NEP in the draft report is that after this initial section, all other natural heritage impacts seem to be addressed only in the context of the PPS, Regional and Local Official Plan policy.

Response: The draft Natural Heritage Assessment report will be revised to clarify the distinction in NEP policy regarding allowance of essential transportation and utility facilities in the Escarpment Natural Area versus in the Escarpment Protection and Rural Areas. The report will also be revised to more fully cite the NEP in addressing natural feature significance, policy protections, and potential impacts.

3. Significant Wildlife Habitat

It is noted on page 25 and page 69 that the PPS contains policies to protect significant wildlife habitat. The NEP also contains policies to protect wildlife habitat, which can include significant wildlife habitat as defined in the PPS (e.g. special concern species). This should be noted in the report.

Response: The Natural Heritage Assessment report will be revised to include reference to the NEP in describing existing protection policies for Significant Wildlife Habitat.

4. Provincially Significant Wetlands

On page 20 of the report it is noted that there are two Provincially Significant Wetland (PSW) complexes in the study area and that a permit would be required from the CVC to “proceed with any alteration or development within these wetland features, or within their adjacent lands”. PSWs are a Designation Criterion for inclusion in the Escarpment Natural Area designation of the NEP where the Objective is to “maintain the most natural Escarpment features, stream valleys and wetlands”. Furthermore, Part 2.6 of the NEP subsections 10-13 state that development shall locate outside wetlands and that development adjacent to wetlands must not result in the loss of wetland function or loss of contiguous wetland area. Section 5.1.3.1 does not reference the Objective and policies for wetlands found in the NEP, only the PPS is noted. This should be corrected.

Any consideration of development in PSWs would not be consistent with the NEP and other alternatives would have to be considered.

Response: The Natural Heritage Assessment report will be updated to reference and address the NEP’s Objective and policies toward wetlands as they apply within the study area. The report will note that development within PSWs is inconsistent with NEP policy without a full evaluation of alternatives.

5. Vegetation Communities

On page 40 of the report in reference to tree species, there is a statement that because the trees were within a plantation they were “not considered significant”. While staff understands that certain tree species may not be identified as significant from a SAR perspective, NEP Part 2.7 New Development in Wooded Areas contains the objective to

“preserve as much as possible of wooded areas”. The report should address consideration of the NEP objective and policies as related to tree preservation on lands within the NEP Area. There is no reference to the NEP provisions in Section 5.1.4.

Response: As noted by the NEC in the statement above, the observed regionally rare species White Spruce (*Picea glauca*) and Red Pine (*Pinus resinosa*) were considered not significant as individual trees due to their planted context. However, Section 5.1.4 of the Natural Heritage Assessment report will be updated to reference the NEP policy regarding protection of existing wooded areas, including plantations.

6. Significant Natural Heritage Features and Habitats

In the discussion in Part 5 of the report with respect to lands within the NEP, the report suggests that in certain instances, avoiding impact to significant natural heritage features may not be feasible and that mitigation and monitoring would be utilized. If a policy in the NEP cannot be satisfied through the proposed road project, mitigation is not necessarily a satisfactory response depending on the policy tests. Mitigation can be used to minimize impacts, but may not be sufficient in demonstrating no negative impacts. In these cases, a Niagara Escarpment Plan amendment could be required, in which a justification for not meeting these tests would have to be made, along with demonstration that the overall Purpose and Objectives of the Plan and Act could be met.

Section 5.1.1 of the report should be corrected because it suggests that the test is “essential” infrastructure in the Escarpment Protection and Rural Areas whereas that test only applies in the Escarpment Natural Area. A full analysis of whether any of the proposed infrastructure is essential will have to be taken as part of the EA process if the works are to be proposed in the Escarpment Natural Area.

Response: Section 5 of the Natural Heritage Assessment report will be revised to clarify that the full evaluation of alternatives should be completed with an objective to *firstly* avoid impacts to significant natural heritage features, *and if this is not possible*, *secondarily* minimize or mitigate potential impacts to the affected significant natural feature(s). The report will note the potential need for a NEP amendment, as described above, if it cannot be demonstrated that there will be no negative impacts to a significant natural feature.

Section 5.1.1 of the report will be updated to clarify the policy associated with essential infrastructure within the NEP Area, including the need to evaluate whether the proposed infrastructure is considered essential.

7. ANSIs

Areas of Natural and Scientific Interest are discussed in Section 5.1.7 of the report. Part 2.14 of the NEP respecting Provincial and Regionally Significant Life Science ANSIs, which states that development shall be directed outside of Provincially Significant and Regionally Significant Life Science ANSIs, is not evaluated. The report should undertake an evaluation of whether the policy test has been satisfied.

Response: The Natural Heritage Assessment report will be revised to include reference to NEP policy associated with protection of provincially and regionally significant Life Science ANSIs. The policy test will be applied as part of the planned evaluation of alternatives and subsequent impact assessment associated with the preferred alternative design.

8. Table 7

Although the NEP is mentioned under general policy considerations at the start of the Table, there is no further analysis in the chart of NEP policy and the related policy considerations. The chart should be revised to include all the relevant NEP policy considerations as discussed in this letter.

Response: Table 7 will be revised to more fully describe relevant NEP policies as noted.

9. “Opportunities”

Section 5.3 contemplates that some works may have to take place outside the road right of way. If any works are proposed on private land and a Development Permit is required, the private land owner would have to consent to the permit application.

Response: Noted. This will be clarified within Section 5.3.

10. Mitigation

We acknowledge the assessment of possible mitigation alternatives to reduce mortality due to road crossings. Further work would have to be done to demonstrate that this has been or could be a successful option to address the issue in consideration of the other options (road closings, speed reductions).

Response: The Natural Heritage Assessment report presents only preliminary recommendations for potential mitigation measures that seek to minimize wildlife impacts associated with road crossings. A more fulsome assessment of appropriate measures will be considered, with associated recommendations, as part of a future impact assessment of the preferred alternative design.

11. Monitoring and Adaptive Management

Monitoring is an important component in the implementation of a project and is an important component of the NEC program as outlined in the Purpose and Objectives section of the NEP. Actions and responsibilities developed as part of an “adaptive management” approach are also a necessary component, such that if an impact is measured, the follow-up actions are identified, the responsibilities have been assigned and the steps outlined to address that impact have been set out in agreement between the agencies. Further work would need to be undertaken in this regard.

Response: Section 6.3 of the Natural Heritage Assessment report states the need for appropriate monitoring as well as an adaptive management approach to properly measure, recognize, and respond to the need for actions and changes in approach if required. This section will be updated to more fully describe this recommended approach as noted above.

12. Appendix 1

The NEC is not listed as being in attendance at the meeting on July 9, 2013. As NEC Staff was in attendance at the meeting, the meeting notes should be corrected.

Response: Please specify NEC’s staff’s attendance at the July 9, 2013 meeting, which focused on discussion regarding updates to MNR deer road crossing data, CVC amphibian monitoring data, and SAR observations. NRSI’s meeting notes indicated the attendance of the following: M. Heaton (MNR), V. Restrepo (HDR), H. Lynn and L.

Murray (CVC), G. Dela Cruz (Region of Peel), and R. Archer (NRSI).

November 18, 2013

Mr. Gino Dela Cruz
Project Manager
Regional Municipality of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, ON L6T 4B9

Dear Mr. Dela Cruz

**RE: Class Environmental Assessment – Notice of PIC #2
Mississauga Road, Old Main Street, Bush Street, Winston Churchill
Boulevard and Olde Base Line Road
Region of Peel**

Thank you for your letter dated November 4, 2013 informing the Board of the PIC #2 to be held November 20, 2013 for the above noted project. Please keep the Board informed of the status of this project and provide the Board with any information you have available so the Board may monitor its progress and provide comments as necessary.

If you require any further information please contact me at 905-890-1010, ext. 2724.

Yours truly,



Branko Vidovic
Planning Assistant
Planning and Accommodation Department

c. D. Dundas, Peel District School Board
T. Gan, HDR

Miss Bush WC PIC2.doc

Trustees
Janet McDougald, Chair
Suzanne Nurse, Vice-Chair
Stan Cameron
Beryl Ford
David Green
Meredith Johnson

Steve Kavanagh
Sue Lawton
Brad MacDonald
Harinder Malhi
Jeff White
Rick Williams

Director of Education and Secretary
Tony Pontes

Associate Director,
Instructional Support Services
Scott Moreash

Associate Director,
Operational Support Services
Jaspal Gill (Acting)



Canadian Environmental
Assessment Agency

55 St. Clair Avenue East,
Room 907
Toronto ON M4T 1M2

Agence canadienne
d'évaluation environnementale

55, avenue St. Clair Est,
pièce 907
Toronto ON M4T 1M2

April 25, 2014

Sent by mail

Ms. Asha Saddi
Public Works, Region of Peel
10 Peel Centre Dr., Suite B
Brampton, ON L6T4B9

Dear Ms. Saddi:

Re: Information on the Canadian Environmental Assessment Act, 2012

Thank you for your correspondence of January 23, 2014 regarding the Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road Project.

As part of the Government of Canada's plan for Responsible Resource Development which seeks to modernize the regulatory system for project reviews, the *Canadian Environmental Assessment Act, 2012* (CEAA 2012) came into force on July 6, 2012. CEAA 2012 focuses federal environmental reviews on projects that have the potential to cause significant adverse environmental effects in areas of federal jurisdiction.

The CEAA 2012 applies to projects described in the *Regulations Designating Physical Activities* (the Regulations). Based on the information provided, your project does not appear to be described in the Regulations. **Kindly review the Regulations to confirm applicability to your project** including section 1 (h), which relates to wildlife areas and migratory bird sanctuaries.

According to section 25 (c) of the Regulations the construction, operation, decommissioning and abandonment of a new all-season public highway that requires a total of 50 km or more of new right of way may require a Federal Environmental Assessment.

For more information on CEAA 2012, please access the following links on the Canadian Environmental Assessment Agency's (the Agency) website:

Overview of CEAA 2012

<http://www.ceaa.gc.ca/default.asp?lang=En&n=16254939-1>

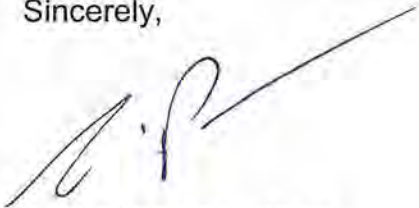
Regulations Designating Physical Activities, and
Prescribed Information for a Description of a Designated Project Regulations
<http://www.ceaa.gc.ca/default.asp?lang=En&n=9EC7CAD2-1>

If it appears that CEAA 2012 may apply to your proposed project, you must provide the Agency with a description of the proposed project. Please see the link below to the Agency's guide to preparing a project description.

Guide to Preparing a Description of a Designated Project
[http://www.ceaa.gc.ca/63D3D025-2236-49C9-A169-DD89A36DA0E6/Guide to Preparing a Description of a Designated Project under CEAA 2012.pdf](http://www.ceaa.gc.ca/63D3D025-2236-49C9-A169-DD89A36DA0E6/Guide%20to%20Preparing%20a%20Description%20of%20a%20Designated%20Project%20under%20CEAA%202012.pdf)

If you believe the project is not subject to a federal environmental assessment, and do not submit a project description, **we kindly request that you remove the Agency from your distribution list**. If you have questions, please get in touch with our office through the switchboard at 416-952-1576.

Sincerely,

A handwritten signature in black ink, appearing to be 'A. P.', with a long horizontal stroke extending to the right.

Anjala Puvananathan
Director, Ontario Region
Canadian Environmental Assessment Agency

Ministry of the Environment

Central Region
Technical Support Section

5775 Yonge Street, 8th Floor
North York, Ontario M2M 4J1

Tel.: (416) 326-6700
Fax: (416) 325-6345

Ministère de l'Environnement

Région du Centre
Section d'appui technique

5775, rue Yonge, 8^{ième} étage
North York, Ontario M2M 4J1

Tél. : (416) 326-6700
Télééc. : (416) 325-6347



April 25, 2014

File: EA 01-06-05

Gino Dela Cruz
Project Manager, Region of Peel
10 Peel Centre Dr., Suite B, 4th Floor
Brampton, ON L6T 4B9

**RE: Mississauga Road, Olde Base Line Road, Winston Churchill Boulevard,
Bush Street and Old Main Street
Regional Municipality of Peel
Municipal Class Environmental Assessment – Schedule C
Response to Draft Environmental Study Report, March, 2014
Technical Support Comments**

We have received the draft Environmental Study Report (ESR) for the above noted environmental assessment. Our understanding is that this project has many preferred alternatives that combine both operational and physical improvements in order to address the different needs and community concerns specific to the five major roads included in this study area. We provide the following comments below for your consideration.

General Comments

1. On page 27 of the Draft ESR Main Text, please change “Indian and Northern Affairs Canada” to “Aboriginal Affairs and Northern Development Canada”.
2. The First Nations Correspondence section in Appendix A only contains responses from three groups. Please clarify whether follow-up was conducted with the other First Nations groups who did not provide a response to initial outreach.

Surface Water Comments

1. We recommend that a statement be added to explain that Enhanced Level Protection may not be met for all areas where the proposed stormwater management facilities of OGS and/or grassed swales are utilized independently. MOE has determined that ditches/enhanced grassed swales or OGS cannot

meet 'Enhanced Water Quality Protection' unless used as part of a treatment train (Stormwater Management Planning and Design Manual, MOE 2003).

OGSs are typically used for small drainage areas (<2 ha). If runoff is over the capacity of the OGS, the potential for by-pass conditions with no treatment occurring during storm events increases. OGS sizing requirements will need to be considered in order to capture and treat at least 90% of the runoff volume that occurs at a site and to achieve a long-term average basis for water quality objectives of 'enhanced protection'. This should be taken into account considering the ditches will continue to drain lands external to the roadway(s).

2. MOE defers any comments on the assessment, impacts, and mitigation of natural features and fish habitat to the Ministry of Natural Resources (MNR). Of particular concern are the effects that changes in water quality and water quantity may have on species identified in the study area classified as Endangered, Threatened or of Special Concern. Please ensure that MNR is consulted and any comments from MNR are incorporated into the final ESR.
3. Hydraulic analysis and proposals for sizing of stream culverts and bridge crossings are not reviewed here and should be sent to the local conservation authority for review by their engineering staff. Please ensure the local conservation authority is consulted and any comments are incorporated into the final ESR.
4. While several sections of the ESR and Appendices acknowledge the need for a PTTW for groundwater pumping in exceedance of 50,000 LPD, it should be noted that a PTTW is also needed for surface water extraction and the active diversion of surface water flows by pumping in exceedance of 50,000 litres/day. A monitoring program for discharge water quality and quantity, as well as a mitigation program, may need to be developed. Please ensure that you consult with the MOE Central Region Permit to Take Water (PTTW) Coordinator prior to detailed design to confirm any approval requirements for water takings during construction or operation.

Air Quality Review Comments

1. During construction, please apply best management practices to mitigate any air quality impacts caused by construction dust. Please note that the ministry recommends that non-chloride dust suppressants be applied.

For a comprehensive list of fugitive dust prevention and control measures, please refer to *Cheminfo Services Inc. Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities*. Report prepared for Environment Canada. March 2005.

<http://www.bieapfrempp.org/Toolbox%20pdfs/EC%20-20Final%20Code%20of%20Practice%20-%20Construction%20%20Demolition.pdf>

Contaminated Soil Comments


1. If soil removed during construction is determined to be contaminated, please ensure that the disposal of contaminated soil is consistent with Part XV.1 of the Environmental Protection Act and Ontario Regulation 153/04, Records of Site Condition, which detail the new requirements related to site assessment and clean up.

Groundwater Review Comments

1. There are over two hundred water supply wells identified within 500 m of the study area. It is conceivable that these wells may be affected by road construction, either because of construction activities or, later, due to additional or more proximate road salt application. We recommend confirming which wells are used domestically prior to construction, to ensure that affected well owners will continue to have water supplies of appropriate quality and in adequate quantities, and to ensure that any work done on affected wells or any replacement wells is done pursuant to O. Reg. 903, Wells (pursuant to the Ontario Water Resources Act).

Thank you for the opportunity to comment on this project. Should you or any members of your project team have any questions, please feel free to contact me at 416-326-5745.

Thanks,



Amanda Graham
Environmental Resource Planner and EA Coordinator
Air, Pesticides and Environmental Planning

Cc: Dan Delaquis, Technical Support APEP Supervisor
Tina Dufresne, Manager, Halton Peel District

Niagara Escarpment Commission

232 Guelph St.
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Commission de l'escarpement du Niagara

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Niagara Escarpment Commission
An agency of the Government of Ontario

May 1, 2014

Mr. Tyrone Gan
Project Manager
HDR Corporation
100 York Blvd., Suite 300
Richmond Hill, ON L4B 1J8

Dear Mr. Gan:

**Re: Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base Line Road
Environmental Assessment – Draft ESR**

Staff of the Niagara Escarpment Commission (NEC) has reviewed the draft Environmental Study Report (ESR) for the above-noted project. As you are aware, the NEC has participated in the Technical Advisory Committee for this project since the fall of 2013 and has commented on drafts of some of the other technical background documents. There has been a thorough process and good opportunities for discussion during the study. We offer the following comments.

General

We appreciate the need to consider road improvements in the Study Area from a safety perspective. In our view, of significant importance to the evaluation of the options for the road improvements within the Niagara Escarpment Plan Area (NEP) is whether the recommended options would be in conflict with the NEP. NEC staff has made it clear throughout the project that the NEP policy is that development shall locate outside wetlands (NEP Part 2.6.10). In addition, we have advised that with respect to wildlife habitat, NEP policy is that new development will not be permitted in identified habitat of endangered (regulated) plant or animal species (NEP Part 2.8.1). There is acknowledgement in the Natural Heritage Report (Appendix B) that an amendment to the NEP would be required with respect to wetlands, depending on the chosen option for road improvement, but there is not a similar statement with respect to proposed development in endangered species habitat.

To recommend an option for a road improvement without noting in the ESR that there is a policy conflict with a Provincial Plan is misleading. Furthermore, it should be noted that even if a Plan Amendment were sought, there is no guarantee that the Amendment would be approved. Recommending the road improvement options in the ESR and deferring the issue of dealing with the policy conflict to the detailed design stage could result in significant delays. Understanding both the policy conflict and possible solutions, such as the "do nothing" alternative in some cases, is preferable before final decisions are made in the EA process about which alternatives are preferred.

While we recognise that some road improvements may be necessary, not all road improvements are inherently "essential" as defined in the NEP. Within the Escarpment Natural Area, "essential transportation and utility facilities" are permitted uses. Essential is defined as that which is deemed necessary to the public interest after all alternatives have been

considered. We do not believe that sufficient justification has been provided for road improvements within the Escarpment Natural Area.

The drawings for the road sections indicate vegetation removal in certain areas. There is no discussion in the ESR about considerations for planting or how visual impact would be addressed in these locations. Part 2.7 of the NEP has an objective that new development should preserve as much as possible of wooded areas. Perhaps additional information could be included as to the approach that will be taken in the project to achieve that preservation and any commitment to undertaking visual impact assessment, where necessary, to evaluate the impact of vegetation removal.

In addition to protecting unique ecologic and historic areas, it is an objective of the NEP in Part 2.12 to inventory, interpret, evaluate, maintain and conserve the cultural heritage features of the Niagara Escarpment Plan Area. We appreciate that you have considered the protection of cemeteries and stone walls in the ESR. A greater commitment to maintaining and conserving the stone walls rather than deferring this consideration to detailed design would have been preferred.

We recommend further discussion between the NEC, the Region and the study team prior to the finalization of the ESR.

Specific

Page 5 – Since Mr. Baran and Mr. Whitbread did not participate in the study you could leave their names out. Alternatively, you could put the names of all NEC staff together.

Page 8 – As part of Phase 5, the ESR should indicate the need for Plan Amendments and Development Permits from the NEC.

Page 9 – The discussion regarding the PPS should be updated to reference the PPS 2014 as it has now been released. This should also be done in the Natural Heritage report.

Page 12 – Given NEC staff comments above, it would be appropriate to state the need for a Plan Amendment and Development Permit (for any road works that are not exempt) from the NEC permit process.

Page 15 – Is it appropriate to discuss transit orientation in the context of this project given that no transit is available, as far as I am aware, to the Study Area?

Page 29 – This section is called Existing Land Use but the discussion focusses more on land use designations in policy documents. It might be more appropriately titled Existing Land Use designations and should include reference to the specific NEP designations within the Study Area, not just the NEP in general.

Page 36 – There is reference here to an “equalization culvert”. I think that the term should be clarified as the term may not be known to members of the public who will be reading the report.

Page 37 – It is not clear what the presence of the Escarpment has to do with limiting archaeological potential. The NEP also protects cultural heritage resources. Also on the same page, Item 1, the third sentence is not clear.

Page 101 – Given that NEC staff has expressed concern about the possible impact of culvert changes on natural heritage I would have expected some comment in this section as to whether the proposed culverts are in the regulated habitat of an endangered species.

Page 122 – There is no discussion here about the timing of construction and de-watering with respect to the impact on wildlife or fish habitat.

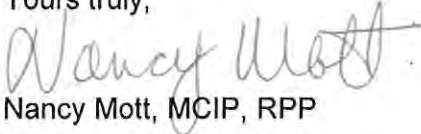
Page 184 – Escarpment Protected Area should read Escarpment Protection Area. Using this chart as an example, the do nothing approach is the recommended option due to conflict with the NEP but another option with impact on the NEP Area is offered. The connection is not made between the policy conflicts identified under the Niagara Escarpment section heading to the evaluation of the options earlier in the chart as it relates to impact on regulated habitat. The overall choices are options that would involve impact to regulated habitat which is misleading given the need for a Niagara Escarpment Plan amendment which might not be successful. There should be consideration for revising the charts for the road sections in the NEP to identify the policy issue and how the overall choice can be justified in light of it.

For the tables evaluating the preferred road improvement options for lands within the NEP Area, we reiterate our concern that road improvements are proposed within or adjacent to the habitat of endangered species. An option would be to combine Figure 4 (Environmental Policy Areas) with Figure 5b (Fish and Wildlife Habitat) to fully appreciate the policy conflicts. This was undertaken in the detailed design for the Winston Churchill Road project presently ongoing and it was helpful to the understanding of the potential conflicts between policy and the preferred alternatives from an engineering perspective.

Please note that the NEC has been contacted by the Belfountain Community Association. They have expressed concern about proposed road changes in Belfountain. We provided information about the EA process in general and encouraged them to review the ESR when it is posted before reaching any conclusions about what might be proposed.

We appreciate the opportunity to provide comments on the draft ESR and trust that these comments are of assistance. If you have any questions or would like an opportunity to discuss them further, please contact me at 905-877-8363.

Yours truly,



Nancy Mott, MCIP, RPP

Senior Strategic Advisor

cc. Ken Whitbread, NEC
Gino Dela Cruz, Region of Peel
Mark Heaton, MNR
Liam Marray, CVC

Appendix A.3
First Nations Consultation

August 3, 2012
Project No. 11-4360

Ms. Heather Levecque, Manager
Consultation Unit
Ministry of Aboriginal Affairs
160 Bloor Street East, 9th Floor
Toronto ON M7A 2E6

Dear Ms. Levecque:

**Re: Class Environmental Assessment Study
Regional Road corridor bounded by Winston Churchill Boulevard, Bush Street, Old Main Street, Mississauga Road and Olde Baseline Road**

In June 2009 the Regional Municipality of Peel began a Schedule C Municipal Class Environmental Assessment (EA) for improvements to Mississauga Road from Olde Base Line Road to Bush Street and Bush Street from Mississauga Road to Winston Churchill Boulevard. We are now expanding the limits of our EA to include Olde Base Line Road from Mississauga Road to Winston Churchill Boulevard, and Winston Churchill Boulevard from Olde Base Line Road to Bush Street.

The study is being carried out in accordance with the planning and design process for Schedule 'C' projects as outlined in the Municipal Engineers Association, Municipal Class Environmental Assessment document (October 2000, as amended in October 2007), which is approved under the Ontario Environmental Assessment Act.

A copy of the project "Notice of Study Commencement" is enclosed for your reference.

Could you please advise whether there are any land claims, litigation and/or Crown land that would apply to our study area (map included in the Notice)?

A key component of the study will be consultation with interested stakeholders. As part of the notification for the study, we have contacted the following First Nations and Aboriginal Peoples. We want to ensure that everyone with an interest in the area has been consulted and has the opportunity to provide input. Do you know of any other First Nations or Aboriginal Peoples that may have an interest in this study?

Chiefs of Ontario 111 Peter Street Suite 804 Toronto, ON M5V 2H1	Credit River Metis Council Ryan McBrid 56 Baccarat Crescent Brampton, ON L7A 1K8	Peel Aboriginal Network Patricia Chrisjohn 5 Wellington Street East Brampton, ON L6W 1Y1
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Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Anishinabek Nation/Union of Ontario Indians Allan Dokis Director - Intergovernmental Affairs Nippissing First Nation PO Box 611 North Bay, ON P1B 8J8	Alderville First Nation Denise Graham Band Administrator 11696 2nd Line Road P.O. Box 46 Roseneath, ON K0K 2X0	Association of Iroquois and Allied Indians Rolanda Elijah Director of Intergovernmental Affairs 387 Princess Avenue London, ON N6B 2A7
Six Nations Council Kate Cave 2498 Chiefswood Road P.O. Box 5000 Oshweken, ON N0A 1M0	Saugeen Ojibway Nation Jake Linklater RR# 5 Warton, ON N0H 2T0	Chippewas of Georgina Island Janice Taylor R.R. #2 P.O. Box N13 Sutton West, ON L0E 1R0
Hiawatha First Nation Chief Laurie Carr 123 Paudash Street R.R.#2 Keene, ON K0L 2G0	Mississaugas of Scugog Island Chief Tracy Gauthier 22521 Island Road Port Perry, ON L9L 1B6	Mississaugas of New Credit First Nation Chief M. Bryan LaForme 8545 Townline Road RR 1 Hagersville, ON N0A 1H0
Métis Nation of Ontario Métis Consultation Unit 500 Old St. Patrick Street Unit D Ottawa, ON K1N 9G4	Beausoleil First Nation Chief Roland Monague 1 Ogema Street Christian Island, ON L0K1C0	Chippewas of Rama K.A. Sandy-McKenzie 8 Creswick Court Barrie, ON L4M 2J7
Curve Lake First Nation Chief I. Keith Knott General Delivery Curve lake, ON K0L 1R0	Mississaugas of Scugog Island First Nation Kathy Brant R.R. #5 22521 Island Road Port Perry, ON L9L 1B6	

We look forward to your assistance and cooperation for this project and thank you for your attention to this matter.

Yours sincerely,

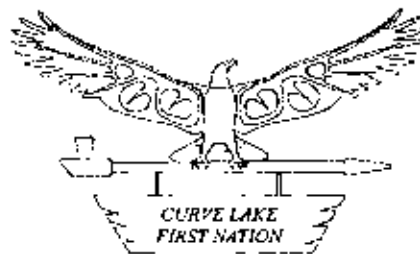


Hitesh Topiwala
Project Manager
Transportation Division, Transportation Program Planning

attachment

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca



CURVE LAKE, ONTARIO N0L 1R0

July 19, 2012

Hitesh Topiwala
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, Ontario L6T 4B9

Dear Hitesh Topiwala,

RE: Notice of Study Commencement for Expanded Study Area Project No. 11-4360

We would like to acknowledge receipt of your correspondence, which we received on 7/13/2012 regarding the above noted project.

As you may be aware, the area in which your project is proposed is situated within the Traditional Territory of Curve Lake First Nation. Our First Nation's Territory is incorporated within the Williams Treaty Territory and is the subject of a claim under Canada's Specific Claims Policy. We strongly suggest that you provide Karry Sandy-Mackenzie, Williams Treaty First Nation Claims Coordinator, 8 Creswick Court, Barrie, ON L4M 2S7, with a copy of your proposal as your obligation to consult to also extend to the other First Nations of the Williams Treaty.

Although we have not conducted exhaustive research nor have we the resources to do so, Curve Lake First Nation Council is not currently aware of any issues that would cause concern with respect to our Traditional, Aboriginal and Treaty rights.

Please note that we have particular concern for the remains of our ancestors. Should excavation unearth bones, remains or other such evidence of a native burial site or any Archaeological findings, we must be notified without delay. In the case of a burial site, Council reminds you of your obligations under the *Cemeteries Act* to notify the nearest First Nation Government or other community of Aboriginal people which is willing to act as a representative and whose members have a close cultural affinity to the interred person. As I am sure you are aware, the regulations further state that the representative is needed before the remains and associated artefacts can be removed. Should such a find occur, we request that you contact our First Nation immediately. Curve Lake First Nation also has available, trained Archaeological Liaisons who are able to actively participate in the archaeological assessment process as a member of a field crew, the cost of which will be borne by the proponent.

If any new, undisclosed or unforeseen issues should arise, that has potential for anticipated negative environmental impacts or anticipated impacts on our Treaty and Aboriginal rights we require that we be notified regarding these as well.

Thank you for recognizing the importance of consultation and respecting your duty to consult obligations as determined by the Supreme Court of Canada.

Should you have further questions or if you wish to hire a liaison for a project, please feel free to contact Melissa Dokis or Krista Coppaway at 705-657-8045x222 or dutytoconsult@curvelakefn.ca.

Yours sincerely,

A handwritten signature in black ink, appearing to read "Phyllis Williams", is written over a circular stamp or seal.

Chief Phyllis Williams
Curve Lake First Nation



Chippewas of RAMA
First Nation

5884 Rama Road, Suite 200
Rama, Ontario L3V 6H6

T 705.325.3611 F 705.325.0879

A Proud Progressive First Nation Community

OFFICE OF THE CHIEF

May 22, 2013

Region of Peel
10 Peel Centre Drive
Suite 8, 4th Floor
Brampton, ON L6T 4B9

Attention: Gino Dela Cruz, Project Manager

Re: Notice of Public Information Centre #1
Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde
Base Line Road

Dear Mr. Dela Cruz:

As a member of the Williams Treaties First Nations, Rama First Nation acknowledges receipt of your letter of April 24, 2013, which was received on April 25, 2013.

A copy of your letter has been forwarded to Karry Sandy-McKenzie, Barrister & Solicitor, Coordinator for Williams Treaties First Nations for further review and response directly to you. Please direct all future correspondence and inquires, with a copy to Rama First Nation, to Ms. Sandy-McKenzie at 8 Creswick Court, Barrie, ON L4M 2J7 or her email address at k.a.sandy-mckenzie@rogers.com. Her telephone number is (705) 792-5087.

We appreciate your taking the time to share this important information with us.

Sincerely,


Chief Sharon Stinson Henry

c: Council, Rama First Nation
Jeff Hewitt, General Counsel
Karry Sandy-McKenzie, Coordinator for Williams Treaties First Nations
Chief Roland Monague, Portfolio Chief for Williams Treaties First Nations



ALDERVILLE FIRST NATION
11696 Second Line
P.O. Box 46
Roseneath, Ontario K0K 2X0
PHONE:(905) 352-2011
Fax: (905) 352-3242

Chief: James R Marsden
Councilor: Pam Crowe
Councilor: Wes Marsden
Councilor: Dave Mowat

May 28th 2013

Gino Dela Cruz
Project Manager, Region of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, ON L6T 4B9

Att: Gino Dela Cruz

**Re: Notice of Public Information Centre #1
Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde
Base line Road**

Dear Gino,

Thank you for your consultation request to Alderville First Nation regarding the **Notice of Public Information Centre #1, Mississauga Road, Old Main Street, Bush Street, Winston Churchill Boulevard and Olde Base line Road** which is being proposed within our Traditional and Treaty Territory. We appreciate the fact that the Ministry of Transportation recognizes the importance of First Nations Consultation and that your office is conforming to the requirements within the Duty to Consult Process.

As per the Alderville First Nation Consultation Protocol, your proposed project is deemed a level

3, having minimal potential to impact our First Nations' rights, therefore, please keep Alderville apprised of any archaeological findings, burial sites or any environmental impacts, should any occur. I can be contacted at the mailing address above or electronically via email, at the email address below.

In good faith and respect,

Dave Simpson

dsimpson@aldervillefirstnation.ca

Lands and Resources

Communications Officer

Tele: (905) 352-2662

Alderville First Nation

Fax: (905) 352-3242



ALDERVILLE FIRST NATION
11696 Second Line
P.O. Box 46
Roseneath, Ontario K0K 2X0
Phone: (905) 352-2011
Fax: (905) 352-3242

Chief: James R. Marsden
Councillor: Julie Bothwell
Councillor: Jody Holmes
Councillor: Dave Mowat
Councillor: Angela Smoke

February 10, 2014

Region of Peel
Public Works
10 Peel Centre Dr., Suite B
Brampton, ON L6T 4B9

Att: Asha Saddi, Technical Analyst

**Re: The Mississauga Road, Old Main Street, Bush Street, Winston Churchill
Boulevard and Olde Base Line Road Project**

Dear Asha Saddi,

Thank you for your consultation request to Alderville First Nation regarding the above noted project which is being proposed within our Traditional and Treaty Territory. We appreciate the fact that the Region of Peel recognizes the importance of First Nations Consultation and that your office is conforming to the requirements within the Duty to Consult Process.

As per the Alderville First Nation Consultation Protocol, your proposed project is deemed a level 3, having minimal potential to impact our First Nations' rights, therefore, please keep Alderville apprised of any changes to your project. I can be contacted at the mailing address above or electronically via email, at the email address below.

In good faith and respect,

Dave Simpson
Lands and Resources
Communications Officer
Alderville First Nation

dsimpson@aldervillefirstnation.ca

Tele: (905) 352-2662
Fax: (905) 352-3242