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Appendix A: Consultation Record

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Appendix A-1: Public Consultation

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A-1-2: Public Information Centre (PIC) #1

- Notice of PIC #1 (Cancelled)
- Notice of PIC #1 (Rescheduled)
- PIC #1 Summary Report (Includes PIC #1 Materials)
- PIC #1 Comment Sheets

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- Notice of PIC #2
- PIC #2 Materials
- PIC #2 Summary Report

A-1

Appendix A-1-1: Notice of Study Commencement

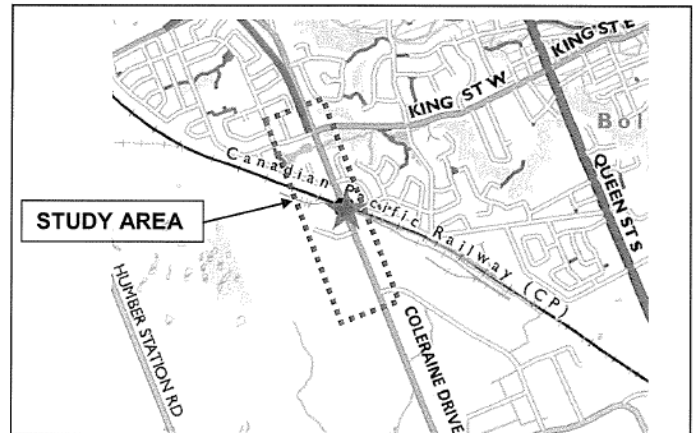
NOTICE OF STUDY COMMENCEMENT

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY FOR COLERAINE DRIVE GRADE SEPARATION

The Study

The Region of Peel has initiated a Municipal Class Environmental Assessment (EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon.

The Region of Peel Long Range Transportation Plan (LRTP) (2012) identifies significant growth expected in Peel Region over the next 20 years and the associated transportation issues including traffic congestion. The Goods Movement Task Force Strategic Plan (2012-2016), which was developed to identify Strategic Actions to improve Goods Movement in Peel, identifies Coleraine Drive as a critical crossing that should be prioritized for a grade separation. The study will assess the impacts and benefits of proposed road improvements in this area on cultural, economic and natural environments as well as safety.



The Process

The study is being conducted in compliance with *Schedule C* of the *Municipal Class Environmental Assessment* process (October 2000, as amended in 2007, 2011 and 2015), which is approved under the *Ontario Environmental Assessment Act*.

The study will define the problem, identify alternative solutions, and determine a preferred solution. Consultation with the public and stakeholders is a key component of the study. Two (2) Public Information Centres will be held to review the study and obtain public input on issues related to alternative solutions and design, environmental impacts and mitigation measures. Details regarding the Public Information Centres will follow with separate notices.

Contact

Please visit our website below for updates on the project:

<http://www.peelregion.ca/pw/transportation/enviro-assess/index-cal.htm>

To provide comments or request additional information about this project, please contact either project manager:

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 ext. 7805
Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Stephen Keen, P.Eng
Project Manager
CIMA Canada Inc. (CIMA+)
3027 Harvester Road, Suite 400
Burlington, ON L7N 3G7
Tel: 289-288-0287 ext. 6834
Fax: 289-288-0285
Email: stephen.keen@cima.ca

The Region of Peel is committed to ensure that all Regional services, programs and facilities are inclusive and accessible for persons with disabilities. Please contact the Project Manager if you need any disability accommodation to participate in the study.

A-1

Appendix A-1-2: Public Information Centre #1

NOTICE OF PUBLIC INFORMATION CENTRE NO.1

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY FOR COLERAINE DRIVE GRADE SEPARATION

The Study

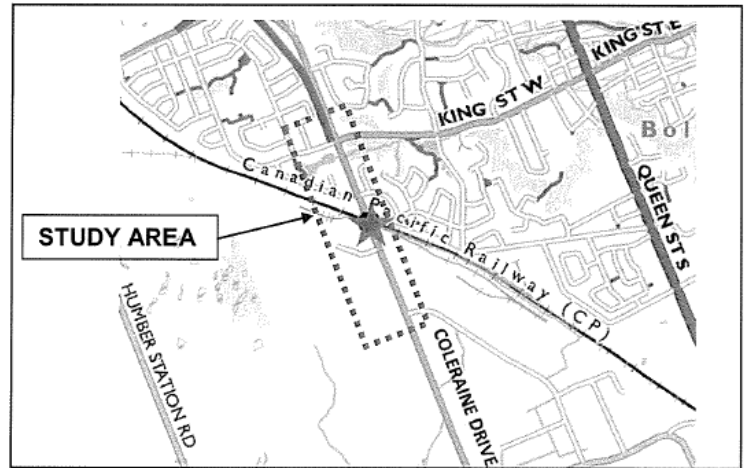
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The study will assess the impacts and benefits of road improvements in this area on cultural, economic and natural environments as well as safety.

The Process

The study is being conducted in compliance with *Schedule C* of the *Municipal Class Environmental Assessment* process (October 2000, as amended in 2007, 2011 and 2015), which is approved under the *Ontario Environmental Assessment Act*. Consultation with the public is a key to the study.

You are invited to attend the first Public Information Centre (PIC), which is being held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity for the public to review project information and ask questions or discuss comments with the project team.



Date: September 26th, 2017

Time: 6:00 p.m. to 8:00 p.m.

Location: Albion Bolton Community Centre
150 Queen St. S.
Bolton, ON L7E 1E3

Contact

For information on the study progress and updates, visit:

www.peelregion.ca/pw/transportation/environ-assess/index-cal.htm

If you have any comments regarding the study and cannot attend the PIC, please contact either project manager:

P. Gino Dela Cruz, P.Eng

Project Manager

Transportation Division, Public Works

Region of Peel

10 Peel Centre Drive, Suite B

Brampton, ON L6T 4B9

Tel: 905-791-7800 ext. 7805 Fax: 905-791-1442

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3027 Harvester Road, Suite 400

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Tel: 289-288-0287 ext. 6834 Fax: 289-288-0285

Email: stephen.keen@cima.ca

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With the exception of personal information, all comments will become part of the public record of the study. The study is being conducted according to the requirements of the Municipal Class Environmental Assessment, which is a planning process approved under Ontario's *Environmental Assessment Act*.

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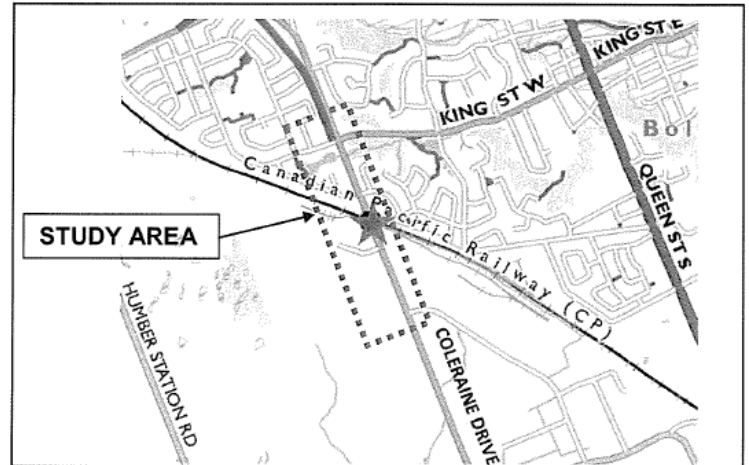
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Date: October 8th, 2019

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150 Queen St. S.
Bolton, ON L7E 1E3

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If you have any comments regarding the study and cannot attend the PIC, please contact either project manager:

Sonya Bubas, MCIP, RPP

Project Manager
Infrastructure Programming & Studies
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 ext. 7801
Email: sonya.bubas@peelregion.ca

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MEMO

TO : Sonya Bubas, RPP
FROM : Stephen Keen, P.Eng
DATE : December 16, 2019
SUBJECT : Region of Peel Coleraine Drive Grade Separation Class EA
Summary of Public Information Centre No. 1

1. INTRODUCTION

CIMA+ was retained by the Region of Peel to conduct a Municipal Class Environmental Assessment Study for the proposed grade separation of the rail tracks on Coleraine Drive, near Old Ellwood Drive. This study follows a Schedule C process as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007, 2011 and 2015).

The Region of Peel held the first Public Information Centre (PIC) for the Class EA study on October 8, 2019. The purpose of this memo is to summarize the results of the PIC.

2. NOTICE OF PUBLIC INFORMATION CENTRE NO. 1

The Notice of Public Information Centre No. 1 was prepared to inform the public and agencies of the opportunity to review the project and provide input. The Notice was advertised in The Caledon Enterprise on September 26th and October 3rd and on Twitter on October 1st, 7th and 8th. The Notice was emailed or mailed to 35 agencies, including 2 First Nations and 4 Elected Officials and 297 property owners within the study limits on September 25, 2019. A covering letter was provided with the Notice to all recipients. A copy of the Notice, letters and the list of agency representatives is included in Appendix A.

The Notice of Public Information Centre No. 1 outlined the purpose of the meeting and identified the time, date, and location for the PIC. The Notice invited public comments on the study by either attending the PIC or contacting the project team.

3. SUMMARY OF PIC NO. 1

The PIC was held on October 8, 2019, from 6:00 PM to 8:00 PM at the Albion Bolton Community Centre (AODA¹ accessible) at 150 Queen Street South in the Town of Caledon. The PIC was held in an open-house format where the public was invited to review display boards, ask questions, and discuss comments with the project team. The display boards described the following:

- Welcome and Introduction
- Purpose of Public Information Centre No. 1
- Study Area Overview
- Region of Peel Context
- Study Process
- Future Traffic Conditions
- Active Transportation Plans
- Existing Conditions Plans
 - Environmental Inventories to be completed
 - Existing Natural Environment Overview
 - Stormwater Management
 - Archaeological Assessment
 - Built Heritage
- Problem and Opportunity Statement
- Alternative Planning Solutions
 - Do Nothing
 - Change Grade of Rail (2A or 2B)
 - Change Grade of Road (2A or 2B)
- Analysis of Alternative Planning Solutions
- Preliminary Preferred Alternative Solution
- Next Steps

A sign-in sheet and comment sheets were provided to record attendance and obtain written comments. A copy of the PIC material is included in Appendix B. Seventeen (17) people signed into the PIC and three (3) comments were received through comment sheets and email correspondence.

¹ Accessibility for Ontarians with Disabilities Act, 2005

4. SUMMARY OF PUBLIC COMMENTS

The following general comments were submitted and noted by the project team during the PIC:

Traffic Operations and Traffic Safety

- Preference for the alternative 2B - depress road under rail
- Current lack of fencing for pedestrian safety

Social Impacts

- Noise concerns related to alternative 2A – raise rail over road
- Lack of noise attenuation barriers
- Lengthy construction time

The project team also noted the following general concerns:

- Consider opportunities to maintain access to residential properties
- Existing skew for southbound and northbound left turn lanes at Coleraine Drive & King Street West
- Unsafe crossing road even at crosswalk at King St; Consensus among many the sightlines are deficient; One resident recommended conversion to a roundabout
- Issues with vibrations caused by trains and foundation damage to existing homes resulting from trucks and train
- Consider a pedestrian underpass from Ellwood Drive to Grapevine Road
 - Access to Ellwood Drive West from Coleraine Drive is higher priority over Grapevine Road
- Old Ellwood connection to Ellwood Drive West would go through a park
- Ellwood Drive West floods under heavy rain events and some residents are experiencing basement flooding
- Residents believe there is only overland flow Stormwater controls and believe it is insufficient
- Update aerial map to include MARS Plant development
- Resident asked if Peel would be liable if there is any damage to their properties caused by construction
- Residents frustrated with industrial development going on around them and believe it is going to increase traffic, especially truck traffic and truck traffic is already negatively impacting them
- Residents frustrated that houses were built “too close to tracks”, mentioned that the CPs required set back was not followed
- Resident questioned signage approaching tracks from North
- Heavy traffic/truck noise all night long
- Unable to enjoy outdoors due to noise
- Resident siding Coleraine indicated preference for “a 17’ wall” to buffer and protect but worried about sunlight
- Majority preferred underpass but understand need to review all options

5. CONCLUSION

The PIC was an opportunity to hear comments from the public on the existing conditions, problems identified within the study area and alternative planning solutions. All comments received will be reviewed by the project team and considered in the selection of the preferred solution. Responses will be provided for all comments received. A second Public Information Centre will be held to present the technically preferred alternative design and is tentatively scheduled for late winter/early spring 2020.

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Appendix A



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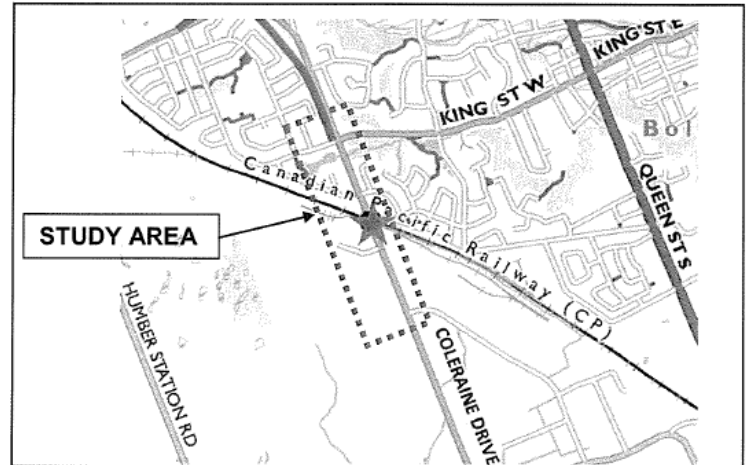
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Sonya Bubas, MCIP, RPP

Project Manager
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Date

Name

Title

Agency

Address

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Sonya Bubas, MCIP, RPP
Project Manager
Infrastructure Programming & Studies
Tel: 905-791-7800 ext. 7801
Email: sonya.bubas@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Date

Name

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

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We look forward to seeing you at the PIC.

Sincerely,

Sonya Bubas, MCIP, RPP
Project Manager
Infrastructure Programming & Studies
Tel: 905-791-7800 ext. 7801
Email: sonya.bubas@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Stakeholder Group	Organization Name	Prefix	First	Last	Title	Address	City	Province	Postal Code	Phone Number	Email
Emergency Service Providers	Peel Regional Police	Ms.	Jennifer	Evans	Chief	7750 Hurontario Street	Brampton	ON	L6V 3W6	905-453-3311	
Emergency Service Providers	Region of Peel Ambulance Service	Mr.	Peter	Dundas	Director-Paramedic Services	5299 Malingate Dr	Mississauga	ON	L4W 1G6	905-791-7800, ext. 3921	peter.dundas@peelregion.ca
First Nations	Mississaugas of the New Credit First Nation	Ms.	Fawn	Sault	Consultation Manager	2789 First Line Rd.	Hagersville	ON	N0A 1H0	905-768-4260	Fawn.Sault@newcreditfirstnation.com
First Nations	Huron Wendat	Chief	Konrad	Sioui	Grand Chief Huron-Wendat Nation Council	255, place Chef-Michel-Laveau	Wendake	QC	G0A 4V0	418 843-3767	administration@cnhw.qc.ca
Municipality	Town of Caledon	Mr.	Kant	Chawla	Senior Transportation Planner, Development Approval and Planning Policy Department	6312 Old Church Road	Caledon East	ON	L7C 1J6	905-254-2272	kant.chawla@caledon.ca
Elected Officials	Town of Caledon	Mr.	Nick	deBoer	Ward 3 and 4 Area Councillor	6311 Old Church Road	Caledon	ON	L7C 1J6	905-880-1370	nick.deboer@caledon.ca
Elected Officials	Town of Caledon	Mr.	Tony	Rosa	Ward 5 Area Councillor	6311 Old Church Road	Caledon	ON	L7C 1J6	905.584.2272 ext. 5130	tony.rosa@caledon.ca
Elected Officials	Town of Caledon	Ms.	Jennifer	Innis	Ward 3 & 4 Regional Councillor	6311 Old Church Road	Caledon	ON	L7C 1J6		jennifer.innis@caledon.ca
Elected Officials	Town of Caledon	Ms.	Annette	Groves	Ward 5 Regional Councillor	6311 Old Church Road	Caledon	ON	L7C 1J6		annette.groves@caledon.ca
Provincial	Ministry of Environment, Conservation and Parks	Mr.	Trevor	Bell	Environmental Resource Planner/EA Coordinator	5775 Yonge St., 8th Floor	Toronto	ON	M2M 4J1	416-326-3577	trevor.bell@ontario.ca
Provincial	Ministry of Environment, Conservation and Parks	Ms.	Aurora	McAllister	Management Biologist	50 Bloomington Rd	Aurora	ON	L4G 0L8		aurora.mcallister@ontario.ca
Provincial	Ministry of Natural Resources and Forestry	Mr.	Steven	Strong	Senior District Planner	50 Bloomington Rd	Aurora	ON	L4G 0L8	905-713-7366	steven.strong@ontario.ca
Provincial	Ministry of Natural Resources and Forestry	Mr.	Mark	Heaton	Area Biologist, Halton/Peel/Toronto	50 Bloomington Rd	Aurora	ON	L4G 0L8	905-713-7406	mark.heaton@ontario.ca
Provincial	Ministry of Tourism, Culture and Sport	Mr.	Dan	Minkin	Heritage Planner - Culture Services Unot	401 Bay Street, Suite 1700	Toronto	ON	M7A 0A7	416-314-7147	dan.minkin@ontario.ca
Provincial	Ministry of Tourism, Culture and Sport	Ms.	Rosi	Zirger	Heritage Planner - Culture Services Unot	401 Bay Street, Suite 1700	Toronto	ON	M7A 0A7	416-314-7159	rosi.zirger@ontario.ca
Provincial	Toronto and Region Conservation Authority	Ms.	Sonia	Dhir	Project Manager	5 Shoreham Drive	Toronto	ON	M3N 1S4	416-661-6600	Sdhir@trca.on.ca
Provincial	Fisheries and Oceans Canada	Mr.	Dan	Thompson	Team Leader-Triage and Planning	867 Lakeshore Road, P.O. Box 5050	Burlington	ON	L7R 4A6	(519) 668-3897	dan.thompson@dfo-mpo.gc.ca
Rail Line	Canadian Pacific Rail	Mr.	Brian	Costigan	Engineer in Training – Public Works	1290 Central Parkway West, Suite 600	Mississauga	ON	L5C 4R3	905-803-3305	Brian_costigan@cpr.ca
School	Peel District School Board		Branko	Vidovic	Planning & Accommodation Support Services	5650 Hurontario Street	Mississauga	ON	L5R 1C6	905-890-1010 ext. 2724	branko.vidovic@peelsb.com
Utilities	TransCanada Pipelines Limited	Ms.	Darlene	Quilty	MHBC Planning, Urban Design & Landscape Architecture	442 Brant Street, Suite 204	Burlington	ON	L7R 2G4	905 639 8686 x 229	dquilty@mhbcplan.com
Utilities	Enbridge Gas Distribution		Meetalpal	Chhina	Planning Technical Specialist	6 Colony Court	Brampton	ON	L6T 4E4	905-458-2159	Meetalpal.Chhina@enbridge.com
Utilities	Enbridge Gas Distribution	Ms.	Diana	Beaulne	Mark-Up Administrator	500 Consumers Road, 4th Floor	North York	ON	M2J 1P8	416-495-5520	markups@enbridge.com
Utilities	Bel Canada Municipal Operations Centre	Ms.	Diana	Velez	c/o Netricom	200 Town Centre Boulevard, Suite 300	Markham	ON	L3R 8G5	905-470-2112 x 40309	bell.moc@netricom.com
Utilities	Hydro One Networks Inc.				Planning and Scheduling Department	40 Olympic Drive	Dundas	ON	L9H 7P5	905-627-6050	westcentralzonescheduling@hydroone.com
Utilities	Hydro One Networks Inc.	Mr.	Ian	Mitchell	EA Coordinator	65 Kelfield Street	Rexdale	ON	M9W 5A3	416-240-6701	ian.mitchell@hydroone.com
Utilities	Rogers Cable	Mr.	Richard	Humpage	EA Coordinator	244 Newkirk Road	Richmond Hill	ON	L4C 3S5	905-780-7014	richard.humpage@rci.rogers.com
Municipality	Caledon Cycling Task Force		Danita	Taccogna	Supervisor, FIS Administration, Finance and Infrastructure Services						danita.taccogna@caledon.ca
Municipality	Caledon Cycling Task Force		Steve	Mathew	Traffic Technologist, Transportation, Finance and Infrastructure Services						Steve.Mathew@caledon.ca

CALEDON'S 5 OLDEST RESTAURANTS

IT'S A TOUGH BUSINESS, BUT SOME, HAVE MADE IT LAST IN CALEDON

MATTHEW STRADER
mstrader@caledon
enterprise.com

Restaurant ownership is said to be one of the most difficult businesses. From staffing issues, to difficulties maintaining profits while contending with changing food prices, to of course, providing a menu that will keep customers coming back.

But some in Caledon, have clearly made it work, as when the oldest restaurants in Caledon are looked into, some have been in the town for more than 60 years.

Here is a look at the oldest restaurants in Caledon, beginning with number five, and ending with num-

ber one, a restaurant café that has been in Caledon for approximately 73 years.

European BBQ Chicken

A traditional Portuguese rotisserie chicken outlet that also features grilled chicken and Italian fare. This restaurant has been in Bolton since its opening in 1976.

Wishbone Restaurant Steakhouse and Tavern

Established in 1969, the family-owned and operated (to this day) establishment is a legend in Caledon, known for its home cooked flavours and menu and friendly atmosphere.

Ray's 3rd Generation Bistro Bakery

An Alton favourite that now is known not just for quality food, but a high-end menu featuring locally sourced products and the ingenious creativity of its

chef, Jason Perkins. Rays was first established approximately 53 years ago, in 1966.

The Terra Cotta Inn Restaurant

Views of the Niagara Escarpment and flavours from the mind of chef Roberto Florindi. Once you're seated at this restaurant on the banks of the Credit River, you'll understand why it's been maintained for approximately 69 years, having first opened in 1950.

The Brampton Flight Centre Restaurant and Café

The oldest restaurant in Caledon is also situated kitty corner to Caledon's Great War Flying Museum. And who would have known the café can battle some of the planes for its lasting heritage? This favourite Caledon spot has been around for approximately 73 years, having first opened in 1946.



Brampton Flight Centre Restaurant-Cafe chef and manager Jim Malliaros.

Matthew Strader Torstar

Public Notice



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Email: stephen.keen@cima.ca

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The Region of Peel is committed to ensure that all Regional services, programs and facilities are inclusive and accessible for persons with disabilities. Please contact the Project Manager if you need any disability accommodation to participate in the study.

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Robyn Wilkinson/Metroland

The Peel District School Board flip-flopped on its decision about not locking front doors at elementary schools. The doors will now remain locked during work-to-rule job action that began Sept. 30.

FIRST OPEN, NOW CLOSED

PEEL BOARD FLIP FLOPS ON DECISION TO UNLOCK ELEMENTARY SCHOOL DOORS DURING JOB ACTION

KAREN MARTIN-ROBBINS
kmartinrobbins@metroland.com

Caledon's public elementary schools will be locked during the school day during job action for school support workers despite a controversial announcement to the contrary last week.

The Peel District School Board initially alerted parents in a letter on Thursday Sept. 19 that as part of Canadian Union of Public Employees' work-to-rule office staff will not answer door buzzers.

Front doors into elementary schools would remain open during the school day, the letter said.

On Sunday, the board reversed that decision stating doors will remain locked.

"Upon review, we have reversed this decision," the update stated.

Elementary schools in Ontario started locking doors in 2013 in reaction to the shooting rampage at

THE ISSUE:

SCHOOL SAFETY DURING JOB ACTION

LOCAL IMPACT:

PEEL BOARD WILL KEEP DOORS LOCKED

Sandy Hook Elementary.

On Dec. 14, 2012, Adam Lanza, 20, went into the Newton, Conn., school shooting and killing 26 people, 20 of them children.

Ontario's Premier Dalton McGuinty implemented the "locked door" policy at its 4,000 elementary schools, earmarking \$10 million for front door locks, entry buzzer systems and security cameras.

Secondary schools in the province are not locked.

Elementary schools in the Catholic school system in Caledon will also remain locked during the job action.

Bruce Campbell, communications manager with the Dufferin Peel Catholic District School Board, said the door buzzer and intercom entry system at elementary schools

will still be in effect.

Both schools board warned parents there may be delays in staff responding to the buzzers.

"All elementary schools will be posting signage to that effect starting Monday," Campbell said.

Along with not answering door buzzers, office staff will also not administer medication to students; custodians will clean classrooms and washrooms but not hallways, office areas or gyms; they won't do any outdoor work.

The Catholic board also issued a letter to parents about the job action last week.

It said that while the job action may not be readily apparent to parents and students, it will impact grass cutting on school properties, cleaning of premises, and cancellation of external permits.

"Despite the potential impact of the withdrawal of services, we know that our schools will remain welcoming, Christ centred environments," the letter said.

STORY BEHIND THE STORY

We have been following closely how job action will impact Caledon schools.

Dr. Andrew Howarth & Dr. Alex Dolan would like to welcome you to their family dental practice at 30 Martha St., Suite 312, Bolton.

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Serving the community for over 20 years offering a full range of dental care for the whole family.

Call 905 857-0475 or email dentalca@yahoo.ca to set up an appointment at your convenience.



Public Notice

Region of Peel
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NOTICE OF PUBLIC INFORMATION CENTRE NO.1

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY FOR COLERAINE DRIVE GRADE SEPARATION

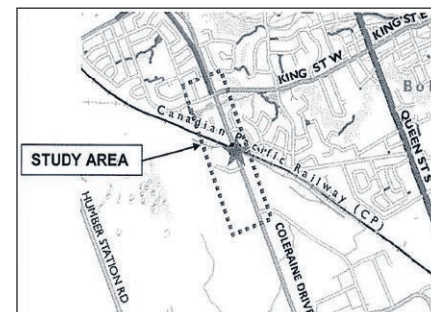
The Study

The Region of Peel has initiated a Municipal Class Environmental Assessment (EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. A grade separation is the realignment of either the roadway or the rail line with one running over or under the other to improve traffic flow.

The Process

The study is being conducted in compliance with Schedule C of the *Municipal Class Environmental Assessment* process (October 2000, as amended in 2007, 2011 and 2015), which is approved under the *Ontario Environmental Assessment Act*. Consultation with the public is a key to the study.

You are invited to attend the first Public Information Centre (PIC), which is being held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity for the public to review project information and ask questions or discuss comments with the project team.



Date: October 8th, 2019

Time: 6:00 p.m. to 8:00 p.m.

Location Albion Bolton Community Centre
150 Queen St. S.
Bolton, ON L7E 1E3

Contact

For information on the study progress and updates, visit:
www.peelregion.ca/pw/transportation/envirom-assess/index-cal.htm

If you have any comments regarding the study and cannot attend the PIC, please contact either project manager:

Sonya Bubas, MCIP, RPP
Project Manager
Infrastructure Programming & Studies
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 ext. 7801
Email: sonya.bubas@peelregion.ca

Stephen Keen, P.Eng
Project Manager
CIMA Canada Inc. (CIMA+)
3027 Harvester Road, Suite 400
Burlington, ON L7N 3G7
Tel: 289-288-0287 ext. 6834
Email: stephen.keen@cima.ca

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Peel Public Works

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On Oct 8, you are invited to our 1st Public Information Centre for the grade separation project in Caledon. This meeting is to review the study & ask questions on the project. It'll run from 6-8pm at the Albion Bolton Community Centre. visit:bddy.me/2p2KSbr

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Tomorrow, join us at the Albion Bolton Community Centre, from 6-8pm for our 1st Public Information Centre for the grade separation project in Caledon. This meeting is to review the study and ask questions with the project team. Visit: bddy.me/2nIXDgC

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Tonight, from 6-8pm at the Albion Bolton Community Centre, you are invited to our 1st Public Information Centre for the grade separation project in Caledon. This meeting is to review the study and ask questions about the project. Visit: bddy.me/2nrqWyq

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B

Appendix B



Public Information Centre No. 1

Municipal Class Environmental Assessment

Coleraine Drive south of Old Ellwood Drive

Town of Caledon



October 8, 2019

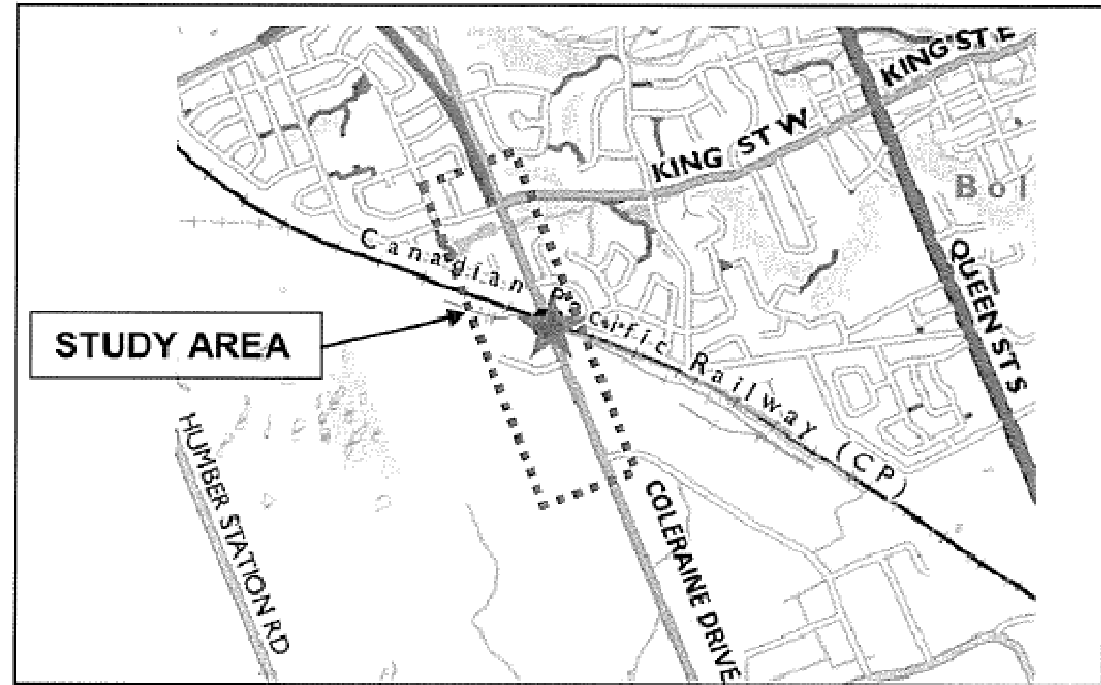
Albion Bolton Community Centre

PURPOSE OF PIC NO. 1

- You are invited to review the project information on display:
 - Background to the study
 - Municipal Class Environmental Assessment Process
 - Existing conditions of the study area
 - Traffic study
 - Alternative planning solutions and preliminary preferred solution
- Ask questions and discuss areas of interest with the study team
- Your feedback is important and will be considered and incorporated in the preferred alternative selection process
- Fill out and submit a comment sheet by **October 22, 2019**

STUDY AREA

Peel Region is carrying out a Municipal Class Environmental Assessment (MCEA) Study to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon.



The study area extends approximately 150 metres north of Harvest Moon Drive to Holland Drive.

REGION OF PEEL CONTEXT

This Environmental Assessment supports the Region's 2015-2035 Strategic Plan and Council Priorities.

The Region of Peel Long Range Transportation Plan (LRTP) (2019)

- The Region of Peel LRTP identifies significant growth expected in Peel Region over the next 20 years and the associated transportation issues including traffic congestion.

Bolton Transportation Master Plan Study (2015)

- Identifies transportation deficiencies and road network issues.
- Identifies Coleraine Drive as the most likely alternative route for north/south and east/west trucks following the restricted truck movements in the downtown core.

The Goods Movement Task Force Strategic Plan (2012-2016)

- Developed to identify Strategic Actions to improve Goods Movement in Peel.
- Identifies Coleraine Drive as a critical crossing that should be prioritized for a grade separation.

The Bolton Commuter Rail Service Feasibility Study (2010)

- Recommends improving service on the Bolton rail line including a new Bolton GO Station near King Street and Humber Station Road in Caledon.

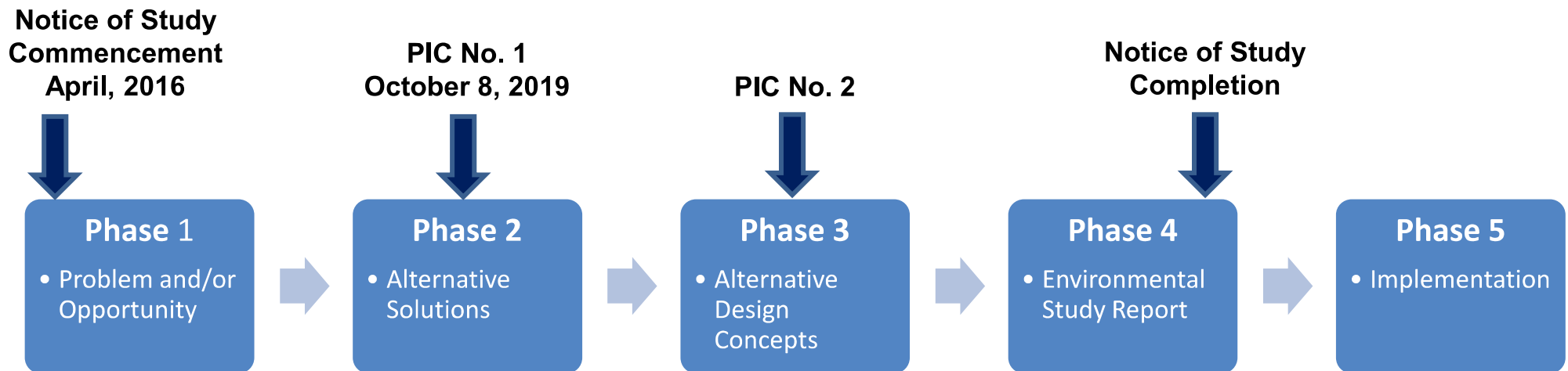


STUDY PROCESS

The Municipal Class EA is a planning and design process approved by the Ministry of Environment, Conservation and Parks to meet the requirements of the Ontario Environmental Assessment Act.

This Study follows the Class EA process for **Schedule 'C'** projects and will complete Phases 1 to 4 as outlined below:

We are here



FUTURE TRAFFIC CONDITIONS

- With an increase in motor vehicle and train traffic projected for 2041, queues generated at the at-grade crossing are expected to extend beyond the intersection of Coleraine Drive & King Street West/Harvest Moon Drive.
- This issue is expected to be more intense as the frequency of trains during peak hours increases, particularly due to the planned extension of GO Train service to a new Bolton station.



2041 AM Southbound Queues During Train Crossing

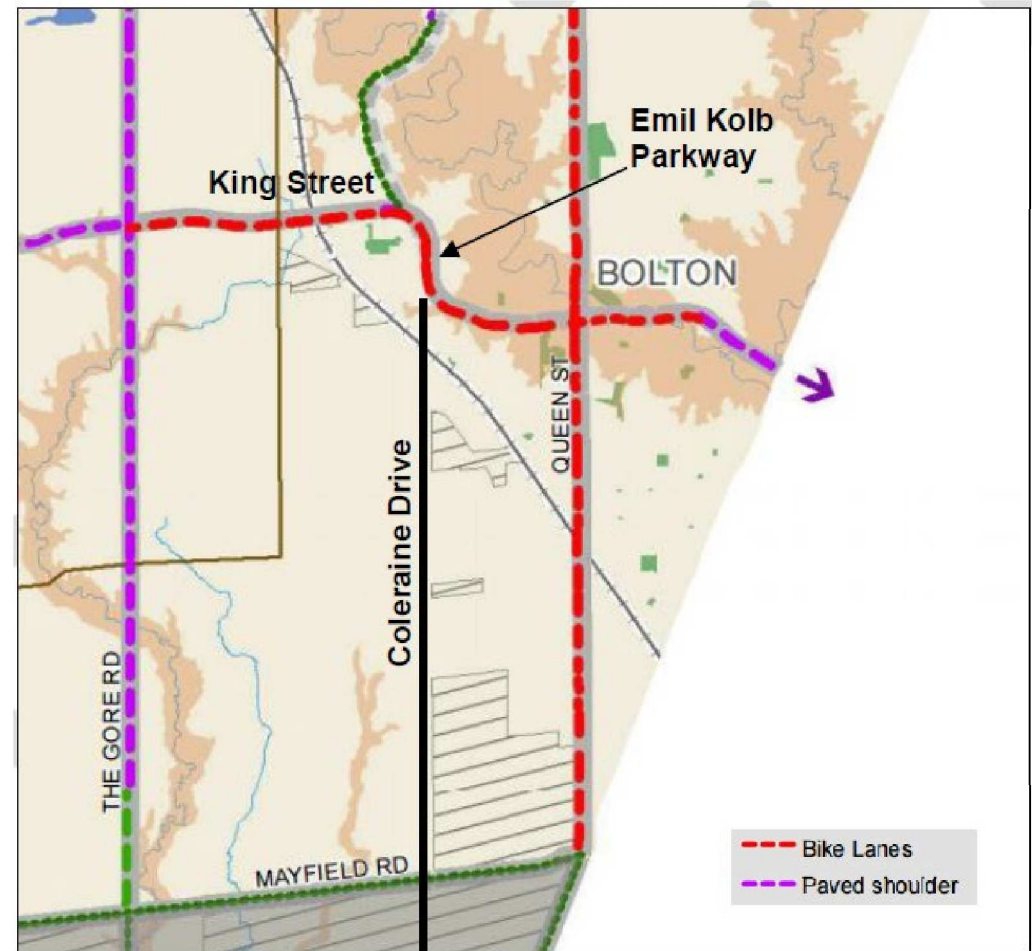
FUTURE TRAFFIC CONDITIONS

Road Section	Direction	2017 Volumes (vph)		2041 Volumes (vph)	
		AM	PM	AM	PM
Coleraine Drive south of King Street W	NB	249	1,070	349	1,500
	SB	933	336	1,824	657
King Street W between Coleraine Drive and Station Road	EB	294	433	486	716
	WB	422	338	982	787

ACTIVE TRANSPORTATION



Left: Bolton Transportation Master Plan identifies a proposed buffered bicycle lane along King Street and Coleraine Drive.



Right: The proposed long-term Regional cycling network indicates that bicycle lanes are planned along King Street. This includes Emil Kolb Parkway, which is a direct continuation of Coleraine Drive north of King Street West.

EXISTING CONDITIONS PLAN

SEE ROLL PLAN

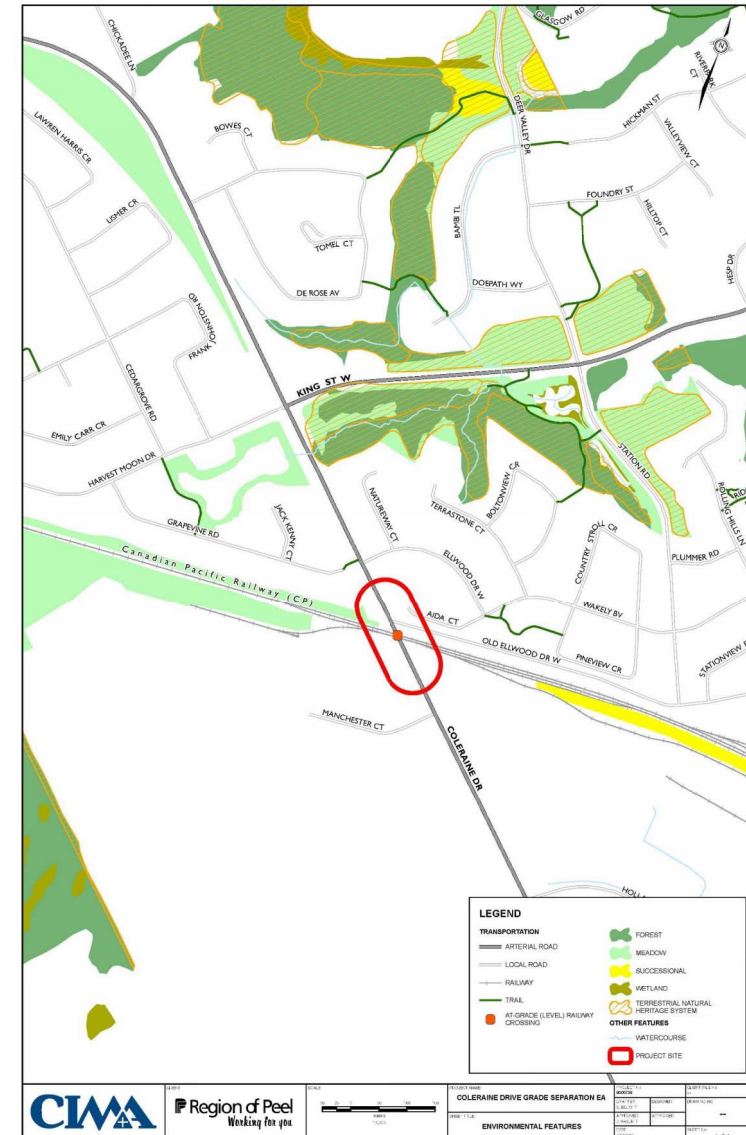
ENVIRONMENTAL INVENTORIES

The following environmental inventories are being completed as part of this study:

- Natural Environment Assessment
- Drainage and Stormwater Management
- Stage 1 Archaeological Assessment
- Cultural and Built Heritage Assessment
- Hydrogeological Investigation
- Geotechnical Investigation and Pavement Design
- Noise Impact
- Air Quality Assessment
- Contaminated Soils Assessment
- Culvert Inspections
- Meander Belt and Fluvial Geomorphology Analysis

NATURAL ENVIRONMENT

- The project is located within the Humber River watershed.
- A small tributary of the Humber River originates at a stormwater pond on the west side of Coleraine Drive, north of the rail line, and flows under the road towards the east.
- TRCA identifies the valley along the watercourse as part of the Natural Heritage System, comprised of meadow and forest.
- CIMA field investigations did not find any rare species of flora or fauna; most of the species encountered were urban tolerant, and/or non-native species.
- Fish habitat is limited to the tributary east of Coleraine Drive, as the outlet of the stormwater pond is a barrier to fish.
- There are no natural features of provincial significance in the study area.
- There are no Core Areas of the Region of Peel Greenlands System in the study area, but the valley of the watercourse is considered an Environmental Policy Area in the Town of Caledon Official Plan.



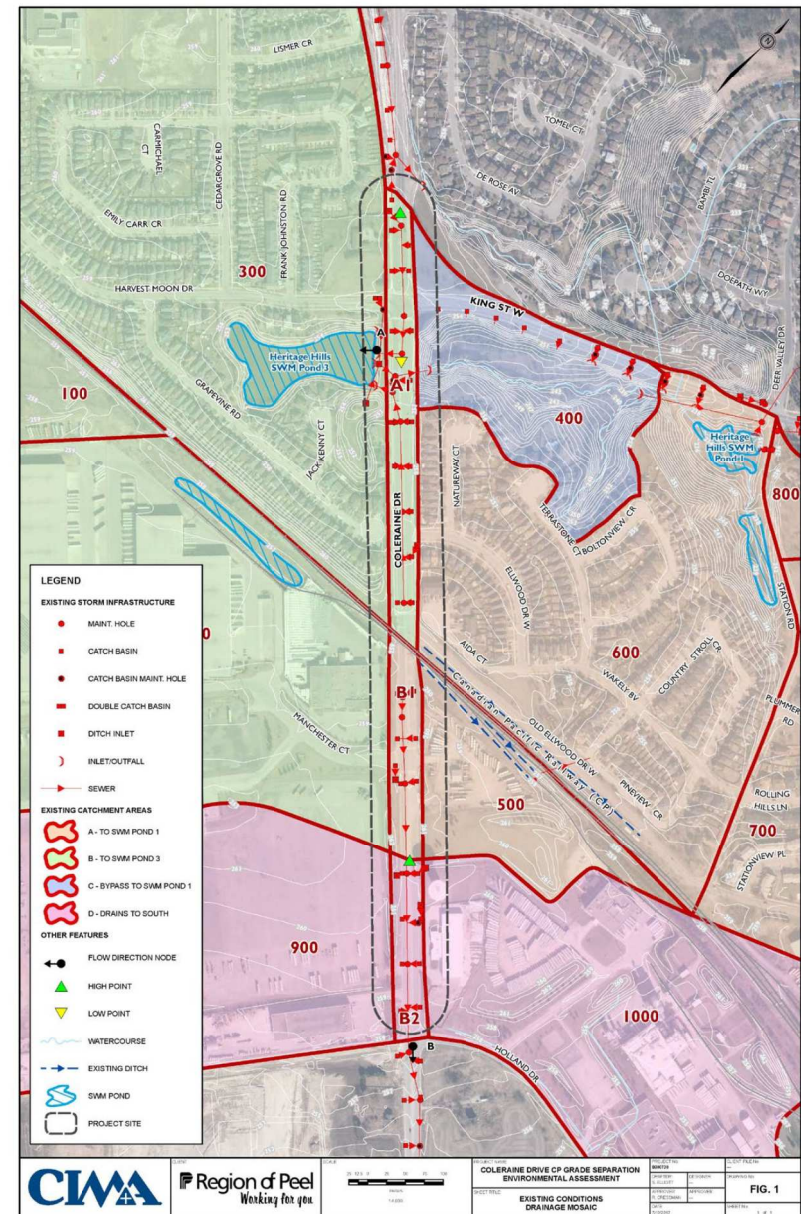
STORMWATER MANAGEMENT

Existing Drainage

- Stormwater drainage from Coleraine Drive drains to the Humber River watershed.
- The soil within the study area consist primarily of clay loam.
- This type of soil is considered to have a moderate to poor drainage capability, meaning very little rainfall in the open areas will soak into the ground, leaving most of the water as runoff.
- Solutions that incorporate Low Impact Development (LID) practices will be investigated for use along Coleraine Drive.

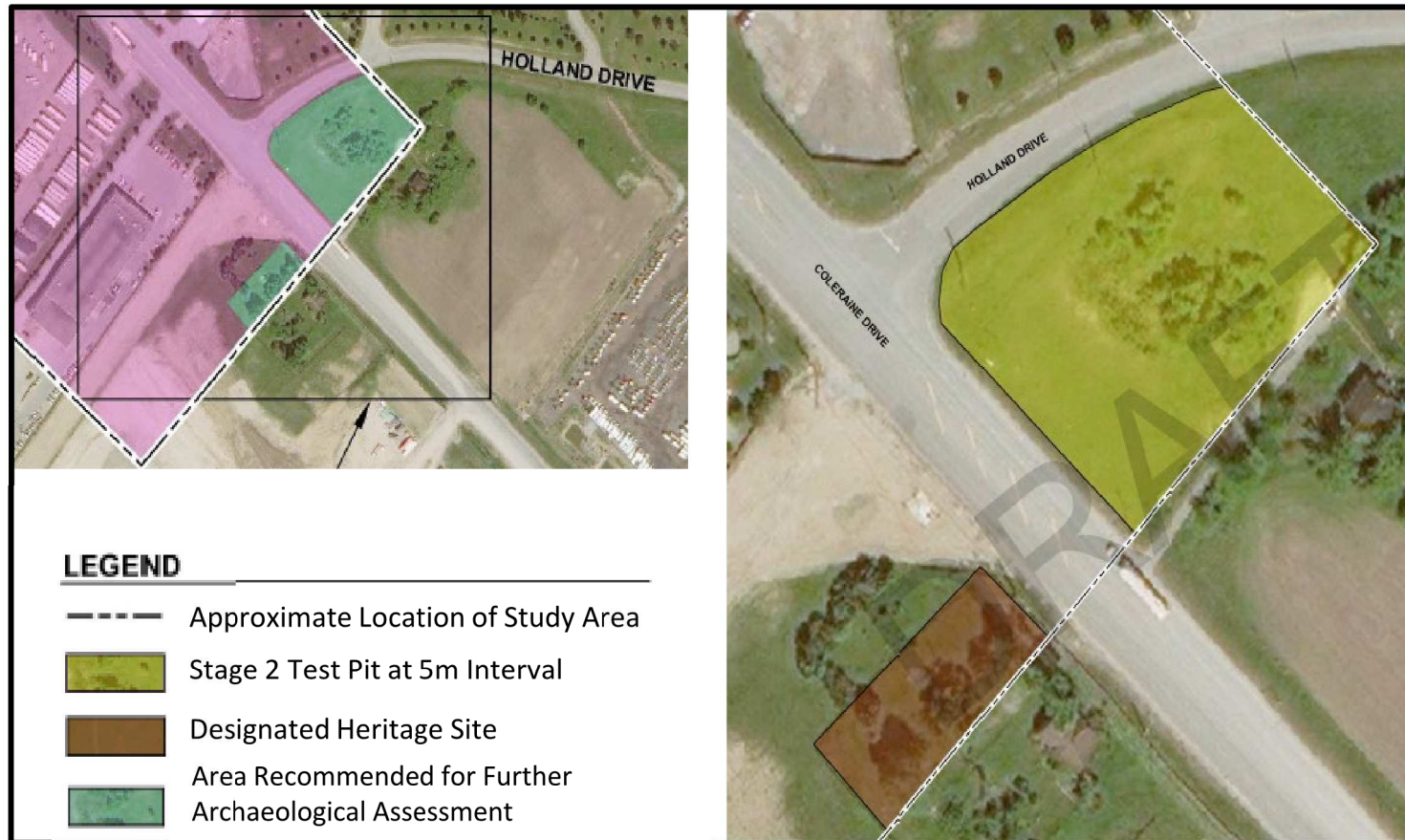
Existing Stormwater Infrastructure

- Coleraine Drive is an urban cross-section with curb and gutter, catch basins, and storm sewers which discharge into the Heritage Hills Stormwater Management Pond to the north and into roadside ditches to the south.
- Adjacent to the roadway there are two existing storm water management ponds constructed as part of the Heritage Hills Residential development that incorporate drainage from Coleraine Drive.



STAGE 1 ARCHAEOLOGICAL ASSESSMENT

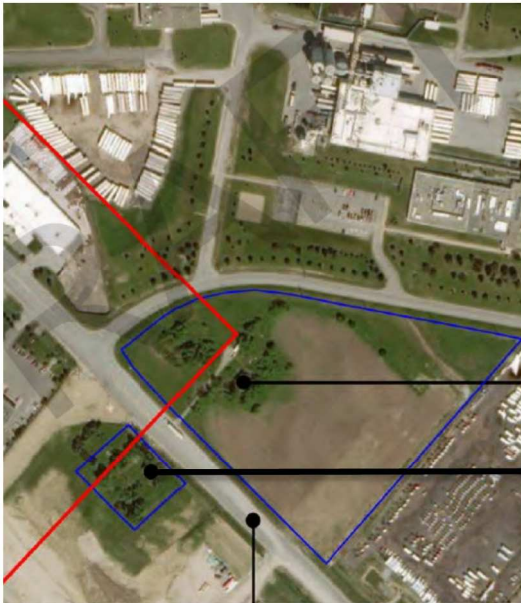
A Stage 1 Archaeological Assessment was completed for the study corridor, some locations are recommended for a Stage 2 Archaeological Assessment.



BUILT HERITAGE

The Cultural Heritage Assessment determined that the following are present in the study area outside of the anticipated limits of construction:

- One protected heritage property
 - Shore-Wakely House, 13304 Coleraine Drive
- One property of cultural heritage value or interest
 - 49 Wakely Boulevard



View facing south of 13304 Coleraine Drive



Study Area, North Portion
Property of Cultural Heritage Value or Interest



View facing northeast of 49 Wakely Boulevard



View facing east

PROBLEM AND OPPORTUNITY STATEMENT

It is a Council priority to improve the efficiency in which goods move within and through Peel in order to manage congestion and enhance Peel's economic competitiveness. The Goods Movements Strategic Plan (2012-2016) was developed and identified Coleraine Drive as a critical crossing that should be prioritized for grade separation.

An increase in motor vehicle and train traffic is projected for 2041 which is expected to result in queues generated at the at-grade crossing extending beyond the intersection of Coleraine Drive & King Street West/Harvest Moon Drive.

This issue is expected to worsen as the frequency of trains during peak hours increases, particularly due to the planned extension of GO Train service to a new Bolton station.

ALTERNATIVE PLANNING SOLUTIONS

Three alternative planning solutions were developed to address the problem statement:

- 1. Do Nothing**
- 2. Change Grade of Rail**
 - 2A) Raise rail over road, or
 - 2B) Depress rail under road
- 3. Change Grade of Road**
 - 3A) Raise road over rail, or
 - 3B) Depress road under rail

(2A) Grade Separation: Raise Rail-over-Road Example



Woodbridge Ave, Vaughan ON



Regional Road 25, Milton ON



Highway 50, Palgrave ON



Parliament St, Toronto ON

(2B) Grade Separation: Depress Rail-under-Road Example



Camp Rd, Australia



A1/N1 Dublin to Belfast motorway



A1/N1 Dublin to Belfast motorway

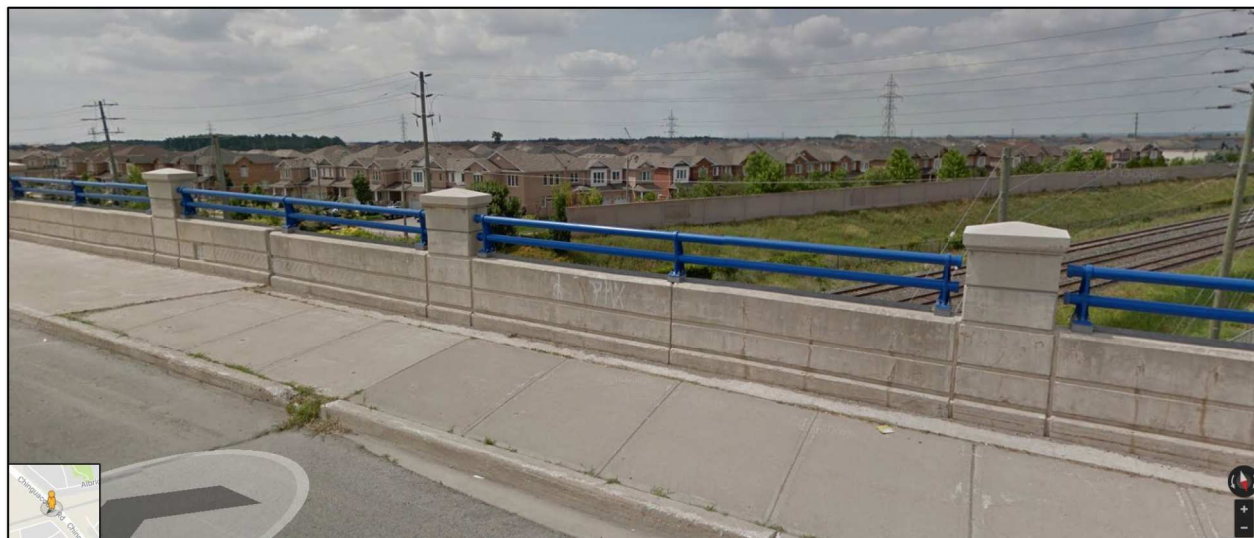
(3A) Grade Separation: Raise Road-over-Rail Example



Norwood Junction, Croydon, United Kingdom

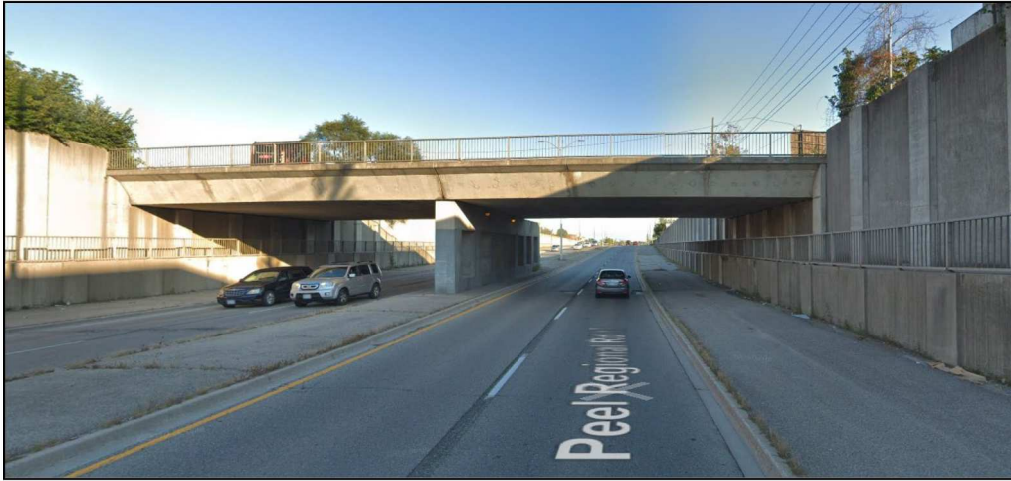


Highway 50, Caledon ON



Chinguacousy Rd, Brampton ON

(3B) Grade Separation: Depress Road-under-Rail Example



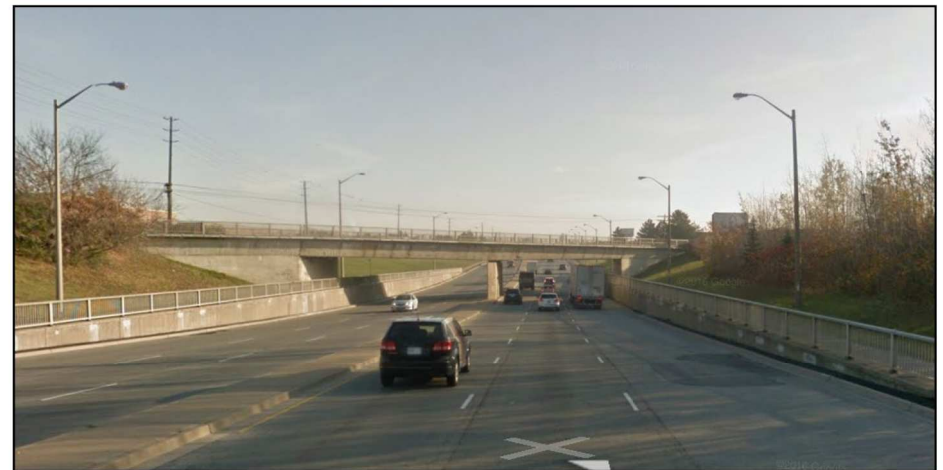
Cawthra Rd, Mississauga ON



Derry Rd, Mississauga ON



Creditview Rd, Brampton ON



Guelph Line, Burlington ON

ANALYSIS OF ALTERNATIVE PLANNING SOLUTIONS

Criteria	Do Nothing	Change Grade of Rail	Change Grade of Road
Transportation	<ul style="list-style-type: none"> No changes to existing transportation system Does not accommodate future transportation needs 	<ul style="list-style-type: none"> Accommodates future transportation needs 	<ul style="list-style-type: none"> Accommodates future transportation needs
Socio-Economic Environment	<ul style="list-style-type: none"> No impact to adjacent properties 	<ul style="list-style-type: none"> Long distance required for grade change (1% max grade) Significant impact to properties adjacent to rail Temporary rail diversion needed 	<ul style="list-style-type: none"> Moderate distance required for grade change (6% max grade) Moderate impact to properties adjacent to Coleraine Drive
Natural Environment	<ul style="list-style-type: none"> No impact to natural environment 	<ul style="list-style-type: none"> Greater impact to meadow lands adjacent to rail Some impact to natural heritage feature 	<ul style="list-style-type: none"> Less impact to meadow lands adjacent to rail line Minimal impact to natural heritage feature
Active Transportation	<ul style="list-style-type: none"> No opportunities for pedestrian or cycling facilities 	<ul style="list-style-type: none"> Opportunity for the provision of pedestrian or cycling facilities on Coleraine Drive 	<ul style="list-style-type: none"> Opportunity for the provision of pedestrian or cycling facilities on Coleraine Drive

PRELIMINARY PREFERRED SOLUTION

The preliminary preferred solution is to change the grade of the road on Coleraine Drive which will address the problem statement developed for the Coleraine Drive study area and is in support of the Region's Long Range Transportation Plan and Goods Movement Strategic Plan endorsed by Region of Peel Council.

This preliminary recommended solution will consider the following:

- Change grade of the road
 - Depress road-under-rail grade separation; OR
 - Raise road-over-rail grade separation
- Provision of pedestrian and bicycle facilities along Coleraine Drive

NEXT STEPS

- Review and confirm preferred planning solutions and assessment based on comments received from the public and other stakeholders
- Develop alternative design concepts
- Complete detailed impact analysis
- Public Information Centre No.2
- Prepare and File Environmental Study Report

Please complete a comment sheet and submit all comments **by October 22, 2019:**

Sonya Bubas, MCIP, RPP

Project Manager
Infrastructure Programming & Studies
Region of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, On L6T 4B9
905-791-7800 ext.7801
Sonya.Bubas@peelregion.ca

Stephen Keen, P. Eng.

Consultant Project Manager
CIMA Canada Inc.
3027 Harvester Road
Burlington ON L7N 3G7
289-288-0287
Stephen.Keen@cima.ca

**CLASS ENVIRONMENTAL ASSESSMENT STUDY
COLERAINE DRIVE GRADE SEPARATION (SOUTH OF OLD ELLWOOD DRIVE)
TOWN OF CALEDON**

**Public Information Centre #1 - Tuesday, October 8, 2019
Albion Bolton Community Centre
COMMENT SHEET**

COMMENTS

Place your completed comment sheet in the box provided or return by October 22, 2019 to:

Stephen Keen, P. Eng.
Project Manager
CIMA Canada Inc. (CIMA+)
3027 Harvester Road, Suite 400
Burlington, Ontario L7N 3G7
Phone: 289-288-0287 ext. 6834 Fax: 289-288-0285
Email: Stephen.Keen@cima.ca

Sonya Bubas, MCIP, RPP
Project Manager, Infrastructure, Programming & Studies
Region of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, Ontario, L6T 4B9
Phone: 905-791-7800 ext. 7801
Email: sonya.bubas@peelregion.ca

PLEASE PRINT CLEARLY

Name: _____

Address: _____

Email: _____

Postal Code: _____

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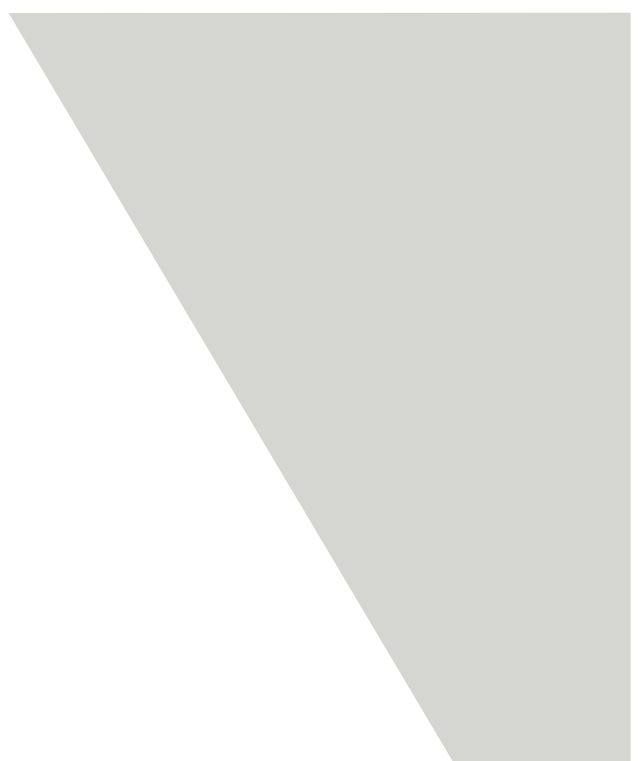
SUBMITTED BY CIMA CANADA INC.

400-3027 Harvester Road

Burlington, ON L7N 3G7

T: 289 288-0287 F: 289 288-0285

cima.ca



**CLASS ENVIRONMENTAL ASSESSMENT STUDY
COLERAINE DRIVE GRADE SEPARATION (SOUTH OF OLD ELLWOOD DRIVE)
TOWN OF CALEDON**

**Public Information Centre #1 - Tuesday, October 8, 2019
Albion Bolton Community Centre**

COMMENT SHEET

COMMENTS

PLEASE ADD ME TO THE EMAIL LIST FOR THIS PROJECT.

MY PREFERENCE WOULD BE FOR THE ROAD TO GO UNDER THE RAIL.

Place your completed comment sheet in the box provided or return by October 22, 2019 to:

Stephen Keen, P. Eng.
Project Manager
CIMA Canada Inc. (CIMA+)
3027 Harvester Road, Suite 400
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PLEASE PRINT CLEARLY

Name: _____

Address: _____

Email: _____

Postal Code: _____

**CLASS ENVIRONMENTAL ASSESSMENT STUDY
COLERAINE DRIVE GRADE SEPARATION (SOUTH OF OLD ELLWOOD DRIVE)
TOWN OF CALEDON**

**Public Information Centre #1 - Tuesday, October 8, 2019
Albion Bolton Community Centre**

COMMENT SHEET

COMMENTS

- We have concerns putting the overpass we feel the best solution would be to put the train below grade.

- The noise level already is excessive.

Place your completed comment sheet in the box provided or return by October 22, 2019 to:

Stephen Keen, P. Eng.
Project Manager
CIMA Canada Inc. (CIMA+)
3027 Harvester Road, Suite 400
Burlington, Ontario L7N 3G7
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PLEASE PRINT CLEARLY

Name: _____

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Email: _____

Postal Code: _____

**CLASS ENVIRONMENTAL ASSESSMENT STUDY
COLERAINE DRIVE GRADE SEPARATION (SOUTH OF OLD ELLWOOD DRIVE)
TOWN OF CALEDON**

**Public Information Centre #1 - Tuesday, October 8, 2019
Albion Bolton Community Centre**

COMMENT SHEET

COMMENTS

- very concerned about the length of time of construction
- very concerned about the lack of proper fencing, noise reduction & safety for the residents of Wacey & Aida Court.
- Further explanation as to why the lowering of the rail line beneath the road is NOT an option?? Even though we were shown pictures at Public Meeting that showed the rail going under, but were told it was not an option

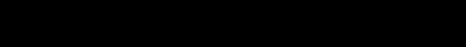
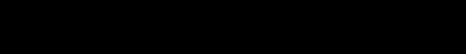
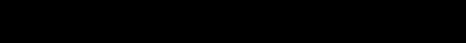
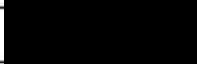
Generally I have many concerns about the Region & Towns ability to come up with a cohesive plan that is best for the residents who are most affected by this.

Place your completed comment sheet in the box provided or return by October 22, 2019 to:

Stephen Keen, P. Eng.
Project Manager
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PLEASE PRINT CLEARLY

Name: 
Address: 
Email:  Postal Code: 

A-1

Appendix A-1-3: Public Information Centre #2

NOTICE OF PUBLIC INFORMATION CENTRE NO. 2

COLERAINE DRIVE GRADE SEPARATION, SOUTH OF OLD ELLWOOD DRIVE, TOWN OF CALEDON MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY

The Region of Peel is conducting a Municipal Class Environmental Assessment (EA) Study for a grade separation of the Canadian Pacific Railway (CP) on Coleraine Drive in the Town of Caledon (see map). The purpose of the study is to consider grade separation improvements of the CP Rail line on Coleraine Drive, south of Old Ellwood Drive.

The Study is being conducted in compliance with Schedule 'C' of the Municipal Class Environmental Assessment (Municipal Engineers Association, as amended in 2015), which is approved under the *Ontario Environmental Assessment Act*.

PUBLIC INFORMATION CENTRE NO. 2

Due to the current COVID-19 guidelines, the Public Information Centre (PIC) will be held in an online format to present the evaluation of alternative design concepts and recommended preliminary design.

To provide your input/comments you can:

- **Visit the Project Website from December 16, 2021 to January 21, 2022** to review project details through an interactive PIC portal and provide your feedback online. The PIC is available via the project website at: <https://peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp>

The Project Team will review and consider the feedback received at the PIC and confirm the recommended preliminary design. The next steps of the study include the preparation of an Environmental Study Report (ESR) to document the planning, consultation and design process. At the end of the study, the ESR will be made available for public review for a minimum of 30 calendar days and at that time, a Notice of Completion will also be issued.

If you have any questions or comments on the Study, please contact either of the following Project Team members:

Tareq Mahmood
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10 Peel Centre Drive, Suite B, 4th Floor
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The Region of Peel is committed to ensure that all Regional services, programs and facilities are inclusive and accessible for persons with disabilities. Please contact the Project Manager if you need any disability accommodation to participate in the study.

Information will be collected in accordance with the *Municipal Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record

Follow [Peel Public Works on Twitter](#) for service updates and changes.



Public Information Centre (PIC) 2

Municipal Class EA Study For Coleraine Drive Grade Separation PRESENTATION 1 - OVERVIEW



December 16, 2021 – January 21, 2022

Land Acknowledgement

- We would like to acknowledge that the land on which we gather, and on which the Region of Peel operates, is part of the Treaty Lands and Territory of the Mississaugas of the Credit.
- For thousands of years, Indigenous peoples inhabited and cared for this land, and continue to do so today.
- In particular we acknowledge the territory of the Anishinbek, Huron-Wendat, Haudenosaunee and Ojibway/Chippewa peoples; and land that is home to the Metis; and most recently, the territory of the Mississaugas of the Credit.
- We are grateful to have the opportunity to work on this land, and by doing so, give our respect to its first inhabitants.

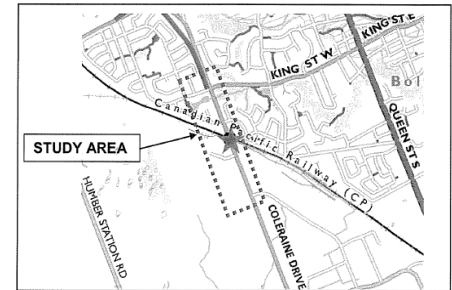
OVERVIEW SUMMARY (3 'W's)

WHAT?

Public Information Centre (PIC) #2 of ongoing Environmental Assessment (EA) Study

WHERE?

Coleraine Drive and CP Rail Crossing, south of Old Ellwood Drive, Town of Caledon, between Holland Drive and Harvest Moon Drive /King Street West

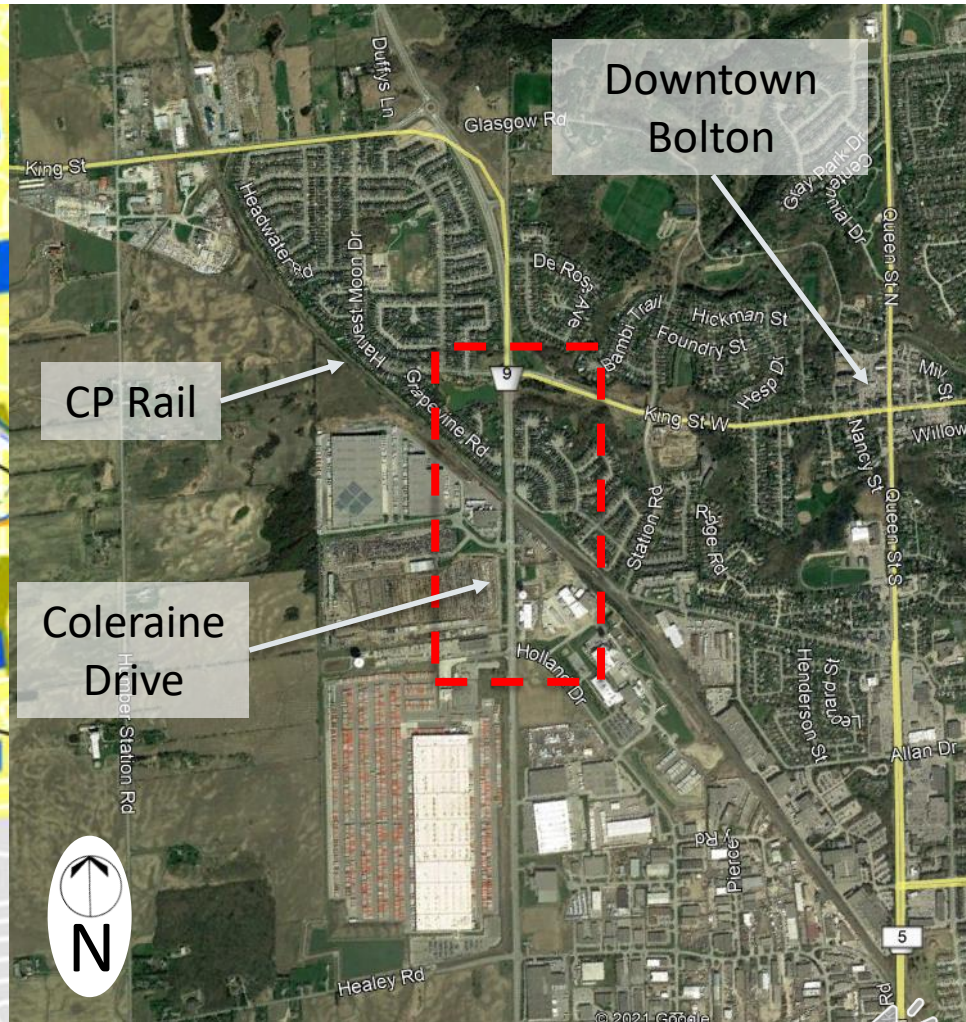
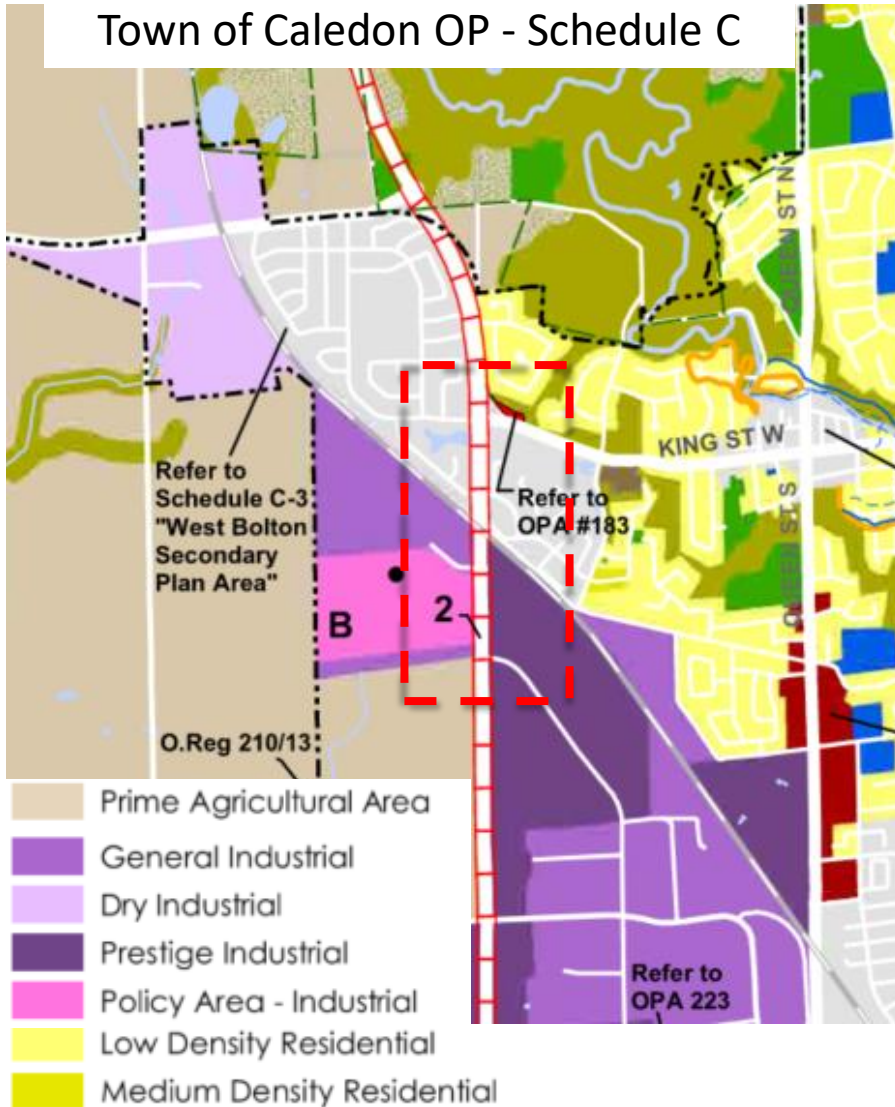


WHY?

- EA Study was initiated through the Region's Long Range Transportation Plan, to investigate grade separation options to serve anticipated traffic growth.
- PIC #1 identified four options. Options to raise or lower the rail grade were screened out. Options to raise or lower the road were carried forward for more detailed evaluation.
- This PIC #2 is presenting the results of this evaluation.

STUDY AREA

Town of Caledon OP - Schedule C

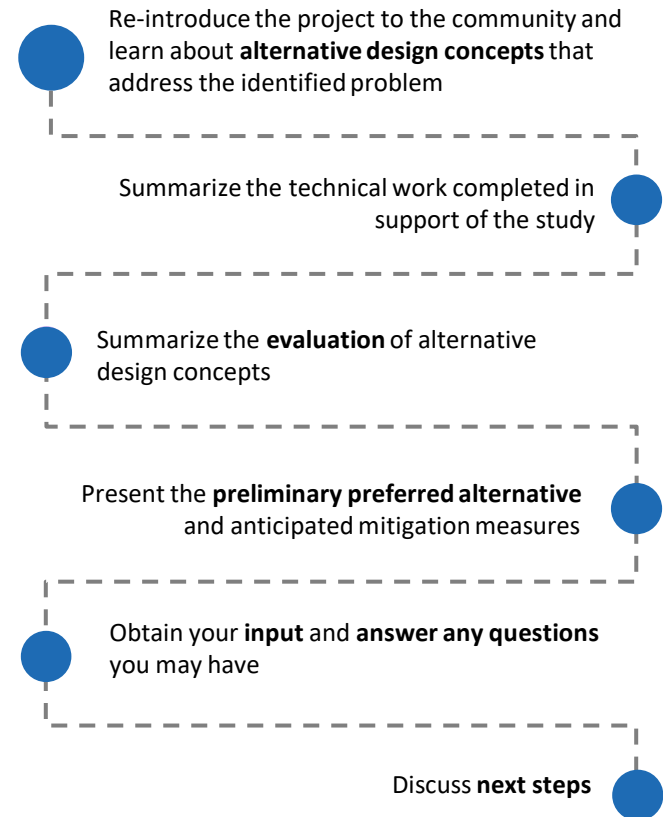


PURPOSE AND OBJECTIVES

Study Purpose

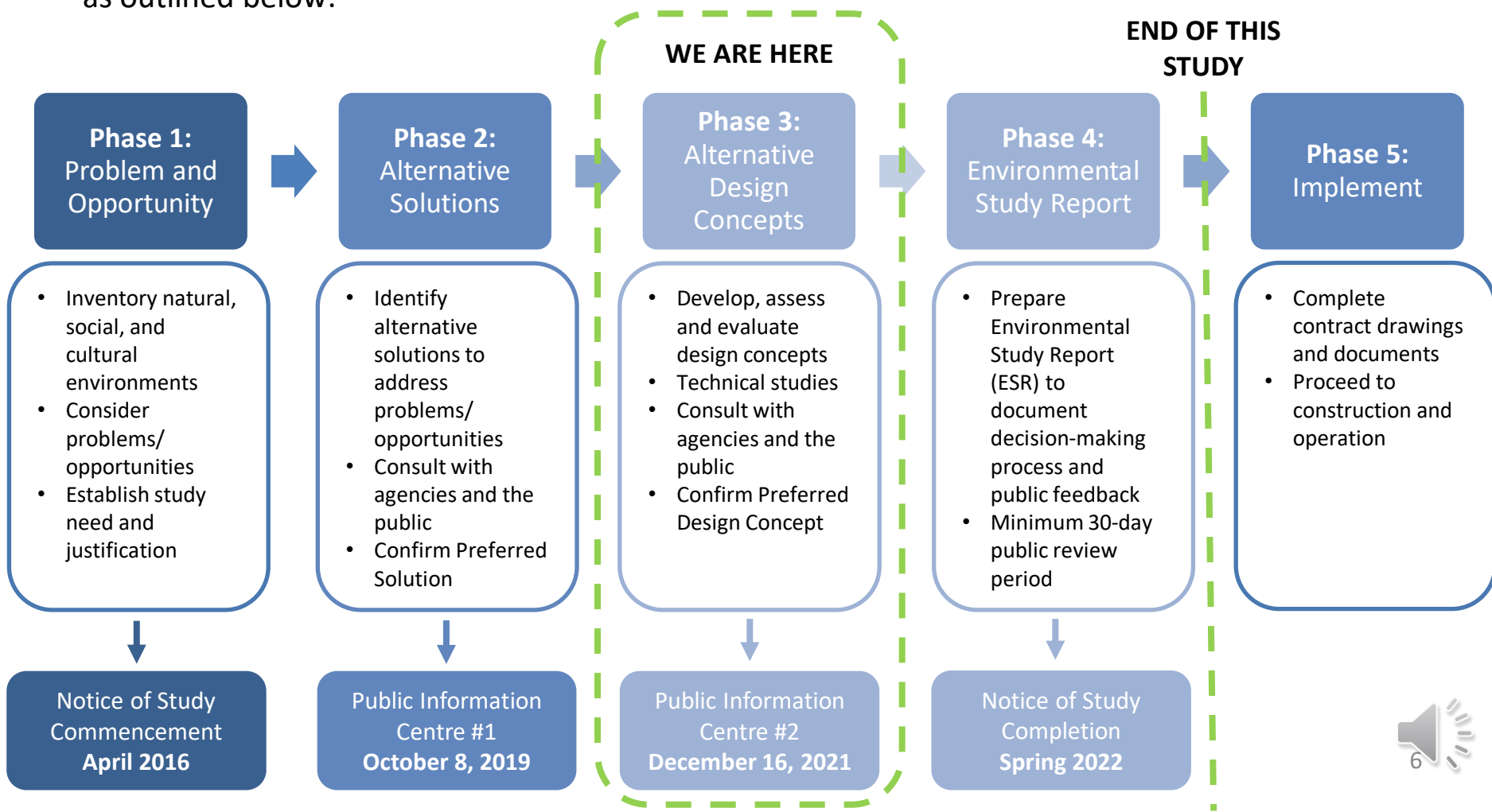
- **Complete** a Municipal Class Environmental Assessment (Schedule 'C') and Preliminary Design for a grade separation of Coleraine Drive and the CP Rail, in Bolton.
- **Identify, define and evaluate** the existing and future transportation needs for the grade separation
- **Determine improvements** to accommodate those needs, including consideration of all modes: Active Transportation (pedestrians / cyclists) and Transit
- **Identify potential impacts** to the natural, social and economic environments as well as cultural heritage and archaeology

Objectives of PIC #2



STUDY PROCESS AND SCHEDULE

- The Municipal Class EA is a planning and design process approved by the Ministry of Environment, Conservation and Parks to meet the requirements of the Ontario Environmental Assessment Act.
- This Study is following the Class EA process for **Schedule 'C'** projects and will complete Phases 1 to 4 as outlined below:



PROBLEM AND OPPORTUNITY STATEMENT

Problems

- An increase in motor vehicle and train traffic is projected for 2041 which is expected to result in queues generated at the at-grade crossing extending beyond the intersection of Coleraine Drive & King Street West/Harvest Moon Drive.
- This issue is expected to worsen as the frequency of trains during peak hours increases, particularly due to the planned extension of GO Train service to a new Bolton station.

Opportunities

- An opportunity to improve active transportation, recognize the importance of goods movement through the corridor, identify safety improvements and enhancing Peel's economic competitiveness.



PIC #1 RECAP: WHAT WE HEARD & ACTIVITIES SINCE

Key PIC No. 1 Comments / Concerns:

- Preference for Road Under Rail however commentors understood the need to review all options
- Existing noise concerns and a request for noise attenuation barriers
- Concerns regarding the long time it would take to construct such large improvements
- Consider a pedestrian underpass from Ellwood Drive to Grapevine Road

Activities Since PIC #1:

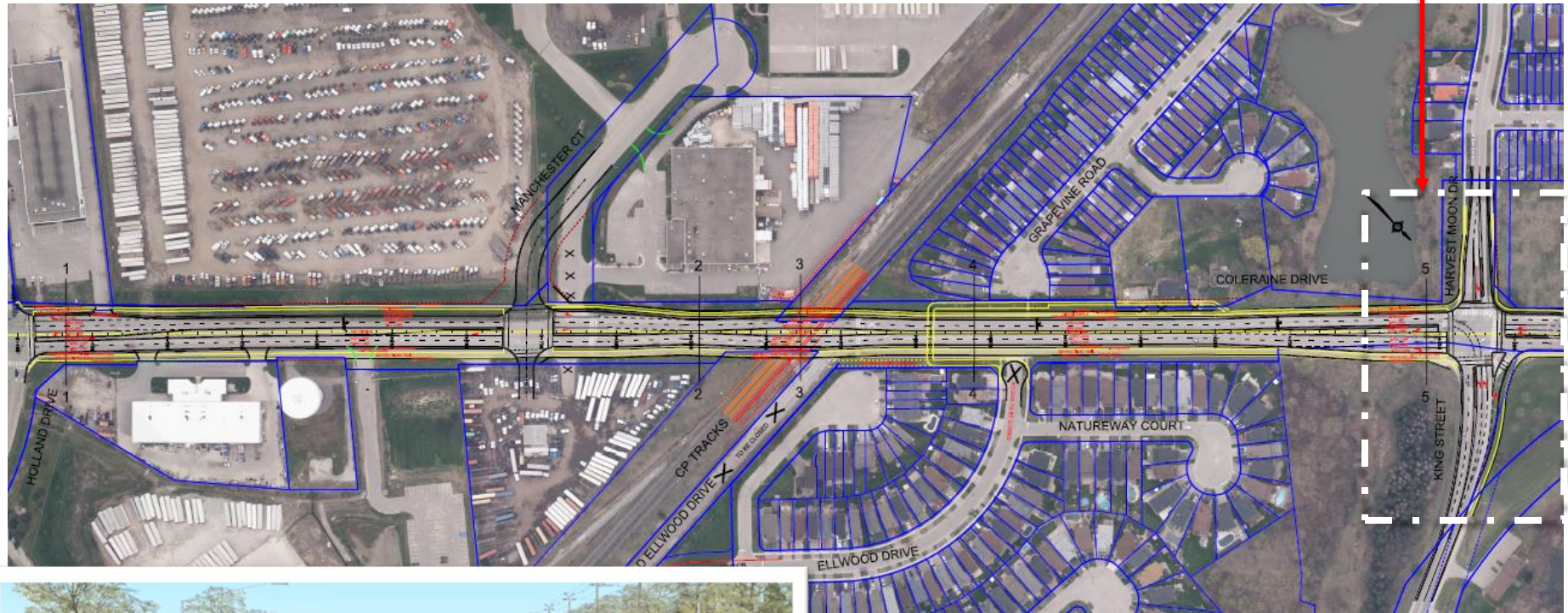
- Responded to comments received
- Developed alternative design concepts
- Completed supporting studies

ALTERNATIVE DESIGN CONCEPTS

Two alternative design concepts have been developed:

1. Road under Rail

Intersection improvements considered separately



ROAD UNDER RAIL (Alternative #1) Looking South towards Ellwood Drive West



- Rail and a dedicated pedestrian & cycling bridge is provided over Coleraine Drive.

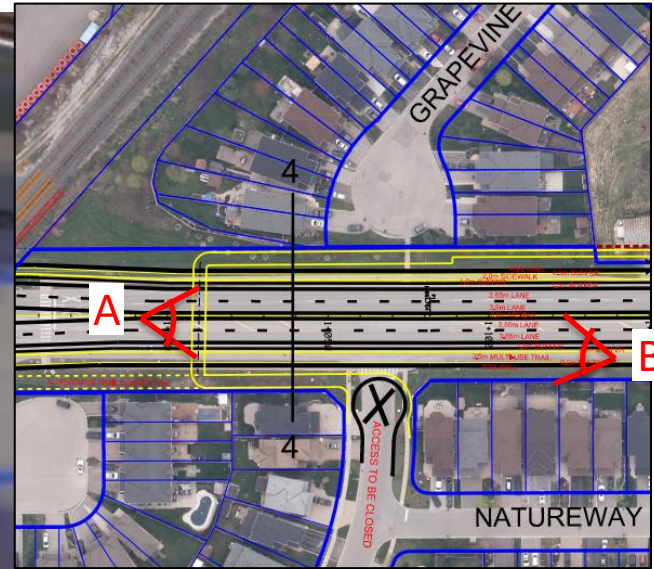


ROAD UNDER RAIL (Alternative #1) Dedicated Active Transportation Bridge

Viewpoint B



Dedicated Active Transportation Bridge



Viewpoint A



- Dedicated Active Transportation (pedestrian and cyclists) bridge provides connection between Ellwood Drive West and Coleraine Drive and Grapevine Road

ROAD UNDER RAIL (Alternative #1) Looking North towards Ellwood Drive West

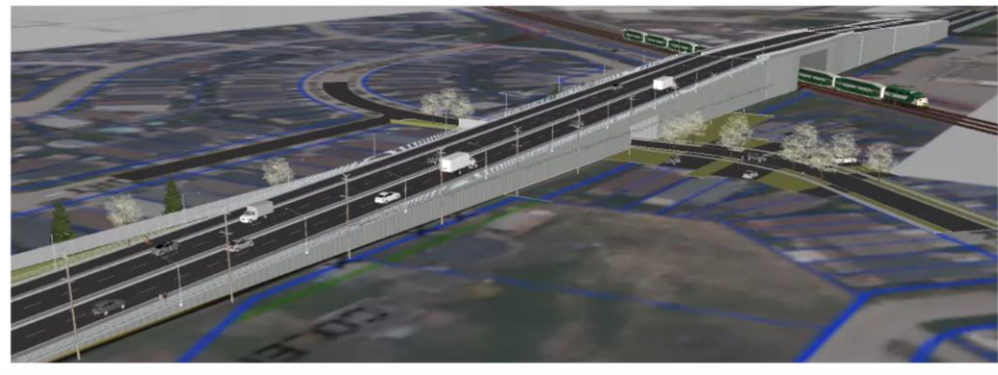
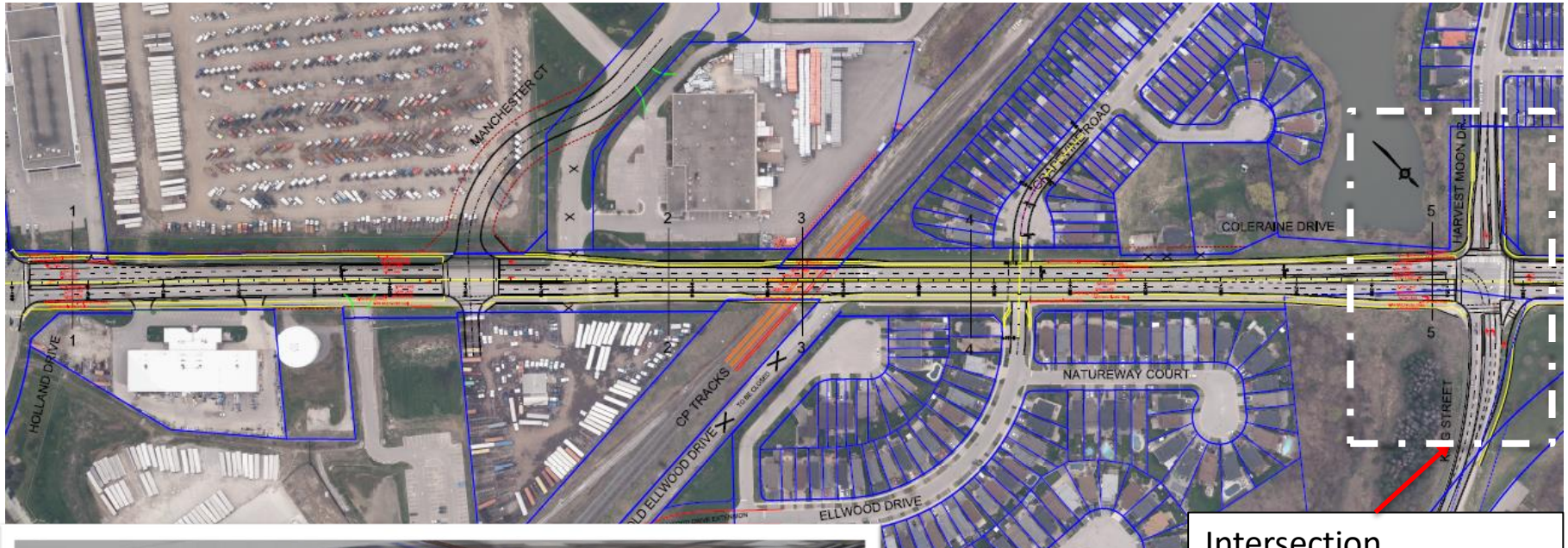


- Road cut under rail using retaining walls.
- Railings used for pedestrian safety.



ALTERNATIVE DESIGN CONCEPTS

2. Road over Rail



Intersection improvements considered separately

ROAD OVER RAIL (Alternative #2) Looking South towards Ellwood Drive West



- Road elevated over rail using embankments on approach and bridge over rail lines.



ROAD OVER RAIL (Alternative #2) Looking North towards Ellwood Drive West



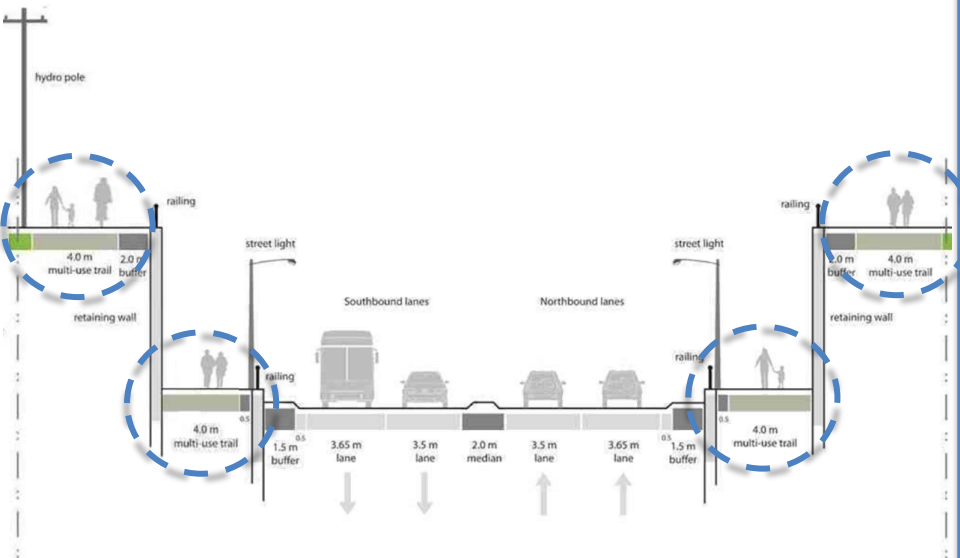
- Road elevated over rail using retaining walls.
- Potential for exterior design / landscaping approaches (cedar tree planting example shown)



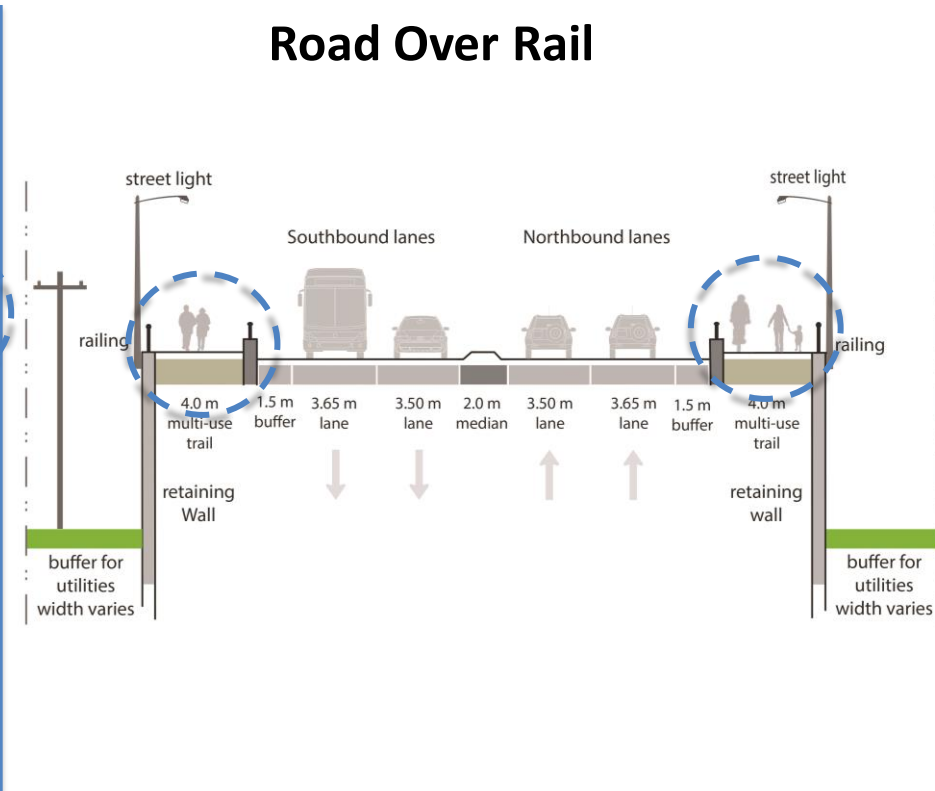
ALTERNATIVE DESIGN CONCEPTS

ACTIVE TRANSPORTATION

Road Under Rail



Road Over Rail



Both alternatives include 4.0 metre multi-use trails on both sides of the roadway

EVALUATION SUMMARY

Road Under Rail



Road Over Rail



Stormwater Management and Groundwater

Road Under Rail will have significant drainage requirements (i.e., permanent pumping) and groundwater impacts. Road Under Rail configurations often encounter flooding.

Noise / Air Quality

Both alternatives will have similar noise/air quality impacts, after mitigation

Access, Property and Aesthetics

Both alternatives have similar property requirements. Road Over Rail is less aesthetically pleasing, however mitigation measures include exterior design. There will be shadow impacts

Constructability

Road Under Rail requires deep, difficult excavation (9 m) and a rail diversion

Cost

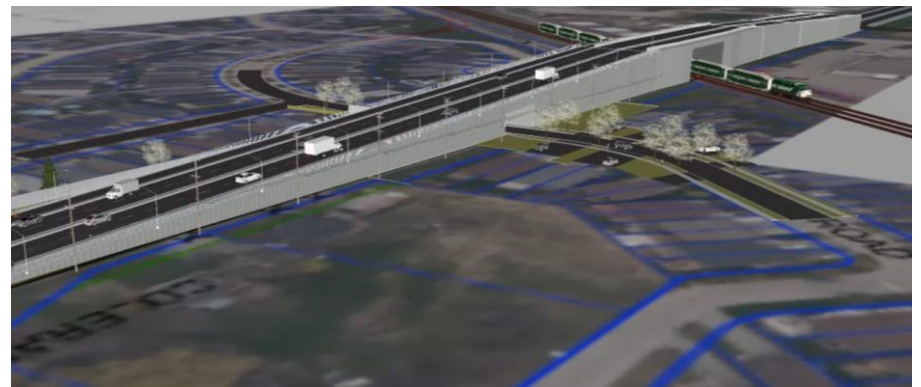
Road Under Rail is significantly more expensive (\$56M) than Road Over Rail (\$36M)

The evaluation of alternatives is further detailed in the supporting documentation available on the study website

PRELIMINARY PREFERRED ALTERNATIVE

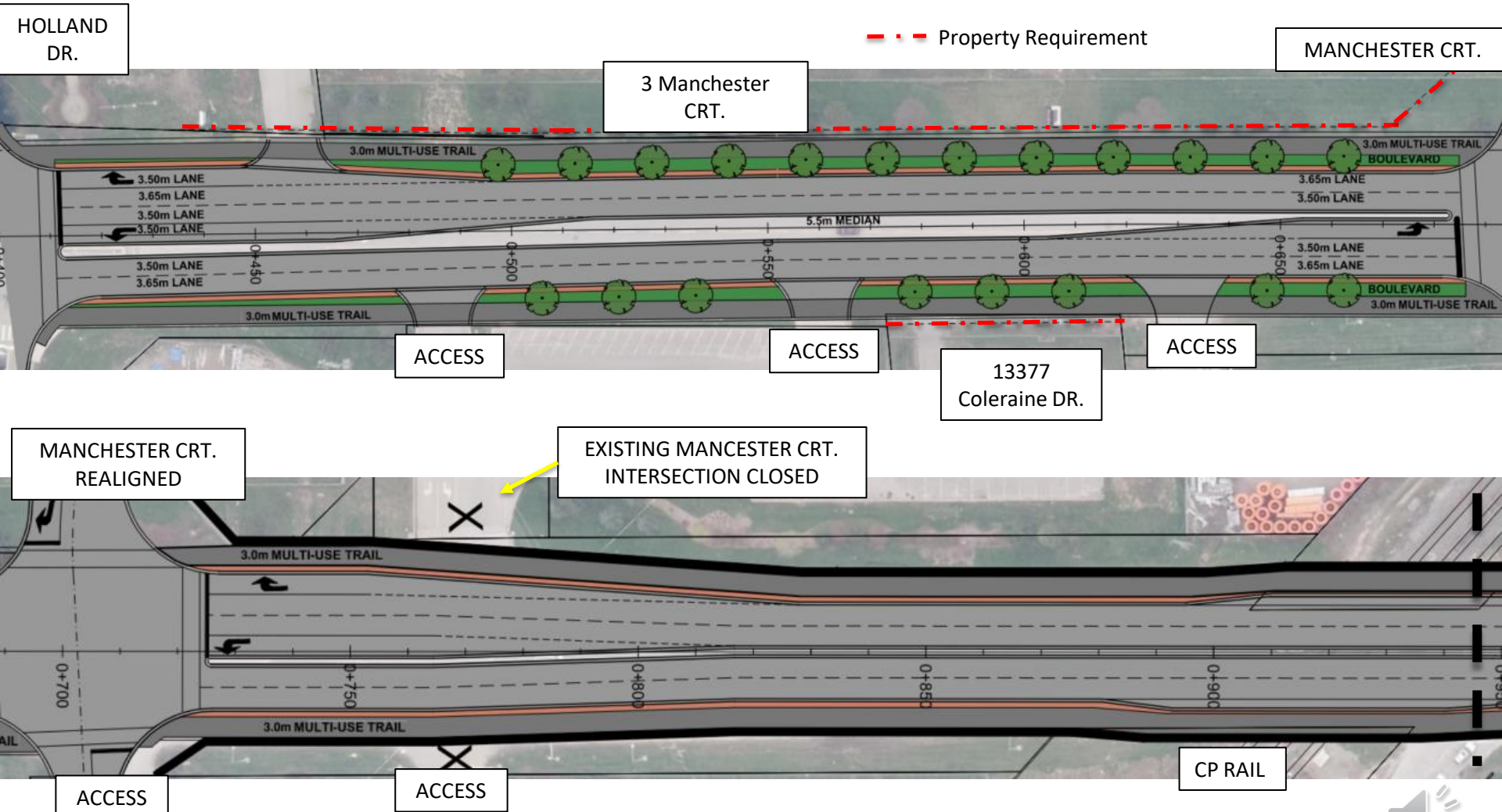
The preliminary preferred alternative is **Road Over Rail**.

- Road Under Rail is not preferred due to a much greater construction complexity (including a rail diversion), greater risk of groundwater impacts and higher cost by ~\$20M.
- Noise impacts (with mitigation measures applied) and property requirements are similar for both options.

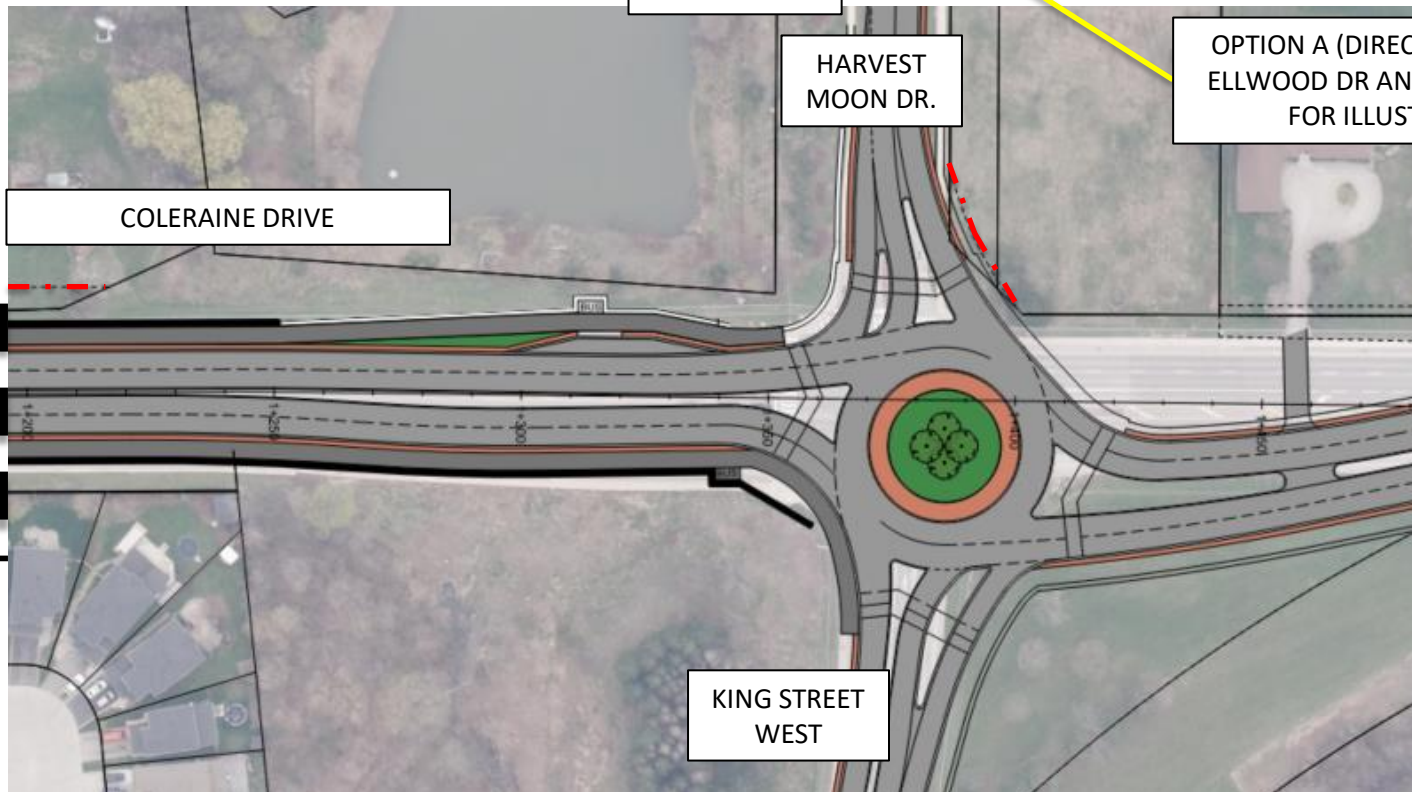
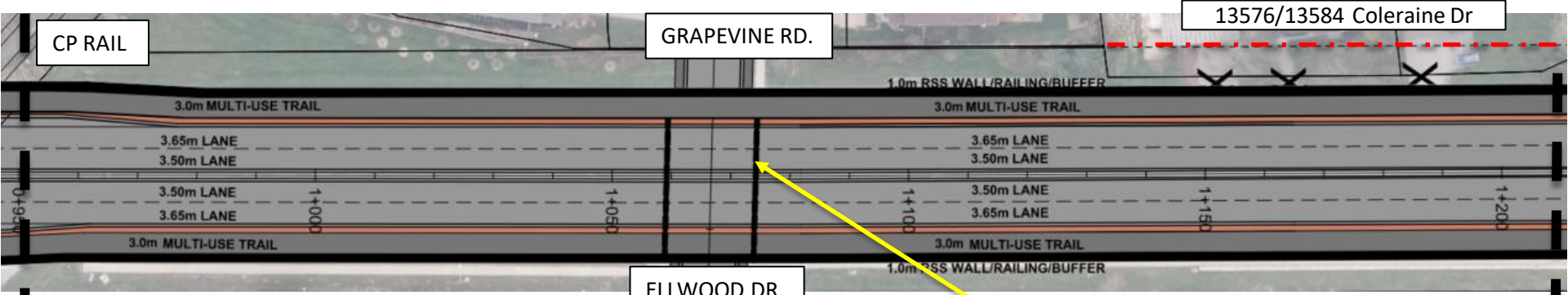


The evaluation of alternatives is further detailed in the supporting documentation available on the study website

PRELIMINARY PREFERRED ALTERNATIVE PLAN: SOUTH OF RAIL CROSSING



PRELIMINARY PREFERRED ALTERNATIVE PLAN: NORTH OF RAIL CROSSING



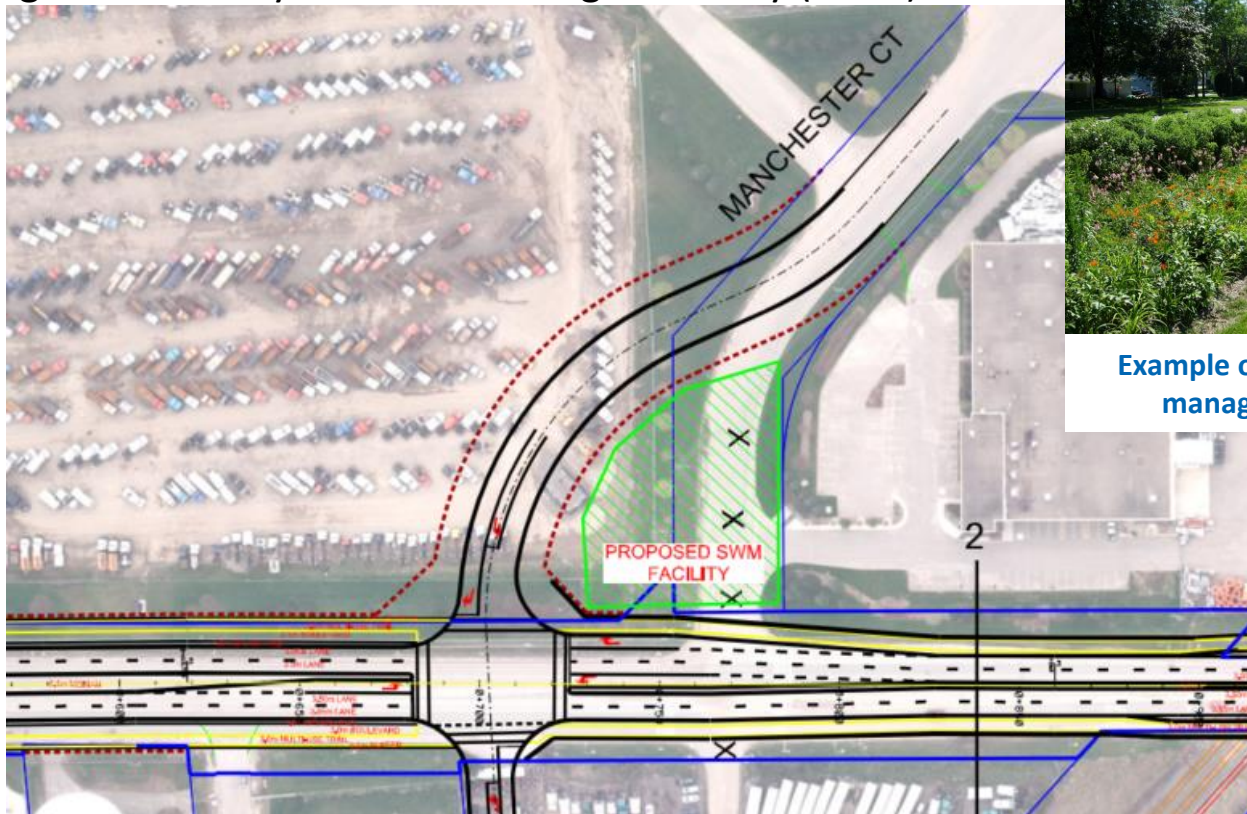
OPTION A (DIRECT CONNECTION BETWEEN ELLWOOD DR AND GRAPEVINE RD) SHOWN FOR ILLUSTRATION PURPOSES

--- Property Requirement



LOCAL ROAD MODIFICATIONS; MANCHESTER COURT

- Both alternatives will require modifications to local roads:
 - Manchester Court is realigned and connects with Coleraine Drive at a new intersection
 - Realignment varies slightly between the Road Over Rail and Road Under Rail options
 - Realignment provides an opportunity for a Low Impact Development (LID) stormwater management facility in the closed Right-of-Way (ROW)

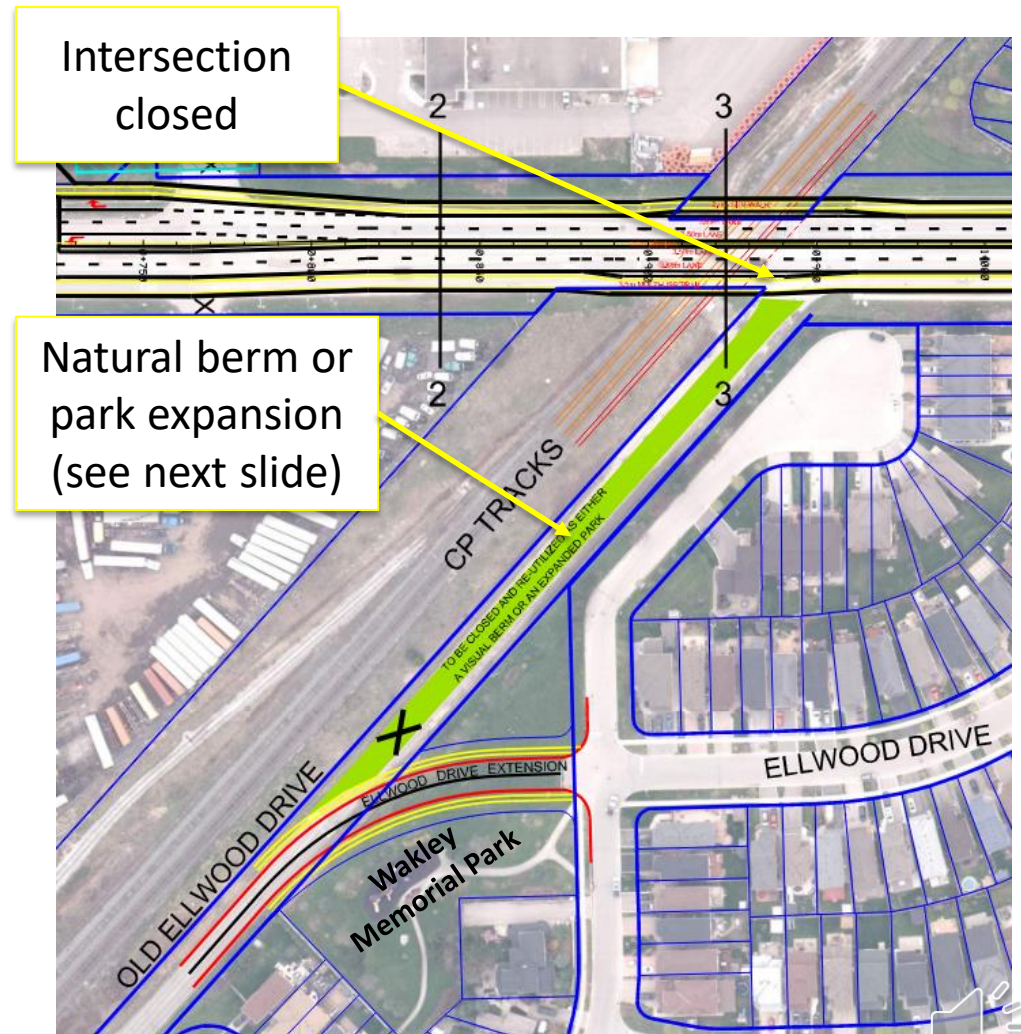


Example of LID stormwater management facility

LOCAL ROAD MODIFICATIONS

BOTH ALTERNATIVES; OLD ELLWOOD DRIVE

- Both alternatives will require modifications to local roads:
 - New connection required as existing Old Ellwood Drive and Coleraine Drive intersection is closed.
 - New connection provided between Old Ellwood Drive and Ellwood Drive, using existing roadway allowance.
 - Disused Old Ellwood Drive Right-of-Way (ROW) to be used as natural berm or park expansion.



LOCAL ROAD MODIFICATIONS

BOTH ALTERNATIVES; OLD ELLWOOD DRIVE

Removing a length of Old Ellwood Drive presents opportunity for neighbourhood enhancement options, such as:

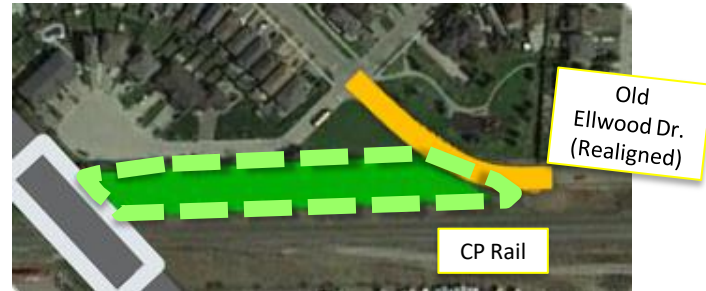
Option A



Typically, 2.5 m to 3.0 m high

Natural Berm

Option B



Park Expansion



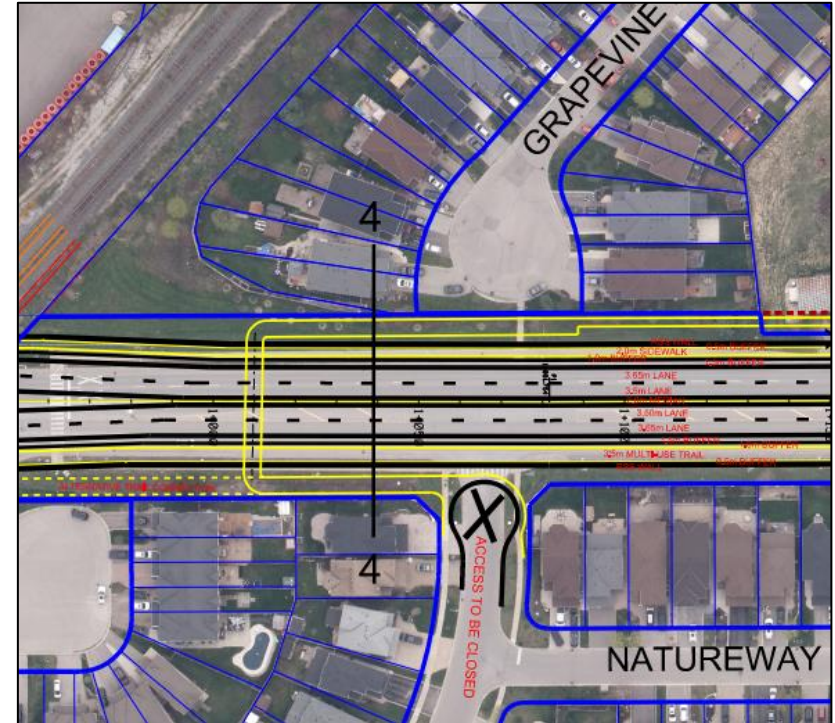
We are seeking the public's thoughts on these options.
Please vote using the interactive tool on the project website.

LOCAL ROAD MODIFICATIONS ROAD UNDER RAIL; ELLWOOD DRIVE WEST

- For the Road Under Rail option:
 - A cul-de-sac is recommended on Ellwood Dr. West.
 - An Active Transportation connection (bridge) is provided over Coleraine Drive.



3D Rendering



LOCAL ROAD MODIFICATIONS ROAD OVER RAIL; ELLWOOD DRIVE



Option A

Option B



Connection between Ellwood Drive and Grapevine Road, accommodating all traffic. (projected volumes approx. 30 - 50 vehicles in the peak hour)

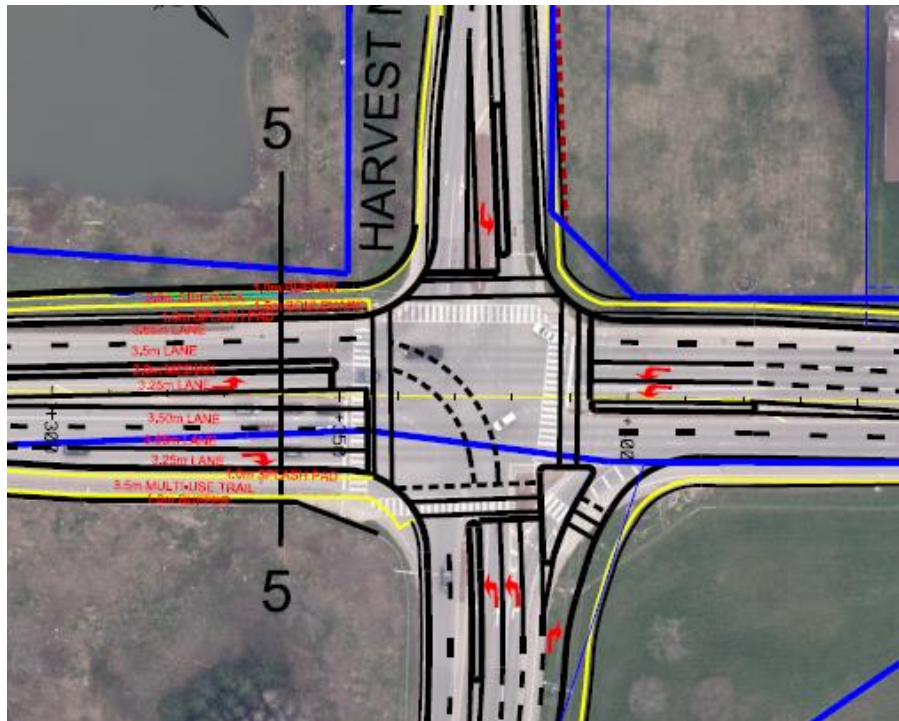


Cul-de-sac Ellwood Drive West and provide only an Active Transportation connection to Grapevine Road

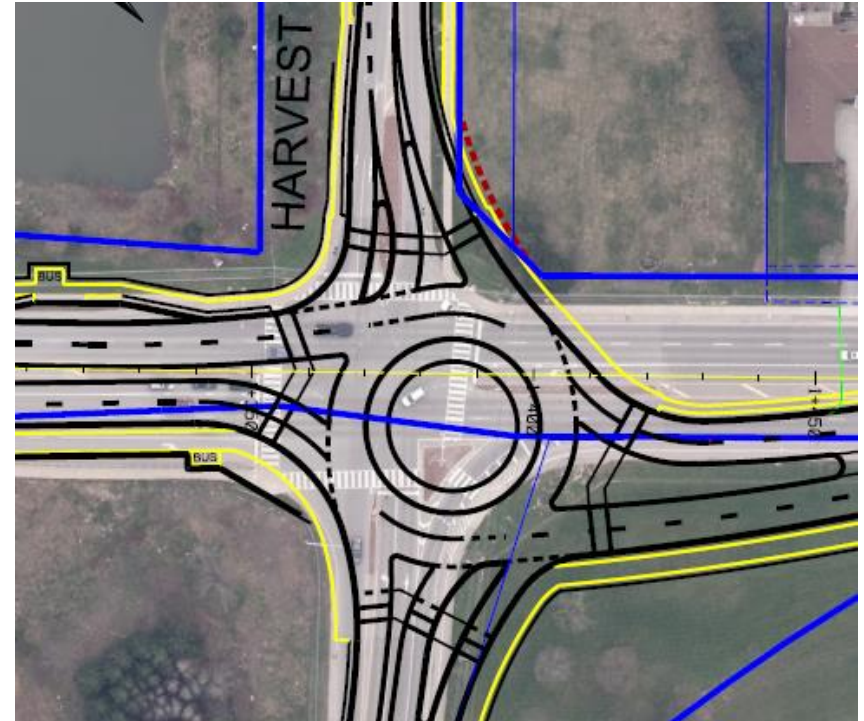
We are seeking the public's thoughts on this option.
Please vote using the interactive tool on the project website.

ALTERNATIVE DESIGN CONCEPTS FOR HARVEST MOON DR. / KING ST. & COLERAINE DR.

Two alternatives were considered for the intersection.



SIGNALIZED INTERSECTION IMPROVEMENTS



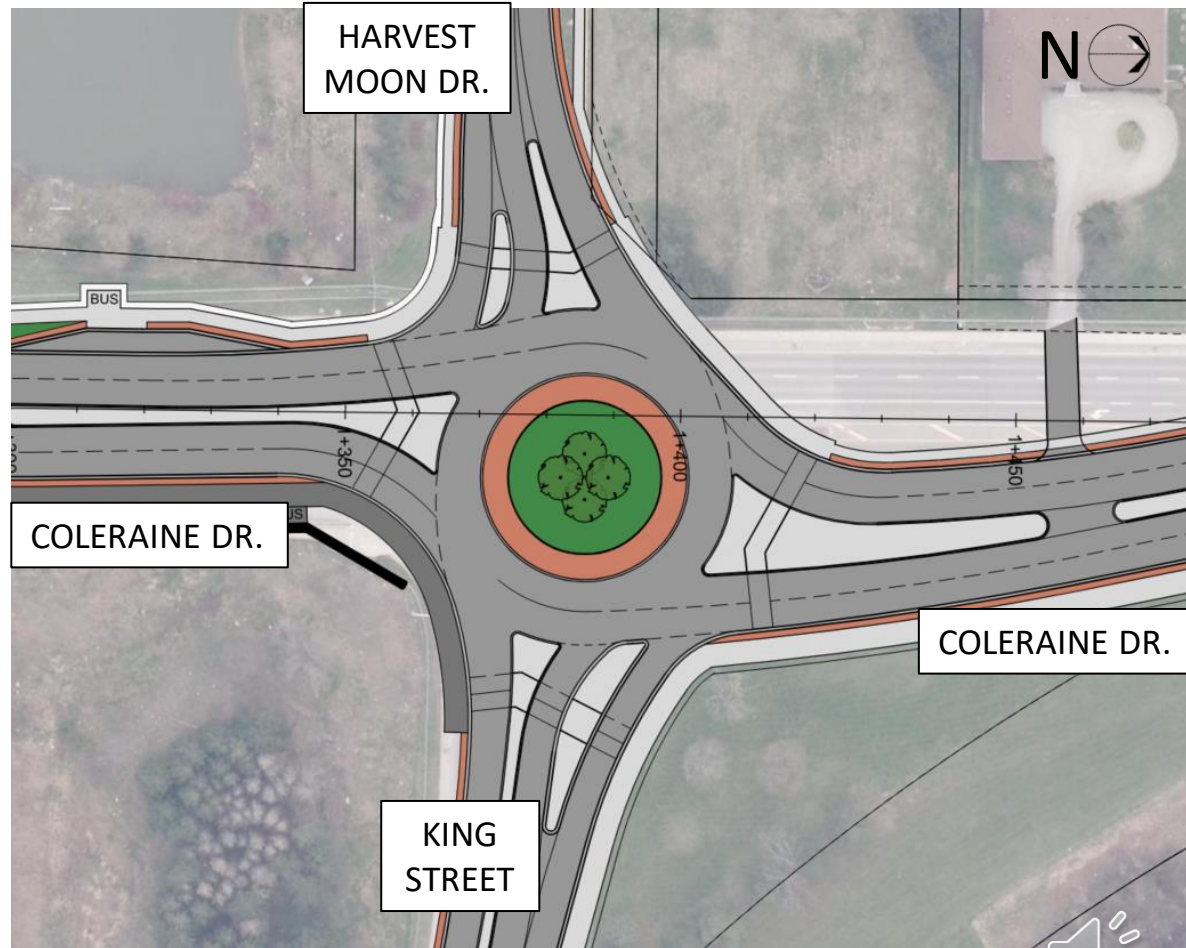
ROUNDBABOUT

HARVEST MOON DR. / KING ST. & COLERAINE DR. INTERSECTION

Preliminary preferred intersection improvement = Roundabout



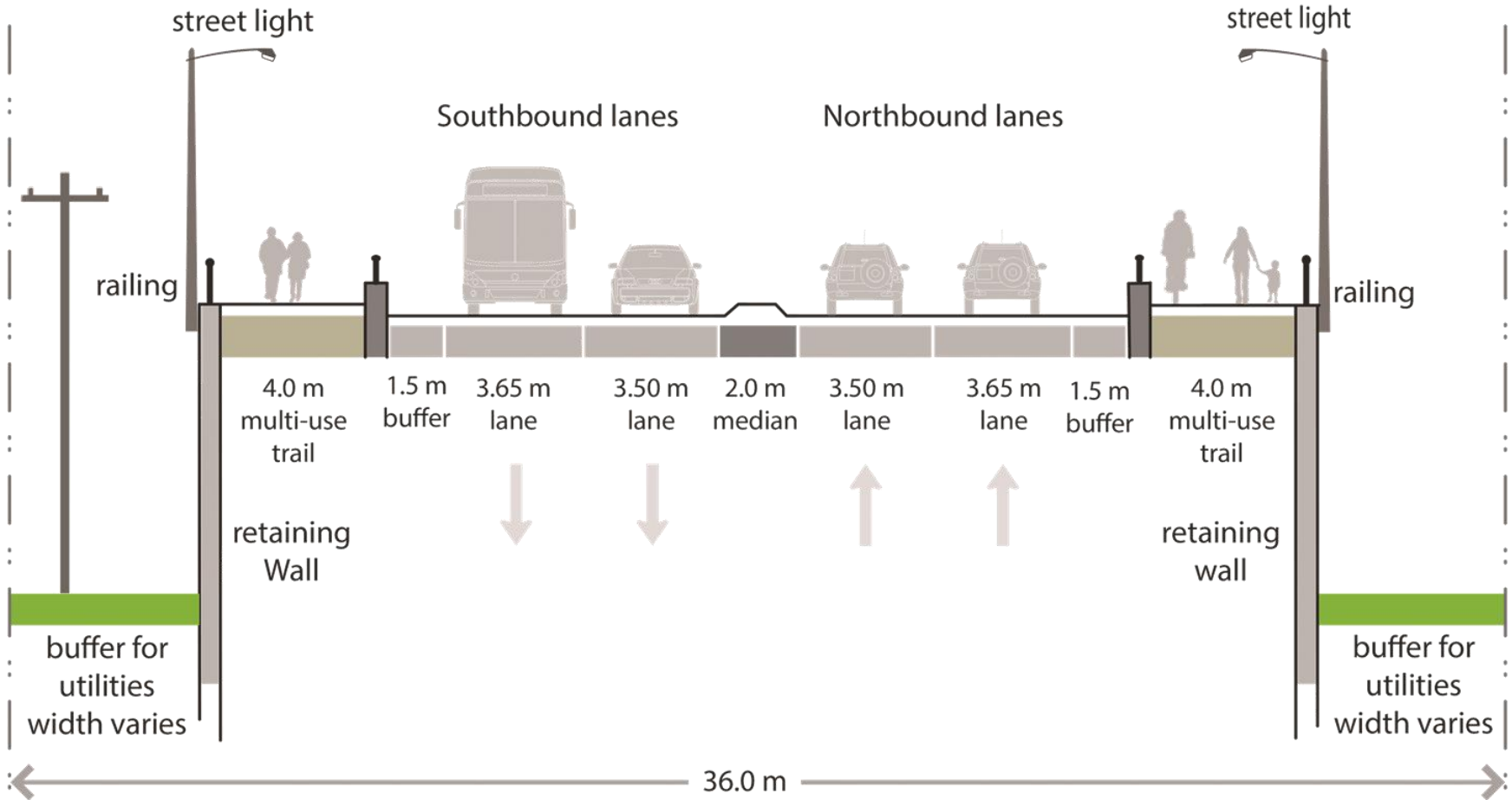
EXISTING



PROPOSED

The evaluation of alternatives is further detailed in the supporting documentation available on the study website

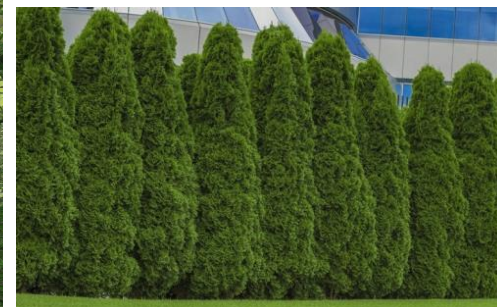
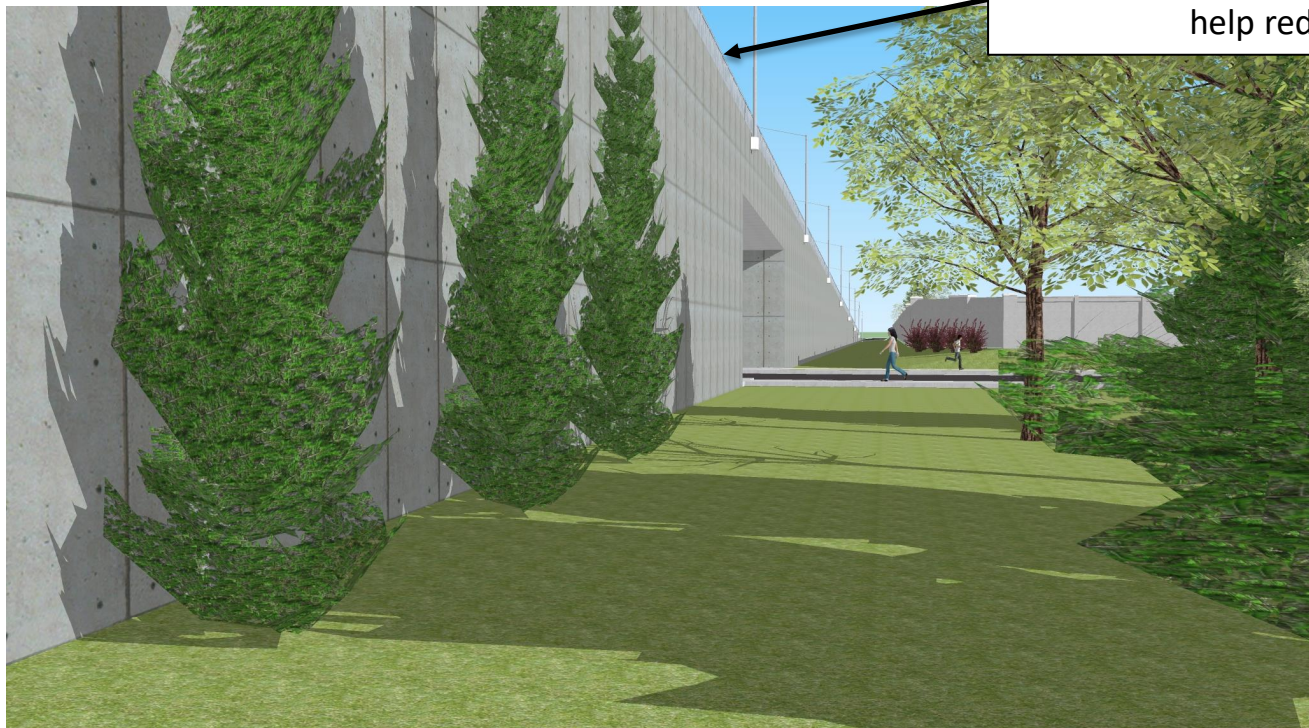
TYPICAL CROSS-SECTION ROAD OVER RAIL



Proposed Cross-section

MITIGATION MEASURES

Concrete barriers on the elevated roadway will help reduce noise impacts



Landscaping opportunities
Example: Wintergreen
Cedar

- High retaining walls can provide opportunities for exterior design or landscaping such as trees, planters, murals or community message boards, and will be confirmed in Detailed Design.
- Other mitigation measures include 1.2 m concrete barriers (typically 0.8 m high) on the roadway to reduce noise impacts.

NEXT STEPS

- Collect and respond to comments received, and tally results of the voting, to inform our decision on the "preferred alternative"
- Complete preliminary design of preferred alternative
- File Environmental Study Report (ESR) for EA approval, in 2022.
- The project will go to the detailed design in 2024, followed by property acquisitions and utility relocations. According to the 2021 Transportation Capital Budget, the construction of the project is scheduled to begin in 2029.

Please complete a comment sheet and submit all comments **by January 21:**

Tareq Mahmood

Project Manager

Infrastructure Programming & Studies

Region of Peel

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Brampton, On L6T 4B9

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Stephen Keen, P. Eng.

Consultant Project Manager

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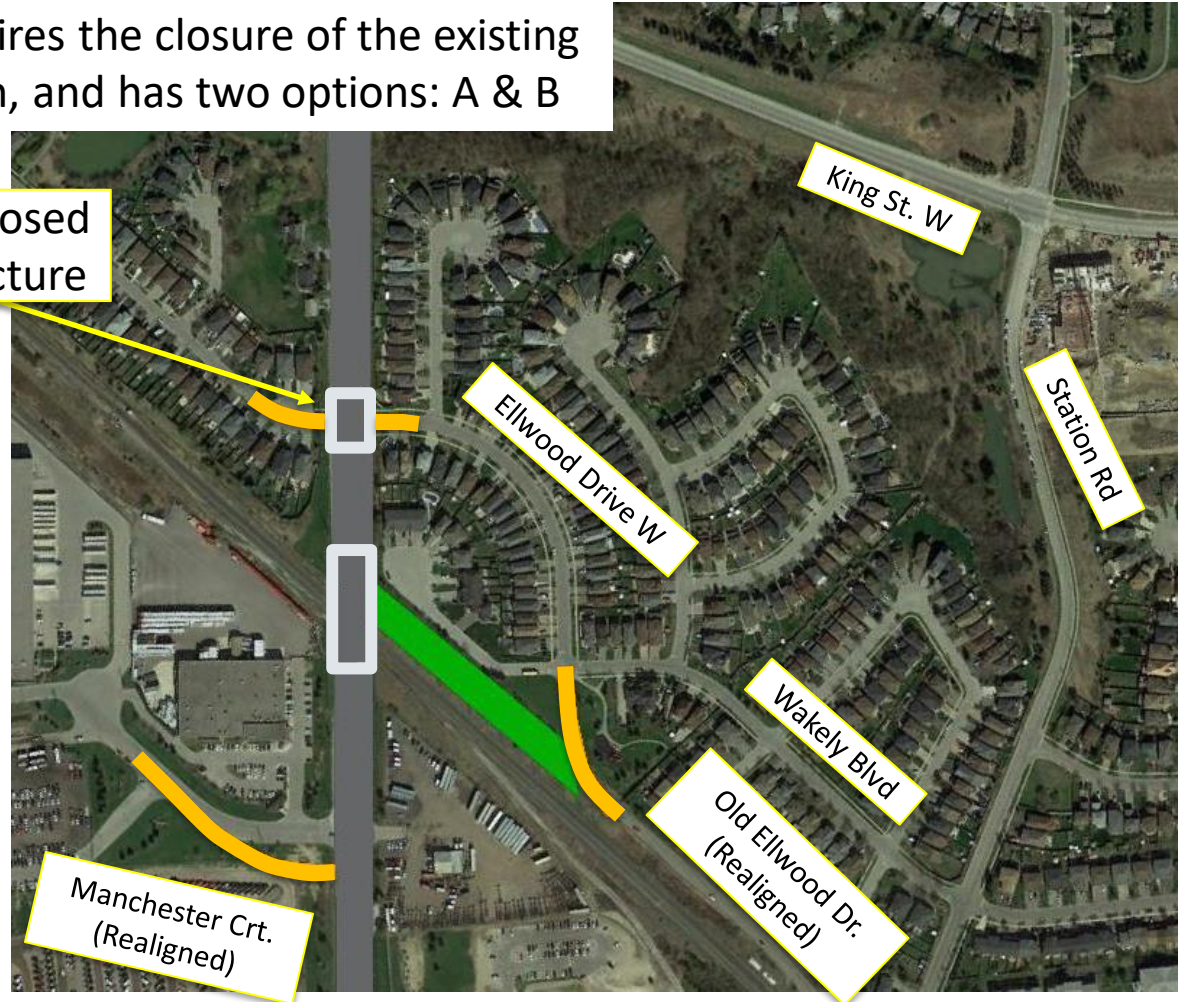
ADDITIONAL

LOCAL ROAD MODIFICATIONS ROAD OVER RAIL; OLD ELLWOOD DRIVE OPTION A

The Road Over Rail option requires the closure of the existing Ellwood Drive West intersection, and has two options: A & B



Proposed structure

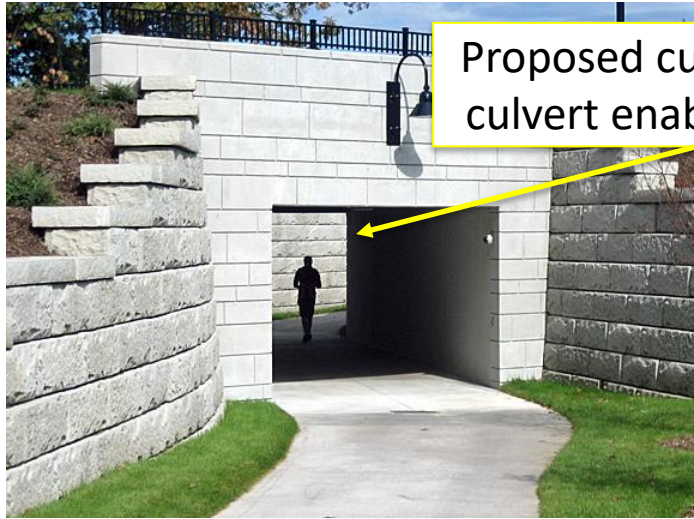


Connection between Ellwood Drive and Grapevine Road accommodates all traffic.



We are seeking the public's thoughts on this option.
Please vote using the interactive tool on the project website.

LOCAL ROAD MODIFICATIONS ROAD OVER RAIL; OLD ELLWOOD DRIVE OPTION B



Proposed cul-de-sac and culvert enabling AT only

Cul-de-sac Ellwood Drive West and provide only an Active Transportation connection to Grapevine Road



We are seeking the public's thoughts on this option.
Please vote using the interactive tool on the project website.

Public Information Centre #2 – Main Overview Presentation – Narration

Slide 1

Welcome to the second Public Information Centre (or PIC) for the Coleraine Drive Grade Separation Environmental Assessment Study, undertaken by the Region of Peel.

Due to the current COVID-19 guidelines, this PIC is being held in a virtual format and will be available online from December 16th to January 21st, 2022. This narrated presentation will be an overview of the study, the alternatives considered and the identified preliminary preferred alternative. Additional information on the Evaluation and Technical Assessments completed in support of the study, is included in a separate narrated slideshow included on the PIC website.

Slide 2

We would like to acknowledge that the land on which we gather, and on which the Region of Peel operates, is part of the Treaty Lands and Territory of the Mississaugas of the Credit.

For thousands of years, Indigenous peoples inhabited and cared for this land, and continue to do so today.

In particular we acknowledge the territory of the Anishinbek, Huron-Wendat, Haudenosaunee and Ojibway/Chippewa peoples; and land that is home to the Metis; and most recently, the territory of the Mississaugas of the Credit.

We are grateful to have the opportunity to work on this land, and by doing so, give our respect to its first inhabitants.

Slide 3

To summarize, we will first answer the three W's: What, Where and Why?

What? This is the second Public Information Centre of this EA Study. The first PIC was held in October 2019.

Where? The study is focused on the Coleraine Drive and CP Rail Crossing, located south of Old Ellwood Drive in the Town of Caledon

Why? This EA study was initiated through the Region's Long Range Transportation Plan, to investigate grade separation options to serve anticipated traffic growth that will be experienced by Coleraine Drive in the future. PIC #1 identified four options, including raising or lowering the railway, and raising or lowering the road. Options to raise or lower the rail grade were screened out. Options to raise or lower the road were carried forward for more detailed evaluation. This PIC#2 will present the road grade design options and the results of their evaluation.

Slide 4

This slide provides an overview of the general location of the study area relative to downtown Bolton, to the east, and in terms of the surrounding land use. The study area extends along Coleraine Drive, from approximately 150 m north of Harvest Moon Drive to Holland Drive.

The map on the left is an excerpt from the Land Use schedule in the Town of Caledon's Official Plan. As shown, south of the CP rail line, the land use is primarily industrial with some prime agricultural area further to the south. North of the CP rail line, the land use is established by the West Bolton Secondary Plan. This planning area is a residential community comprised of two neighbourhoods with a mix of low, medium, and high-density housing, and a range of community uses such as parks and schools.

Slide 5

The Region is carrying out this 'Schedule C' Municipal Class Environmental Assessment study to consider a range of options for a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. This PIC will:

- Re-introduce the study and present the alternative design concepts considered
- Summarize the technical work completed in support of the study
- Summarize the evaluation of the alternative design concepts
- Present the preliminary preferred alternative
- Obtain public input and answer questions

Slide 6

The Municipal Class Environmental Assessment or Class EA, is a planning and approval process for municipal infrastructure that follows Ontario's Environmental Assessment Act. This study is being carried out as a Schedule 'C' Class EA and this graphic summarizes the study process and schedule. We are currently in Phase 3 of 4 and following this PIC an Environmental Study Report will be prepared to document the study recommendations and decision-making process. The Environmental Study Report will be made available for a minimum 30-day public review period at the end of the study.

Slide 7

The first phase of the EA process is to identify the problems and opportunities in the study area, and this was presented at the previous PIC #1. In summary, the problems include:

- An increase in motor vehicle and train traffic that will result in significant queuing on Coleraine Drive in the future, extending from the rail crossing to beyond the King Street and Harvest Moon intersection.
- This issue is expected to worsen as the frequency of trains increase, particularly with the extension of GO train service to a new Bolton station.

The opportunities therefore include a grade separation that would not only improve transportation, however, also improve active transportation connections, recognize the importance of goods movement through the corridor, provide safety improvements, and enhance Peel's economic competitiveness.

Slide 8

In response to the material presented at PIC #1 there was a general preference from the public for the Road Under Rail option, concerns about noise and the time it would take to construct the improvements.

Since PIC #1, the study has responded to the comments received, and has moved into Phase 3 of the EA process in the development of design alternatives and their evaluation. Two design concepts for grade separation have been developed, including:

- Road under Rail
- Road over Rail

More information about the alternative design concepts is presented on the following slides.

Slide 9

The image on this slide is an overview of the plan for Alternative Design Concept 1, Rail under Road. In this alternative, Coleraine Drive is lowered under the CP rail line. A dedicated bridge for active transportation users, such as pedestrians and cyclists, is provided over Coleraine Drive, north of the rail bridge, and will provide connections between Ellwood Drive and Grapevine Road, and Coleraine Drive.

The image on the bottom of the slide is a 3D rendering of this design concept from the perspective of someone standing on Coleraine Drive, looking north towards the rail crossing. Further 3D renderings are included in the next three slides show what the road under rail alternative could look like from different perspectives.

Slide 10

This viewpoint is looking south on Coleraine Drive towards Ellwood Drive West and the rail bridge. Adjacent to the vehicle lanes there are multi-use paths on both sides of Coleraine Drive, separated from the vehicle lanes by a buffer and railing.

The asphalt path on the right-hand side of the image is a multi-use path that runs along side Coleraine Drive at the existing grade, providing connections to Ellwood Drive West and Grapevine Road.

Slide 11

This slide shows the dedicated active transportation bridge over Coleraine Drive, for the Road Under Rail Alternative. The bridge provides a connection between Coleraine Drive, Ellwood Drive and Grapevine Road.

Viewpoint A is from the perspective of someone standing on the bridge looking north towards Ellwood Drive.

Viewpoint B is an aerial perspective from north of the active transportation bridge, looking south towards the rail line.

Slide 12

This viewpoint is from the multi-use path on east side of Coleraine Drive looking north towards Ellwood Drive West. Coleraine Drive is on the left side of the image at an elevation below the existing ground, having passed under the CP rail line. A railing is provided for safety.

Slide 13

This is an overview of the plan for the Alternative Design Concept 2, Road Over Rail. In this alternative, Coleraine Drive is raised over the CP rail line. The image on the bottom of the slide is a 3D rendering of this design concept from an aerial perspective. Further 3D renderings are included in the next two slides to further illustrate this alternative.

Slide 14

This viewpoint is of Coleraine Drive looking south towards Ellwood Drive West. The road is raised above the ground on approach to the bridge over the CP rail line.

Slide 15

This is the viewpoint from the ground adjacent to Coleraine Drive looking north towards Ellwood Drive West. Retaining walls are used to raise Coleraine Drive over the CP Rail Line and above Ellwood Drive West. There are two options for the configuration of Ellwood Drive West at this location, and these are further detailed later in this presentation.

Slide 16

In terms of Active Transportation on Coleraine Drive, multi-use trails are provided on both sides of the roadway through the length of the study area, for both alternative design concepts. For the Rail Under Road alternative (shown on the left side of the slide) multi-use trails are also provided at the existing ground level.

Slide 17

This slide provides a summary of the key items identified when evaluating the two design concepts. Further information on the evaluation can be found in the additional presentation, available on the project website.

In terms of key differences:

Regarding Stormwater Management and Groundwater - Road under Rail will have a significantly larger drainage requirement than Road Over Rail, due to the excavation resulting in groundwater impacts and a need for pumping to accommodate drainage. Road Under Rail configurations often encounter flooding.

Regarding Noise and Air Quality - Both alternatives will have similar impacts. Mitigation measures for noise are spoken to later in this presentation.

Regarding Access, Property and Aesthetics - Both alternatives will have similar property requirements, however Road Over Rail will be less aesthetically pleasing and will have shadow impacts due to the high retaining walls. Mitigation measures for the look of the walls include landscaping or planting opportunities and are spoken to later in this presentation.

Regarding Constructability - The excavation required for Road under Rail will be deep, at approximately 9 m, meaning its construction will be more complex. The Road Under Rail will also require the construction of a rail diversion, increasing construction requirement and cost.

Regarding Cost – Road Under Rail is significantly more expensive than Road Over Rail, with a difference of approximately \$20M.

Slide 18

Through the evaluation, the Road Over Rail design concept has been identified as the preliminarily preferred alternative.

The Road Under Rail was not preferred due to a much greater construction complexity, (including the construction of a rail diversion), greater risk of drainage and groundwater impacts and higher cost.

Noise impacts (with mitigation measures applied) and property requirements are similar for both options.

The Road Over Rail design is further detailed in the following slides and the public is encouraged to provide comments on the evaluation and the proposed recommendations, through either the PIC website or directly to the project contacts. Contact information will be provided at the end of this presentation.

Slide 19

For presentation, the preliminary preferred alternative has been segmented in the following slides. A complete plan of the design is available for download on the PIC website.

South of the rail, the design ties into the existing Holland Drive intersection at the southern limits. Manchester Court is realigned to intersect Coleraine Drive further south of the rail

crossing, to help accommodate raising the grade of Coleraine Drive to cross over the rail line. Local road modifications are further explained later in this presentation. Property requirements are illustrated by the red dashed line in these images and in the complete plan available for download.

Slide 20

North of the rail line, Coleraine Drive is still elevated at the locations of Old Ellwood Drive and Ellwood Drive West and so the existing intersections of Coleraine Drive and Old Ellwood Drive and Ellwood Drive will be closed. The modifications to these roads are further detailed later in this presentation. A roundabout is proposed at the intersection of Harvest Moon Drive and King Street, and Coleraine Drive will tie back into the existing roadway north of the roundabout.

Slide 21

Modifications to local roads are required to accommodate the proposed changes to Coleraine Drive. Manchester Court is realigned and connects with Coleraine Drive at a new intersection. There is an opportunity for Low Impact Development Stormwater Management Facility in the closed right-of-way of the existing Manchester Court.

Slide 22

With the raising of Coleraine Drive over the rail line, the intersection of Coleraine Drive and Old Ellwood Drive will be closed. A new connection between Old Ellwood Drive and Ellwood Drive is provided east of Coleraine Drive, using an existing roadway allowance. The unused right-of-way of Old Ellwood Drive may be used as a natural berm or to expand the park, further detailed on the following slide.

Slide 23

As mentioned, there are potential neighbourhood enhancement options for the unused right-of-way of Old Ellwood Drive and the Region would like to take this opportunity to ask for feedback on these options.

Option A includes providing a natural berm to help provide a natural horizontal and vertical barrier between the subdivision and the rail line.

Option B includes using the space to extend the area of Wakely Memorial Park. In this option, trees but also other plants can be planted along the unused right-of-way, next to the rail line. It is noted the Town of Caledon and the Toronto Region Conservation Authority are currently looking at planting opportunities at the Wakely Memorial Park, potentially as part of the West Bolton Sustainable Neighbourhood Action Program (SNAP).

Please vote for your preferred option using the voting tool available on the PIC website.

Slide 24

For both Road Over Rail and Road Under Rail options, the intersection at Coleraine Drive and Ellwood Road West will be closed. For the Road Under Rail, a cul-de-sac is recommended on Ellwood Drive West, as a direct connection to Grapevine Road over Coleraine Drive is geometrically challenging. As described before, an Active Transportation bridge is provided over Coleraine Drive, and this is done by locating the bridge closer to the rail crossing.

Slide 25

For both Road Over Rail and Road Under Rail options, the existing intersection at Coleraine Drive and Ellwood Drive West will be closed. This slide presents the two potential configuration options for Ellwood Drive West, and the Region would like to take this opportunity to also ask for feedback on these options.

Option A provides a direct connection between Ellwood Drive West and Grapevine Road, accommodating all traffic.

Option B provides a connection between Ellwood Drive West and Grapevine Road, only for Active Transportation users.

Please vote for your preferred option using the voting tool available on the PIC website.

Slide 26

For the intersection of Coleraine Drive, Harvest Moon Drive and King Street, two improvement options were considered by this study. Like the Road Over Rail and Road Under Rail, these two intersection improvement alternatives were evaluated to determine a preferred intersection improvement. The improvement alternative included typical signalized intersection improvements or reconfiguring the intersection into a roundabout.

Slide 27

For the intersection of Coleraine Drive, Harvest Moon Drive and King Street, the identified preliminary preferred intersection improvement is the roundabout and will be constructed in conjunction with the Road Over Rail design. Further details into its evaluation is provided in the additional presentation available on the PIC website.

Slide 28

To recap the Road Over Rail cross-section, the design will accommodate two lanes in each direction, including turning lanes where required, and accommodate multi-use trails on both sides of Coleraine Drive.

Slide 29

In terms of Mitigation Measures, the high retaining walls provide opportunities for exterior design approaches or landscaping, including trees, planters, murals, community message boards or similar. These approaches will be confirmed in Detailed Design.

Other mitigation measures include those for Noise, which involve using slightly higher roadside barriers to help reduce noise impacts.

Slide 30

The next steps in the study include completing the preliminary design of the preliminary preferred alternative and preparing an Environmental Study Report to document the study recommendations and decision-making process.

The Environmental Study Report will be made available for a minimum 30-day public review period at the end of the study. Following approval of this Environmental Assessment study, the project will move into a detailed design phase. It is anticipated that the project will go to the detailed design in 2024, followed by property acquisitions and utility relocations. According to the 2021 Transportation Capital Budget, the construction of the project is scheduled to begin in 2029.

We are looking forward to hearing your feedback on the materials presented at this Public Information Centre and to provide your input and feedback, please fill-in the comment boxes and vote in the polls available on the PIC website.

If you have any questions or would like an alternate way to provide feedback, please contact either project manager via the contact information shown here and on the PIC website.

Public Information Centre (PIC) 2

Municipal Class EA Study

For Coleraine Drive Grade Separation

EVALUATION AND TECHNICAL ASSESSMENTS

P 16-4315





December 16, 2021 – January 21, 2022

Land Acknowledgement

- We would like to acknowledge that the land on which we gather, and on which the Region of Peel operates, is part of the Treaty Lands and Territory of the Mississaugas of the Credit.
- For thousands of years, Indigenous peoples inhabited and cared for this land, and continue to do so today.
- In particular we acknowledge the territory of the Anishinbek, Huron-Wendat, Haudenosaunee and Ojibway/Chippewa peoples; and land that is home to the Metis; and most recently, the territory of the Mississaugas of the Credit.
- We are grateful to have the opportunity to work on this land, and by doing so, give our respect to its first inhabitants.

EVALUATION

Criteria	Alt 1: Road Under Rail	Alt 2: Road Over Rail
Traffic Operations and Safety	Both alternatives eliminate excessive queuing and reduce number of intersections	
Natural Environment	Both alternatives will have minimal impact	
Stormwater Management and Groundwater	Complex groundwater /drainage impacts (would require pumping)	Less complex groundwater / drainage impacts
Pedestrian and Cyclist Safety	Both alternatives provide equal opportunity for new and improved active transportation facilities	
Noise / Air Quality	Both alternatives will have similar noise/air quality impacts	
Archaeology and Cultural Heritage	Both alternatives have no cultural heritage impacts and the same impact to areas of archaeological potential	
Access, Property and Aesthetics	Both alternatives have similar property requirements	
	More desirable aesthetics	Less desirable aesthetics, including shadow impacts
Constructability	Very Complex	Less Complex
Cost	More expensive (\$56M)	Less expensive (\$36M)
OVERALL		

PREFERRED DESIGN



TRAFFIC ANALYSIS

- Queues for the rail crossing are expected to extend beyond the intersection of Coleraine Drive and King Street West/Harvest Moon Drive in the future.
- Queuing will worsen as trains increase due to the planned extension of GO Train services.
- Benefits of providing either Road Over Rail or Road Under Rail include:
 - Elimination of queuing and reduced risk of train collisions
 - Reduction of intersections
 - Improved truck network and reliability for movement of goods
 - Opportunity for bicycle, pedestrian and transit facility upgrades.

NOISE ASSESSMENT

- A noise assessment was undertaken for the two alternatives
- With **noise mitigation, the options are similar** and will improve on existing conditions. There is not a strong preferred alternative from a noise perspective

	Existing Without Project (dBA)	Alt: 1 Road under Rail (dBA)	Alt: 2 Road over Rail (dBA)
OLA001	53.6	48.4	53.1
OLA002	58.8	52.9	50.8
OLA003	58.8	51.1	50.6
OLA004	58.6	55.9	53.6
OLA005	55.9	54.2	54.7
OLA006	59.1	56.1	56.6



dBA = Decibels

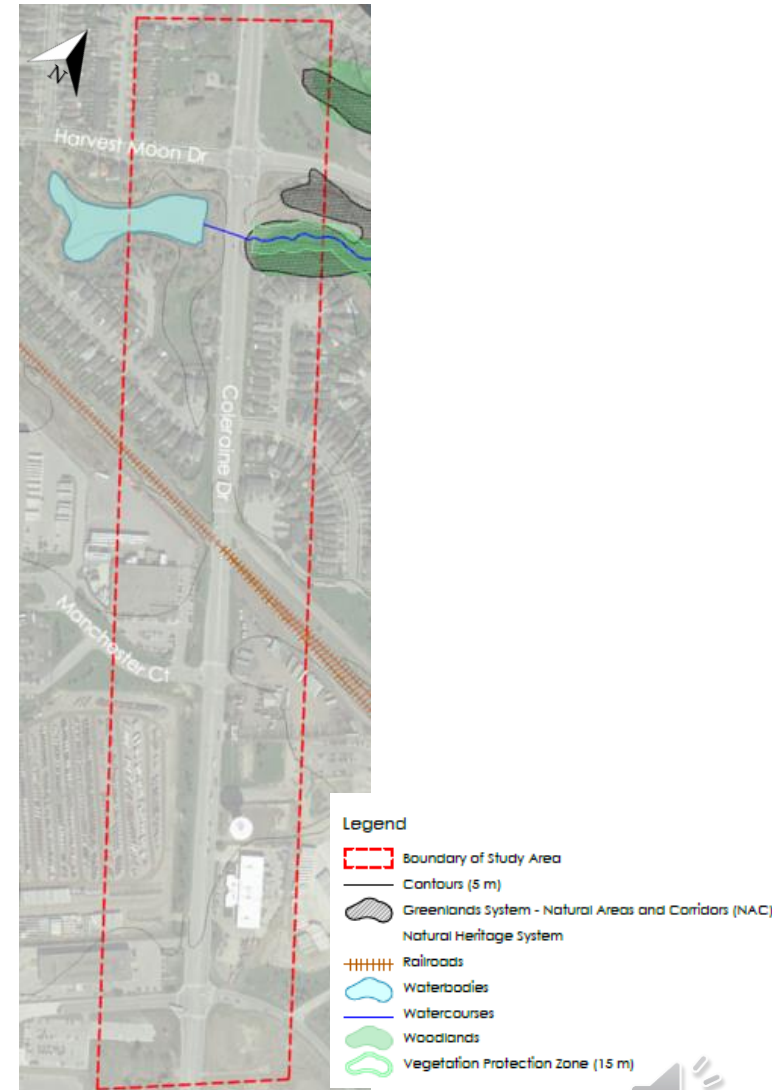
OLA = Outdoor Living Area

- Noise mitigation options for the Road Over Rail include raised roadside barriers.



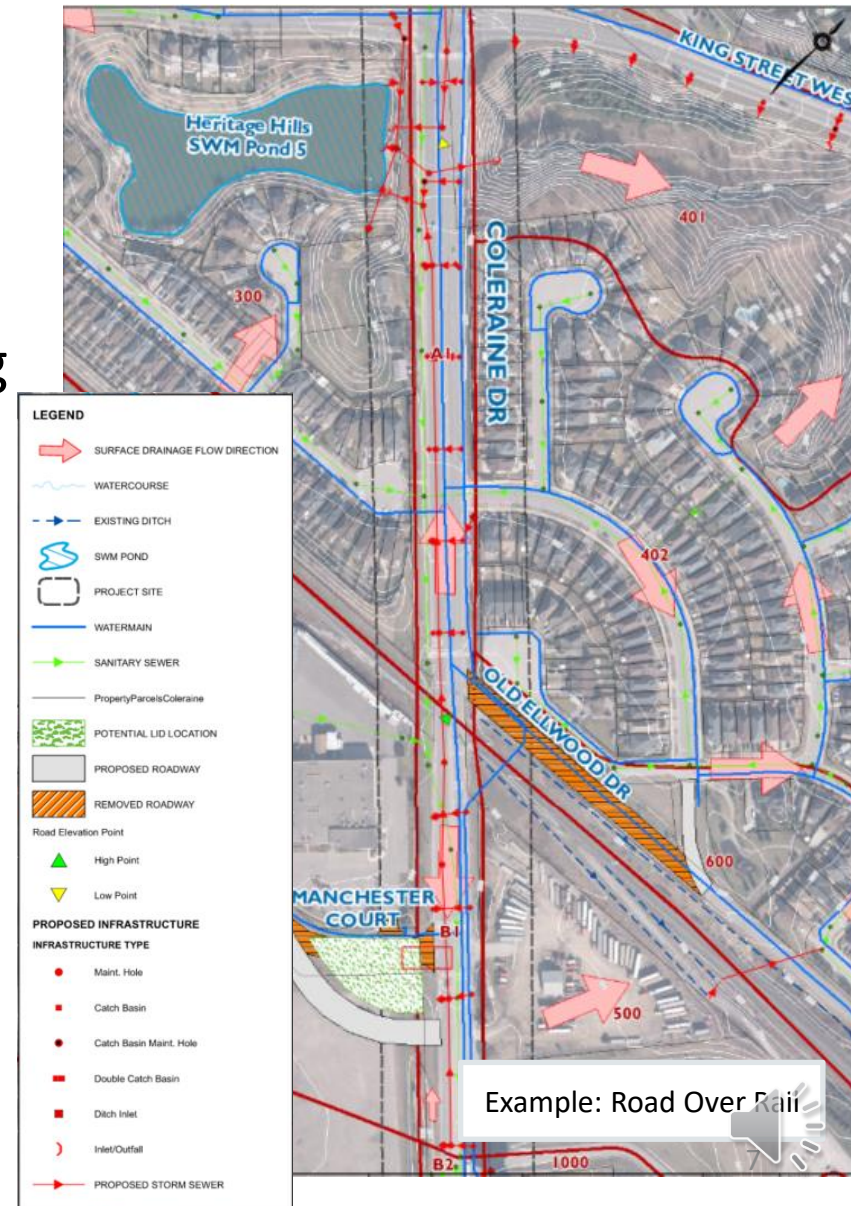
NATURAL ENVIRONMENT

- Study area within the Humber River Watershed and the Main Humber sub-watershed, regulated by Toronto and Region Conservation Authority (TRCA).
- Woodlands are located northeast of the study area.
- Stormwater pond west of Coleraine Drive does not support fish habitat.
- No species at risk (SAR) were observed.
- Mitigation measures are recommended for construction activities.



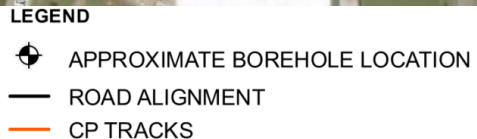
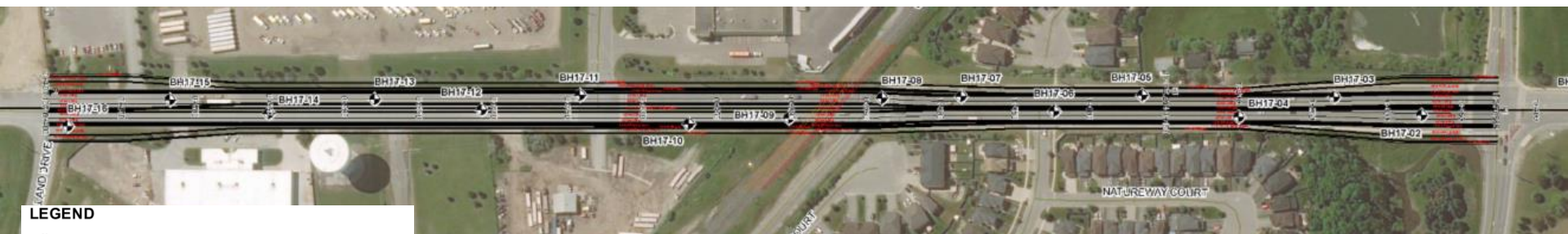
STORMWATER MANAGEMENT

- Both alternatives will increase impervious area within study area.
- Water quality, quantity and erosion controls will be provided by existing stormwater management pond, and proposed Low Impact Development (LID) facility at Manchester Court
- Storm sewer system is recommended to be upgraded to 10-year storm-event design standard, to improve drainage and help combat climate change.



HYDROGEOLOGY

- Assessment completed to determine the hydrogeological (groundwater) conditions of the area
- Groundwater table is shallow, and the soils are susceptible to erosion.
- Road Under Rail will require permanent measures (i.e., pumping) to mitigate environmental impacts including groundwater seepage and erosion;
 - **Key disadvantage** compared to Road Over Rail.



ARCHAEOLOGY & CULTURAL

- Archaeological Assessment

- Stage 1 Assessment identified no archaeological potential for most of the study area.
- Vegetated area at Holland Drive and Coleraine Drive intersection has archaeological potential and a Stage 2 Assessment is recommended.

- Cultural Heritage

- The Shore-Wakely House is designated and retains cultural heritage value.
(Not impacted by the proposed works)

LEGEND

- APPROXIMATE LOCATION OF STUDY AREA
- STAGE 2 TEST PIT AT 5m INTERVAL
- DESIGNATED HERITAGE SITE; FURTHER CULTURAL HERITAGE VALUE OR INTEREST



Shore-Wakely House

CONSTRUCTABILITY

ROAD OVER RAIL

- Simpler construction as deep excavation is not required.
- No impacts to rail operations
- Tall retaining walls required

ROAD UNDER RAIL

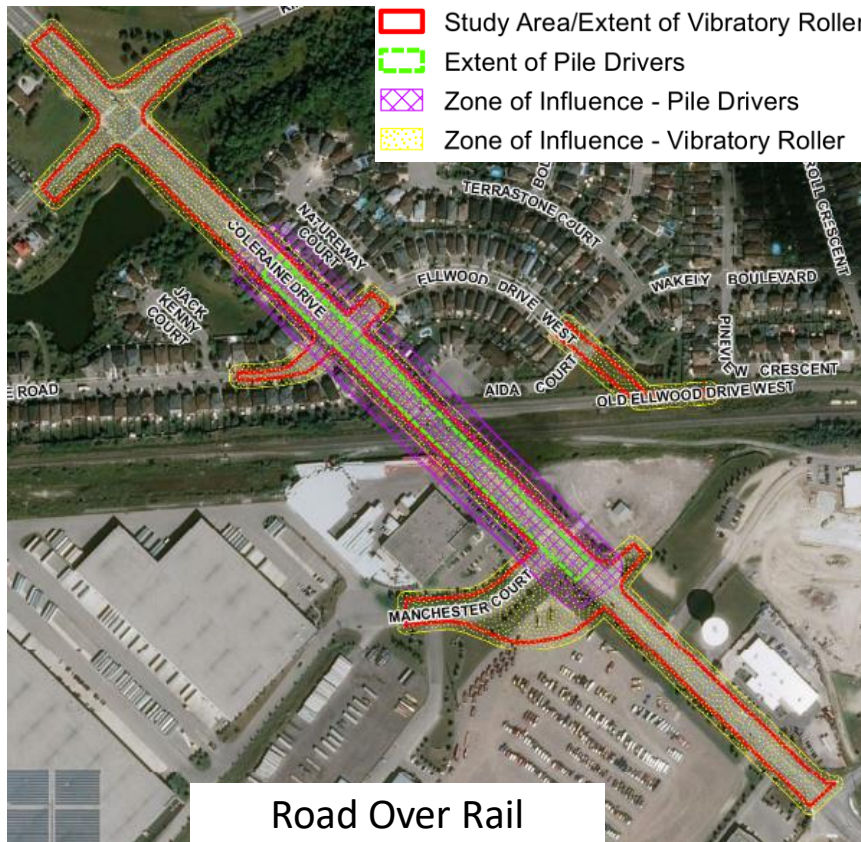
- Deep excavation (up to 9 m) required, increasing difficulty.
- Complex dewatering during construction.
- Requires permanent drainage system (i.e., pumping)
- Significant and costly impacts to rail operations (requires track diversion)
- Water table and Well impacts
- Potential for longer duration of construction

Less complex

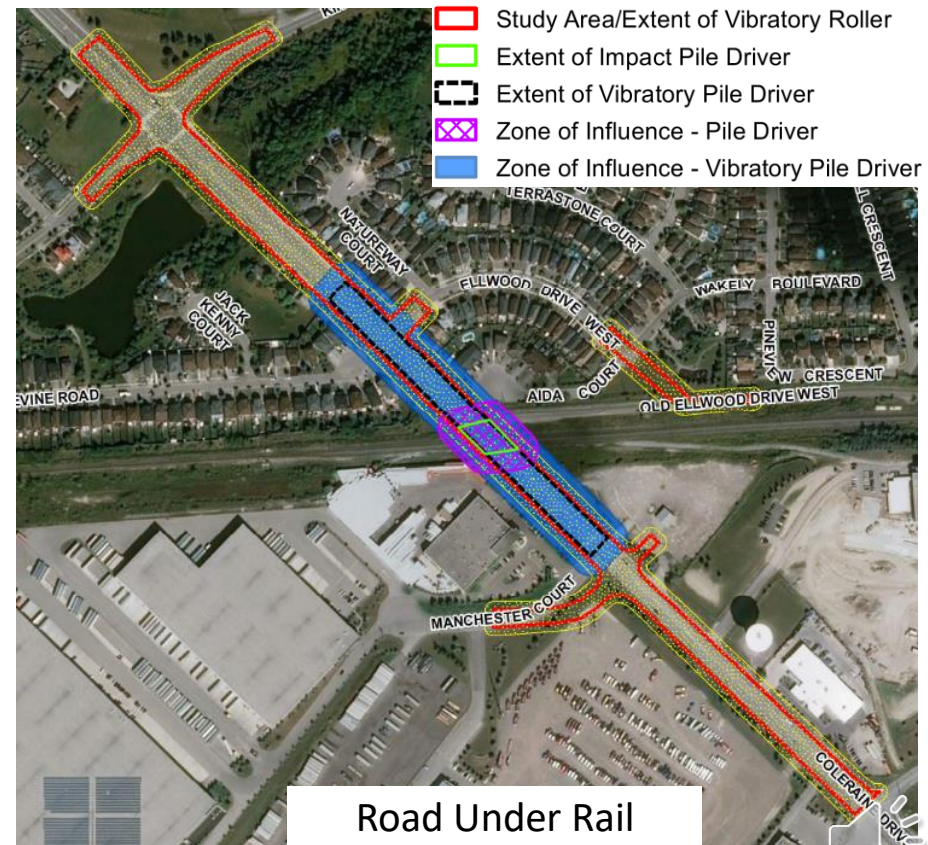
More complex

VIBRATION ASSESSMENT

- Vibration during construction anticipated to be below guideline limits.
- Induced ground vibrations during construction will be felt by surrounding residences



Road Over Rail
Zone of Influence



Road Under Rail
Zone of Influence

SHADOW IMPACT ANALYSIS

- A Shadow Impact Analysis was undertaken using Town of Caledon guidelines



MARCH
(Early evening)



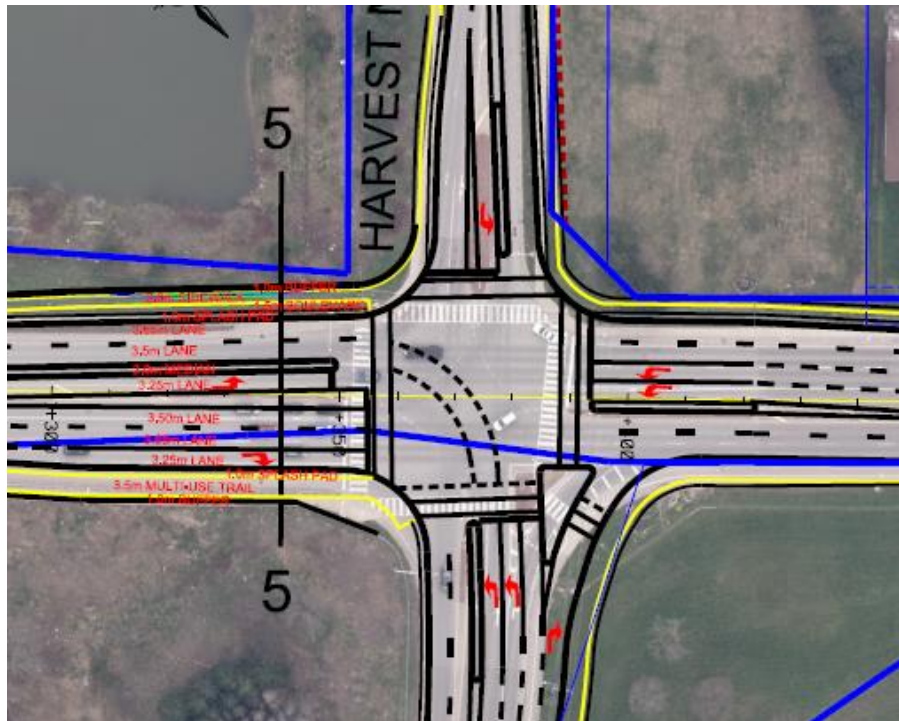
DECEMBER
(Early evening)

- Analysis concluded:
 - Shadow on neighbouring streets and sidewalks from the proposed road structure will be minimal and not considered an impact.
 - Shadow on neighbouring low-rise residential amenity spaces (i.e. gardens) east of will not meet the requirements of the Town's guidelines:
 - Some properties will have less than 5 consecutive hours of sunlight at certain times of the year and/or shadow on over 50% of their gardens.

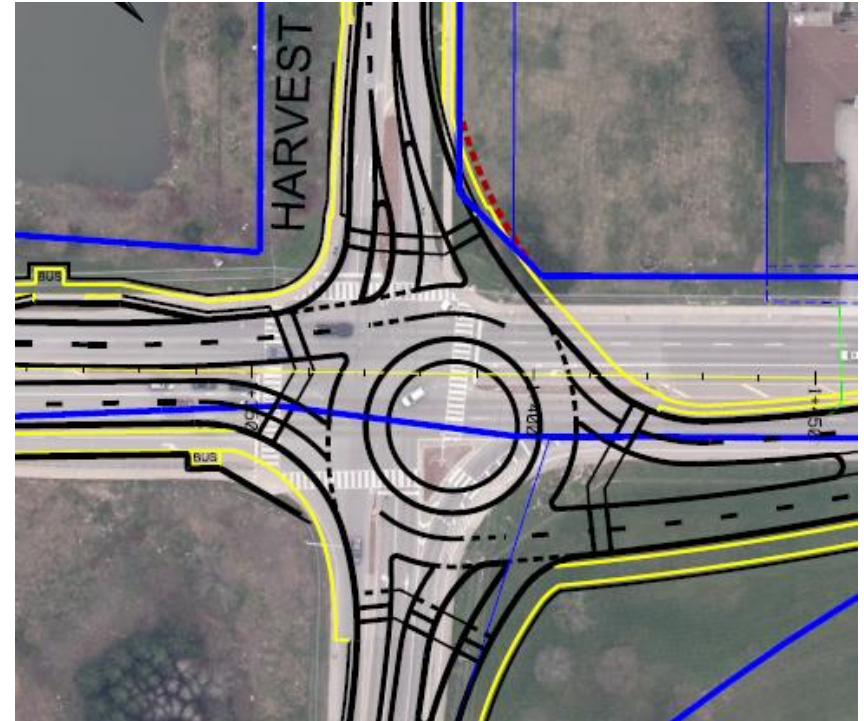


ALTERNATIVE DESIGN CONCEPTS FOR HARVEST MOON DR. / KING ST. & COLERAINE DR.

Two alternatives were considered for the intersection.




SIGNALIZED INTERSECTION IMPROVEMENTS



ROUNDBABOUT

HARVEST MOON DR. / KING ST. & COLERAINE DRIVE INTERSECTION EVALUATION

	Signalized	Roundabout
Key Advantages	<ul style="list-style-type: none"> Easier to construct. Provides slightly better operations during peak hours. 	<ul style="list-style-type: none"> Less impact to sensitive natural environment areas. More safety benefits, by always encouraging a slower speed. Reduces severity of collisions, although it may increase non-fatal collisions. More streetscaping potential. Less delay during off-peak hours. Less utility impacts.
Key Disadvantages	<ul style="list-style-type: none"> Collisions are more severe as vehicles only slowed/stopped by red light. More delay during off-peak hours. Little streetscaping potential. More utility impacts. 	<ul style="list-style-type: none"> Requires more construction staging. Slightly more property impacts.
	Not Recommended	Recommended 

NEXT STEPS

- Collect and respond to comments received, and tally results of the voting, to inform our decision on the "preferred alternative"
- Complete preliminary design of preferred alternative
- File Environmental Study Report (ESR) for EA approval, in 2022.
- The project will go to the detailed design in 2024, followed by property acquisitions and utility relocations. According to the 2021 Transportation Capital Budget, the construction of the project is scheduled to begin in 2029.

Please complete a comment sheet and submit all comments **by January 21:**

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Infrastructure Programming & Studies

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Public Information Centre #2 – Technical and Environmental Assessments Presentation – Narration

Slide 1

Hello, welcome to the for Public Information Centre #2 for the Municipal Class EA Study for the Coleraine Drive Grade Separation, undertaken by the Region of Peel.

This presentation is in addition to the Main Overview presentation and provides further details on the evaluation and technical assessments completed in support of the study.

We encourage you to provide feedback on the PIC using the comment boxes and voting tools on the PIC website.

Slide 2

I would like to acknowledge that the land on which we gather, and on which the Region of Peel operates, is part of the Treaty Lands and Territory of the Mississaugas of the Credit.

Peel Region - the traditional territory of the Anishinabek (Ojibway), Huron-Wendat, Haudenosaunee (Iroquois), and home to the Métis, was most recently, the territory of the Mississaugas of the Credit First Nation.

On this day our meeting place is still occupied by many Indigenous peoples (i.e., First Nations, Métis, and Inuit) from across Turtle Island (North America). We are grateful to have the opportunity to work on this land, and by doing so, we give our respect to its first inhabitants.

A shared understanding of how our collective past brought us to where we are today will help us walk together into a better future.

Slide 3

This slide summarizes the evaluation completed for the Road Under Rail and Road Over Rail options. The evaluation will be detailed in the final Environmental Study Report, completed at the end of this study.

The two design options were evaluated through key criteria:

From a Traffic perspective, both alternatives are similar in that they will eliminate queuing and improve operations

From a Natural Environment perspective, both alternatives will have minimal impact to natural resources in the study area

From a Stormwater Management and Groundwater perspective, Road Under Rail is less preferred as its excavation will result in significant drainage requirements (including an

increased impervious area and the requirement for a pumping station) and groundwater impacts. Road Under Rail configurations often encounter flooding.

From the Pedestrian and Cyclists perspective, both alternatives will accommodate similar upgraded facilities

From the Noise and Air Quality perspectives, both alternatives will have similar impacts

From the Archeological and Cultural Heritage perspectives, both alternatives will have similar impacts

In terms of Access and Property, both alternatives will have similar impacts however the Road Over Rail will have less desirable aesthetics due to the high retaining walls required. Mitigation measures include using exterior design approaches and landscaping (as presented in the Main Overview presentation) however there will be shadow impacts.

In terms of constructability and cost, the Road Under Rail is less desirable due to its large construction staging requirement, deep excavation requirements, need for a rail diversion and increased cost.

Overall, the Road Over Rail option has been identified as the preliminary preferred design alternative.

Slide 4

A Transportation and Traffic Study was completed as part of this study to review the existing and future traffic conditions in the study area.

With the increase in motor vehicle and train traffic, queues generated at the at-grade crossing are expected to extend beyond the intersection of Coleraine Drive & King Street West/Harvest Moon Drive in the future years. This issue is expected to become worse due to the planned extension of GO Train services to a new Bolton Station, increasing the frequency of trains during peak hours.

Benefits of including either Road Over Rail or Road Under Rail options include:

- Elimination of queuing and reduced risk of train collisions
- Reduction of intersections
- Improved truck network and reliability for movement of goods
- Opportunity for bicycle, pedestrian, and transit facility upgrades.

Slide 5

A Noise Assessment was undertaken to determine the potential noise impact on neighbouring noise sensitive areas, due to the improvements.

Noise sensitive areas include a range of land uses including private homes, townhouses, hospitals, etc., and the outdoor living area associated with them. Noise prediction modelling was completed to determine the potential noise impact and a summary of the predicted noise levels is shown in the table on the slide. As indicated, comparable noise levels are expected between the two options. The Road Over Rail levels assume mitigation measures are applied, and these include slightly higher roadside barriers.

Slide 6

A Natural Heritage Study was conducted to determine the presence and extent of natural heritage features and associated constraints in the study area. The study area is located within the Humber River Watershed and the Main Humber sub-watershed, regulated by the Toronto and Region Conservation Authority. The natural features in the study area are shown in the map on the right side of the slide. Woodlands have been identified in the northeast portion of the study area and there is a stormwater pond west of Coleraine Drive. A perched culvert outlet drains into the pond. The pond does not support fish habitat because the culvert is a barrier to fish species. No Species at Risk were observed during field assessment. The alternatives were evaluated based on the habitats, species, and features identified in the study area. Measures to mitigate impacts to the environment are recommended for construction activities.

Slide 7

Both the Rail Under Road and Rail Over Road alternatives will increase the imperious areas in the study area. Water quality, quantity and erosion controls will be provided by the existing stormwater management pond west of Coleraine Drive and a proposed Low Impact Development stormwater management facility near Manchester Court. The storm sewer system is recommended to be upgraded to accommodate a 10-year design storm to improve drainage and help combat climate change.

Slide 8

A Hydrogeological Assessment was completed to determine the hydrogeological conditions in the study area. Boreholes were advanced along Coleraine Drive revealed soil characterized by a low hydraulic conductivity. The groundwater table is relatively shallow and the soils are potentially susceptible to internal erosions. Therefore, the Road Under Rail grade separation will require measures to permanently manage groundwater seepage and systems to prevent ground loss or erosion. This is a key disadvantage to the Road Under Rail option compared to Road Over Rail.

Slide 9

A Stage 1 Archaeological Assessment was conducted to evaluate the study area's archeological potential and recommend strategies for Stage 2 assessment, if required.

The archaeological potential of the study area was evaluated through an examination of its geography, history, previous archaeological fieldwork and current land conditions.

The Stage 1 Assessment determined that the study area did have some archaeological potential but that this potential has been removed for most parts of the area due to deep and extensive disturbances resulting from past urban and industrial development, or sloped areas. Some sections of manicured lawn and overgrown fields still retain archaeological potential, such as the vegetated area at the intersection of Holland Drive and Coleraine Drive. Stage 2 assessment is recommended for this area.

A Cultural Heritage Assessment Report was prepared to research and evaluate properties for their cultural heritage value or interest.

The Cultural Heritage Assessment Report determined there is a protected heritage property and property of culture heritage value or interest in the study area. This heritage property, known as the Shore-Wakely Stone house, is designated under the Ontario Heritage Act for its architectural and historical value, and will not be impacted by the proposed works.

Slide 10

An assessment of constructability was also undertaken between the two design options. Constructability refers to the ease and efficiency with which the modifications can be built.

The Road over Rail alternative was found to be less complex to construct overall compared with the Road under Rail alternative because it does not require deep excavation, the requirement of a pumping station, or a rail diversion which increases construction requirements and cost. The Road Under Rail option therefore also has the potential for a longer duration of construction.

Slide 11

A Vibration Assessment was carried out for the study to analyze the Zone of Influence impacted by ground vibrations produced during the anticipated construction operations and their potential impact on neighbouring residential structures.

Machinery of high vibration levels, such as pile drivers and vibratory rollers will likely be used during construction and the vibrations from these are anticipated to be below standard vibration control limits, however, the induced ground vibrations will be felt at surrounding residences. It will be recommended that vibration monitoring be conducted for the duration of construction activities.

Slide 12

A Shadow Impact Analysis was also undertaken as part of this study. The analysis followed the Requirements for Shadow Studies, as outlined by the Town of Caledon's Town-Wide Design

Guidelines. Example images of the analysis are shown in this slide, with the details included in the Shadow Impact Analysis Report that is available on the region's project website.

The analysis concluded that there will be minimal shadow on neighbouring streets and sidewalks, and therefore not considered an impact by the guidelines.

The shadow on properties adjacent to Coleraine Drive, particularly to the east, will not meet the requirements of the Town's guidelines. The analysis identified that some properties will have less than five consecutive hours of sunlight at certain times of the year and/or shadow on over 50% of their gardens.

Slide 13

As presented in the Main Overview presentation, typical signalized intersection improvements or a reconfiguration into a roundabout were considered for the intersection of Coleraine Drive at Harvest Moon Drive and King Street. These intersection improvements are independent of the Road Under Rail or Road Over Rail design alternatives. The two intersection improvement alternatives were evaluated, and the key advantages and disadvantages of each alternative are discussed on the following slide.

Slide 14

Ultimately, a reconfiguration into a roundabout was selected as preferred as it provides safety benefits by encouraging slower speeds and reducing the severity of collisions, reduces delay during off peak hours compared with a conventional signalized intersection, provides for more streetscaping potential, and has less impact to utilities. The roundabout does have a larger footprint compared with the signalized intersection, however the footprint has less impact to sensitive natural features in the area.

Slide 15

The next steps in the study include completing the preliminary design of the preliminary preferred alternative and preparing an Environmental Study Report to document the study recommendations and decision-making process.

The Environmental Study Report will be made available for a minimum 30-day public review period at the end of the study. Following approval of this Environmental Assessment study, the project will move into a detailed design phase. It is anticipated that the project will go to the detailed design in 2024, followed by property acquisitions and utility relocations. According to the 2021 Transportation Capital Budget, the construction of the project is scheduled to begin in 2029.

We are looking forward to hearing your feedback on the materials presented at this Public Information Centre and to provide your input and feedback, please fill-in the comment boxes and vote in the polls available on the PIC website.

If you have any questions or would like an alternate way to provide feedback, please contact either project manager via the contact information shown here and on the PIC website.

Criteria for Identification and Evaluation of Alternative Design Concepts

Coleraine Drive Grade Separation Evaluation Criteria

Evaluation Criteria	Measures
Transportation	
Traffic Operations	<ul style="list-style-type: none"> • Peak Hour / Off Peak Performance
Traffic Safety	<ul style="list-style-type: none"> • Potential for collisions
Natural Environment	
Environmentally Sensitive Areas	<ul style="list-style-type: none"> • Significant woodlands, wetlands, protection areas
Slope Stability	<ul style="list-style-type: none"> • Impacts to slope located at southeast corner of King St. and Coleraine Drive intersection.
Tree Impacts	<ul style="list-style-type: none"> • Removal and protection of trees
Stormwater Management	
Stormwater Management	<ul style="list-style-type: none"> • Quality and quantity control
Groundwater	<ul style="list-style-type: none"> • Dewatering for bridge construction
Climate Change	<ul style="list-style-type: none"> • Flooding risks
Healthy Communities	
Active Transportation	<ul style="list-style-type: none"> • Pedestrian and Cyclist infrastructure
Air Quality	<ul style="list-style-type: none"> • Vehicle emissions
Noise	<ul style="list-style-type: none"> • Vehicles, trains
Socio-Economic Environment	
Archaeology	<ul style="list-style-type: none"> • Burial sites or artifacts
Cultural and Built Heritage	<ul style="list-style-type: none"> • Heritage properties or structures
Property Impacts	<ul style="list-style-type: none"> • Property Acquisition
Land Use / Property Access	<ul style="list-style-type: none"> • Road Closures / Realignment
Aesthetics	<ul style="list-style-type: none"> • Streetscaping / Views of residents

Evaluation Criteria	Measures
Constructability / Engineering	
Utilities	<ul style="list-style-type: none"> • Relocations
Geometry	<ul style="list-style-type: none"> • Design Standards
Construction Staging	<ul style="list-style-type: none"> • Detours / Rerouting / Closures
Construction Cost	<ul style="list-style-type: none"> • Dollar Value

Coleraine Drive and King Street/Harvest Moon Drive Intersection Evaluation Criteria

Evaluation Criteria	Measures
Traffic Operations	<ul style="list-style-type: none"> • Peak Hour • Off Peak Performance
Traffic Safety	<ul style="list-style-type: none"> • Potential for collisions
Pedestrian Accommodation	<ul style="list-style-type: none"> • Crossings • Exposure Time
Cyclist Accommodation	<ul style="list-style-type: none"> • Facilities • Crossings • Exposure
Natural Environment	<ul style="list-style-type: none"> • Environmentally Sensitive Areas • Slope Stability
Socio-Economic Environment	<ul style="list-style-type: none"> • Property Impacts • Speed Control • Streetscaping
Constructability/Engineering	<ul style="list-style-type: none"> • Utilities • Geometry • Construction Staging • Cost



Detailed Evaluation of Alternative Design Concepts Coleraine Drive Grade Separation and King Street/Harvest Moon Drive Intersection

1. INTRODUCTION

The purpose of this memo is to summarize the assessment and evaluation of the alternative design concepts identified by the Coleraine Drive Grade Separation EA, undertaken by the Region of Peel.

The study investigated 1) design concepts for the Coleraine Drive / King Street and Harvest Moon Drive intersection, and 2) grade separation concepts for the rail tracks on Coleraine Drive, near Old Ellwood Drive in the Town of Caledon. As the improvement concepts would not influence each-other's design, alternative concepts were generated separately for each.

2. COLERAINE DRIVE AND KING STREET/HARVEST MOON DRIVE INTERSECTION

Two alternatives were identified for the Coleraine Drive / King Street and Harvest Moon Drive intersection:

1. Signalized Intersection with dual left turn lanes on Southbound and Westbound approaches
2. Two-Lane Roundabout

The evaluation criteria used is summarized in **Table 1** and the detailed assessment and evaluation is included in **Table 3**.

Table 1 Coleraine Drive / King Street and Harvest Moon Drive Intersection Evaluation Criteria

Evaluation Criteria	Measures
Traffic Operations	Peak Hour / Off Peak Performance
Traffic Safety	Potential for collisions
Pedestrian Accommodation	Crossings / Exposure Time
Cyclist Accommodation	Facilities / Crossings / Exposure
Natural Environment	Environmentally Sensitive Areas and Slope Stability
Socio-Economic Environment	Property Impacts/ Speed Control / Streetscaping
Constructability/Engineering	Utilities / Geometry / Construction Staging / Cost

As detailed in the table, **Alternative #2 – Roundabout** was identified as the overall preferred alternative.

Coleraine Drive Grade Separation EA - Assessment and Evaluation of Alternative Design Concepts

3. COLERAINE DRIVE GRADE SEPARATION

Two alternatives were identified for the Coleraine Drive grade separation:

1. Road Under Rail
2. Road Over Rail

The evaluation criteria used is summarized in **Table 2** and the detailed assessment and evaluation is included in **Table 4**.

Table 2 Coleraine Drive Grade Separation Evaluation Criteria

Evaluation Criteria	Measures
Transportation	
Traffic Operations	Peak Hour / Off Peak Performance
Traffic Safety	Potential for collisions
Natural Environment	
Environmentally Sensitive Areas	Significant woodlands, wetlands, protection areas
Slope Stability	Impacts to slope located at southeast corner of King St. and Coleraine Drive intersection.
Tree Impacts	Removal and protection of trees
Stormwater Management	
Stormwater Management	Quality and quantity control
Groundwater	Dewatering for bridge construction
Climate Change	Flooding risks
Healthy Communities	
Active Transportation	Pedestrian and Cyclist infrastructure
Air Quality	Vehicle emissions
Noise	Vehicles, trains
Socio-Economic Environment	
Archaeology	Burial sites or artifacts
Cultural and Built Heritage	Heritage properties or structures
Property Impacts	Property Acquisition
Land Use / Property Access	Road Closures / Realignments
Aesthetics	Streetscaping / Views of residents
Constructability / Engineering	
Utilities	Relocations
Geometry	Design Standards
Construction Staging	Detours / Rerouting / Closures
Construction Cost	Dollar Value

As detailed in the table, **Alternative #2 – Road Over Rail** was identified as the overall preferred alternative. Plans of the alternative design concepts are attached to this memo.

Table 4 Coleraine Drive Grade Separation– Detailed Evaluation and Assessment

Criteria	Do Nothing	Alternative 1 - Road Under Rail	Alternative 2 - Road Over Rail
Transportation			
Traffic Operations	<p>With an increase in motor vehicle and train traffic projected for 2041, queues generated at the at-grade crossing are expected to extend beyond the intersection of Coleraine Drive & King Street West/Harvest Moon Drive.</p> <p>This issue is expected to be more intense as the frequency of trains during peak hours increases, particularly due to the planned extension of GO Train service to a new Bolton station.</p> <p>No opportunities to improve traffic operations and eliminate queues generated at the at-grade rail crossing.</p>	<p>No queues generated at the rail crossing. Eliminates the potential for queues backing up to the King Street and Coleraine Drive intersection.</p>	<p>No queues generated at the rail crossing. Eliminates the potential for queues backing up to the King Street and Coleraine Drive intersection.</p>
Traffic Safety - Vehicular	<p>No opportunity to improve traffic safety.</p>	<p>Eliminates potential vehicle conflicts at the Coleraine Drive and Old Ellwood Drive intersection and the Coleraine Drive and Ellwood Drive intersection because Old Ellwood Drive and Ellwood Drive are closed at Coleraine Drive.</p>	<p>Eliminates potential vehicle conflicts at the Coleraine Drive and Old Ellwood Drive intersection and the Coleraine Drive and Ellwood Drive intersection because Old Ellwood Drive and Ellwood Drive are closed at Coleraine Drive.</p>
Transportation Summary	<p>Both Alternatives improve operations to a similar extent, by resulting in no queuing at the rail crossing and eliminates potential of queues backing up to King Street and Coleraine Drive Intersection. Both alternatives have similar safety benefits, in the reduction of intersections resulting in reduction of conflict points. Both alternatives are the same from the Transportation perspective.</p>		
Natural Environment			
Environmentally Sensitive Areas	<p>No impact to the Greenlands System (Natural Areas and Corridors), Woodlands and Vegetation Protection Zone.</p>	<p>Avoids impact to the Greenlands System (Natural Areas and Corridors), Woodlands and Vegetation Protection Zone.</p>	<p>Avoids impact to the Greenlands System (Natural Areas and Corridors), Woodlands and Vegetation Protection Zone.</p>
Slope Stability	<p>No impact to the slope located at the southeast corner of the King Street and Coleraine Drive intersection.</p>	<p>Avoids impact to the slope located at the southeast corner of the King Street and Coleraine Drive intersection.</p>	<p>Avoids impact to the slope located at the southeast corner of the King Street and Coleraine Drive intersection.</p>

Criteria	Do Nothing	Alternative 1 - Road Under Rail	Alternative 2 - Road Over Rail
Tree Impacts	No impact to trees.	Potential impact to trees on the west side of Coleraine Drive north of Grapevine Road and at the relocated Manchester Court. There are similar impacts between each alternative. Construction management measures can be implemented during construction to minimize impacts to trees.	
Natural Environment Summary	Both alternatives impact natural environment resources to a similar extent as there are similar potential impacts to trees, which will be recommended to be mitigated through construction management measures, and similar avoidance to the Greenlands System, Woodlands and Vegetation Protection Zone and natural slopes. Both alternatives are the same from the Natural Environment perspective.		
Stormwater Management			
Stormwater Management	<p>Portions of the existing roadway north of the CP railway currently discharge to the Heritage Hills SWM Pond 5 for both water quality and quantity.</p> <p>Portions of the existing roadway south of the CP railway currently do not provide any stormwater management for quality and quantity.</p> <p>No improvements to stormwater discharge.</p>	<p>More complex option for accommodating stormwater.</p> <p>Minor stormwater flow will change directions from existing southerly direction and to north with the major flow. This will increase the amount of water that needs to be attenuated/treated.</p> <p>Water must be pumped from the underpass to either a discharge point located within the existing pond or an existing storm sewer system. There is likely no opportunity to have the water discharge by gravity. Therefore, a pump house would be required.</p> <p>There is an opportunity to mitigate stormwater impacts by discharging to the existing Heritage Hills SWM Pond 5.</p> <p>The low point in the roadway will change from the stormwater management pond to the CP underpass. Safe pedestrian and vehicle ingress/regress will need to be considered during major storm events should the roadway be overtopped.</p> <p>Stormwater management analysis will need to be performed to assess the capacity of existing stormwater management pond and ensure that there is sufficient capacity to handle any new flows, whether from rerouting or an increase in impervious area, to the stormwater management pond.</p>	<p>Less complex option for accommodating stormwater.</p> <p>Minor storm drainage could remain the same as existing conditions.</p> <p>The new high point in the roadway caused by the roadway/bridge would change major drainage flows.</p> <p>The impact to downstream receiving capacities would need to be mitigated.</p> <p>There is an opportunity to mitigate stormwater impacts by discharging flows to the existing Heritage Hills SWM Pond 5.</p> <p>The low point in the roadway would remain the same, and safe pedestrian and vehicle ingress/regress not a concern in major storm events should the roadway be overtopped.</p> <p>Stormwater management analysis will need to be performed to assess the capacity of existing stormwater management pond and ensure that there is sufficient capacity to handle any new flows, whether from rerouting or an increase in impervious area, to the stormwater management pond.</p> <p>Road over rail (i.e., bridge) is more susceptible to roadway icing and freezing.</p>

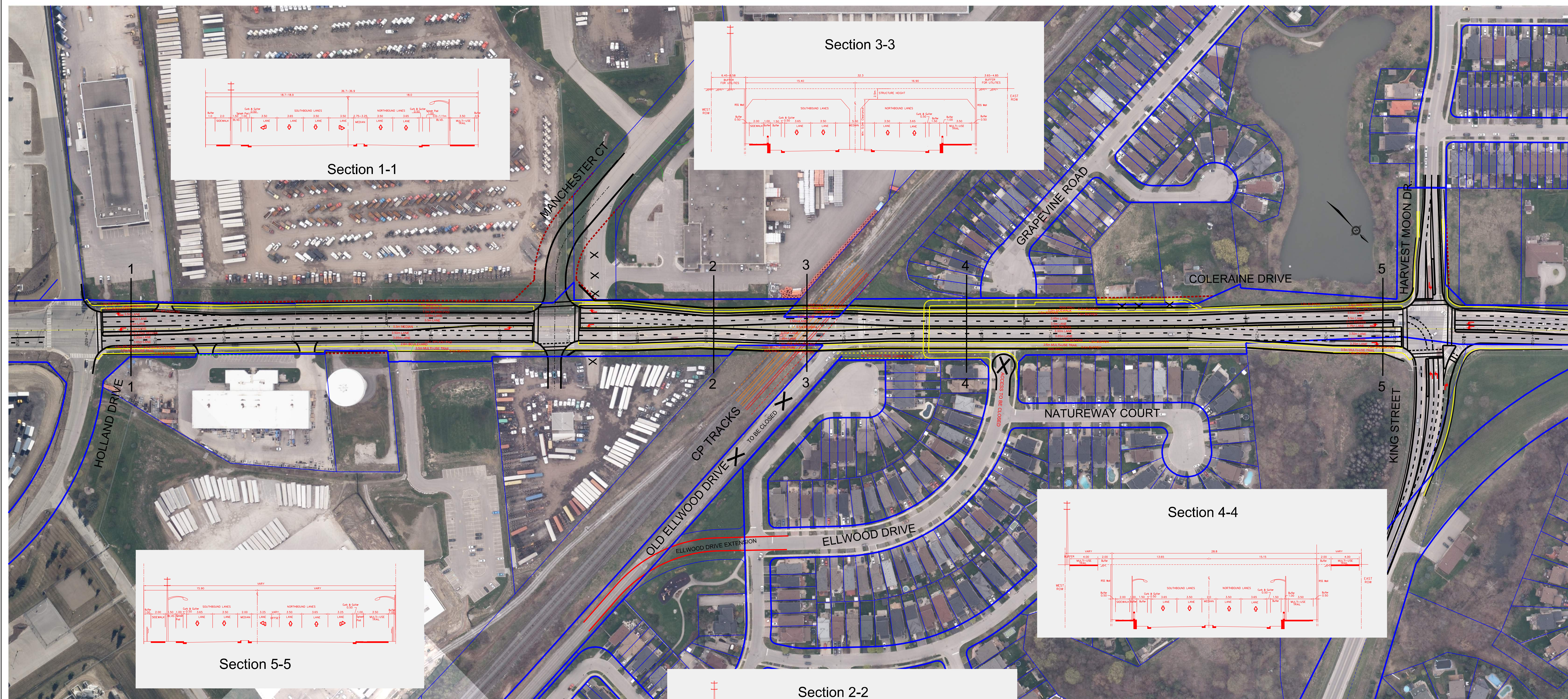
Criteria	Do Nothing	Alternative 1 - Road Under Rail	Alternative 2 - Road Over Rail
Groundwater	No impact.	High potential groundwater impacts due to temporary dewatering during construction. Earth excavation for roadway could permanently lower groundwater table.	Potential groundwater impacts due to temporary dewatering for bridge foundation during construction.
Climate Change	No improvements to stormwater infrastructure to improve resilience.	Alternative more susceptible to flooding.	Opportunity to improve resilience of stormwater infrastructure.
Stormwater Management Summary	Alternative 1 – Road Under Rail is less preferred than Alternative 2 – Road Over Rail, due to the complex stormwater management requirements, including the need for a permanent pumping system, and due to its higher potential of groundwater impacts, as a result of the deep excavations required. Also, Alternative 2 will provide the opportunity to improve the stormwater infrastructure in the area. Due to the significant impacts of Alternative 1, Alternative 2 – Road Over Rail is preferred from the Stormwater Management perspective.		
Healthy Communities			
Active Transportation - Pedestrians and Cyclists	No opportunity to improve pedestrian and cyclist safety. No pedestrian facilities provided south of the rail crossings. No dedicated bicycle facilities provided along Coleraine Drive. No protected pedestrian/cyclist rail crossing.	Provides dedicated pedestrian facilities along the full length of the corridor. Provides shared cycling facilities (multi-use trail) along the full length of the corridor. A grade separated trail crossing is provided across Coleraine Drive. Provides a protected crossing for cyclists and pedestrians.	Provides dedicated pedestrian facilities along the full length of the corridor. Provides shared cycling facilities (multi-use trail) along the full length of the corridor. A trail crossing can be provided across Coleraine Drive by underpass from Old Ellwood Drive. Alternatively, an at-grade crossing between Ellwood Drive and Grapevine Road could be provided by way of the road connection (shared between vehicles and bicycles).
Air Quality	Air quality could decrease over time due to additional vehicle delays and queuing.	The grade separation will act to minimize the air quality impact of increased traffic through improved traffic flows and reduced queuing times at intersections within the local vicinity of the project. As a result, the impact is anticipated to be positive.	
Noise	Noise levels at Outdoor Living Areas (i.e., sensitive receptor locations) will increase as traffic volumes increase.	Predicted noise levels with the road under rail alternative do not exceed the expected future noise levels without the project (i.e., the noise levels will not exceed the future 'Do Nothing' scenario levels). Comparable noise levels are expected at Outdoor Living Areas between the two grade separation alternatives. Mitigation measures will be required to reduce potential 'tunneling effect'.	Predicted noise levels with the road over rail alternative do not exceed the expected future noise levels without the project (i.e., the noise levels will not exceed the future 'Do Nothing' scenario levels). With the consideration of mitigation measures such as a localized barrier, comparable noise levels are expected at Outdoor Living Areas between the two grade separation alternatives.

Criteria	Do Nothing	Alternative 1 - Road Under Rail	Alternative 2 - Road Over Rail
Healthy Communities Summary	Both alternatives will help improve the air quality in the area due to the elimination of vehicles queuing at the rail crossing. The alternatives also provide the opportunity to improve the active transportation infrastructure in the area thus improving the safety of pedestrians/cyclists. Both alternatives will have a similar noise levels which will not exceed the future “Do Nothing” scenario. Both alternatives are the same from the Healthy Communities perspective		
Socio-Economic Environment			
Archaeology	No impact.	Portions of the study area require Stage 2 Archaeological Assessment (south of Holland Drive).	Portions of the study area require Stage 2 Archaeological Assessment (south of Holland Drive).
Cultural and Built Heritage	No impact.	No impact to any heritage properties or properties of cultural heritage value or interest.	No impact to any heritage properties or properties of cultural heritage value or interest.
Property Impacts	No impact to private property.	Property is required at the following locations: <ul style="list-style-type: none"> • Northwest corner of the King Street and Coleraine Drive intersection • West side of Coleraine Drive north of Grapevine Road • Adjacent to Coleraine Drive south of Manchester Court • For the Manchester Court realignment. Property requirements are the same as the road over rail alternative.	Property is required at the following locations: <ul style="list-style-type: none"> • Northwest corner of the King Street and Coleraine Drive intersection • West side of Coleraine Drive north of Grapevine Road • Adjacent to Coleraine Drive south of Manchester Court • For the Manchester Court realignment. Property requirements are the same as the road under rail alternative.
Land Use/ Property Access	No impact to property access.	Residential access will not be permitted onto Coleraine Drive. No potential for a direct connection between Ellwood Drive west and Grapevine Road due to geometric challenges, and so the relatively large subdivision east of Coleraine Road would only have one access (at the Station Road and Wakely Blvd Intersection). Lack of connection would also hamper emergency service response and routing. Access to commercial properties will be maintained. Some accesses will be restricted to right-in-right-out movements only. Old Ellwood Drive will be closed upstream of Coleraine Drive.	Residential access will not be permitted onto Coleraine Drive. Potential for a direct connection between Ellwood Drive west and Grapevine Road – with the existing Ellwood Drive West and Coleraine Road intersection closed, the relatively large subdivision east of Coleraine Road would only have one access (at the Station Road and Wakely Blvd Intersection). Access to commercial properties will be maintained. Some accesses will be restricted to right-in-right-out movements only. Old Ellwood Drive will be closed upstream of Coleraine Drive.

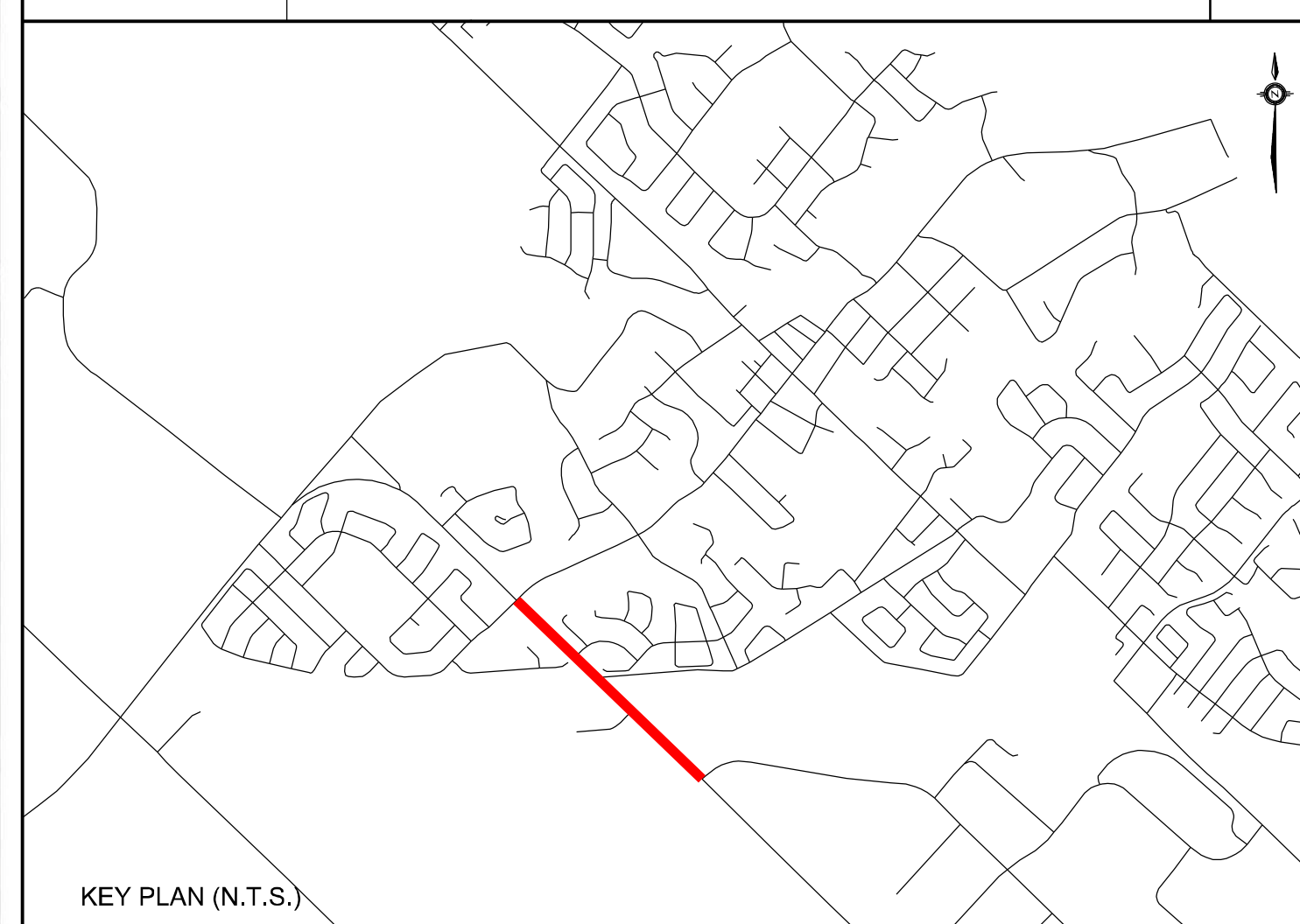
Criteria	Do Nothing	Alternative 1 - Road Under Rail	Alternative 2 - Road Over Rail
		Manchester Court will be realigned to accommodate the grade change of Coleraine Drive (less significant realignment than road over rail alternative).	Manchester Court will be realigned to accommodate the grade change of Coleraine Drive (more significant realignment than road under rail alternative).
Aesthetics	No impact.	More desirable aesthetics as no new bridge structure is required.	Less desirable aesthetics as road over rail option requires a new bridge structure located adjacent to residential properties. The close location will result in shadow impacts to adjacent properties.
Socio-Economic Environment Summary	Both alternatives will require a Stage 2 Archaeology Assessment but will not have any impact on Cultural/Built Heritage properties within the area. Similar property requirements are needed for both alternatives, however Alternative 2 will require a slightly larger requirements due to the larger realignment of Manchester Court. Alternative 1 will have significant impacts to accessibility within the local road network, due to the preclusion of a connection between Ellwood Drive West and Grapevine Road. In terms of aesthetics, Alternative 2 will be much less desirable for residents due to the bridge structure located adjacent to homes and due to shadow impacts. Overall, Alternative 2 – Road Over Rail is preferred from the Socio-Economic perspective.		
Constructability/Engineering			
Utilities	No impact.	Requires relocation of the following utilities: <ul style="list-style-type: none"> • Hydro facilities on the west side of Coleraine Drive • 1050 watermain • 250 PVC sanitary pipeline Offers less available space to accommodate utility relocations. Two stage relocation may be required during construction to protect facilities (i.e., water supply).	Requires relocation of the following utilities: <ul style="list-style-type: none"> • Hydro facilities on the west side of Coleraine Drive • 1050 watermain • 250 PVC sanitary pipeline Offers more available space to accommodate utility relocations.
Geometry (i.e., design speed, minimum radius, maximum grade, etc.)	No impact.	Both alternatives conform to TAC and Municipal Standards, including accommodating trucks and active transportation facilities.	
Construction Staging	No construction staging required.	More complex construction staging compared to road over rail alternative, due to extensive excavation required (9 m depth) Construction staging would require the temporary rerouting of rail tracks.	Less complex construction staging compared to road under rail alternative. Construction staging would not require the temporary rerouting of rail tracks.

Criteria	Do Nothing	Alternative 1 - Road Under Rail	Alternative 2 - Road Over Rail
Construction Cost	No cost.	Significant construction cost compared to the road over rail alternative - \$56M	Lesser construction cost compared to the road under rail alternative - \$36M
Constructability/Engineering Summary	Alternative 1 – Road Under Rail will be much more complex to design and construct due to the utility relocation challenges and rail rerouting. This will result in a much more expensive construction cost for Alternative 1. Therefore, from a Constructability/Engineering perspective, Alternative 2 – Road Over Rail is preferred.		
Summary of Evaluation			
Overall Summary	<p>Alternative 2 – Road Over Rail is the preferred alternative.</p> <p>Both alternatives have similar impacts regarding mitigating operational issues (Transportation), vegetation and tree impacts (Natural Environment), and noise and air quality impacts (Healthy Communities). While the Road Over Rail is less desirable aesthetically and would result in shadow impacts (Socio-Economic Environment), the Road Under Rail has significant disadvantages by requiring large drainage/stormwater management requirements, including pumping, and groundwater impacts (Stormwater Management), as well as a large construction staging requirement, including temporary tracks. The large construction staging requirements and impacts also result in a significant increased cost for Alternative 1 – Road Under Rail (\$56M) compared to Alternative 2 – Road Over Rail (\$36M).</p>		
Recommendation	Not Recommended	Not Recommended	Recommended

Very Low Impact (Most Positive)	Fairly Low Impact	Medium/Ambivalent Impact	Fairly High Impact	Very High Impact (Least Positive)



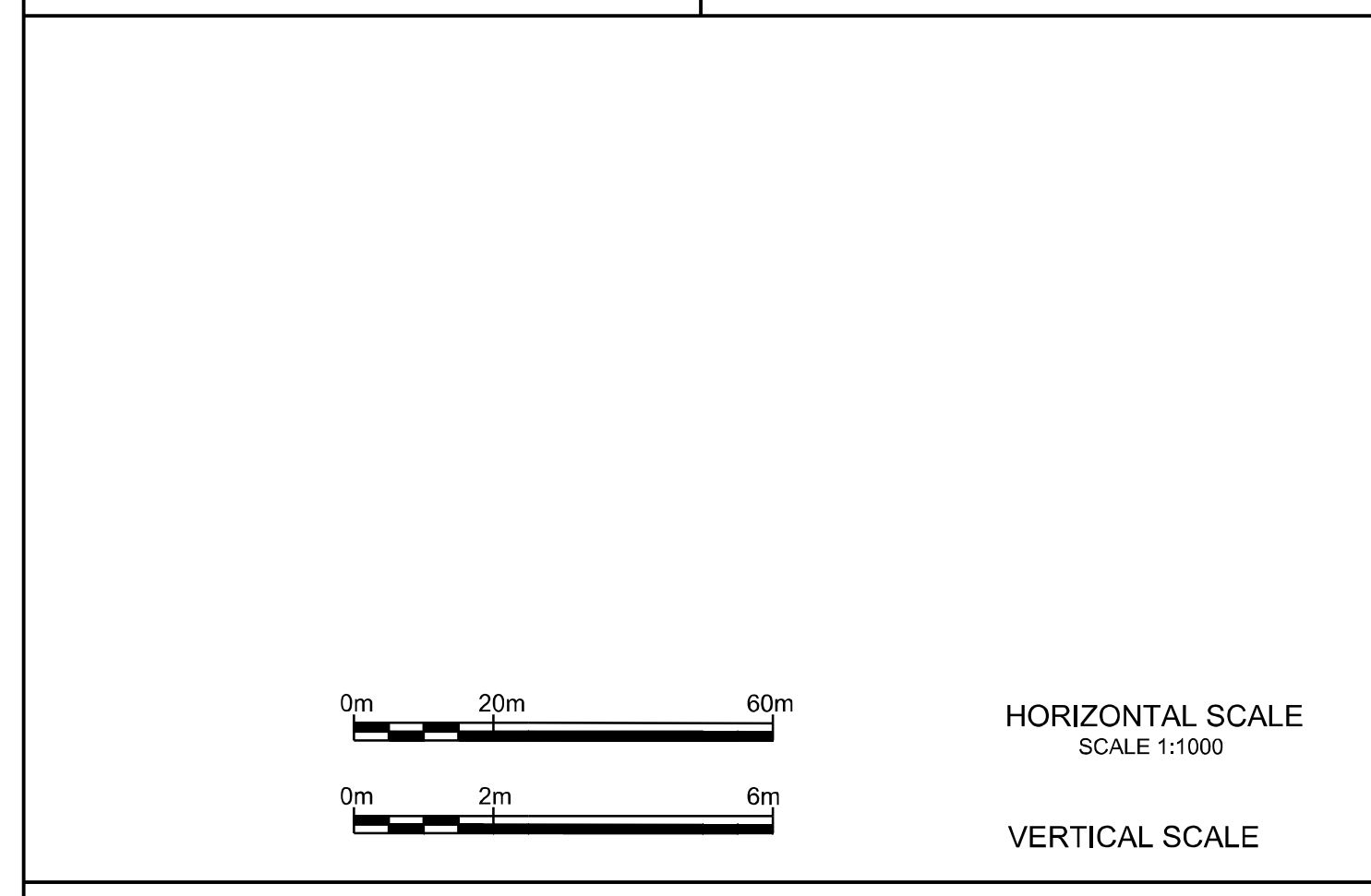
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SERVICE	DATE	INIT.	SERVICE	DATE	INIT.
REVISIONS					
DATE	DETAILS			INIT.	
September 2019	ISSUED FOR REVIEW			S.K.	
June 2020	REVISED FOR REVIEW			S.K.	



General Notes

Designed by _____ Chkd. _____

Approved by _____

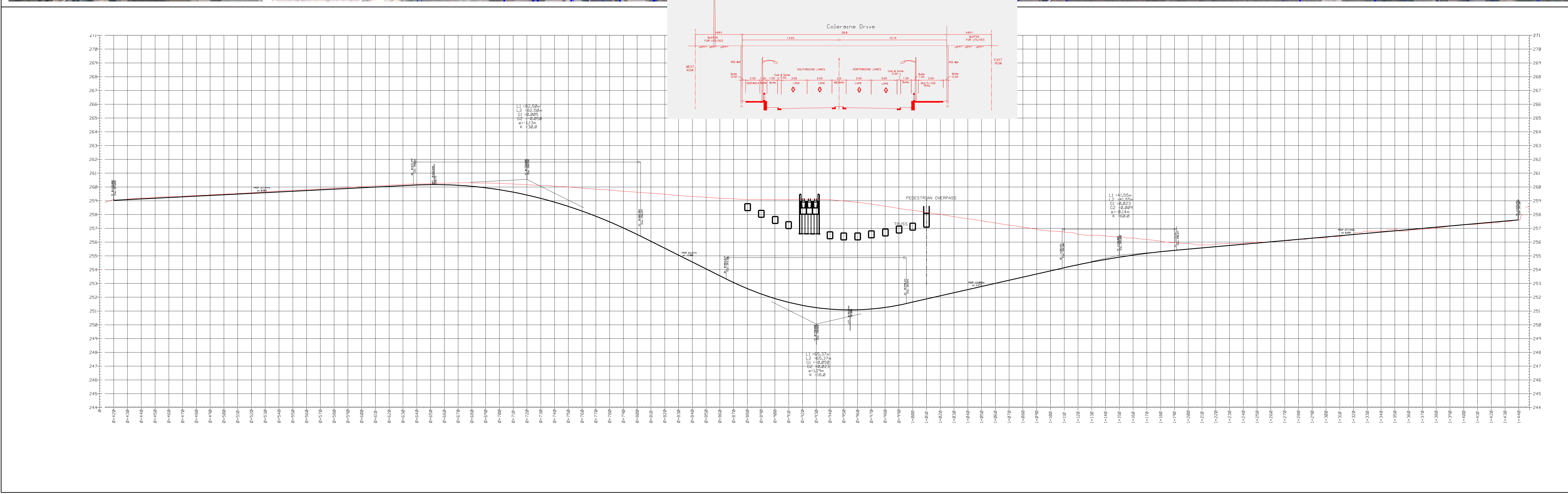


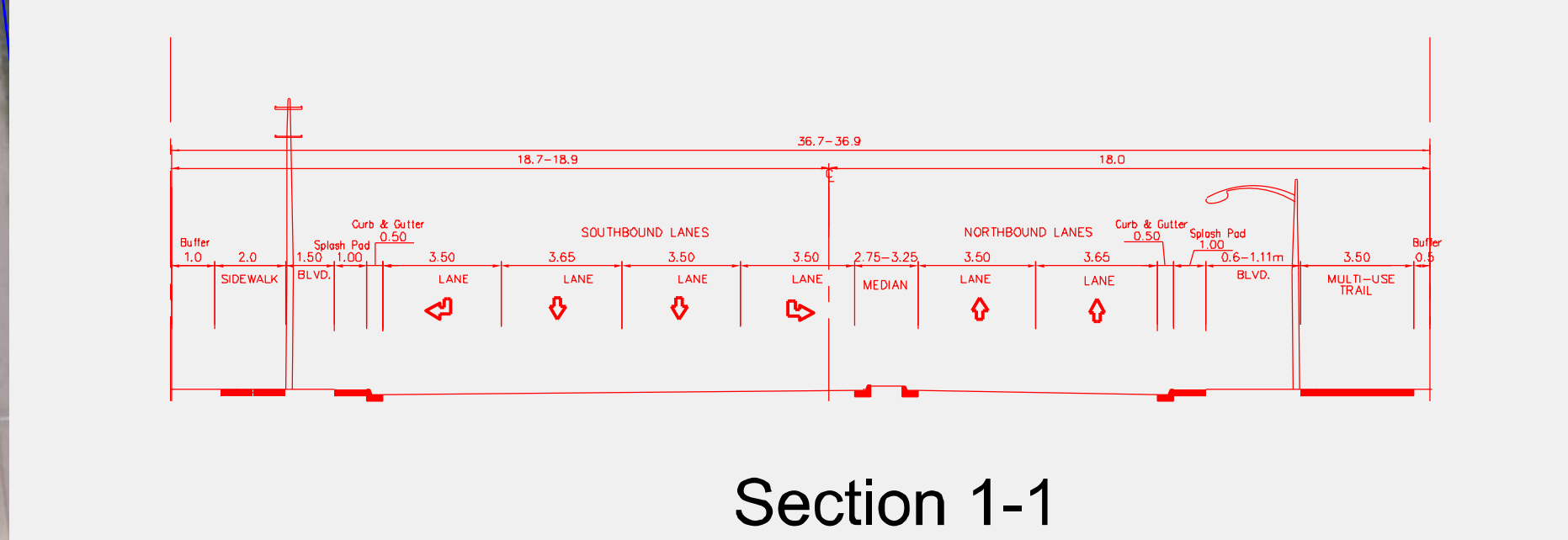
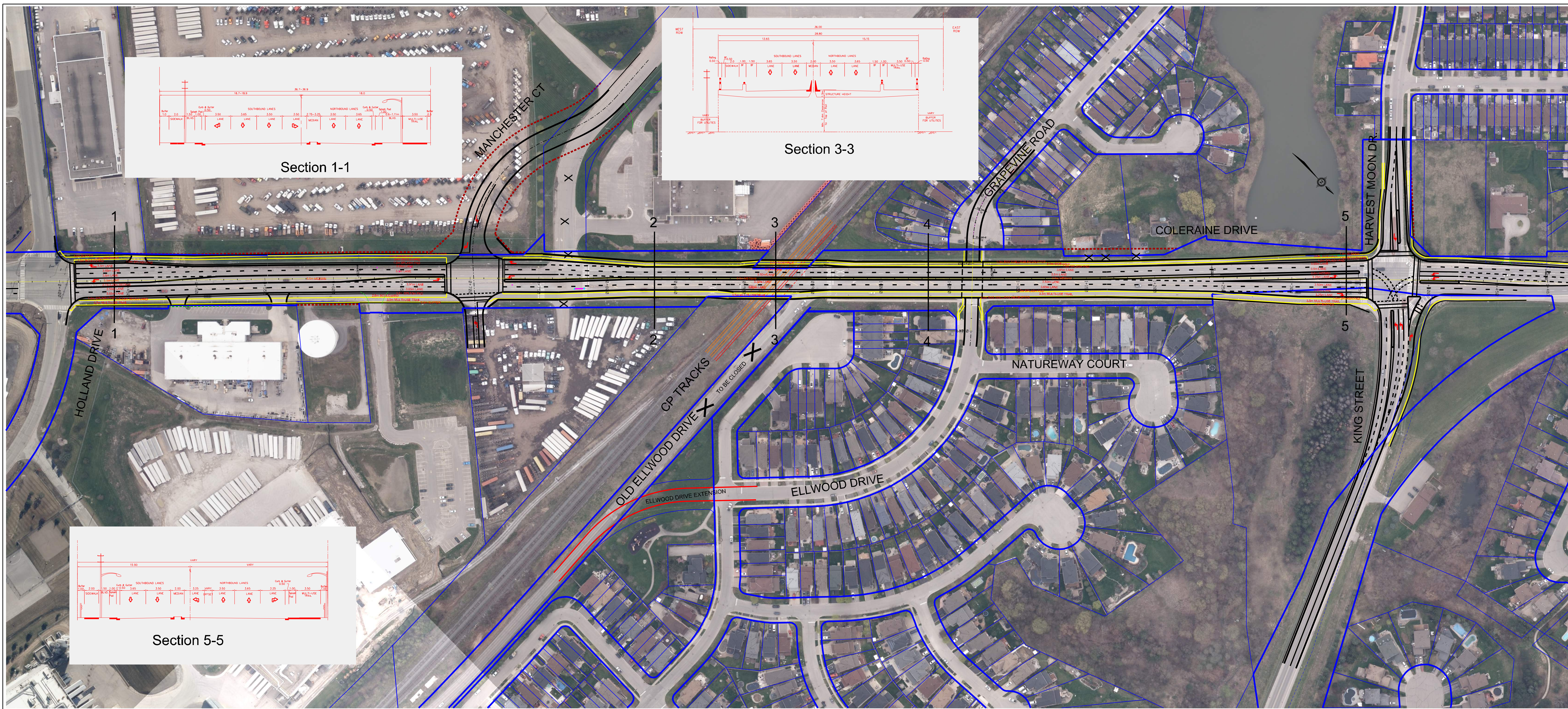
Region of Peel
Working for you

COLERAINE DRIVE
(HOLLAND DRIVE TO HARVEST MOON DRIVE)
RAIL OVER ROAD OPTION
PLAN & PROFILE

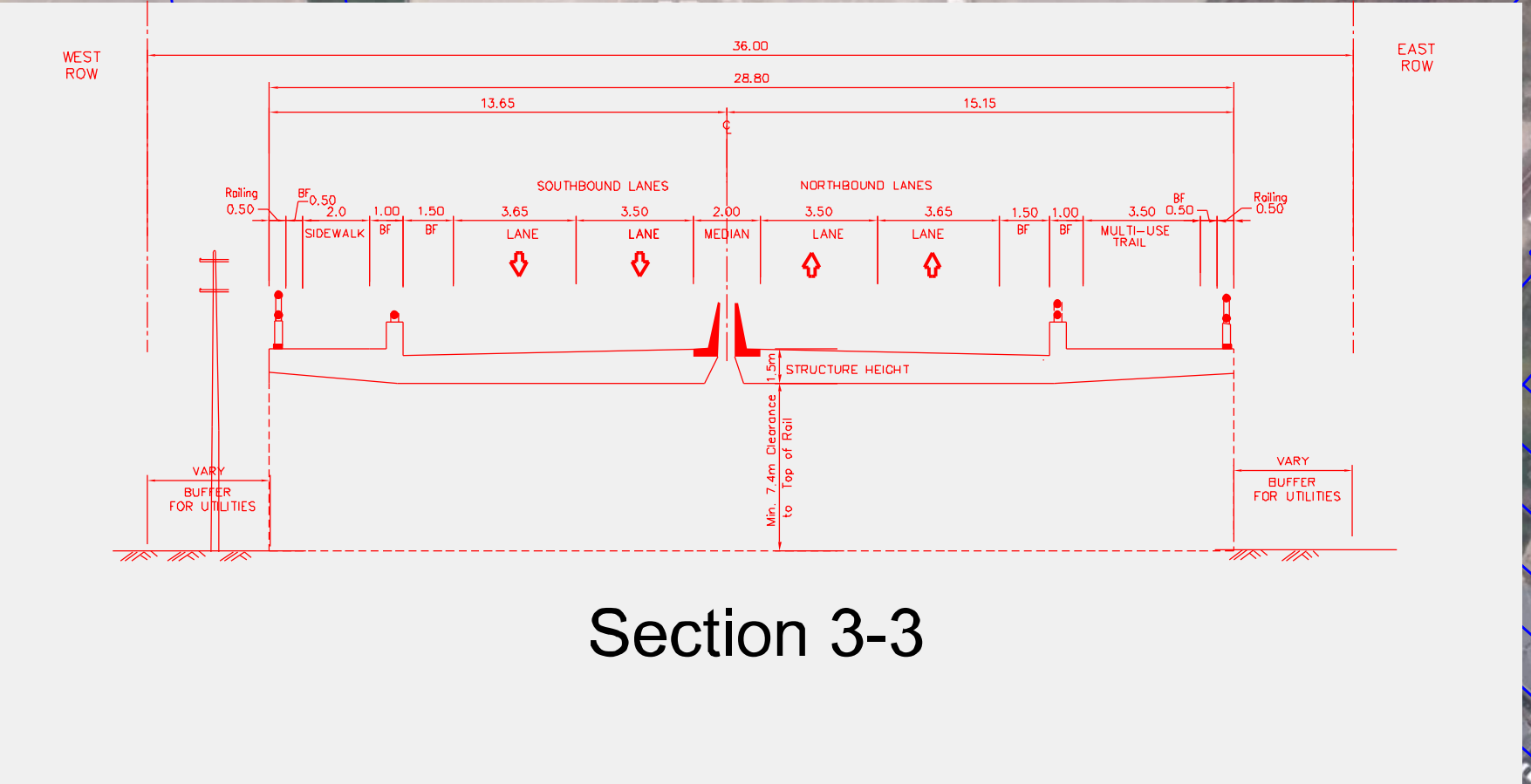
STA. 0+400 TO STA. 1+500

CAD Area	X-XX	Area	X-XX	Project No.	B000738
Checked by	S.K.	Drawn by	H.G.	Plan No.	0004-D
Date	June 2020	Sheet	4 of 8		

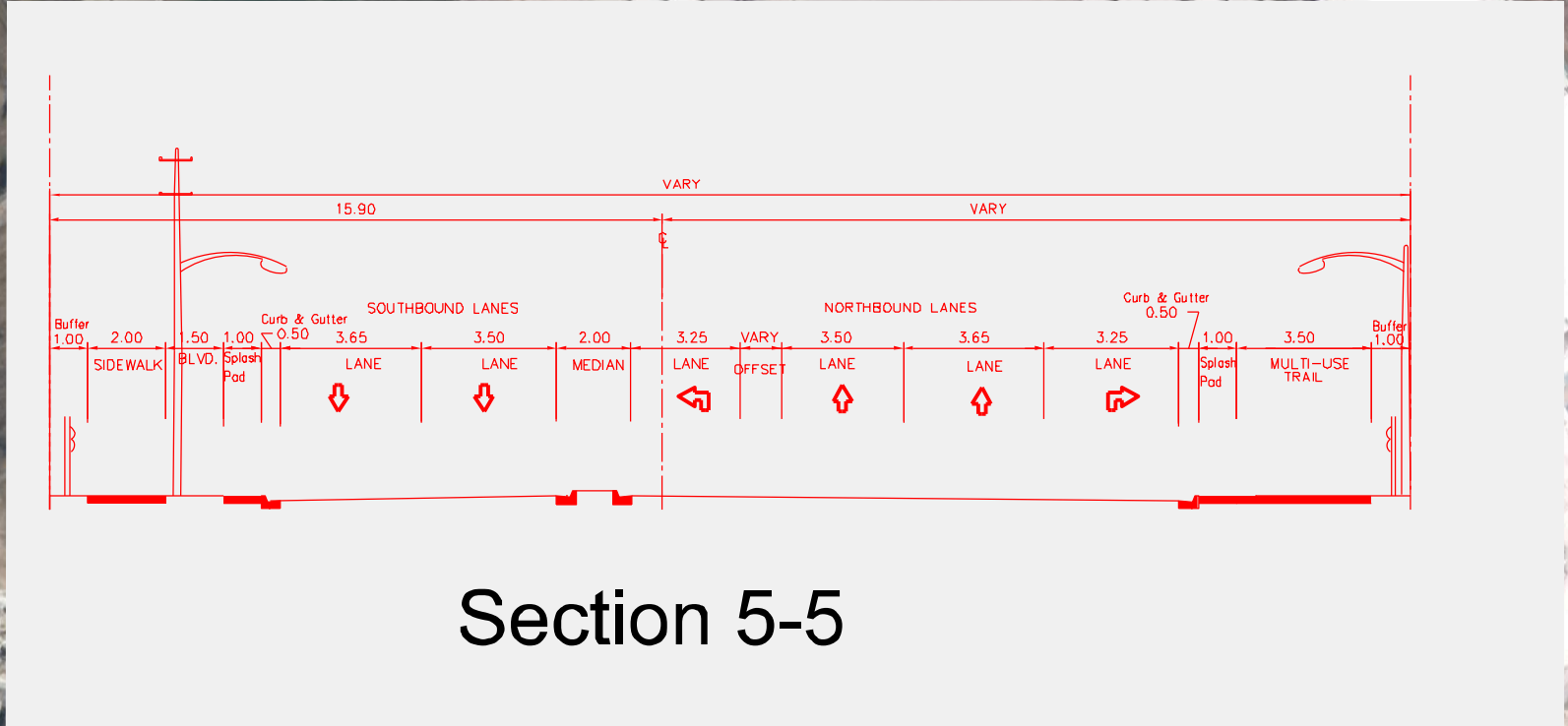




Section 1-1



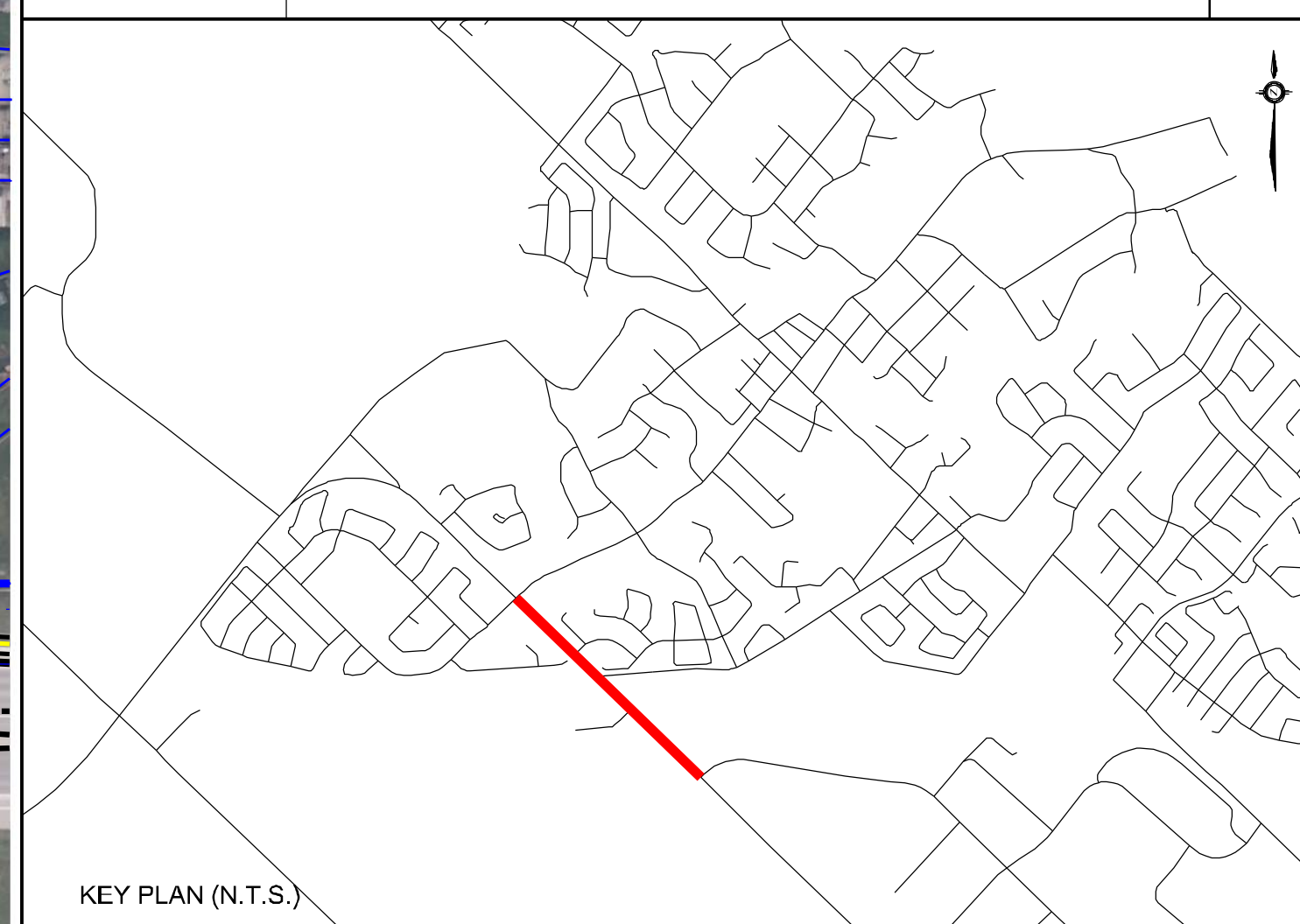
Section 3-3



Section 5-5

SERVICE DATA					
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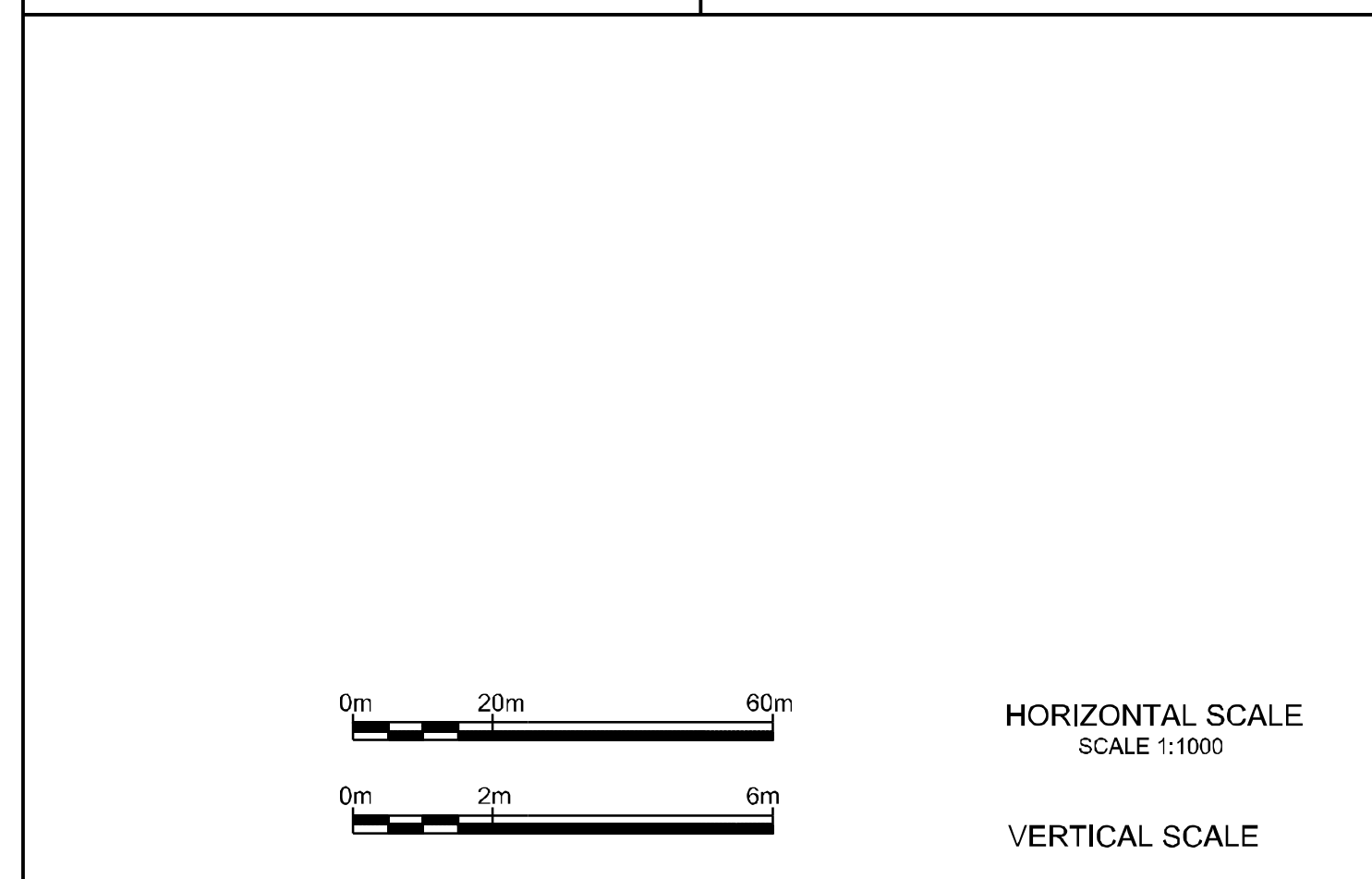
REVISIONS		
DATE	DETAILS	INIT.
September 2019	ISSUED FOR REVIEW	S.K.
June 2020	REVISED FOR REVIEW	S.K.



General Notes

Designed by _____
 Chkd _____
 Approved by _____

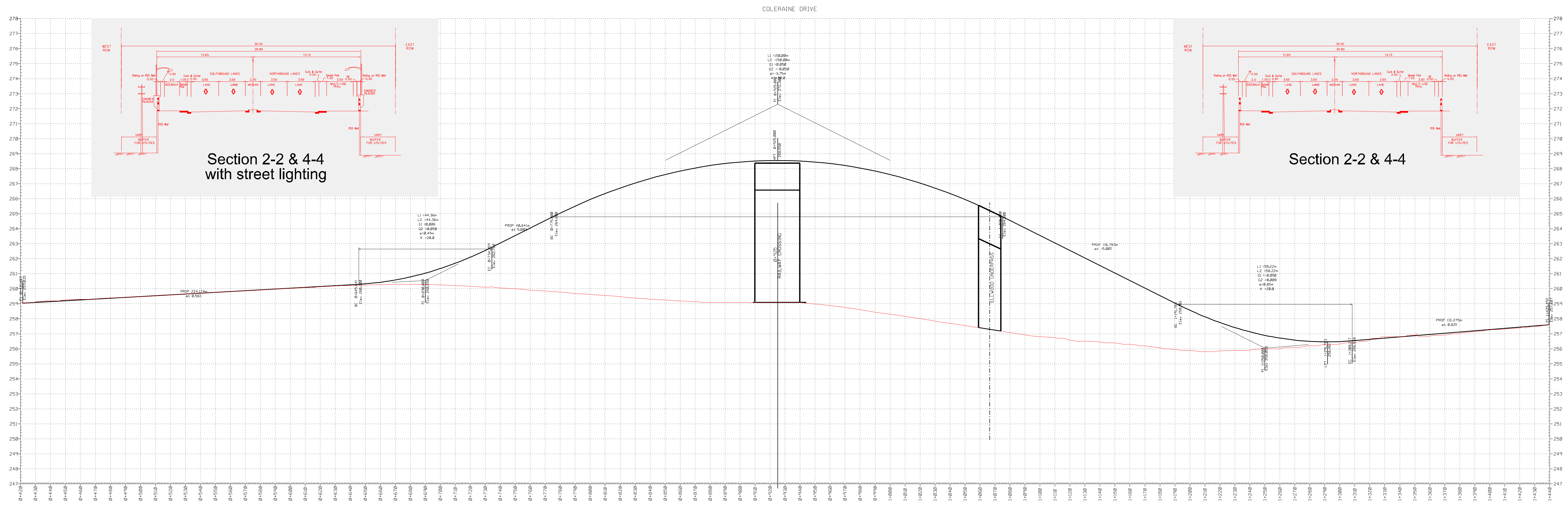
Designed by _____
 Chkd _____
 Approved by _____



Region of Peel
Working for you

COLERAINE DRIVE
 (HOLLAND DRIVE TO HARVEST MOON DRIVE)
 ROAD OVER RAIL OPTION
 PLAN & PROFILE

STA. 0+400		TO STA. 1+500	
CAD Area	X-XX	Area	X-XX
Checked by	S.K.	Drawn by	H.G.
Date	June 2020	Sheet	2 of 8
Project No.	B000738	Plan No.	0002-D



Section 2-2 & 4-4 with street lighting

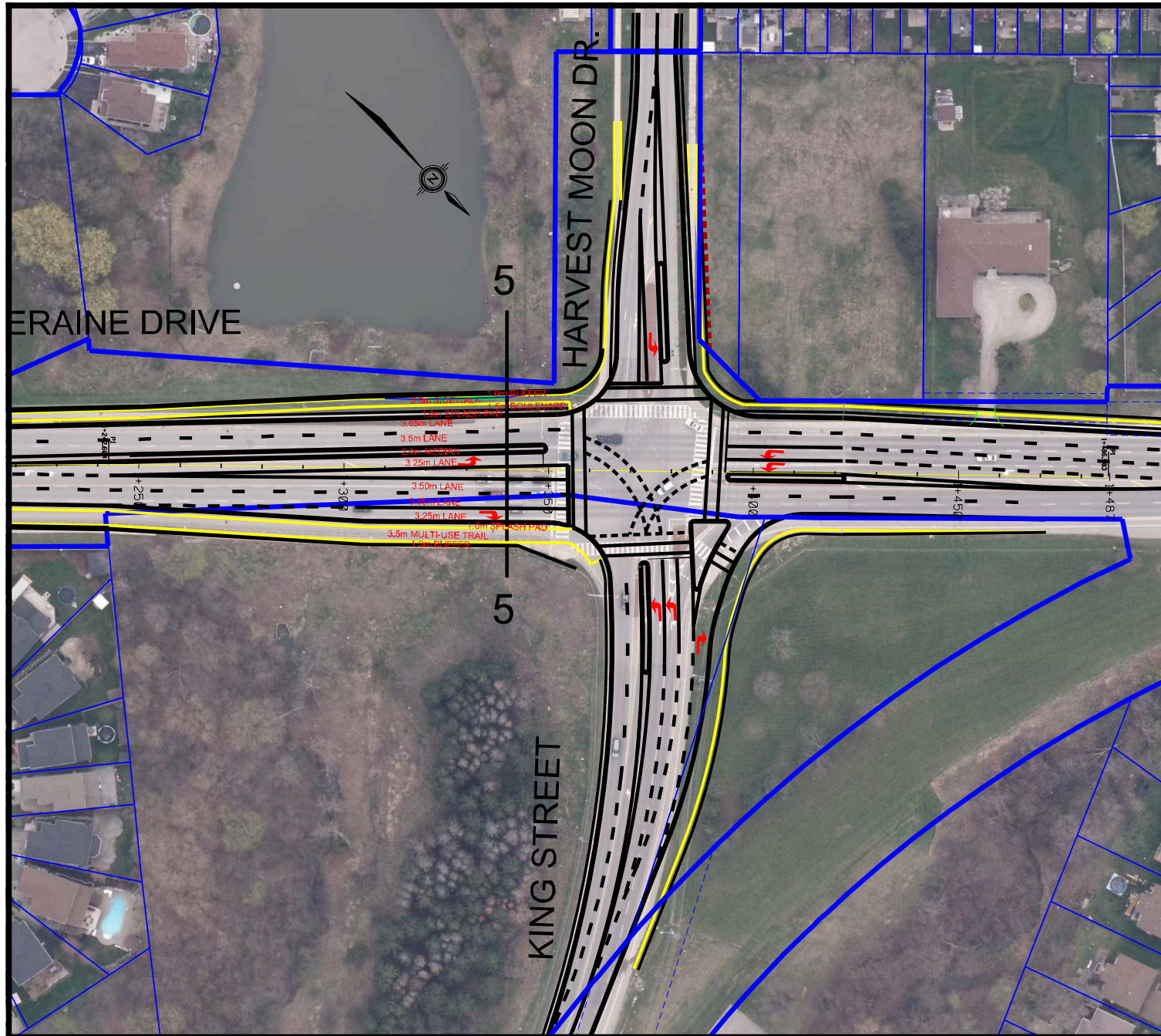
Section 2-2 & 4-4

Table 3 Coleraine Drive / King Street and Harvest Moon Drive Intersection – Detailed Evaluation and Assessment

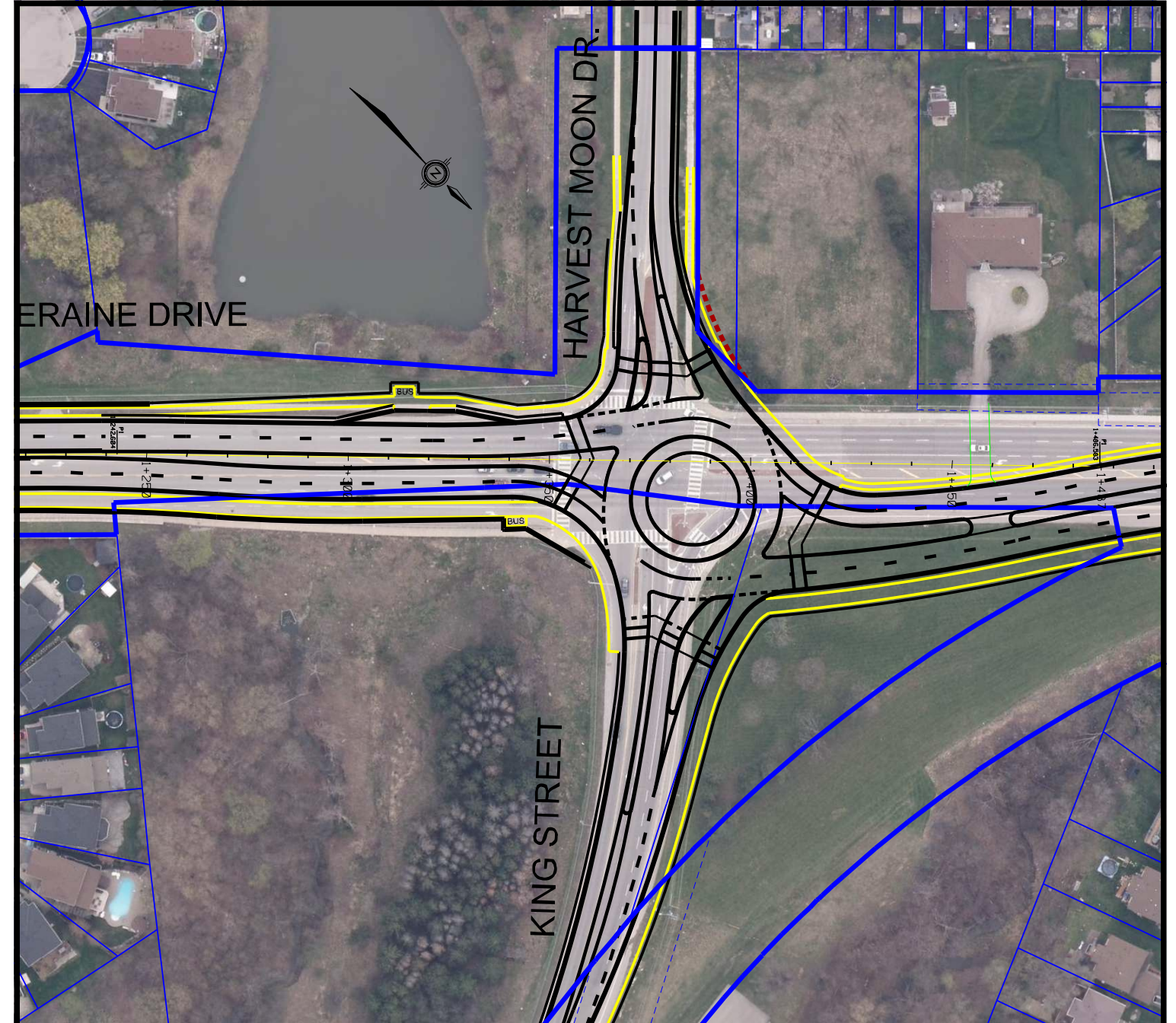
Criteria	Alternative 1 – Signalized Intersection	Alternative 2 – Roundabout
Transportation		
Peak Hour Traffic Operations	Overall level of service D in the AM peak hour, and intersection delay of 53 seconds. Overall level of service D in the PM peak hour, and intersection delay of 50 seconds.	Overall level of service E in the AM peak hour, and intersection delay of 45 seconds. Longer delays possible on Harvest Moon Drive. Overall level of service B in the PM peak hour, and intersection delay of 11 seconds.
Off-Peak Traffic Operations	Moderate wait times for side street traffic.	Low delays for all traffic movements.
Traffic Safety	Potential for high-speed turning movement and angle collisions, increasing severity.	Statistically, roundabouts tend to result in fewer injury collisions than signalized intersections and encourage slower speeds. Roundabouts reduce the severity if collisions, although it may increase non-fatal collisions.
Pedestrian Accommodation	Pedestrians have controlled crossings with audible pedestrian signals, but long exposure times (crossing up to six lanes at a time) to high-speed turning traffic.	Pedestrians have to find or create their own gap in traffic. However, crossing distances are shorter (crossing one or two lanes at a time) and pedestrians only have to look for traffic from one direction at a time.
Cyclist Accommodation	Cyclists on a multi-use path can use cross rides and not have to dismount to cross the intersection but will encounter higher-speed traffic. Cyclists in a bike lane will find left turns challenging. Can tie into the existing multi-use path on Emil Kolb Parkway.	Cyclists can claim the lane and ride with traffic, or dismount and cross at pedestrian crosswalks. If there are bike lanes, then they must terminate before and resume after the roundabout. Can tie into the existing multi-use path on Emil Kolb Parkway.
Natural Environment		
Environmentally Sensitive Areas and Slope Stability	With removal of boulevard, no impact to environmentally sensitive area on east side of Coleraine Drive at the intersection. More impact on west side of Coleraine Drive.	No impact to environmentally sensitive area on east side of Coleraine Drive at the intersection. Less impact on west side of Coleraine Drive than the signalized intersection.
Socio-Economic Environment		
Property Impacts	Property impacts on northwest corner of the intersection: 70 m ² .	Property impacts on northwest corner of the intersection: 320 m ² .
Speed Control	Ability to control vehicle speeds only during red signal indication.	Ability to control vehicle speeds at all times, allowing the area posted speed limits of 60 km/h and 40 km/h to be potentially attained.
Streetscaping Potential	Some potential with decorative concrete in medians.	More potential with decorative concrete in splitter islands and landscaping in central island.
Constructability/Engineering		
Utilities	Both options result in similar impact to underground utilities. Requires the relocation of 6 hydro poles.	Both options result in similar impact to underground utilities. Requires the relocation of 4 hydro poles.

Criteria	Alternative 1 – Signalized Intersection	Alternative 2 – Roundabout
Geometry	Preferred intersection lane configurations (as identified in Traffic Report) can be provided. Geometry can accommodate a WB-20 design vehicle.	Multi-lane roundabout can be provided as per capacity analysis. Geometry can accommodate a WB-20 design vehicle.
Construction Staging	Periodic lane closures will be necessary.	Construction staging will be more difficult than for signalized intersection, and period of construction will be longer.
Construction Cost	Approximately \$2.33M.	Approximately \$2.10M.
Summary		
Summary	A signalized intersection will be easier to stage and construct and will work reasonably well in terms of traffic operations. However, it is expected to be less safe than a roundabout, it will create more off-peak delay, and it will lead to drivers exceeding area posted speed limits.	A roundabout will be more difficult to stage and construct and will result in more property impact at the intersection. However, it will result in less environmental impact, and it will have a number of Transportation and Socio-Economic advantages over a signalized intersection.
Recommendation	Not Recommended	Recommended

Very Low Impact (Most Positive)	Fairly Low Impact	Medium/Ambivalent Impact	Fairly High Impact	Very High Impact (Least Positive)

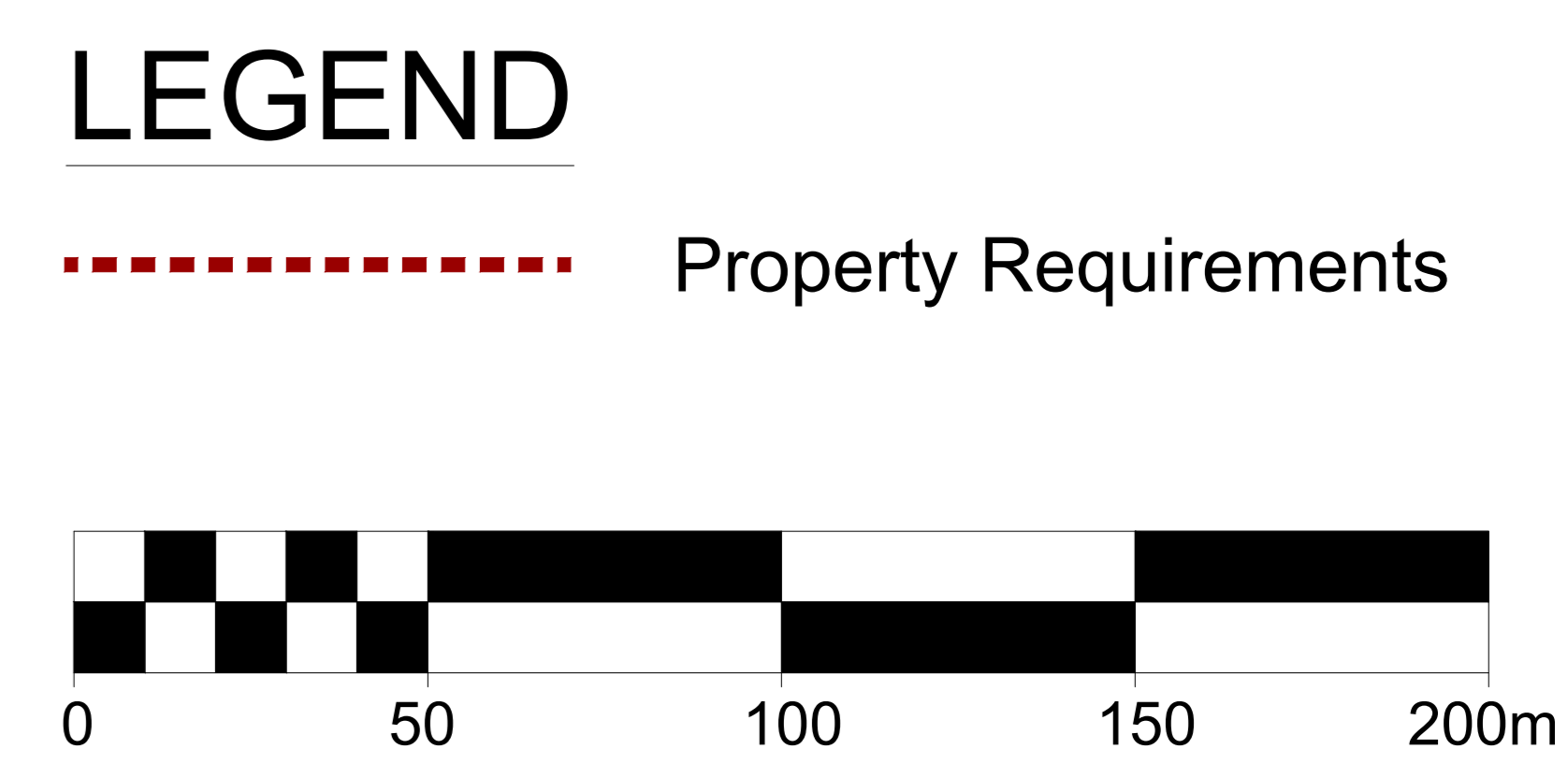
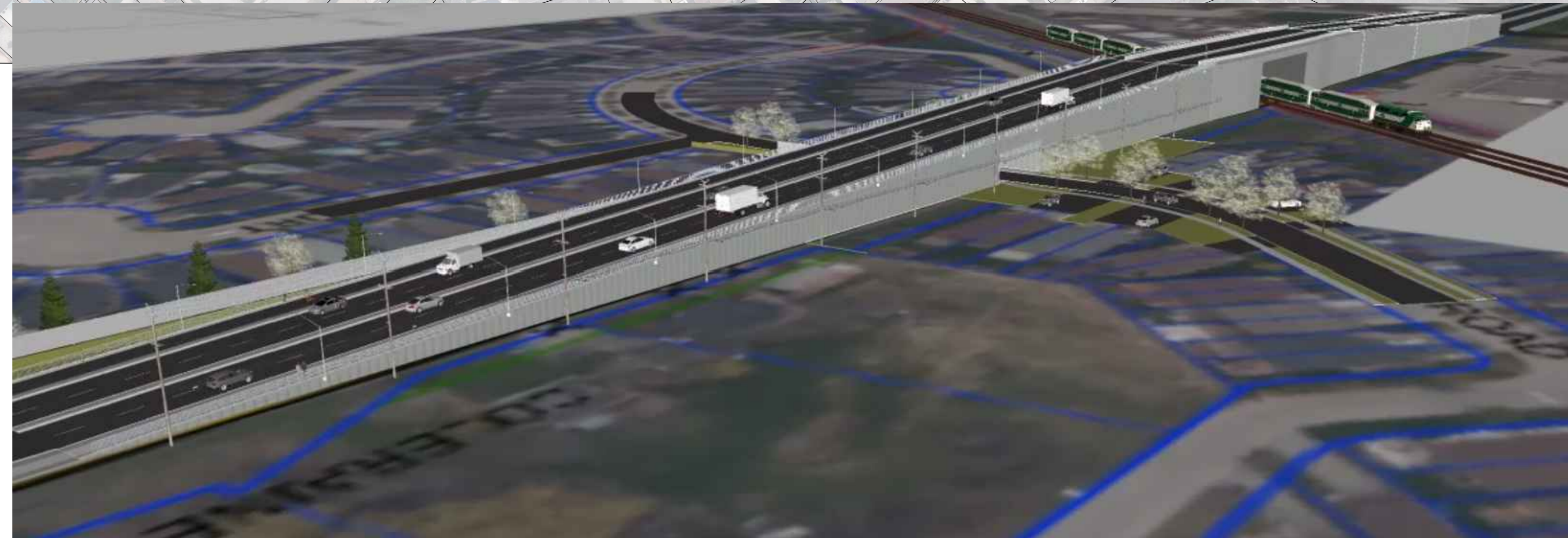
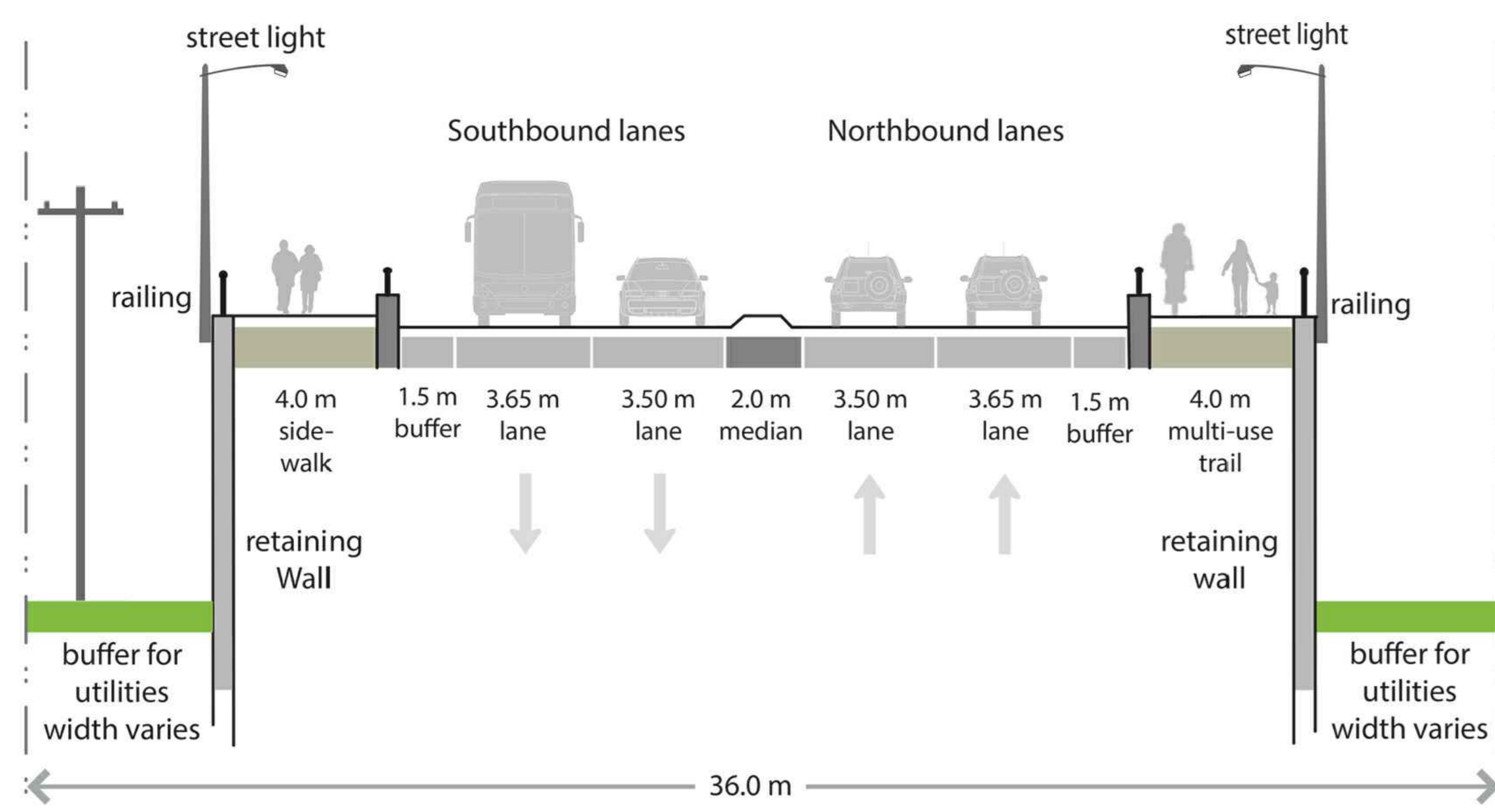
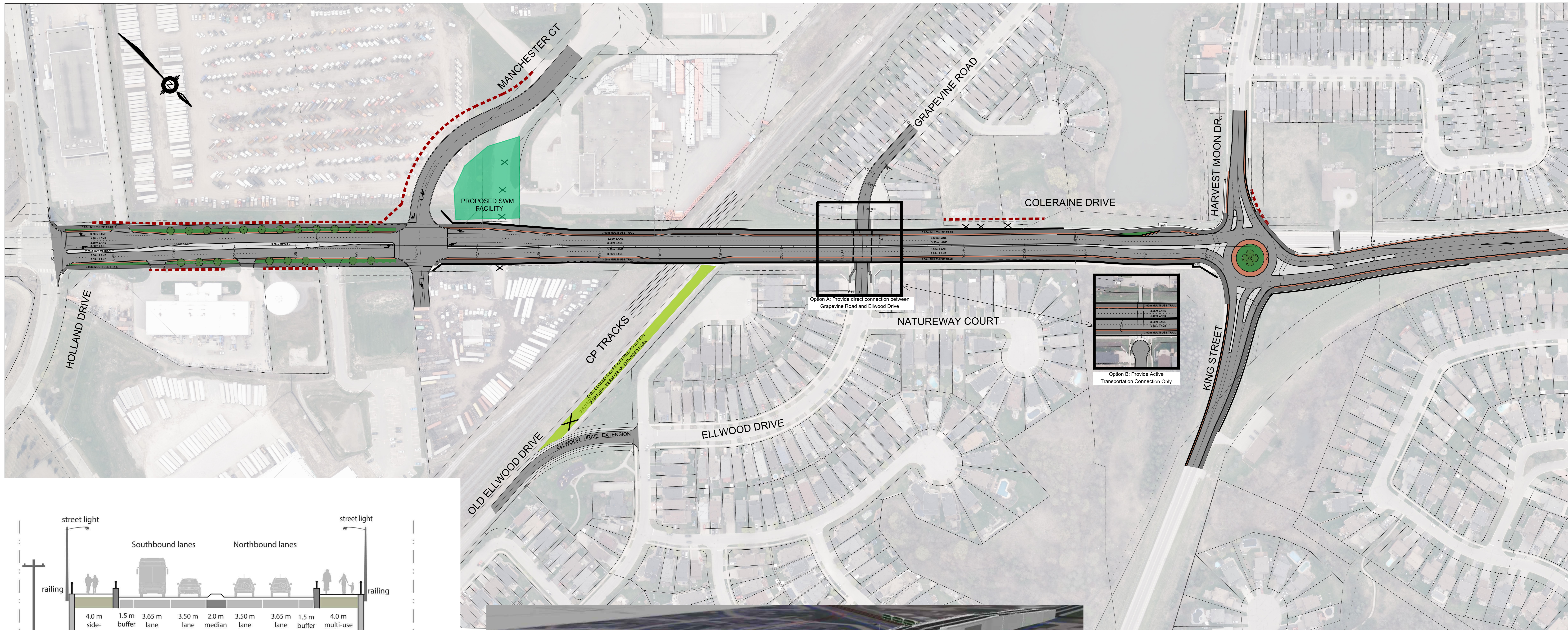


ALTERNATIVE 1 - Signalized Intersection with dual left-turn lanes on Southbound and Westbound approaches



ALTERNATIVE 2 - Two-Lane Roundabout

COLERAINE DRIVE AND KING STREET/HARVEST MOON DRIVE INTERSECTION
ALTERNATIVE DESIGN OPTIONS



COLERAINE DRIVE GRADE SEPARATION EA

(HOLLAND DRIVE TO HARVEST MOON DRIVE)

PRELIMINARY PREFERRED ALTERNATIVE - ROAD OVER RAIL

STA. 0+400 TO STA. 1+500



C11ProjectC11-0818000738_Peel-CP Grade Separation EA/400_DRAWINGS/8000738 Road over Rail Roundabout e05v for PIC-Coleraine Drive.dwg

MEMO

TO : Tareq Mahmood (Region of Peel)

FROM : Stephen Keen, David Hiatt, Kate Barclay (CIMA+)

DATE : February 22, 2022

SUBJECT : Region of Peel Coleraine Drive Grade Separation Class EA
Summary of Public Information Centre No. 2

1. INTRODUCTION

CIMA+ has been retained by the Region of Peel to conduct a Municipal Class Environmental Assessment Study for the proposed grade separation of the rail tracks on Coleraine Drive, near Old Ellwood Drive, within the Town of Caledon. The study is following the Schedule C process as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007, 2011 and 2015).

The first Public Information Centre (PIC) for the study was held in person on October 8, 2019. The second PIC was held virtually from December 16, 2021, to January 21, 2022. The purpose of this memo is to summarize the PIC notification and feedback received from the public and other stakeholders.

2. NOTICE OF PUBLIC INFORMATION CENTRE NO. 2

A public notice was prepared to inform the public and agencies of the opportunity to review the study and provide input. The Notice summarized the purpose and logistics of the virtual PIC and invited public comments by visiting the study website or directly contacting the project team by email or phone.

The Notice was circulated to property owners, businesses agencies, stakeholders, and First Nations via mail and email. Members of the public were made aware of the PIC by way of the Notice published in The Caledon Enterprise, the Caledon Citizen, and PIC information tweeted from the Region's Twitter account. An email was sent to the study mailing list during the PIC to remind potentially interested parties about the PIC and the requested comment deadline. Notification dates are summarized in **Table 1**.

Table 1: PIC 2 Notification

Notification	Date
Notice (hard copy) mailed to property owners / mailing list	– December 6, 2021
Notice emailed to mailing list	– December 16, 2021
Notice published in The Caledon Enterprise and the Caledon Citizen	– December 2, 2021 (both papers) – December 9, 2021 (both papers)
PIC information tweeted by the Region (@PeelRegion and @peelpublicworks)	– December 16, 2021 – December 20, 2021 – January 6, 2022 – January 10, 2022 – January 12, 2022 – January 17, 2022
PIC reminder mail sent to mailing list	– January 13, 2022
Unsolicited article published in The Star	– January 13, 2022

A copy of the Notice, tear sheets from the Caledon Enterprise, Caledon Citizen, and The Star, and notification emails are included in **Appendix A**.

3. SUMMARY OF PIC NO. 2

The PIC was held virtually from December 16, 2021, to January 21, 2022, on the Region’s project website. The PIC materials were presented via a project portal designed and managed by Civic Plan (subconsultant). The purpose of this PIC was to provide an opportunity for the public and other stakeholders to review and comment on the assessment of the alternative design concepts, the selection of the preferred alternative, and the preliminary preferred alternative. The PIC included a summary click-through slide show, two narrated presentations about the study, 3D visualizations of the two design alternatives, and interactive comment/voting tools that enabled participants to share their feedback directly with the project team. In addition to the narrated presentations accessible via project portal, the following supporting materials were posted on the Region’s website:

- Narrated Main Presentation (video)
- Narrated Technical and Environmental Assessments Presentation (video)
- Supporting Documents:

- Static Presentation File
- Presentation Transcript
- Static Technical and Environmental Assessments Presentation File
- Technical and Environmental Assessments Presentation Transcript
- Evaluation Criteria and Assessment
- Preferred Preliminary Design
- Alternative Designs:
 - Alternative 1: Road under Rail (3D video)
 - Alternative 2: Road over Rail (3D video)
- Technical Studies:
 - Draft Shadow Impact Study
 - Draft Noise Impact Study
 - Draft Zone of Influence Study – Vibration Analysis
- Comment Sheet

The virtual PIC had a total of 101 visitors and of those visitors, 14 participated in the virtual PIC using the interactive tools. CivicPlan monitored and reported on the activity on the project portal. The comments received at the PIC are summarized in Section 4 and the original summary report produced by CivicPlan is included in **Appendix B**.

4. SUMMARY OF PUBLIC FEEDBACK

Participants were asked to share their feedback on the following design options:

- Neighbourhood enhancements near Old Ellwood Drive;
- Local road modifications on Ellwood Drive West;
- Intersection modifications at Harvest Mood Drive / King Street & Coleraine Drive; and,
- The preliminary preferred alternative – Alternative #2: Road Over Rail.

The project portal enabled participants to vote to show their preferences, submit comments, and add content to a map of the study area. Feedback could also be submitted directly to the project team via email or phone.

The following feedback was received in response to the questions posted at the virtual PIC.

4.1. Neighbourhood Enhancements near Old Ellwood Drive

Removing a section of Old Ellwood Drive presents an opportunity for neighbourhood enhancements such as a natural berm or park expansion. Participants were asked to vote and comment on both options.

There are potential neighbourhood enhancement options for the unused right-of-way of Old Ellwood Drive and the Region took this opportunity to ask for community feedback on these options. Option A includes providing a natural berm to help provide a natural horizontal and vertical barrier between the subdivision and the rail line. Option B includes using the space to extend the area of Wakley Memorial Park, where trees and other plants can be planted along the unused right-of-way, next to the rail line. The Town of Caledon and the Toronto Region Conservation Authority consider planting opportunities at Wakely Memorial Park, potentially as part of the West Bolton Sustainable Neighbourhood Action Program (SNAP). Poll results showed a marginal preference for Option A: Natural Berm, with 57% favoring it compared to the 43% for Option B: Park Expansion. This narrow margin suggests a slight leaning towards the Natural Berm option, but opinions are relatively balanced

Figure 1 and **Table 2** summarize the comments received. In general, more participants “liked” Option A: Natural Berm compared with Option B: Park Expansion. Comments are provided verbatim.

Figure 1: Voting Preferences for Neighbourhood Enhancements

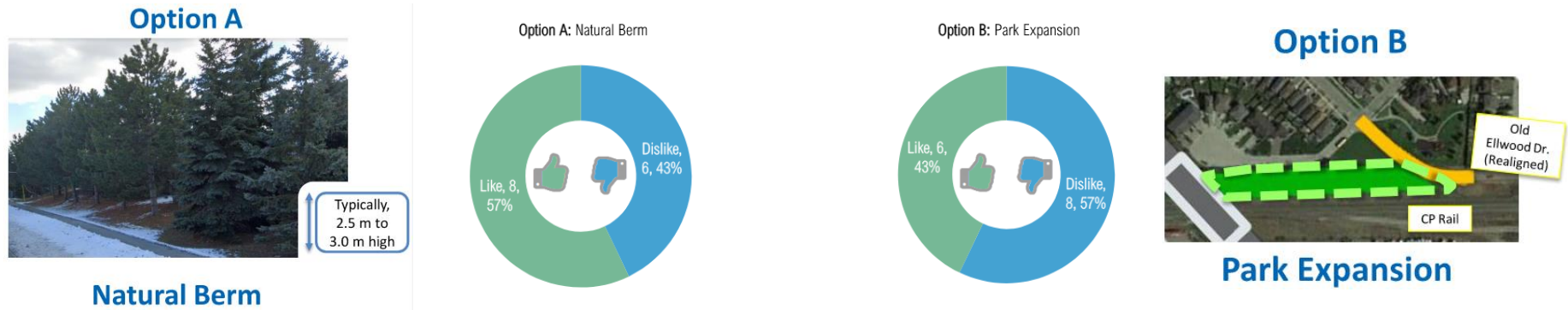


Table 2: Comments on Neighbourhood Enhancements

Option A: Natural Berm		Option B: Park Expansion	
Like	Dislike	Like	Dislike
<p>First off I don't think Old Ellwood should be connected to Ellwood Dr W. Old Ellwood Dr should now end at Station Road. Ellwood Dr W is a residential street and already has too many cars on it. Recently there has been numerous cars hitting and killing children on residential streets. We should be doing everything we can to reduce the number of cars using residential streets as major roads. If this connection with Old Ellwood is made and then the connection is made to Grapevine Dr under Coleraine it will basically be used for car races. Please think about the people that live here and not the cars driving on the streets. As for the natural berm anything is better than what it currently is, which is basically the train is visible from my front door.</p>	<p>Feels like a waste of space, and creates a dark gathering area</p>	<p>Again I don't believe the connection from Old Ellwood Dr to Ellwood Dr W should be made. Old Ellwood should end at Station Road. Keep the cars on the main roads and off the streets where kids play. Additional park space would be great as this area needs more trees and natural spaces. There is nowhere for a kid to kick a ball. Again think about the residents and the cars driving through the area.</p>	<p>Having an open park space right next to a train is not esthetically pleasing. None of these plans are.</p>

Option A: Natural Berm		Option B: Park Expansion	
The Berm will help diminish the view of the train.		Planting of Large full trees is environmentally responsible and will assist in defining a residential neighborhood vs. large industrial - also will allow for better mental	The Berm will help diminish the view of the train.
Ecological natural sound barrier air cleaner and filtering smog barrier all in one			
Planting of Large full trees is environmentally responsible and will assist in defining a residential neighborhood vs. large industrial		If you could also ensure planting of several trees and greenery and have it incorporated; an extension to a park area would be great for young families and families in surrounding neighborhoods	It comes down to I don't believe the town will be able to maintain another park.
I think its a wonderful idea to plant a natural berm which will allow the esthetics of the existing industrial area to be softened.		Allows for use of space by the community	
The trees could help act as a natural sound barrier. The train traffic is really loud because of the proximity of the tracks to houses.		Yes, good idea, but will require adequate fencing along the rail line and newly aligned Ellwood. Will also require design and enforcement to ensure that this does not simply become an informal off-leash dog park	
Provides slight noise barrier for adjacent residential space.			
I prefer this option for the wildlife. Environmentally speaking this is the better option then having grass			

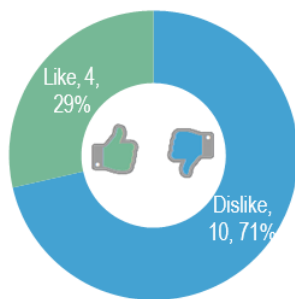
4.2. Road Modifications on Ellwood Drive West

Participants were presented with two options for road modifications on Ellwood Drive West. Option A provides a connection between Ellwood Drive West and Grapevine Road that accommodates all traffic. Option B provides a cul-se-sac on Ellwood Drive West and a connection between Ellwood Drive West and Grapevine Road for active transportation users

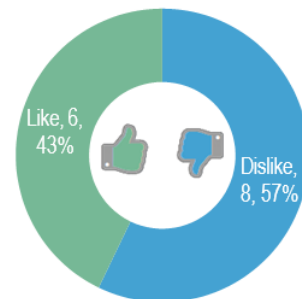
There was more support for Option B: Cul-de-sac and Active Transportation connection to Grapevine Road is preferred compared with Option A: Connection between Ellwood Drive West and Grapevine Road for all traffic. **Figure 2** and **Table 3** show the voting preferences and comments on the options. Comments are provided verbatim.

Figure 2: Voting Preferences for Road Modifications

Option A: A connection between Ellwood Drive West & Grapevine Road (accommodating all traffic)



Option B: Cul-de-sac Ellwood Drive West & provide an Active Transportation connection to Grapevine Road



Option A



Connection between Ellwood Drive and Grapevine Road, accommodating all traffic. (projected volumes approx. 30 - 50 vehicles in the peak hour)

Option B



Cul-de-sac Ellwood Drive West and provide only an Active Transportation connection to Grapevine Road

Table 3: Comments on Road Modifications

Option A: Connection between Ellwood Drive West & Grapevine Road		Option B: Cul-de-sac Ellwood Drive	
Like	Dislike	Like	Dislike
<p>This will keep traffic off of Station Road (yes, I know it is outside of the study area). I feel bad for residents on Grapevine especially, since it is currently a dead-end, but for the greater good, this connection makes the most sense. Again, I don't see the overall project as necessary, but if it goes ahead, please connect Ellwood and Grapevine.</p>	<p>residential street is not meant to become a thru street with ongoing traffic dangerous for families and elderly residents.</p>	<p>This is the only option. If this does not happen I will be forced to move as well as multiple families in the area that I have personally spoken to. Please take the residents side on this and not the side of the cars driving through the area. Why not keep travelers on main roads. By creating a cul-de-sac you can make this neighborhood better. There is Who logistical need to connect Ellwood Dr W to Grapevine Dr under Coleraine.</p>	<p>While this is likely preferred option for residents closest to the proposed cul-de-sac, this option forces westbound traffic onto Station, then King. Station speed limits are not currently enforced, and a dramatic increase to traffic would be a major safety concern, especially now that visitors to the Sorrento development park on Station, on a downhill curve.</p>
<p>Having additional through points for motor vehicle traffic (in case of accidents/closures) is much preferred</p>	<p>Absolutely not. This will increase the traffic immensely.</p>	<p>Better option for family with young kids.</p>	<p>Dislike because the Road Over Rail is not a smart option and one I am against and do not approve.</p>
<p>It is always better to have more ways to flow traffic.</p>	<p>Dislike because the Road Over Rail is not a smart option and one I am against and do not approve.</p>	<p>Cul de sac with only active transportation access</p>	<p>It is always better to have more ways to flow traffic.</p>
	<p>Dislike very much.</p> <p>█ █ █ the court was a big selling point for us</p>	<p>This is a better option as there will be local traffic only.</p>	

4.3. Crowd Map

Four Crowd Map locations were submitted through the project portal. The locations and their associated comments are presented below.

Figure 3: Crowd Map Results



4.4. Overall Comments

Participants submitted comments through the project portal and directly to the project team via email and phone. A summary of the comments received during the PIC is below. Comments are provided verbatim in **Table 4**.

- 12 participants submitted comments through the project portal
- 4 participants submitted comments directly to the project team
- 6 participants expressed their lack of support for the preliminary preferred alternative (road over rail)
- 4 participants expressed their support for a signalized intersection at

Table 4 PIC #2 Comments and Responses

Full Name	Email	Please share any comments you have about the preliminary preferred alternative here	Please share any comments you have about the preliminary preferred intersection improvement here.	If you have additional comments you would like to share, please include them here.	Project Team Response
		I still prefer the road under rail as it will have a lesser visual impact		Pls keep me up to date. Thanks	<p>Thank you for your comments, your preference for Road Under Rail has been noted.</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>
		Regardless of option is there any current plans to alter the current railroad crossing at King east of Humber Station as any overpass will increase traffic on that stretch of road way? Currently there are long lines when there's an interruption of traffic flow due to train crossing. if so should any work be done simultaneously?	Current stretches of coleraine north of king/harvest moon harbours an increase in traffic speeds and noise. Would prefer traffic lights as it would minimize this. This stretch at times sounds like a racing circuit as cars use the roundabout and return in the opposite direction at increased speeds.		<p>Thank you for your comments.</p> <p>Regarding the rail crossing at King Street, a driver behind this study is the pursuit of completing Action 4 of the Region's Goods Movement Strategic Plan. Action 4 includes the prioritization of improvements to at-grade rail crossings in the Region and the review of these locations is documented in the 'Improvements to At-Grade Rail Crossings, Prioritizing Crossings for Grade-Separation: A Recommendation Report' (2014). The report concluded that the two at-grade rail crossings located in Bolton, the Coleraine Drive crossing followed by the King Street crossing, should be further studied for grade separation. A grade separation at King Street is the ultimate goal of the Region, however timeframes for its study and/or construction are currently unknown. The region has initiated updating the Comprehensive Transportation Master Plan for the next cycle. The construction timeline for the grade separation at King Street will be reviewed through this undertaking.</p> <p>Your comments regarding Harvest Mood Drive / King Street and Coleraine Drive intersection are noted. Further regarding the roundabout, its implementation would provide benefits to speed and noise as roundabouts</p>

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					<p>act as speed-management controls, due to the need to always slow down to navigate the intersection. Conversely, if a signalised intersection is approached when it is green, there is no need to slow down.</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>
		<p>This is the worst rail crossing I have ever driven over. I have to put my hazards on and crawl across it. This is extremely dangerous as I worry about being hit from behind. Glad something is being done. Can you also please look at adding barriers at the end of Ellwood Drive West? It is so easy for people to walk on to the tracks and if a train derails, the immediate neighbourhood will be at serious risk. I also think the speed of trains should be considered.</p>	<p>A roundabout is a good idea.</p>	<p>This will take several years to implement. Please look at some short term solutions for the Coleraine/Old Ellwood intersection in the meantime. It really is horrible.</p>	<p>Thank you for your comments and your recommendations of short-term improvements at the intersection have been noted.</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>

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		<p>There is so much more train traffic and some operators are travelling at a very high speed through this area. Thanks</p>			
		<p>I disappointed</p> <p>We will have significant noise and light blockage. We will definitely have to move which is completely unfair and unacceptable.</p>			<p>Thank you for your comments and we understand your objections. The connection of Grapevine Road to Ellwood Drive West will increase traffic, however it is anticipated to be around 30-50 vehicles in the peak hour. In terms of light impacts, further detail can be found in the Draft Shadow Impact Study available on the Region's website (link below). While there will be some impacts on Grapevine Road (to the west of the grade separation), a longer duration of shadow impacts will be experienced to the east. Hopefully this provides some further information, and please contact us again if you require any more information or have any further comments.</p> <p>https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>

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		<p>The preferred design (road over rail) provides the following advantages which make it a better choice than road under rail:</p> <ol style="list-style-type: none"> 1. Cheaper cost 2. Less prone to unanticipated environmental challenges 3. More resilient to changing climate conditions (e.g. heavy rain/snowfall) 	<p>The existing signalized intersection is preferred here. I do not see a benefit to reconstructing the intersection when it is already signalized. There may be the idea of reduced speed at the intersection, however there is quite a bit of traffic that flows through that intersection and I anticipate (with increases in residency) that the volume of traffic that would result would be too great for a roundabout. Thus the likelihood of non-fatal incidents as mentioned would increase much more than the risk of having a slightly higher speed with a signalized intersection. Also there is a roundabout just north of that intersection already as a traffic calming measure.</p>		<p>Thank you for your comments regarding the preferred alternative and the Harvest Moon / King Street and Coleraine Drive intersection. As part of this study, traffic analysis was completed, and this included an assessment of the roundabout in terms of operational performance. It was concluded that the roundabout would operate acceptably in the peak hours and provide a better performance over a signalized intersection in the off-peak hours.</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>

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		<p>My preferred choice is the road under rail. I understand that there is additional costs but this is a residential area and more than just cost should be considered. My family is going to have to live beside this massive concrete structure and it will negatively effect the entire neighborhood. I would understand making a decision based on cost in a industrial area but this area is not industrial area. Think about the residents first.</p>	<p>Yes. I am in favor of a roundabout.</p>	<p>I really think everything possible should be done by the region to make this area more livable. By connecting Ellwood Drive West to Grapevine Road you will only increase traffic in this residential area. Ellwood Drive W should become a cul-de-sac. By creating a cul-de-sac the neighborhood will be more peaceful and the numerous young families that live here will be able to let their kids play on their driveways without the fear of cars racing down the street. Grapevine Road and the surrounding neighborhood west of Coleraine is already easily accessible via 2 access points. Harvest Moon Drive off of Coleraine, and Harvest Moon Drive also connects to King Street, north of this same neighborhood.</p>	<p>Thank you for your comments and your previous correspondence regarding the project. Your active participation in this study has been greatly appreciated.</p> <p>Your comments and preference for the Road Under Rail and cul-de-sac of Ellwood Drive West are understandable and noted. As noted in the PIC #2 material, the identification of the preferred alternative was not just identified through Cost, however also Constructability and Stormwater Management / Drainage. The large excavation required for the Road Under Rail poses significant risks of erosion and flooding, and thus permanent pumping will be required. Also, the requirement to detour the rail during construction adds both construction complexity and duration, in addition to costs.</p> <p>The Road Over Rail option will have notable aesthetic impacts due to its large construction, however mitigation measures are being proposed to be further investigated in the Detailed Design phase. These include landscaping (i.e., trees / planters / flower beds) and streetscaping (i.e., benches) in the space adjacent the walls, and the high retaining walls offer opportunities for items like murals and community message boards.</p> <p>Regarding the Ellwood Drive West cul-de-sac or connection, the increase in traffic is expected to be around 30-50 vehicles in the peak hour. The final configuration of Ellwood Drive West will be confirmed by the Town of Caledon, using the information gained by this study, and after the filing of this Environmental Study Report.</p> <p>Regarding the traffic counts taken in support of this study, they were conducted prior to the pandemic, in 2017 and at the time of study initiation. The transportation analysis and assessment will be included in the final Environmental Study Report, which will document all study findings.</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon</p>

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				<p>There is no need for an additional access point to Grapevine Road and the surrounding neighborhood. I really feel that this is the most important decision. Please Cul-de-sac Ellwood Drive West & provide an Active Transportation connection to Grapevine Road. Also there is a lack of green space in this area so I think its very important to increase the number of trees and any additional steps that can be taken to reduce the noise from both Coleraine Drive and the existing railway. Again I just wanted to point out that the region was counting cars on these roads during the height of the pandemic. Thus the number of cars on the roads at the time was substantially less then normal.</p> <p>This should also be</p>	<p>completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>

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				<p>taken into account. Thank you very much for letting me have a say. I hope the Region of Peel keeps the residents of this area top of mind when these decisions are made.</p>	
		<p>Your preliminary preferred alternative is the NOT the best way to go - in fact, its a HUGE mistake. Although costs are higher with Road Under Rail, this is the best option for the adjacent neighborhoods off Coleraine. As a resident of</p>	<p>Leave existing traffic light controlled intersection. Too many trucks travel this route and a round-about will result in accidents with these trucks.</p>	<p>The adjacent neighborhoods of Coleraine have already suffered enough. With all the large industrial being allowed to be built; having Coleraine the main throughway for truck traffic; and the</p>	<p>Thank you for your comments and we understand your objections. Regarding the roundabout, the roundabout will be specifically designed to accommodate trucks, and this will include the use of truck aprons (elevated paved portions) that provide an area for truck wheels to track over, rather than encroaching into adjacent lanes. Also, the roundabout can be designed with pedestrian and active transportation crossings on each leg. While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at</p>

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		<p>this neighborhood, we already live with issues (the active rail line with noise and pollution along with all the major large active industrial and high truck traffic on Coleraine). Putting concrete walls up with a major active roadway in the sky is the most illogical thing. Planting 'tree's' along this concrete nightmare will not hide the unsightly concrete jail like walls and definitely not make one look up in awe of watching tractor trailers and cars travelling at the rooflines.</p>	<p>Traffic Light controlled intersection provides better safety for the community and for people walking / biking, etc.</p>	<p>existing CP rail line that runs all hours of the day and night - now proposing to put a 4 lane roadway up in the sky at our rooflines is what you want to do? Please - do not go this route - It is understood that the rail line has to be made accommodating for future GO Service and town expansion which is fine, but please do so with the Road Under Rail option regardless of the high cost. Do the right thing - the large cost of Road Under Rail will be covered with the growing expansion of the town coming over the years.</p>	<p>https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>
		<p>I would prefer road under rail.</p>	<p>I prefer the roundabout.</p>	<p>Please consider that there are a lot of family with young kids in the area when you finalize your plan and what is best for the community. It will be safer for if you make Ellwood Drive West a cul-de-sac. Thank you.</p>	<p>Thank you for your comment and your preference for Road Under Rail and a cul-de-sac for Ellwood Drive West has been noted.</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon</p>

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					<p>completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>
		<p>The city has failed us once again. This is absolutely infuriating. For once, do what's best for your residents. As a resident who will be immediately affected by either option, the Road Over option is infuriating. NO resident wants to stare at a giant concrete wall, the train is bad enough. The city failed when they allowed this housing development to be built too close to the train. Now you want to make it worse by having huge barriers and transport truck flying high above residential homes and all to save a few million dollars! Maybe the town should have these billion dollar corporations, such as Amazon and Canadian Tire, who they've allowed into the community pay their fair share of taxes. I'm sure you can come up with that \$20 million dollars in no time. Stop punishing your residents like this.</p>			<p>Thank you for your comments, your objections are understandable and have been noted.</p> <p>The Road Over Rail option will have notable aesthetic impacts due to its large construction, however mitigation measures are being proposed to be further investigated in the Detailed Design phase. These include landscaping (i.e., trees / planters / flower beds) and streetscaping (i.e., benches) in the space adjacent the walls, and the high retaining walls offer opportunities for items like murals and community message boards</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>

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		<p>It doesn't look as aesthetically pleasing as the other one, however one also needs to take into account of more severe weather we have been having. Having the cars go over the rail then under does make sense as one needs to think about potential flooding. With the change of the weather we have been having the past few years, I would take the preferred alternative for this reason. As well it would be fiscally responsible as well given the big difference of costs. When digging underground one never know what one may find.</p>	<p>I don't understand why we need to change what is already in place and has been working very well. I feel that this is a waste of money.</p>	<p>This is something that is long overdue.</p> <p>I love the green spaces and town life we have here. However it is important to always look forward to the future. With bring the GO train up here we will need this, and I feel that this project has been left to do at the last minute.</p>	<p>Thank you for your comments and your support for the Road Over Rail alternative has been noted.</p> <p>Regarding the Harvest Moon Drive / King Street and Coleraine Drive intersection, the traffic analysis undertaken by this study identified that the intersection would perform poorly in the future, due to increased traffic growth and that the grade separation would improve flow towards the intersection. The study reviewed options of signal improvements or a roundabout, to identify a mitigation measure to ensure the intersection performs adequately in the future, with the grade separation.</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>

Full Name	Email	Please share any comments you have about the preliminary preferred alternative here	Please share any comments you have about the preliminary preferred intersection improvement here.	If you have additional comments you would like to share, please include them here.	Project Team Response
		<p>Absolutely not!!!! I understand the \$20M cost savings however this is not the way to go! As a resident of this community with my home backing /facing the already disgusting rail line, you now want me to look at a concrete monstrosity with truck traffic at all hours of the day? Oh no ... no way with your so called projected increases of residency for this town, I'm sure you can find the \$20M extra to go the alternate way of roadway under rail. Going road over rail is a cheap short cut in my opinion and that of</p> <p>regarding the rail line and the fact that it was allowed for our homes to be built that close to that line; now you want to go the road over rail option? Please take into consideration and put yourselves in our position - would you enjoy starting at concrete walls with trucks and cars in the sky? Please relook at the option of</p>	<p>Honestly, I think the existing should remain - with the amount of truck traffic that runs on this corridor as the by-pass for trucking; it should remain as traffic light controlled. A round-a-bout with these trucks will be a guaranteed collision intersection with possible loss of life.</p>	<p>I would like to ask that you truly take into consideration of what is being proposed as the 'preferred' option - Road Over Rail. This is not the way to go for many reasons, as I understand the costs are significant, but over the long run, going Road Under Rail will be the most beneficial to the existing residences and to any of the new residences coming into Bolton.</p>	<p>Thank you for your comments, your objections are understandable and have been noted.</p> <p>The Road Over Rail option will have notable aesthetic impacts due to its large construction, however mitigation measures are being proposed to be further investigated in the Detailed Design phase. These include landscaping (i.e., trees / planters / flower beds) and streetscaping (i.e., benches) in the space adjacent the walls, and the high retaining walls offer opportunities for items like murals and community message boards.</p> <p>Regarding the roundabout, the roundabout will be specifically designed to accommodate trucks, and this will include the use of truck aprons (elevated paved portions) that provide an area for truck wheels to track over, rather than encroaching into adjacent lanes. Also, the roundabout can be designed with pedestrian and active transportation crossings on each leg.</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>

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		Road Under Rail - I realize the amount of work needed and costs; however long term this is the best way to go.			
		I think that this option makes sense, between the two suggestions. Ellwood to Grapevine will become a major thoroughfare. I would like to know what will happen to the area	I think a roundabout here would be a disaster. Spend 10 minutes at either roundabout in town between 6:30 and 9 any morning, or 4:30 and 6 any evening and you'll see that years after they were	This project seems overkill for a minor inconvenience of occasionally having to wait for a train crossing.	Thank you for your comments and your support for the Road Over Rail alternative has been noted. Regarding Old Ellwood Drive, the area of roadway that will not be required will either become a location of a natural berm or an extension of the park. The PIC #2 included this consideration for informational purposes, as the final decision will be made by the Town of Caledon, as the roadway is under their jurisdiction.

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		<p>west of Ellwood, where the current road footprint sits. Will that become greenspace, or will it just be a vacant area for debris?</p> <p>Or will the property be sold and developed?</p> <p>Overall, I think that spending this kind of money to avoid queues occasionally doesn't make sense. There is a rail overpass in town, and although that does not help trucks, so be it. Coleraine has been overdeveloped. Coleraine speed limits are ignored, especially by trucks and construction vehicles. Before dropping tens of millions in construction and development that will make that section feel even more like a highway, how about adding speed cameras, pay for additional/dedicated enforcement along the study stretch, and at the King/Harvest Moon/Coleraine intersection? Likewise, why not add a right turn lane from Ellwood onto Coleraine? Or put in obstacles so that drivers</p>	<p>opened, people still don't use the roundabouts properly. Speed and aggressive driving are bad enough as is.</p> <p>A changed intersection isn't required, enforcement of existing laws and speed limits are.</p> <p>Put in red light and speed cameras. Get OPP there on a regular yet unpredictable basis, so that residents and truck drivers are afraid to be ticketed.</p>	<p>Spend a fraction of the budget on improving existing conditions and enforcement through policing and cameras.</p> <p>If the businesses along Coleraine don't like waiting for trains, they should have considered that, and they should reroute their trucks down to Mayfield and up Coleraine, rather than across King and down Coleraine.</p> <p>There are solutions that would cost less and be less disruptive to local residents.</p>	<p>Regarding the roundabout, the roundabout will be specifically designed to accommodate trucks, and this will include the use of truck aprons (elevated paved portions) that provide an area for truck wheels to track over, rather than encroaching into adjacent lanes. Also, the roundabout will act as a speed-management control due to the low speeds required to navigate the intersection.</p> <p>The grade separation is being pursued as an increase in motor vehicle and train traffic is projected for 2041. This is expected to result in traffic queues generated at the at-grade crossing extending beyond the intersection of Coleraine Drive & King Street West/Harvest Moon Drive. This issue is expected to worsen as the frequency of trains during peak hours increases, particularly due to the planned extension of GO Train service to a new Bolton station. The grade separation also comes at the recommendation of previous study undertaken by the Region in support the Region's Goods Movement Strategic Plan. The grade separation improvements are also consistent with Provincial and Regional planning policies.</p> <p>While the PIC #2 has been concluded, the material presented is still available for review on the Region's website at https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp</p> <p>The next phase in the study includes the confirmation of the preferred alternative and the completion of the Environmental Study Report (ESR), which will document the EA study and its decision-making process. Upon completion, the ESR will be available of a public review period for a minimum of 30 days. You will be contacted again at the time of filing.</p> <p>Please let us know if you have any further comments or questions,</p>

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		<p>can no longer drive on the shoulder and over the curb to make that right hand turn?</p> <p>I am concerned about changing that intersection to a roundabout. People and large vehicles seem to have difficulty managing the existing roundabouts in the region, nevermind one at a major intersection. Recognizing it is outside of the study area, there are unintended consequences with this project, mainly an increase on traffic volumes and speed on Station Road, during construction, but also after construction, as people will use Station to get to King and head west on Harvest Moon.</p>			

A

Appendix A



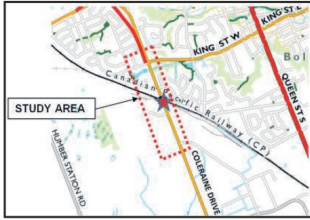
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THIS NOTICE WAS FIRST ISSUED ON DECEMBER 2, 2021

NEWS

HORSES SAVED FROM BARN FIRE

CHRIS CLAY
cclay@mississauga.net

A fire at a Caledon barn ended without the loss of life as more than 20 horses were evacuated before the structure became fully engulfed in flames.

However, one person was taken to hospital following the fire that happened around 8 a.m. at a property in the area of Innish Lake Road, north of Old Church Road, on Nov. 27. Both officers with Caledon OPP and crews with Caledon Fire and Emergency Services were called to the blaze.

The barn was fully engulfed when first responders arrived on scene. However, residents of the property were able to evacuate the animals in the barn, saving them all.

One resident suffered smoke inhalation and was



Caledon OPP photo
Caledon OPP and Caledon Fire and Emergency Services were called to a barn fire on Saturday, Nov. 27.

taken to hospital.

Damage to the barn is estimated to be more than

\$1.5 million. Police said the fire is not considered to be suspicious.



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NEWS

MAJORITY OF OUTBREAKS RECORDED IN ELEMENTARY SCHOOLS

SABRINA GAMROT
sgamrot@mississauga.net

A study of recent COVID-19 outbreaks in Peel Region found a majority were recorded in elementary schools.

According to the most recent epidemiological report from Peel Public Health (PPH) since Oct. 25, 33 outbreaks were declared.

Out of those outbreaks, 39 per cent were found in schools, specifically in elementary or joint elementary/secondary schools.

The second-highest amount of outbreaks were recorded in manufacturing/warehouse and transportation settings at 18.2 per cent, followed by congregate care at 15.2 per cent.

Roughly 6.1 per cent of outbreaks were reported in office and business services and recreation settings, respectively.

Child care, banquet hall and event centres, retail, place of worship and "other" settings were reported each at three per cent.

No confirmed outbreaks were reported at restaurants, bars or nightclubs as well as fitness centres and gyms, according to



Jonathan Hayward/Metroland

Out of the outbreaks, 39 per cent were found in schools.

the report.

Though not specified in the report, zero outbreaks in these settings could have occurred since the introduction of the Sept. 22 proof of vaccination program was introduced by the province.

Since the beginning of the fourth wave, which started around July 18, PPH said 156 COVID-19 outbreaks were recorded.

As of Nov. 24, PPH reports the weekly COVID-19 incidence rate for the region remains steady at 19 per 100,000, much lower than the provincial rate of 32 per 100,000.

"In Peel, COVID-19 outbreaks continue to occur in settings where there is a higher risk of close and prolonged contact and in settings where there are larger numbers of susceptible individuals," read the report.



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Canada increases border restrictions as Omicron variant poses new COVID-19 threat

BY ROB PAUL

Last week, the World Health Organization classified the COVID-19 Omicron variant as a variant of concern. Since then, it has been found in multiple countries, including several travel-related cases in Canada.

The variant became a concern last week when it was detected in South Africa and is viewed as potentially more highly contagious than the Delta variant. As a result, the Federal Government announced additional border measures to reduce the risk of it entering Canada.

Canada has put into effect a mandate that any foreign nationals who have been in any of the countries on the entry prohibition list within the previous 14 days will not be permitted entry into Canada. The countries on the list are Botswana, Egypt, Eswatini, Lesotho, Malawi, Mozambique, Namibia, Nigeria, South Africa, and Zimbabwe.

"We are taking quick action at our borders to mitigate travel related importations of the Omicron variant," said Jean-Yves Duclos, Canada's

Minister of Health. "While our monitoring systems are working well, we now know that the Omicron variant is present in Canada. We need to remain vigilant in our own actions. Vaccination and simple public health measures such as masking and limiting the number of persons we interact with slow down transmission, reduce hospitalization and death, and protect our health systems from being overwhelmed. We must all continue to do our part to protect all people in Canada."

At an announcement in Mississauga on December 1 for funding towards Trillium Health Partners, Premier Doug Ford and Deputy Premier and Minister of Health Christine Elliott spoke about the Omicron variant.

"Cause for concern, but not a cause for panic," said Ford. "Every day we learn more about the Omicron virus and how quickly it can spread and how severe it may be. We're learning about how effective our vaccines are and every day we hold off more cases from entering our country, the more time we have to learn and prepare. The best thing we can do right now is fortify our borders.

Our best defence is keeping the variant out of our country. I want to thank the Feds for taking action and being decisive on the borders.

"We will continue to call on them to be proactive and not wait to implement every measure needed to keep Canadians safe. The good news is that each of us has the power to slow the spread of this variant and all variants. If you haven't already done so, please get vaccinated today. If you've put off getting your second dose then please get your second dose, and if you're eligible for your third dose then please book your booster appointment."

There will also be increased screening at airports to help mitigate the spread of COVID-19 due to the contagious variant, but the severity of impact the variant could have in Ontario and Canada is still not known.

"There's still a lot we don't know about the Omicron variant, the extent of the transmissibility, the virulence, and how safe and effective our vaccines are, but we're continuing with all of our precautions," said Elliott. "We have a great system for testing and case and contact management that has followed up with all 375 people who had travelled to the South African countries. We'll continue to test and follow up while ensuring their quarantine. We're also continuing with our vaccinations and doing really well, we're at 89.9 per cent with 12 years and up for first dose and

87 per cent with people with the second dose and have already vaccinated over 109,000 children (five to 11) in the last 10 days."

Peel itself has 75 per cent of all residents fully vaccinated with 78.4 per cent at one dose, 89.7 per cent of all adults (18+) in the Region have at least one dose and 86.2 per cent have both doses while those 12 and older are at 89.7 per cent single dose coverage and 86 per cent double dose coverage.

In total, the Region has administered 2,579,638 doses of the vaccine to 1,331,226 individuals. 1,214,490 individuals have both doses and 33,922 have received a third dose.

COVID cases in the Region remain steady with 309 new cases this week to bring its total to 119,318, but there were no additional deaths as the number stayed at 1,031.

In Caledon, there were 14 new cases that bring the total to 4,979 as the death count remains at 22 for the Town.

To book a vaccine in the Region of Peel, visit www.peelregion.ca/coronavirus/vaccine/book-appointment/.

To download or print a copy of your proof of vaccination, visit covid-19.ontario.ca/covid-19-vaccine-booking-support#proof-of-vaccination. To learn more about the official QR code system Ontario has implemented and to access yours, visit covid-19.ontario.ca/get-proof.

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Headwaters Hospital confirms unauthorized access in suspicious email activity

BY PAULA BROWN

Headwaters Health Care Centre (HHCC) has confirmed the cause of suspicious email activity that led to the shutdown of their internal systems, and the closure of the COVID-19 assessment centre.

On Friday, November 26, the local hospital released an updated notice on their website noting their system had been subjected to "unauthorized access" resulting in a number of spam emails.

"We are working with cybersecurity experts who will help us safely restore our IT services and investigate what happened and whether any sensitive data was exposed," said the hospital.

The hospital's information technology system noticed suspicious email activity the day previous (November 25), with a number of spam emails sent from the CEO Kim Delahunt's email account to staff members of the hospital. As a result of the security breach, the hospital shut down their internal system as well as access to the internet and key external partners.

"As an organization we take cybersecurity very seriously and have numerous measures in place to protect our data," wrote the hospital in a November 25 notice. "Thankfully, our team noticed unusual activity quickly and [acted] immediately."

The suspicious email, repeatedly sent from Delahunt's email address on November 25 was from Lorenz ransomware, stating all the hospital files across its entire system has been en-

cryptated, which includes private medical data.

The email continues, "We will publish all the contents of your company on our site includes all your confidential medical history, employers information, documentation, catalogs, reports, configs, mail, database's, invoice's, signature's etc."

The email says to prevent the attack, visit its website and follow its instructions which entails downloading a TOR browser and paying money to recover the files.

Lorenz is a new variant of S240 ransomware, which is designed to encrypt data and demand ransom for decryption. This means Lorenz renders affected files inaccessible and then asks for payment to regain access.

Meanwhile, the shutdown of Headwaters Health Care Centre's systems isn't impacting patient care, as the local hospital said they have "robust processes" in place.

Scheduled surgeries or procedures have not been impacted at the time of print, and the emergency department remains open 24/7.

With the COVID-19 assessment centre temporarily closed, alternative testing locations in Peel Region and in Wellington-Dufferin-Guelph have been given to residents.

At the time of print, there is no timeline for when the hospital expects to have their systems back up and running.

"This process will take some time. We are committed to being transparent and will notify individuals if we learn that personal information has been exposed," said Headwaters.

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Public Notice



NOTICE OF PUBLIC INFORMATION CENTRE NO. 2 COLERAINE DRIVE GRADE SEPARATION, SOUTH OF OLD ELLWOOD DRIVE, TOWN OF CALEDON MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY

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This notice was first issued on December 2, 2021

Six-year-old Caledon girl seizure-free thanks to medical cannabis

Changed the way Neurology Centre of Toronto's medical director views medicine

BY ROB PAUL

Right around her second birthday, Quinn, from Caledon, began experiencing debilitating seizures. Now, six years old, she has been seizure-free for over a year after being treated with medical cannabis.

Approximately four years ago, Quinn and her mother, Carrie, were referred to Dr. Evan Cole Lewis, Medical Director, Neurology Centre of Toronto and Chief Medical Advisor, JMCC Group, a specialist in paediatric and adult epilepsy.

Quinn was diagnosed with epilepsy by Dr. Lewis and prescribed a standard anti-seizure drug.

During Quinn's first year of treatment, Dr. Lewis tried several standard medications, none of which stopped Quinn's seizures.

Approximately one in every 100 Canadians has epilepsy and one third of people with epilepsy will continue to have seizures despite taking anti-seizure drugs and approximately 42,000 children/youth in Canada are living with epilepsy.

When a child is experiencing the frequent epileptic seizures that Quinn was, Dr. Lewis says it can quickly lead down a path that stunts development and leads to long-term effects beyond the seizures themselves. That was the biggest fear when they weren't seeing the prescribed medication work for her.

"In 2017, when Quinn came to me and we made the diagnosis of epilepsy, we went through the standard treatment with anti-epileptic drugs for her type of epilepsy and it didn't work," said Dr. Lewis. "About 30 per cent of patients have what we call drug-resistant epilepsy and that means you have failed two or more medications. What started to happen with her—we sometimes see this in young kids who develop really bad epilepsy and have ongoing abnormal brain activity—she started to lose her developmental skills. It's called regression, if a child is able to walk, they start to stumble or they can't use their hands as well as they had been, or their language starts to deteriorate; we started to see that with Quinn. It's due to excessive electricity in the brain that doesn't allow the brain to communicate with itself and it starts to lose its skills and prevents a child from learning new skills. That was the real big concern."

Eventually, Carrie broached the subject of

treating Quinn with medical cannabis oil and Dr. Lewis agreed to try it.

"It came out of my own research and before my daughter was diagnosed with epilepsy, I was aware that CBD was being used in the medical world," said Carrie. "I always did have it in the back of my head when she was diagnosed but being new to epilepsy I wanted to follow the guidance of the doctor and I think we just got to the point where we weren't seeing success with the typical AEDs that are prescribed to kids with epilepsy. I didn't view medical marijuana in a negative light, it didn't have a stigma to me, it was an opportunity and a potential way to control seizures, so why not try it?"

Having dabbled in medical cannabis before with patients, Dr. Lewis was well-versed in the topic enough to commit himself to understanding what it could do for a kid like Quinn, which ultimately led to him and Carrie agreeing to go down that path.

"Really it was Carrie who first brought up using cannabis; concurrently, I had been working with cannabis for some patients over the last few years prior to that but had never really gone out and prescribed cannabis to a lot of people," said Dr. Lewis. "For me, it was a bit nerve-racking as well. I didn't have a big sample of patients that I had used it on. Carrie and I had a few conversations about it and she had done a lot of work on it and knew a lot about it at that point. We went through the risks and benefits of starting it at that time and ultimately concluded that the other medications weren't working. Given all the data we knew at that time and that I had explored with my colleagues, we opted to try it for Quinn."

After one week of starting CBD-dominant medical cannabis, Quinn's seizures stopped, and she steadily began to walk normally with no falls—something she hadn't done in nearly two months.

"Obviously we were super emotional making that decision because it wasn't the case where we were able to take her off her other drugs right away, you have to kind of do a slow wean and just doing that and with the addition of CBD, her having the ability to walk—which we hadn't seen for weeks—was an, 'oh, my God' moment and just solidified for us that we made the right decision," Carrie said.

Dr. Lewis and his colleagues realized quite

quickly that medical cannabis was working for Quinn and that it exceeded their expectations with the positive results and ultimately had a greater effect than the regularly prescribed medication for epilepsy.

"There are variations of responders, some patients are robust responders and respond very quickly while some take a lot of time. For Quinn, she responded to this within a couple days," he said. "In terms of reduction of seizures, it happened nearly right away and within one week her seizures stopped completely. Not only that, but her clumsiness and inability to walk normalized and she returned to her normal movements, language, and cognition. Even more so, we tested her EEG before and after cannabis and the EEG—which measures the electro activity—became normal within six weeks of starting cannabis. That's not normal, we don't see that a lot—there's very few epilepsies that you treat with a medication and the EEG comes back normal when it's that bad. That was one of the biggest things because the more your EEG is normal, the better chance you have for your brain to develop properly."

However, Quinn's seizures returned one year later and Dr. Lewis suggested adding THC (or Delta-9-tetrahydrocannabinol) to Quinn's treatment, as THC can work with CBD to stabilize the endocannabinoid system—a system that is believed to play a role in the development and/or exacerbation of some epilepsies.

"She's over a year seizure free and just had her sixth birthday in November," Carrie said. "She's a twin, so when she had the diagnosis, we just wanted to ensure her milestones were on pace with her sister's—that was our baseline. Throughout everything she continued to hit her milestones and her development was never significantly impacted in a long-term way. She's just a regular kid."

Seeing firsthand what medical cannabis can do, Carrie wants people to understand that it's not just the drug it's been stigmatized, as it's so much more, and because of it her daughter is able to live a normal life.

"The stigma is there simply because marijuana is a drug and it's a drug that was historically used to get high recreationally," she said.

"There's certain associations people have with marijuana and the negative connotation of getting high, and I think it's a generational

stigma. Now, I think it's more readily acceptable in my generation and younger, especially now that it's been legalized. I think it's the way that it was always portrayed and the association of what a person who smokes marijuana looks like."

Dr. Lewis has treated a couple individuals with medical cannabis, with Quinn being the most prominent in terms of positive results. It's fully changed his outlook on medicine and has led him to committing himself to research in that realm.

"It was a paradigm shift for me," he said. "It has defined my medical career, it's defined where I'm going, and it's completely changed how I've looked at medicine and how I was taught medicine. I'm not exaggerating, there were things leading up to Quinn, but then certainly thereafter and where I am today has to do with my experience with Quinn and what I was privy to in having that opportunity to work with her. It has shifted everything for me."

As for how he sees the stigma, Dr. Lewis doesn't see it going away in his lifetime, but he's hopeful with stories like Quinn's being told, it will lead to a wider acceptance in the medical world because it can have a difference on someone else's life the way it did for her.

"At this point in time, there's certainly a trend towards destigmatization and broader acceptance. I would say that's the case both due to surveys that have been published and my own personal experiences with colleagues," he said. "I don't think we'll ever eliminate the stigma through the next two generations. I think it will take a real change of the guard for this to be completely destigmatized, it's hard to eliminate that when people were born in a culture where this stuff was vilified completely. It's going to take time, but if I were to classify it right now, I'd say there's a relaxed stigmatization. It's still there but there's a relaxation around the stiffness of the stigma and there's a big more openness to it now. You have a greater number of individuals now who are willing to listen rather than shut it out before they've even heard it."

In 2021, Dr. Lewis published a paper in the Canadian Journal of Neurological Sciences on Quinn's case and four others with pediatric epilepsy who responded to the addition of THC to their regimen of CBD-dominant medical cannabis—all with promising results.

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Shoebox Project returns for sixth year

The holidays are a time to give back, and with an ongoing housing crisis that has only been worsened by COVID-19, giving back this holiday season is more important than ever. On Monday (Dec. 6) The Shoebox Project announced its partnership with Allstate for the sixth consecutive year, to raise funds and support those affected by the housing crisis—specifically the thousands of Canadian women impacted by homelessness. Continuing its mission to fill shoeboxes with gifts, basic essentials and messages of support for women, this year The Shoebox Project will also be offering "virtual gifts" in the form of donations and gift cards. In fact, for

this week only (Dec 6–12), all donations will be matched by Allstate (up to \$25,000), with funds directly going to local chapters including Orangeville and shelters serving Indigenous women in remote regions.

Locally, Family Transition Place, Catholic Family Services Dufferin-Peel, and Salvation Army Family Resource Centre will be receiving the shoeboxes to distribute to women in need.

Those looking to create a virtual shoebox can visit: shooboxproject.ca/virtual/ca-en/ home or if you want to make a physical shoebox and drop off in person, go to www.shooboxproject.ca/ways-to-give/make-a-shoobox

Public Notice



NOTICE OF PUBLIC INFORMATION CENTRE NO. 2 COLERAINE DRIVE GRADE SEPARATION, SOUTH OF OLD ELLWOOD DRIVE, TOWN OF CALEDON MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY

The Region of Peel is conducting a Municipal Class Environmental Assessment (EA) Study for a grade separation of the Canadian Pacific Railway (CP) on Coleraine Drive in the Town of Caledon (see map). The purpose of the study is to consider grade separation improvements of the CP Rail line on Coleraine Drive, south of Old Ellwood Drive.

The Study is being conducted in compliance with Schedule "C" of the Municipal Class Environmental Assessment (Municipal Engineers Association, as amended in 2015), which is approved under the Ontario Environmental Assessment Act.

PUBLIC INFORMATION CENTRE NO. 2

Due to the current COVID-19 guidelines, the Public Information Centre (PIC) will be held in an online format to present the evaluation of alternative design concepts and recommended preliminary design.

To provide your input/comments you can:

- Visit the Project Website from December 16, 2021 to January 21, 2022 to review project details through an interactive PIC portal and provide your feedback online. The PIC is available via the project website at: <https://peel.ca/en/peel-transportation/construction/environmental-assessment/coleraine-drive.asp>

The Project Team will review and consider the feedback received at the PIC and confirm the recommended preliminary design. The next steps of the study include the preparation of an Environmental Study Report (ESR) to document the planning, consultation and design process. At the end of the study, the ESR will be made available for public review for a minimum of 30 calendar days and at that time, a Notice of Completion will also be issued.

If you have any questions or comments on the Study, please contact either of the following Project Team members:

Tarvo Malmood
Region of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, ON L6T 4B9
Tel: 905 791 7800 ext. 7828
Toll free: 1-888-919-7800
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Stephen Keen, P.Eng.
Consultant Project Manager
CIMA Canada Inc. (CIMA+)
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Burlington, ON L7N 3G7
Tel: 289-288-0287 ext. 6834
Email: skeen@peelregion.ca

The Region of Peel is committed to ensure that all Regional services, programs and facilities are inclusive and accessible for persons with disabilities. Please contact the Project Manager if you need any disability accommodation to participate in the study.

Information will be collected in accordance with the *Municipal Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

Follow [Peel Public Works on Twitter](https://www.facebook.com/peelpublicworks) for service updates and changes.

This notice was first issued on December 2, 2021

What's going on here? Peel mulls putting Coleraine Drive over CP line in Bolton to mitigate traffic concerns

Region hosting virtual public information centre on intersection crossing until Jan. 21



[Set Caledon as My Local news](#)

By Karen Martin-robins

Thu., Jan. 13, 2022 timer 1 min. read

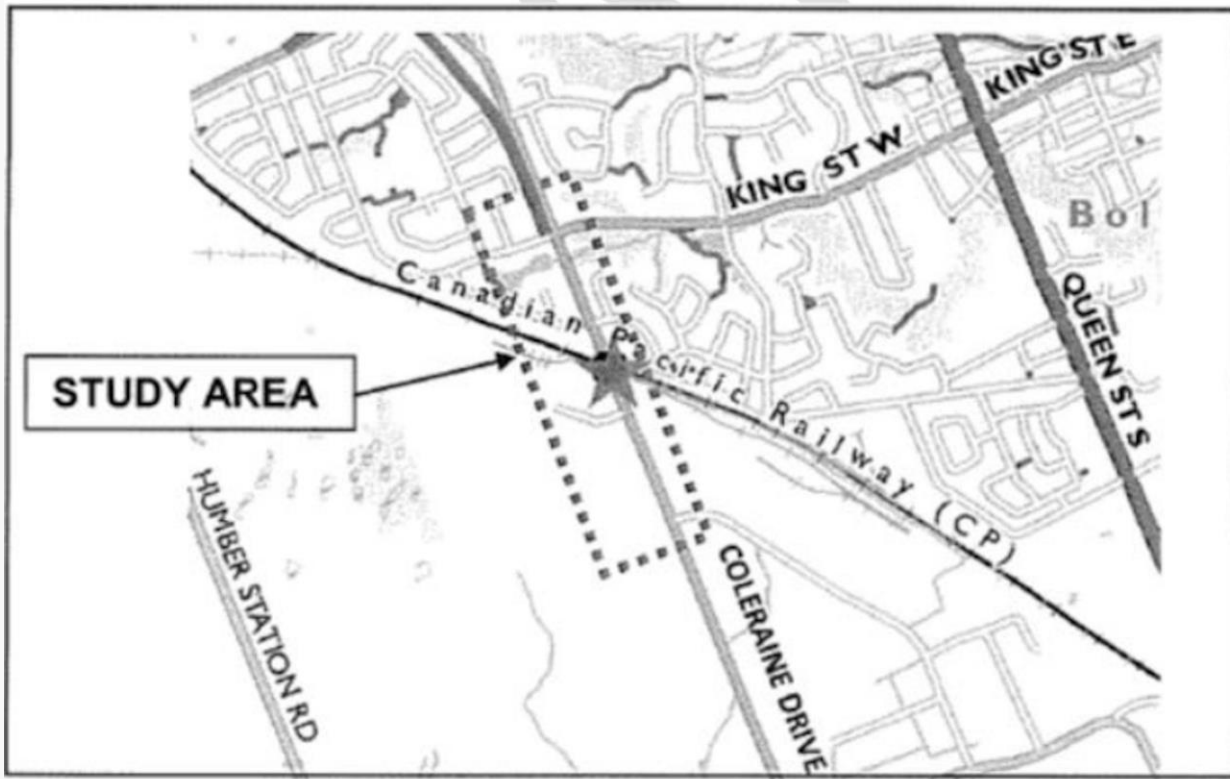
This concept photo shows one of the options the Region of Peel is considering for improving the CP Rail crossing at Coleraine Drive in Bolton.



The Region of Peel has been studying what to do about the CP rail line road crossing at Coleraine Drive and King Street West in Bolton, which is currently at grade level.

The issue is expected to get worst as the frequency of trains during peak hours increases, which will cause even more queues of traffic, according to the region.

JUST THE FACTS:



This map shows the study area for the Region of Peel's environmental assessment. (Region of Peel map)

- Peel is doing an environmental assessment study to look into the grade separation options for Coleraine Drive and the rail crossing.
- The area being studied is Coleraine Drive and CP Rail Crossing, south of Old Ellwood Drive, between Holland Drive and Harvest Moon Drive and King Street West.
- Options to raise or lower the rail grade were discarded after an initial public information centre in Oct. 2019. The region is still considering raising or lowering the road, including possible pedestrian crossings.

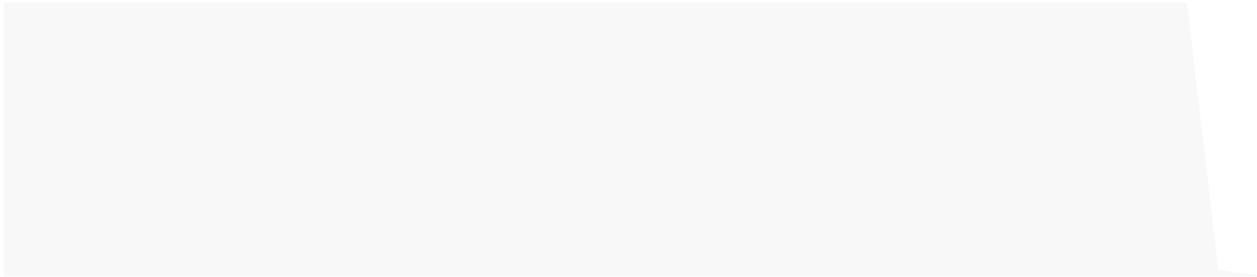
YOU MIGHT BE INTERESTED IN...

- At this time, the region's "preferred" option is putting the road over the rail line because it is less complicated for construction, has less risk of groundwater impacts and lower cost.

- Due to the current COVID-19 guidelines, the second public information centre is being held in an online format to present the evaluation of alternative design concepts and recommended preliminary design.
- Members of the public can participate online and give feedback.
- Once the feedback is received, the region will complete a preliminary design of the preferred option. The project is expected to go to detailed design in 2024, with construction in 2029.

RELATED STORIES

Proposed new transit-oriented community called ‘Caledon Station’ may be coming to Bolton



B

Appendix B





Virtual Public Information Centre for the Municipal Class Environmental Assessment Study for the Coleraine Drive Grade Separation Study

Results Summary
February 2022

The Virtual Public Information Centre (vPIC) was conducted using the PlanLocal™ public engagement service developed by Civicplan. Maps and graphics developed by Civicplan. Graphics of potential solutions provided by Cima+.

About Civicplan

Civicplan helps people shape communities through award-winning participatory planning, public engagement and strategies. For more information visit civicplan.ca

Context

The Region of Peel is undertaking an Environmental Assessment (EA) Study to investigate the grade separation options for Coleraine Drive and the Canadian Pacific (CP) Rail Crossing. As part of the study, a Virtual Public Information Centre (vPIC) was held online to present the evaluation of alternative design concepts and recommended preliminary design.

A vPIC is an online engagement process that allows participants to learn about the project and provide input on the key issues and concerns in the study area. The vPIC was held from December 2021 to January 2022 and outreach and promotion for the virtual process was conducted by the Region of Peel through a variety of channels. The vPIC also included a crowd map that allowed participants to “pin” comments to a map of the study area.

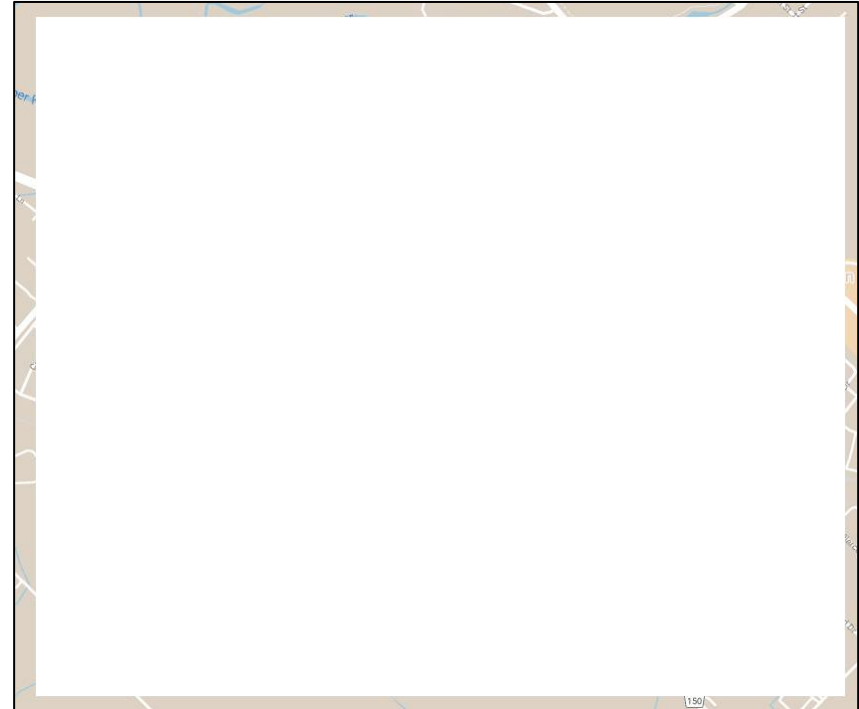
This report provides a summary of the results.

The screenshot shows the vPIC website interface. At the top, there is a navigation menu with items: 1. Welcome, 2. Study Area, 3. Objectives, 4. Alternative #1, 5. Alternative #2, 6. Privacy. The main content area features the Region of Peel logo and the text: "Welcome to the virtual Public Information Centre for the Municipal Class Environmental Assessment Study for the Coleraine Drive Grade Separation Study". Below this is a paragraph explaining the EA study and a "Privacy Statement". A registration form is present with fields for "Full Name" (Jane Doe), "Your Address" (123 Street Avenue), "Postal Code" (L0L 0L0), and "Email Address" (paul@civicplan.ca). To the right, there is a "Study Area and Crowd Map" section with a map showing a red pin and a comment box that reads: "I feel that Old Elwood should end at Station Road and not connect to Elwood Drive West. There is no need for this connection. It will only increase through traffic on a residential road." Below the map is a "Video Presentations" section with a play button icon and the text: "Click the images below to watch the video presentations". A video player is shown with the title "Public Information Centre (PIC) 2 Municipal Class EA Study For Coleraine Drive Grade Separation PRESENTATION 1 - OVERVIEW" and a timestamp of "December 06, 2021 - January 23, 2022".

vPIC Summary



Map of vPIC Participants



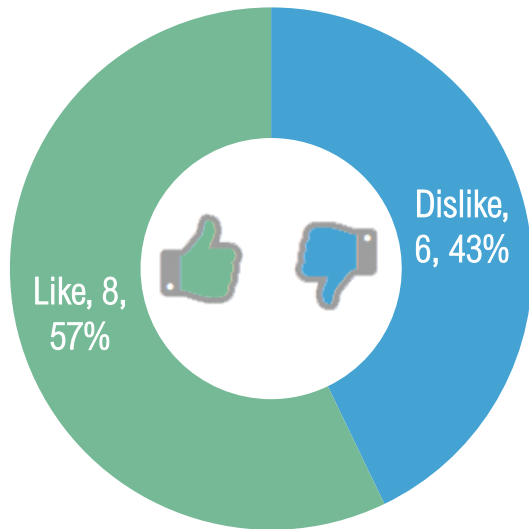
Note: Map displays postal codes of vPIC participants.

Note: Visitors are people who visited the vPIC and are defined as New Users. Whereas Participants are those who visited and submitted information.

vPIC Results: Neighbourhood Enhancements

Removing a length of Old Ellwood Drive presents an opportunity for neighbourhood enhancement options. Participants were presented with two options and asked to provide their feedback on each. The results of voting preferences are presented below, while comments about voting choices are presented in Table 1 on the following pages.

Option A: Natural Berm



Option B: Park Expansion

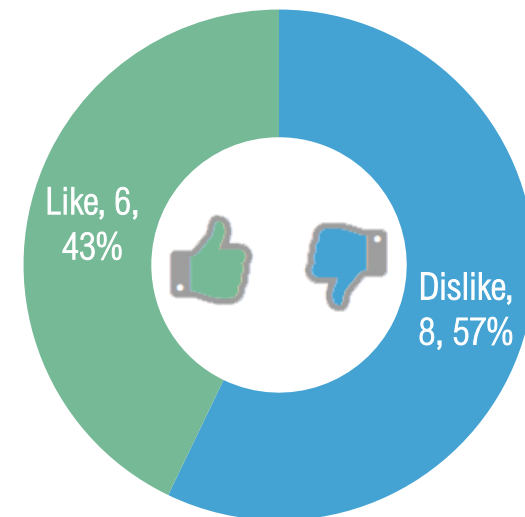


Table 1: Voting Comments on Neighbourhood Enhancements

Natural Berm		Park Extension	
Like	Dislike	Like	Dislike
<p>First off I don't think Old Ellwood should be connected to Ellwood Dr W. Old Ellwood Dr should now end at Station Road. Ellwood Dr W is a residential street and already has to many cars on it. Recently there has been numerous cars hitting and killing children on residential streets. We should be doing everything we can to reduce the number of cars using residential streets as major roads. If this connection with Old Ellwood is made and then the connection is made to Grapevine Dr under Coleraine it will basically be used for car races. Please think about the people that live here and not the cars driving on the streets. As for the natural berm anything is better then what it currently is, which is basically the train is visible from my front door.</p>	<p>Feels like a waste of space, and creates a dark gathering area</p>	<p>Again I don't believe the connection from Old Ellwood Dr to Ellwood Dr W should be made. Old Ellwood should end at Station Road. Keep the cars on the main roads and off the streets where kids play. Additional park space would be great as this area need more trees and natural spaces. There is nowhere for kid to kick a ball. Again think about the residents no the cars driving through the area.</p>	<p>Having an open park space right next to a train is not esthetically pleasing. None of these plans are.</p>

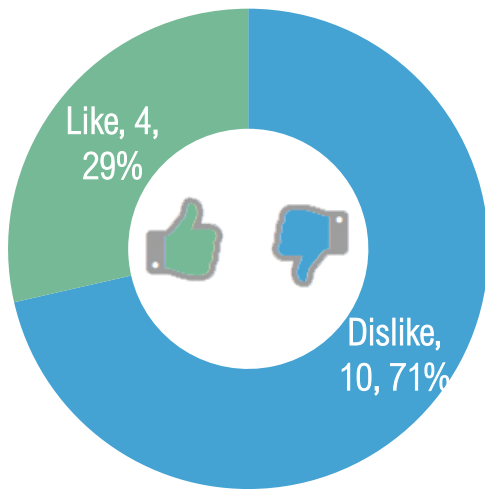
Natural Berm		Park Extension	
Like	Dislike	Like	Dislike
The Berm will help diminish the view of the train.		Planting of Large full trees is environmentally responsible and will assist in defining a residential neighborhood vs. large industrial - also will allow for better mental health of the community	Would rather not have the additional park expansion so close to the railway.
Ecological natural sound barrier air cleaner and filtering smog barrier all in one			
Planting of Large full trees is environmentally responsible and will assist in defining a residential neighborhood vs. large industrial		If you could also ensure planting of several trees and greenery and have it incorporated; an extension to a park area would be great for young families and families in surrounding neighborhoods	It comes down to I don't believe the town will be able to maintain another park.
I think its a wonderful idea to plant a natural berm which will allow the esthetics of the existing industrial area to be softened.		Allows for use of space by the community	
The trees could help act as a natural sound barrier. The train traffic is really loud because of the proximity of the tracks to houses.		Yes, good idea, but will require adequate fencing along the rail line and newly aligned Ellwood. Will also require design and enforcement to ensure that this does not simply become an informal off-leash dog park	

Natural Berm		Park Extension	
Like	Dislike	Like	Dislike
Provides slight noise barrier for adjacent residential space.			
I prefer this option for the wildlife. Environmentally speaking this is the better option then having grass to take care of.			

vPIC Results: Road Modifications

Participants were presented with two options for road modifications at Old Ellwood Drive in the Road Over Rail alternative and asked to provide their feedback on each. The results of voting preferences are presented below, while comments about voting choices are presented in Table 2 on the following pages.

Option A: A connection between Ellwood Drive West & Grapevine Road (**accommodating all traffic**)



Option B: Cul-de-sac Ellwood Drive West & provide an **Active Transportation** connection to Grapevine Road

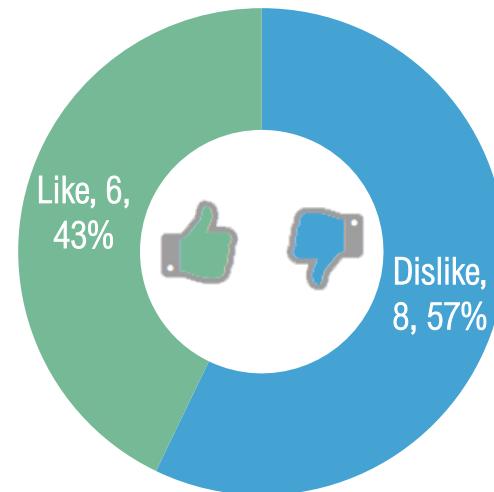
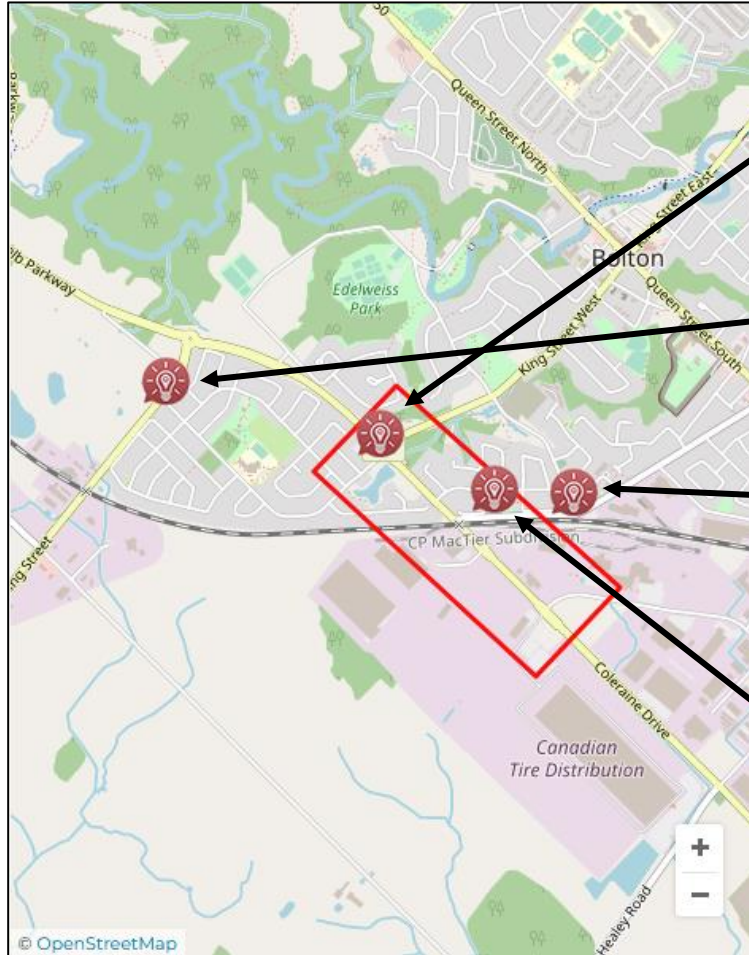


Table 2: Voting Comments on Road Modifications

Connection b/w Ellwood and Grapevine		Cul-de-Sac	
Like	Dislike	Like	Dislike
<p>This will keep traffic off of Station Road (yes, I know it is outside of the study area). I feel bad for residents on Grapevine especially, since it is currently a dead-end, but for the greater good, this connection makes the most sense. Again, I don't see the overall project as necessary, but if it goes ahead, please connect Ellwood and Grapevine.</p>	<p>residential street is not meant to become a thru street with ongoing traffic dangerous for families and elderly residents.</p>	<p>This is the only option. If this does not happen I will be forced to move as well as multiple families in the area that I have personally spoken to. Please take the residents side on this and not the side of the cars driving through the area. Why not keep travelers on main roads. By creating a cul-de-sac you can make this neighborhood better. There is no logistical need to connect Ellwood Dr W to Grapevine Dr under Coleraine.</p>	<p>While this is likely preferred option for residents closest to the proposed cul-de-sac, this option forces westbound traffic onto Station, then King. Station speed limits are not currently enforced, and a dramatic increase to traffic would be a major safety concern, especially now that visitors to the Sorrento development park on Station, on a downhill curve.</p>
<p>Having additional through points for motor vehicle traffic (in case of accidents/closures) is much preferred</p>	<p>Absolutely not. This will increase the traffic immensely.</p>	<p>Better option for family with young kids.</p>	<p>Dislike because the Road Over Rail is not a smart option and one I am against and do not approve.</p>
<p>It is always better to have more ways to flow traffic.</p>	<p>Dislike because the Road Over Rail is not a smart option and one I am against and do not approve.</p>	<p>Cul de sac with only active transportation access</p>	<p>It is always better to have more ways to flow traffic.</p>
	<p>Dislike very much. as we have _____ and the _____ was a big selling point for us</p>	<p>This is a better option as there will be local traffic only.</p>	

Crowd Map Results

Four Crowd Map locations were submitted. The locations and associated comments are presented below.



“Harvest Moon and Coleraine: This is the first access point to this neighborhood. There is no need to connect Ellwood Drive West to Grapevine Road. The only benefit I see is efficiency in garbage pickup and snow removal. Please cul-de-sac Ellwood Drive West.”

“King Street and Harvest Moon: This is the second access point. Again there is no need to connect Ellwood Drive West to Grapevine Road as there is already two access points to gain entry into this neighborhood.”

“Old Ellwood and Station Road: I feel that Old Ellwood should end at Station Road and not connect to Ellwood Drive West. There is no need for this connection. It will only increase through traffic on a residential road.”

“Old Ellwood Drive and Ellwood Drive West: This is the only green area around where kids can play. Being that it’s the only green area in the immediate vicinity I find it to be a bad decision by the region to pave a road through this area. This will have little to no benefit to the area residents. Please do not connect Old Ellwood to Ellwood Drive West it will only reduce the green space of this neighborhood. If my kids want to kick a soccer ball I shouldn’t have to get in my car to take them to a park miles away.”

vPIC Results: Participant Comments

Participants were asked to share comments on the preliminary preferred alternative of road over rail as well as the suggested intersection improvement of a roundabout at the Harvest Moon Drive / King Street & Coleraine Drive intersection. Finally, participants were able to share general comments. Table 3 provides the full list of comments received for these three questions.

Please Note: The privacy statement at the beginning of the vPIC advised participants that their comments and feedback would be reported anonymously. Thus, Table 3 presents comments received, as is, without names and contact information.



Table 3: Full Participant Comments

Q: "Please share any comments you have about the preliminary preferred alternative here."
<p>This is the worst rail crossing I have ever driven over. I have to put my hazards on and crawl across it. This is extremely dangerous as I worry about being hit from behind. Glad something is being done. Can you also please look at adding barriers at the end of Ellwood Drive West? It is so easy for people to walk on to the tracks and if a train derails, the immediate neighbourhood will be at serious risk. I also think the speed of trains should be considered. There is so much more train traffic and some operators are travelling at a very high speed through this area. Thanks!</p>
<p>I think that this option makes sense, between the two suggestions. Ellwood to Grapevine will become a major thoroughfare. I would like to know what will happen to the area west of Ellwood, where the current road footprint sits. Will that become greenspace, or will it just be a vacant area for debris? Or will the property be sold and developed? Overall, I think that spending this kind of money to avoid queues occasionally doesn't make sense. There is a rail overpass in town, and although that does not help trucks, so be it. Coleraine has been overdeveloped. Coleraine speed limits are ignored, especially by trucks and construction vehicles. Before dropping tens of millions in construction and development that will make that section feel even more like a highway, how about adding speed cameras, pay for additional/dedicated enforcement along the study stretch, and at the King/Harvest Moon/Coleraine intersection? Likewise, why not add a right turn lane from Ellwood onto Coleraine? Or put in obstacles so that drivers can no longer drive on the shoulder and over the curb to make that right hand turn? I am concerned about changing that intersection to a roundabout. People and large vehicles seem to have difficulty managing the existing roundabouts in the region, nevermind one at a major intersection. Recognizing it is outside of the study area, there are unintended consequences with this project, mainly an increase on traffic volumes and speed on Station Road, during construction, but also after construction, as people will use Station to get to King and head west on Harvest Moon.</p>
<p style="text-align: center;">I am very disappointed. We will have significant noise and light blockage. We will definitely have to move which is completely unfair and unacceptable.</p>

The city has failed us once again. This is absolutely infuriating. For once, do what's best for your residents. As a resident who will be immediately affected by either option, the Road Over option is infuriating. NO resident wants to stare at a giant concrete wall, the train is bad enough. The city failed when they allowed this housing development to be built too close to the train. Now you want to make it worse by having huge barriers and transport truck flying high above residential homes and all to save a few million dollars! Maybe the town should have these billion dollar corporations, such as Amazon and Canadian Tire, who they've allowed into the community pay their fair share of taxes. I'm sure you can come up with that \$20 million dollars in no time. Stop punishing your residents like this.

I still prefer the road under rail as it will have a lesser visual impact

Regardless of option is there any current plans to alter the current railroad crossing at King east of Humber Station as any overpass will increase traffic on that stretch of road way? Currently there are long lines when there's an interruption of traffic flow due to train crossing. if so should any work be done simultaneously?

I would prefer road under rail.

My preferred choice is the road under rail. I understand that there is additional costs but this is a residential area and more then just cost should be considered. My family is going to have to live beside this massive concrete structure and it will negatively effect the entire neighborhood. I would understand making a decision based on cost in a industrial area but this area is not industrial area. Think about the residents first.

Q: "Please share any comments you have about the preliminary preferred intersection improvement here."
A roundabout is a good idea.
Current stretches of coleraine north of king/harvest moon harbours an increase in traffic speeds and noise. Would prefer traffic lights as it would minimize this. This stretch at times sounds like a racing circuit as cars use the roundabout and return in the opposite direction at increased speeds.
Honestly, I think the existing should remain - with the amount of truck traffic that runs on this corridor as the by-pass for trucking; it should remain as traffic light controlled. A round-a-bout with these trucks will be a guaranteed collision intersection with possible loss of life.
I don't understand why we need to change what is already in placed and has been working very well. I feel that this is a waste of money.
I prefer the roundabout.
<p>I think a roundabout here would be a disaster. Spend 10 minutes at either roundabout in town between 6:30 and 9 any morning, or 4:30 and 6 any evening and you'll see that years after they were opened, people still don't use the roundabouts properly.</p> <p>Speed and aggressive driving are bad enough as is.</p> <p>A changed intersection isn't required, enforcement of existing laws and speed limits are.</p> <p>Put in red light and speed cameras.</p> <p>Get OPP there on a regular yet unpredictable basis, so that residents and truck drivers are afraid to be ticketed.</p>
<p>Leave existing traffic light controlled intersection.</p> <p>Too many trucks travel this route and a round-a-bout will result in accidents with these trucks.</p> <p>Traffic Light controlled intersection provides better safety for the community and for people walking / biking, etc.</p>
roundabout much better option

The existing signalized intersection is preferred here. I do not see a benefit to reconstructing the intersection when it is already signalized. There may be the idea of reduced speed at the intersection, however there is quite a bit of traffic that flows through that intersection and I anticipate (with increases in residency) that the volume of traffic that would result would be too great for a roundabout. Thus the likelihood of non-fatal incidents as mentioned would increase much more than the risk of having a slightly higher speed with a signalized intersection. Also there is a roundabout just north of that intersection already as a traffic calming measure.

Yes. I am in favor of a roundabout.

Q: "If you have additional comments you would like to share, please include them here."

I really think everything possible should be done by the region to make this area more livable. By connecting Ellwood Drive West to Grapevine Road you will only increase traffic in this residential area. Ellwood Drive W should become a cul-de-sac. By creating a cul-de-sac the neighborhood will be more peaceful and the numerous young families that live here will be able to let their kids play on their driveways without the fear of cars racing down the street. Grapevine Road and the surrounding neighborhood west of Coleraine is already easily accessible via 2 access points. Harvest Moon Drive off of Coleraine, and Harvest Moon Drive also connects to King Street, north of this same neighborhood. There is no need for an additional access point to Grapevine Road and the surrounding neighborhood. I really feel that this is the most important decision. Please Cul-de-sac Ellwood Drive West & provide an Active Transportation connection to Grapevine Road. Also there is a lack of green space in this area so I think its very important to increase the number of trees and any additional steps that can be taken to reduce the noise from both Coleraine Drive and the existing railway. Again I just wanted to point out that the region was counting cars on these roads during the height of the pandemic. Thus the number of cars on the roads at the time was substantially less then normal. This should also be taken into account. Thank you very much for letting me have a say. I hope the Region of Peel keeps the residents of this area top of mind when these decisions are made.

Mr. Mahmood and Mr. Keen,
I would like to ask that you truly take into consideration of what is being proposed as the 'preferred' option - Road Over Rail. This is not the way to go for many reasons, as I understand the costs are significant, but over the long run, going Road Under Rail will be the most beneficial to the existing residences and to any of the new residences coming into Bolton.

Please consider that there are a lot of family with young kids in the area when you finalize your plan and what is best for the community. It will be safer for if you make Ellwood Drive West a cul-de-sac. Thank you.

Pls keep me up to date. Thanks

The adjacent neighborhoods of Coleraine have already suffered enough. With all the large industrial being allowed to be built; having Coleraine the main throughway for truck traffic; and the existing CP rail line that runs all hours of the day and night - now proposing to put a 4 lane roadway up in the sky at our rooflines is what you want to do? Please - do not go this route - It is understood that the rail line has to be made accommodating for future GO Service and town expansion which is fine, but please do so with the Road Under Rail option regardless of the high cost. Do the right thing - the large cost of Road Under Rail will be covered with the growing expansion of the town coming over the years.

This is something that is long overdue. I love the green spaces and town life we have here. However it is important to always look forward to the future. With bring the GO train up here we will need this, and I feel that this project has been left to do at the last minute.

This project seems overkill for a minor inconvenience of occasionally having to wait for a train crossing. Spend a fraction of the budget on improving existing conditions and enforcement through policing and cameras. If the businesses along Coleraine don't like waiting for trains, they should have considered that, and they should reroute their trucks down to Mayfield and up Coleraine, rather than across King and down Coleraine. There are solutions that would cost less and be less disruptive to local residents.

This will take several years to implement. Please look at some short term solutions for the Coleraine/Old Ellwood intersection in the mean time. It really is horrible.

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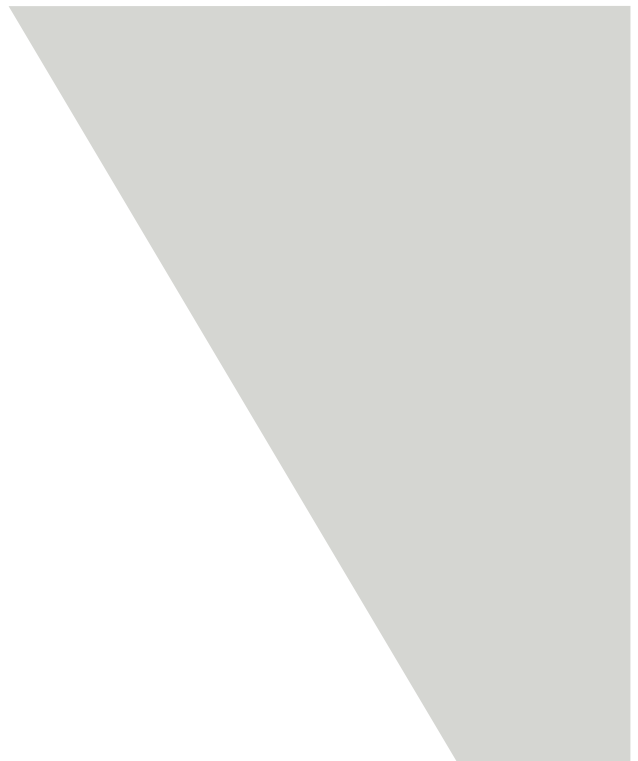
SUBMITTED BY CIMA CANADA INC.

400-3027 Harvester Road

Burlington, ON L7N 3G7

T: 289 288-0287 F: 289 288-0285

cima.ca



A-2

Appendix A-2: Agency Consultation

- A-2-1: Ministry of Indigenous Affairs**
- A-2-2: Ministry of Environment, Conservation and Parks (MCEP)**
- A-2-3: Ministry of Tourism, Culture and Sport (MTCS)**
- A-2-4: Ministry of Northern Development, Mines, Natural Resources, and Forestry (MNDMNR)**
- A-2-5: Toronto and Region Conservation Authority (TRCA)**
- A-2-6: Transport Canada**
- A-2-7: Fisheries and Oceans Canada**
- A-2-8: Technical Agencies Committee**

A-2

Appendix A-2-1: Ministry of Indigenous Affairs

Kate Barclay

From: Jessica Dorgo
Sent: March 3, 2017 1:42 PM
To: 'maa.ea.review@ontario.ca'
Cc: Jennifer Haslett
Subject: RE: B738_Peel Coleraine Drive Grade Separation EA_First Nations and Metis Communities

Good afternoon,

This message is to follow up with our inquiry sent January 26th, 2017. Please see the message below regarding the Region of Peel Municipal Class Environmental Assessment (EA) for Coleraine Drive south of Old Ellwood Drive in the Town of Caledon.

We have identified the following First Nations and Metis communities as potential interested stakeholders in our study:

- Credit River Metis Council
- Curve Lake First Nation
- Mississaugas of Scugog First Nation
- Chippewas of Georgina Island
- Alderville First Nation
- Mississaugas of the New Credit First Nation

We like to request your assistance in identifying additional First Nations or Metis communities that may have interest in our study.

Thank you,

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation

CIMA+
Partners in Excellence

3027 Harvester Road, Suite 400
Burlington Ontario L7N 3G7
CANADA
Tel: 289-288-0287 ext. 6819 / Fax: 289-288-0285



CONFIDENTIALITY WARNING This e-mail is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: Jessica Dorgo
Sent: Thursday, January 26, 2017 9:09 AM
To: maa.ea.review@ontario.ca
Cc: Jennifer Haslett <Jennifer.Haslett@cima.ca>
Subject: B738_Peel Coleraine Drive Grade Separation EA_First Nations and Metis Communities

Good morning,

The Region of Peel has initiated a Municipal Class Environmental Assessment (EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. A key map of the study area is provided below.

The Region of Peel Long Range Transportation Plan (LRTP) 2012 identifies significant growth expected in Peel Region over the next 20 years and the associated transportation issues including traffic congestion. The study will assess the impacts and benefits of proposed road improvements in this area on cultural, economic and natural environments as well as safety.

The study is being conducted in compliance with *Schedule C* of the *Municipal Class Environmental Assessment* process (October 2000, as amended in 2007, 2011 and 2015), which is approved under the *Ontario Environmental Assessment Act*.

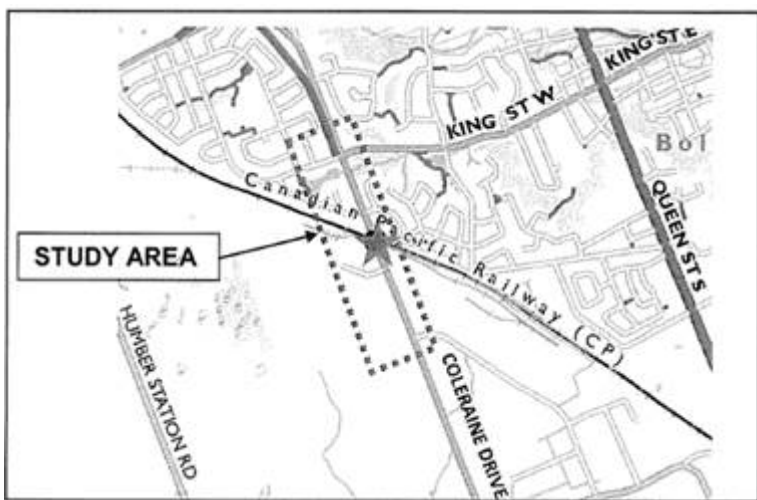
The study will define the problem, identify alternative solutions, and determine a preferred solution. Consultation with the public and stakeholders including First Nations and Metis communities is a key component of the study. Two (2) Public Information Centres will be held to review the study and obtain public input on issues related to alternative solutions and design, environmental impacts and mitigation measures.

We have identified the following First Nations and Metis communities as potential interested stakeholders in our study:

- Credit River Metis Council
- Curve Lake First Nation
- Mississaugas of Scugog First Nation
- Chippewas of Georgina Island
- Alderville First Nation
- Mississaugas of the New Credit First Nation

We like to request your assistance in identifying additional First Nations or Metis communities that may have interest in our study.

I can be contacted via email at Jessica.dorgo@cima.ca or by phone 289-2888-0287 ext. 6819.



Thank you,

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation

A-2

Appendix A-2-2: Ministry of Environment, Conservation and Parks (MECP)

Kate Barclay

From: Jessica Dorgo
Sent: February 2, 2017 9:48 AM
To: 'trevor.bell@ontario.ca'
Cc: Jennifer Haslett
Subject: B738_Peel Coleraine Drive Grade Separation EA_First Nations and Metis Communities

Good morning Mr. Bell,

The Region of Peel has initiated a Municipal Class Environmental Assessment (EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. A key map of the study area is provided below.

The Region of Peel Long Range Transportation Plan (LRTP) 2012 identifies significant growth expected in Peel Region over the next 20 years and the associated transportation issues including traffic congestion. The study will assess the impacts and benefits of proposed road improvements in this area on cultural, economic and natural environments as well as safety.

The study is being conducted in compliance with *Schedule C* of the *Municipal Class Environmental Assessment* process (October 2000, as amended in 2007, 2011 and 2015), which is approved under the *Ontario Environmental Assessment Act*.

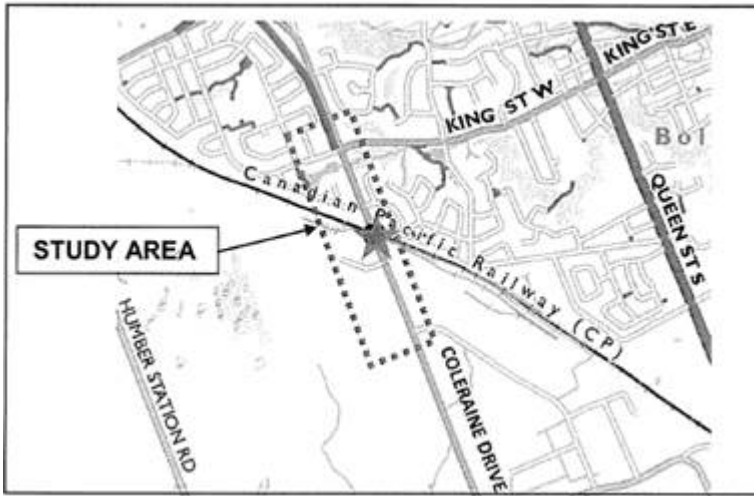
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We have identified the following First Nations and Metis communities as potential interested stakeholders in our study:

- Credit River Metis Council
- Curve Lake First Nation
- Mississaugas of Scugog First Nation
- Chippewas of Georgina Island
- Alderville First Nation
- Mississaugas of the New Credit First Nation

We like to request your assistance in identifying additional First Nations or Metis communities that may have interest in our study.

I can be contacted via email at Jessica.dorgo@cima.ca or by phone 289-2888-0287 ext. 6819.



Thank you,

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation

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Kate Barclay

From: Bell, Trevor (MOECC) <Trevor.Bell@ontario.ca>
Sent: February 10, 2017 12:20 PM
To: Jessica Dorgo
Cc: Martin, Paul (MOECC); Webb, Tim (MOECC)
Subject: Coleraine Drive Grade Separation Schedule C Municipal Class EA
Attachments: TSS_NoSC_Response Letter_Coleraine Drive Grade Separation_signed.PDF; TSS_NoSC_Response Letter_Coleraine Drive Grade Separation.docx; A Proponent's Introduction to the Delegated Aspects of Consultation with....pdf

Follow Up Flag: Follow up
Flag Status: Flagged

Good afternoon,

Please find attached a letter from the Ministry of the Environment and Climate Change, Central Region Technical Support Section regarding the above mentioned project. Feel free to contact me directly with any questions or concerns you may have.

Sincerely,

Trevor Bell

Environmental Resource Planner and EA Coordinator
Technical Support Section | Central Region
Ministry of the Environment and Climate Change
5775 Yonge St., 8th Floor
Toronto, ON M2M 4J1
T: 416-326-3577
E: trevor.bell@ontario.ca

Ministry of the Environment
and Climate Change

Central Region
Technical Support Section

5775 Yonge Street, 8th Floor
North York, Ontario M2M 4J1

Tel.: (416) 326-6700
Fax: (416) 325-6347

Ministère de l'Environnement et de
l'Action en Matière de Changement Climatique

Région du Centre
Section d'appui technique

5775, rue Yonge, 8^{ième} étage
North York, Ontario M2M 4J1

Tél. : (416) 326-6700
Télec. : (416) 325-6347



February 10, 2017

File No.: EA 01-06-05

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation
CIMA+
jessica.dorgo@cima.ca

Re: **Coleraine Drive Grade Separation
Region of Peel
Schedule C Municipal Class Environmental Assessment
Response to Notice of Commencement**

Dear Ms. Dorgo:

This letter acknowledges that the Region of Peel has initiated a Schedule C project under the Municipal Engineers Association's Municipal Class Environmental Assessment (Class EA) for the Coleraine Drive Grade Separation in the Town of Caledon.

The attached "Areas of Interest" document provides guidance regarding the ministry's interests with respect to the Class EA process. Please identify the areas of interest which are applicable to your project and ensure they are addressed. Proponents who address all of the applicable areas of interest can minimize potential delays to their project schedule.

The Crown has a legal duty to consult Aboriginal communities when it has knowledge, real or constructive, of the existence or potential existence of an Aboriginal or treaty right and contemplates conduct that may adversely impact that right. Before authorizing this project, the Crown must ensure that its duty to consult has been fulfilled, where such a duty is triggered. Although the duty to consult with Aboriginal peoples is a duty of the Crown, the Crown may delegate procedural aspects of this duty to project proponents while retaining oversight of the consultation process.

Your proposed project may have the potential to affect Aboriginal or treaty rights protected under Section 35 of Canada's *Constitution Act* 1982. Where the Crown's duty to consult is triggered in relation to your proposed project, **the MOECC is delegating the procedural aspects of rights-based consultation to you through this letter.** The Crown intends to rely on the delegated consultation process in discharging its duty to consult and maintains the right to participate in the consultation process as it sees fit.

Based on information you have provided to date and the Crown's preliminary assessment you are required to consult with the following communities who have been identified as potentially affected by your proposed project:

- Mississaugas of the New Credit First Nation
- The Huron Wendat should be notified in instances where there is potential for archaeological resources to be discovered

Steps that you may need to take in relation to Aboriginal consultation for your proposed project are outlined in the “Code of Practice for Consultation in Ontario’s Environmental Assessment Process” which can be found at the following link: <https://www.ontario.ca/document/consultation-ontarios-environmental-assessment-process>

Additional information related to Ontario’s Environmental Assessment Act is available online at: www.ontario.ca/environmentalassessments

Please also refer to the document “A Proponent’s Introduction to the Delegation of Procedural Aspects of consultation with Aboriginal Communities” for further information.

You must contact the Director of Environmental Approvals Branch under the following circumstances subsequent to initial discussions with the communities identified by MOECC:

- Aboriginal or treaty rights impacts are identified to you by the communities
- You have reason to believe that your proposed project may adversely affect an Aboriginal or treaty right
- Consultation has reached an impasse
- A Part II Order request or elevation request is expected

The Director of the Environmental Approvals Branch can be notified either by email with the subject line “Potential Duty to Consult” to EAASIBGen@ontario.ca or by mail or fax at the address provided below:

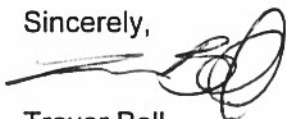
Email:	EAASIBGen@ontario.ca Subject: Potential Duty to Consult
Fax:	416-314-8452
Address:	Environmental Approvals Branch 135 St. Clair Avenue West, 1 st Floor Toronto, ON, M4V 1P5

The MOECC will then assess the extent of any Crown duty to consult for the circumstances and will consider whether additional steps should be taken, including what role you will be asked to play in them.

A draft copy of the Environmental Study Report (ESR) should be sent to this office prior to the filing of the final report, allowing a minimum of 30 days for the ministry’s technical reviewers to provide comments. Please also forward the Notice of Completion and final ESR to me when completed.

Should you or any members of your project team have any questions regarding the material above, please contact me at (416) 326-3577 or by email at trevor.bell@ontario.ca.

Sincerely,



Trevor Bell
Environmental Resource Planner and EA Coordinator
Air, Pesticides and Environmental Planning
Central Region Technical Support Section
Ministry of the Environment and Climate Change

- c. P. Martin, Supervisor, APEP, Central Region, MOECC
T. Webb, Manager (A), Halton Peel District Office, MOECC
Central Region EA File
A & P File

AREAS OF INTEREST

It is suggested that you check off each applicable area after you have considered / addressed it.

Source Water Protection (all projects)

The Clean Water Act, 2006 (CWA) aims to protect existing and future sources of drinking water. To achieve this, several types of vulnerable areas have been delineated around surface water intakes and wellheads for every municipal residential drinking water system that is located in a source protection area. These vulnerable areas are known as a Wellhead Protection Areas (WHPAs) and surface water Intake Protection Zones (IPZs). Other vulnerable areas that have been delineated under the CWA include are Highly Vulnerable Aquifers (HVAs), Significant Groundwater Recharge Areas (SGRAs), Event-based modelling areas (EBAs), and Issues Contributing Areas (ICAs). Source protection plans have been developed that include policies to address existing and future risks to sources of municipal drinking water within these vulnerable areas.

Projects may include activities that, if located in a vulnerable area, could be a threat to sources of drinking water (i.e. have the potential to adversely affect the quality or quantity of drinking water sources) and the activity could therefore be subject to policies in a source protection plan. Where an activity poses a risk to drinking water, policies in the local source protection plan may impact how or where that activity is undertaken. Policies may prohibit certain activities, or they may require risk management measures for these activities. Municipal Official Plans, planning decisions, Class EA projects (where the project includes an activity that is a threat to drinking water) and prescribed instruments must conform with policies that address significant risks to drinking water and must have regard for policies that address moderate or low risks.

- As part of the project, the proponent should clearly document how the proximity of the project to sources of drinking water (municipal or other) and any delineated vulnerable areas was considered and assessed, whether there were any source protection plan policies that applied, and if so, how they impacted the project, as well as identify mitigating measures to address any negative environmental impacts to those sources (considering natural, economic and social/cultural environmental impacts). As you may be aware, in October 2015, the MEA Parent Class EA document was amended to include reference to the Clean Water Act (Section A.2.10.6) and indicates that proponents undertaking a Municipal Class EA project must identify early in their process whether a project is or could potentially be occurring with a vulnerable area. Given this requirement, the proponent should include a section in the ESR on source water protection.
- While most source protection plans focused on including policies for significant drinking water threats in the WHPAs and IPZs it should be noted that even though source protection plan policies may not apply in HVAs, these are areas where aquifers are sensitive and at risk to impacts and within these areas, activities may impact the quality of sources of drinking water for systems other than municipal residential systems.
- In order to determine if this project is occurring within a vulnerable area, proponents can use this mapping tool: <http://www.applications.ene.gov.on.ca/swp/en/index.php> The mapping tool will also provide a link to the appropriate source protection plan in order to identify what policies may be applicable in the vulnerable area.
- For further information on the maps or source protection plan policies which may relate to their project, proponents should contact the Project Manager for Drinking Water Source Protection at the local source protection authority (i.e., conservation authority).

More Information

For more information on the Clean Water Act, source protection areas and plans, including specific information on the vulnerable areas and drinking water threats, please refer to Conservation Ontario's website where you will also find links to the local source protection plan/assessment report.

A list of the prescribed drinking water threats can be found in section 1.1 of Ontario Regulation 287/07 made under the Clean Water Act. In addition to prescribed drinking water threats, some source protection plans may include policies to address additional "local" threat activities, as approved by the MOECC.

□ **Ecosystem Protection and Restoration**

- Any impacts to ecosystem form and function must be avoided where possible. The ESR should describe any proposed mitigation measures and how project planning will protect and enhance the local ecosystem.
- All natural heritage features should be identified and described in detail to assess potential impacts and to develop appropriate mitigation measures. The following sensitive environmental features may be located within or adjacent to the study area:
 - Areas of Natural and Scientific Interest (ANSIs)
 - Rare Species of flora or fauna
 - Watercourses
 - Wetlands
 - Woodlots

We recommend consulting with the Ministry of Natural Resources and Forestry (MNR), Fisheries and Oceans Canada (DFO) and your local conservation authority to determine if special measures or additional studies will be necessary to preserve and protect these sensitive features. In addition, you may consider the provisions of the Rouge Park Management Plan if applicable.

□ **Surface Water**

- The ESR must include a sufficient level of information to demonstrate that there will be no negative impacts on the natural features or ecological functions of any watercourses within the study area. Measures should be included in the planning and design process to ensure that any impacts to watercourses from construction or operational activities (e.g. spills, erosion, pollution) are mitigated as part of the proposed undertaking.
- Additional stormwater runoff from new pavement can impact receiving watercourses and flood conditions. Quality and quantity control measures to treat stormwater runoff should be considered for all new impervious areas and, where possible, existing surfaces. The ministry's Stormwater Management Planning and Design Manual (2003) should be referenced in the ESR and utilized when designing stormwater control methods. We recommend that a Stormwater Management Plan should be prepared as part of the Class EA process that includes:
 - Strategies to address potential water quantity and erosion impacts related to stormwater draining into streams or other sensitive environmental features, and to ensure that adequate (enhanced) water quality is maintained
 - Watershed information, drainage conditions, and other relevant background information
 - Future drainage conditions, stormwater management options, information on erosion and sediment control during construction, and other details of the proposed works
 - Information on maintenance and monitoring commitments.

- Ontario Regulation 60/08 under the Ontario Water Resources Act (OWRA) applies to the Lake Simcoe Basin, which encompasses Lake Simcoe and the lands from which surface water drains into Lake Simcoe. If the proposed sewage treatment plant is listed in Table 1 of the regulation, the ESR should describe how the proposed project and its mitigation measures are consistent with the requirements of this regulation and the OWRA.

Groundwater

- The status of, and potential impacts to any well water supplies should be addressed. If the project involves groundwater takings or changes to drainage patterns, the quantity and quality of groundwater may be affected due to drawdown effects or the redirection of existing contamination flows. In addition, project activities may infringe on existing wells such that they must be reconstructed or sealed and abandoned. Appropriate information to define existing groundwater conditions should be included in the ESR.
- If the potential construction or decommissioning of water wells is identified as an issue, the ESR should refer to Ontario Regulation 903, Wells, under the OWRA.
- Potential impacts to groundwater-dependent natural features should be addressed. Any changes to groundwater flow or quality from groundwater taking may interfere with the ecological processes of streams, wetlands or other surficial features. In addition, discharging contaminated or high volumes of groundwater to these features may have direct impacts on their function. Any potential effects should be identified, and appropriate mitigation measures should be recommended. The level of detail required will be dependent on the significance of the potential impacts.
- Any potential approval requirements for groundwater taking or discharge should be identified in the ESR. In particular, a Permit to Take Water (PTTW) under the OWRA will be required for any water takings that exceed 50,000 litres per day.

Air Quality, Dust and Noise

- If there are sensitive receptors in the surrounding area of this project, an air quality/odour impact assessment will be useful to evaluate alternatives, determine impacts and identify appropriate mitigation measures. The scope of the assessment can be determined based on the potential effects of the proposed alternatives, and typically includes source and receptor characterization, a quantification of air quality impacts by determining emission rates and conducting dispersion modelling, and an assessment of effects. The assessment will compare to all available standards for any contaminants of concern. Please contact this office during the scoping process to confirm the appropriate level of assessment.
- Dust and noise control measures should be addressed and included in the construction plans to ensure that nearby residential and other sensitive land uses within the study area are not adversely affected during construction activities.
- The ESR should consider the potential impacts of increased noise levels during the operation of the undertaking due to potentially higher traffic volumes resulting from this project. The proponent should explore all potential measures to mitigate significant noise impacts during the assessment of alternatives.

Servicing and Facilities

- Any facility that releases emissions to the atmosphere, discharges contaminants to ground or surface water, provides potable water supplies, or stores, transports or disposes of waste must have an Environmental Compliance Approval (ECA) before it can operate lawfully. Please consult with the Environmental Approvals Access and Service Integration Branch (EAASIB) to determine whether a new or amended ECA will be required for any proposed infrastructure.
- We recommend referring to the ministry's "D-Series" guidelines – Land Use Compatibility to ensure that any potential land use conflicts are considered when planning for any infrastructure or facilities related to wastewater, pipelines, landfills or industrial uses.

Contaminated Soils

- Since the removal or movement of soils may be required, appropriate tests to determine contaminant levels from previous land uses or dumping should be undertaken. If the soils are contaminated, you must determine how and where they are to be disposed of, consistent with *Part XV.1 of the Environmental Protection Act (EPA)* and Ontario Regulation 153/04, *Records of Site Condition*, which details the new requirements related to site assessment and clean up. Please contact the ministry's District Offices for further consultation if contaminated sites are present.
- Any current or historical waste disposal sites should be identified in the ESR. The status of these sites should be determined to confirm whether approval pursuant to Section 46 of the EPA may be required for land uses on former disposal sites.
- The location of any underground storage tanks should be investigated in the ESR. Measures should be identified to ensure the integrity of these tanks and to ensure an appropriate response in the event of a spill. The ministry's Spills Action Centre must be contacted in such an event.
- The ESR should identify any underground transmission lines in the study area. The owners should be consulted to avoid impacts to this infrastructure, including potential spills.

Mitigation and Monitoring

- Design and construction reports and plans should be based on a best management approach that centres on the prevention of impacts, protection of the existing environment, and opportunities for rehabilitation and enhancement of any impacted areas.
- All waste generated during construction must be disposed of in accordance with ministry requirements.
- Contractors must be made aware of all environmental considerations so that all environmental standards and commitments for both construction and operation are met. Mitigation measures should be clearly referenced in the ESR and regularly monitored during the construction stage of the project. In addition, we encourage proponents to conduct post-construction monitoring to ensure all mitigation measures have been effective and are functioning properly. The proponent's construction and post-construction monitoring plans should be documented in the ESR.

Planning and Policy

- Parts of the study area may be subject to the Oak Ridges Moraine Conservation Plan, Niagara Escarpment Plan, Greenbelt Plan, Lake Simcoe Protection Plan, or Growth Plan for the Greater Golden Horseshoe. The ESR should demonstrate how the proposed study adheres to the relevant policies in these plans.
- The Provincial Policy Statement (2014) contains policies that protect Ontario's natural heritage and water resources, including designated vulnerable areas mapped in source water protection assessment reports under the *Clean Water Act* (CWA). Applicable policies should be referenced in the ESR, and the proponent should demonstrate how this proposed project is consistent with these policies. Assessment reports can be found on the Conservation Ontario website at: <http://www.conservation-ontario.on.ca/uncategorised/143-otherswpreionsindex>.

□ **Class EA Process**

- If this project is a Master Plan: there are several different approaches that can be used to conduct a Master Plan, examples of which are outlined in Appendix 4 of the Class EA. The Master Plan should clearly indicate the selected approach for conducting the plan, in particular by identifying whether the levels of assessment, consultation and documentation are sufficient to fulfill the requirements for Schedule B or C projects. Please note that any Schedule B or C projects identified in the plan would be subject to Part II Order Requests under the *Environmental Assessment Act* (EAA), although the plan itself would not be.
- The ESR should provide clear and complete documentation of the planning process in order to allow for transparency in decision-making. The ESR must also demonstrate how the consultation provisions of the Class EA have been fulfilled, including documentation of all public consultation efforts undertaken during the planning process. Additionally, the ESR should identify all concerns that were raised and how they have been addressed throughout the planning process. The Class EA also directs proponents to include copies of comments submitted on the project by interested stakeholders, and the proponent's responses to these comments.
- The Class EA requires the consideration of the effects of each alternative on all aspects of the environment. The ESR should include a level of detail (e.g. hydrogeological investigations, terrestrial and aquatic assessments) such that all potential impacts can be identified and appropriate mitigation measures can be developed. Any supporting studies conducted during the Class EA process should be referenced and included as part of the ESR.
- Please include in the ESR a list of all subsequent permits or approvals that may be required for the implementation of the preferred alternative, including MOECC's PTTW and ECAs, conservation authority permits, and approval under the *Canadian Environmental Assessment Act* (CEAA).
- Ministry guidelines and other information related to the issues above are available at <http://www.ontario.ca/environment-and-energy/environment-and-energy> under the publications link. We encourage you to review all the available guides and to reference any relevant information in the ESR.

A PROPONENT'S INTRODUCTION TO THE DELEGATION OF PROCEDURAL ASPECTS OF CONSULTATION WITH ABORIGINAL COMMUNITIES

DEFINITIONS

The following definitions are specific to this document and may not apply in other contexts:

Aboriginal communities – the First Nation or Métis communities identified by the Crown for the purpose of consultation.

Consultation – the Crown's legal obligation to consult when the Crown has knowledge of an established or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. This is the type of consultation required pursuant to s. 35 of the *Constitution Act, 1982*. Note that this definition does not include consultation with Aboriginal communities for other reasons, such as regulatory requirements.

Crown – the Ontario Crown, acting through a particular ministry or ministries.

Procedural aspects of consultation – those portions of consultation related to the process of consultation, such as notifying an Aboriginal community about a project, providing information about the potential impacts of a project, responding to concerns raised by an Aboriginal community and proposing changes to the project to avoid negative impacts.

Proponent – the person or entity that wants to undertake a project and requires an Ontario Crown decision or approval for the project.

I. PURPOSE

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that may adversely impact that right. In outlining a framework for the duty to consult, the Supreme Court of Canada has stated that the Crown may delegate procedural aspects of consultation to third parties. This document provides general information about the Ontario Crown's approach to delegation of the procedural aspects of consultation to proponents.

This document is not intended to instruct a proponent about an individual project, and it does not constitute legal advice.

II. WHY IS IT NECESSARY TO CONSULT WITH ABORIGINAL COMMUNITIES?

The objective of the modern law of Aboriginal and treaty rights is the *reconciliation* of Aboriginal peoples and non-Aboriginal peoples and their respective rights, claims and interests. Consultation is an important component of the reconciliation process.

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. For example, the Crown's duty to consult is triggered when it considers issuing a permit, authorization or approval for a project which has the potential to adversely impact an Aboriginal right, such as the right to hunt, fish, or trap in a particular area.

The scope of consultation required in particular circumstances ranges across a spectrum depending on both the nature of the asserted or established right and the seriousness of the potential adverse impacts on that right.

Depending on the particular circumstances, the Crown may also need to take steps to accommodate the potentially impacted Aboriginal or treaty right. For example, the Crown may be required to avoid or minimize the potential adverse impacts of the project.

III. THE CROWN'S ROLE AND RESPONSIBILITIES IN THE DELEGATED CONSULTATION PROCESS

The Crown has the responsibility for ensuring that the duty to consult, and accommodate where appropriate, is met. However, the Crown may delegate the procedural aspects of consultation to a proponent.

There are different ways in which the Crown may delegate the procedural aspects of consultation to a proponent, including through a letter, a memorandum of understanding, legislation, regulation, policy and codes of practice.

If the Crown decides to delegate procedural aspects of consultation, the Crown will generally:

- Ensure that the delegation of procedural aspects of consultation and the responsibilities of the proponent are clearly communicated to the proponent;
- Identify which Aboriginal communities must be consulted;
- Provide contact information for the Aboriginal communities;
- Revise, as necessary, the list of Aboriginal communities to be consulted as new information becomes available and is assessed by the Crown;
- Assess the scope of consultation owed to the Aboriginal communities;

- Maintain appropriate oversight of the actions taken by the proponent in fulfilling the procedural aspects of consultation;
- Assess the adequacy of consultation that is undertaken and any accommodation that may be required;
- Provide a contact within any responsible ministry in case issues arise that require direction from the Crown; and
- Participate in the consultation process as necessary and as determined by the Crown.

IV. THE PROPONENT'S ROLE AND RESPONSIBILITIES IN THE DELEGATED CONSULTATION PROCESS

Where aspects of the consultation process have been delegated to a proponent, the Crown, in meeting its duty to consult, will rely on the proponent's consultation activities and documentation of those activities. The consultation process informs the Crown's decision of whether or not to approve a proposed project or activity.

A proponent's role and responsibilities will vary depending on a variety of factors including the extent of consultation required in the circumstance and the procedural aspects of consultation the Crown has delegated to it. Proponents are often in a better position than the Crown to discuss a project and its potential impacts with Aboriginal communities and to determine ways to avoid or minimize the adverse impacts of a project.

A proponent can raise issues or questions with the Crown at any time during the consultation process. If issues or concerns arise during the consultation that cannot be addressed by the proponent, the proponent should contact the Crown.

a) What might a proponent be required to do in carrying out the procedural aspects of consultation?

Where the Crown delegates procedural aspects of consultation, it is often the proponent's responsibility to provide notice of the proposed project to the identified Aboriginal communities. The notice should indicate that the Crown has delegated the procedural aspects of consultation to the proponent and should include the following information:

- a description of the proposed project or activity;
- mapping;
- proposed timelines;
- details regarding anticipated environmental and other impacts;
- details regarding opportunities to comment; and
- any changes to the proposed project that have been made for seasonal conditions or other factors, where relevant.

Proponents should provide enough information and time to allow Aboriginal communities to provide meaningful feedback regarding the potential impacts of the project. Depending on the nature of consultation required for a project, a proponent also may be required to:

- provide the Crown with copies of any consultation plans prepared and an opportunity to review and comment;
- ensure that any necessary follow-up discussions with Aboriginal communities take place in a timely manner, including to confirm receipt of information, share and update information and to address questions or concerns that may arise;
- as appropriate, discuss with Aboriginal communities potential mitigation measures and/or changes to the project in response to concerns raised by Aboriginal communities;
- use language that is accessible and not overly technical, and translate material into Aboriginal languages where requested or appropriate;
- bear the reasonable costs associated with the consultation process such as, but not limited to, meeting hall rental, meal costs, document translation(s), or to address technical & capacity issues;
- provide the Crown with all the details about potential impacts on established or asserted Aboriginal or treaty rights, how these concerns have been considered and addressed by the proponent and the Aboriginal communities and any steps taken to mitigate the potential impacts;
- provide the Crown with complete and accurate documentation from these meetings and communications; and
- notify the Crown immediately if an Aboriginal community not identified by the Crown approaches the proponent seeking consultation opportunities.

b) What documentation and reporting does the Crown need from the proponent?

Proponents should keep records of all communications with the Aboriginal communities involved in the consultation process and any information provided to these Aboriginal communities.

As the Crown is required to assess the adequacy of consultation, it needs documentation to satisfy itself that the proponent has fulfilled the procedural aspects of consultation delegated to it. The documentation required would typically include:

- the date of meetings, the agendas, any materials distributed, those in attendance and copies of any minutes prepared;
- the description of the proposed project that was shared at the meeting;
- any and all concerns or other feedback provided by the communities;
- any information that was shared by a community in relation to its asserted or established Aboriginal or treaty rights and any potential adverse impacts of the proposed activity, approval or disposition on such rights;

- any proposed project changes or mitigation measures that were discussed, and feedback from Aboriginal communities about the proposed changes and measures;
- any commitments made by the proponent in response to any concerns raised, and feedback from Aboriginal communities on those commitments;
- copies of correspondence to or from Aboriginal communities, and any materials distributed electronically or by mail;
- information regarding any financial assistance provided by the proponent to enable participation by Aboriginal communities in the consultation;
- periodic consultation progress reports or copies of meeting notes if requested by the Crown;
- a summary of how the delegated aspects of consultation were carried out and the results; and
- a summary of issues raised by the Aboriginal communities, how the issues were addressed and any outstanding issues.

In certain circumstances, the Crown may share and discuss the proponent's consultation record with an Aboriginal community to ensure that it is an accurate reflection of the consultation process.

c) Will the Crown require a proponent to provide information about its commercial arrangements with Aboriginal communities?

The Crown may require a proponent to share information about aspects of commercial arrangements between the proponent and Aboriginal communities where the arrangements:

- include elements that are directed at mitigating or otherwise addressing impacts of the project;
- include securing an Aboriginal community's support for the project; or
- may potentially affect the obligations of the Crown to the Aboriginal communities.

The proponent should make every reasonable effort to exempt the Crown from confidentiality provisions in commercial arrangements with Aboriginal communities to the extent necessary to allow this information to be shared with the Crown.

The Crown cannot guarantee that information shared with the Crown will remain confidential. Confidential commercial information should not be provided to the Crown as part of the consultation record if it is not relevant to the duty to consult or otherwise required to be submitted to the Crown as part of the regulatory process.

V. WHAT ARE THE ROLES AND RESPONSIBILITIES OF ABORIGINAL COMMUNITIES' IN THE CONSULTATION PROCESS?

Like the Crown, Aboriginal communities are expected to engage in consultation in good faith. This includes:

- responding to the consultation notice;
- engaging in the proposed consultation process;
- providing relevant information;
- clearly articulating the potential impacts of the proposed project on Aboriginal or treaty rights; and
- discussing ways to mitigate any adverse impacts.

Some Aboriginal communities have developed tools, such as consultation protocols, policies or processes that provide guidance on how they would prefer to be consulted. Although not legally binding, proponents are encouraged to respect these community processes where it is reasonable to do so. Please note that there is no obligation for a proponent to pay a fee to an Aboriginal community in order to enter into a consultation process.

To ensure that the Crown is aware of existing community consultation protocols, proponents should contact the relevant Crown ministry when presented with a consultation protocol by an Aboriginal community or anyone purporting to be a representative of an Aboriginal community.

VI. WHAT IF MORE THAN ONE PROVINCIAL CROWN MINISTRY IS INVOLVED IN APPROVING A PROPONENT'S PROJECT?

Depending on the project and the required permits or approvals, one or more ministries may delegate procedural aspects of the Crown's duty to consult to the proponent. The proponent may contact individual ministries for guidance related to the delegation of procedural aspects of consultation for ministry-specific permits/approvals required for the project in question. Proponents are encouraged to seek input from all involved Crown ministries sooner rather than later.

Kate Barclay

From: Dela Cruz, Gino <gino.delacruz@peelregion.ca>
Sent: September 26, 2017 3:04 PM
To: Lee, Thomas; Lapos, Natalie; Khan, Sanya; Masley, Mark; Lo, Arthur; Nieuwenhuysen, Bob; Simms, Joy; Detaramani, Tina; Kant Chawla; Sharon Lingertat; mark.heaton@ontario.ca; trevor.bell@ontario.ca; Jennifer Benedict; Hamdani, Hashim Ali; Thomsen, Lori-Ann; Stockman, Angela; Caughey, Rebecca; Ansari, Seema
Cc: Jessica Dorgo; Stephen Keen
Subject: RE: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

Good afternoon,

Please note that the PIC scheduled for tonight has been cancelled and will be re-scheduled for a later date.

Sorry for the inconvenience and we will keep you updated on the future date for this meeting.

Regards,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Roads – Design and Construction
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: Dela Cruz, Gino
Sent: September 19, 2017 4:38 PM
To: Lee, Thomas; Lapos, Natalie; Khan, Sanya; Masley, Mark; Lo, Arthur; Nieuwenhuysen, Bob; Simms, Joy; Detaramani, Tina; 'Kant Chawla'; 'Sharon Lingertat'; 'mark.heaton@ontario.ca'; 'trevor.bell@ontario.ca'; Jennifer Benedict; Hamdani, Hashim Ali; Thomsen, Lori-Ann; Stockman, Angela; Caughey, Rebecca; Ansari, Seema
Cc: 'Jessica Dorgo'; Stephen Keen
Subject: FW: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

FYI – please see attached notice and boards for the PIC #1 on September 26, 2017.

From: Dela Cruz, Gino
Sent: September 12, 2017 4:12 PM
To: Dale, Frank; Thompson, Allan; Groves, Annette; Innis, Jennifer
Cc: Smith, Janette; Kocialek, Gary; Saiyed, Sabbir; Rook, Sally
Subject: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

Good afternoon,

I am reaching out to let you know that the first Public Information Centre (PIC) for the Coleraine Drive Grade Separation Environmental Assessment (South of Old Ellwood Drive) is being held on September 26, 2017 at the Albion Bolton Community Centre. Additional details are noted in the attached notice which will be published in the Caledon Enterprise and Caledon Citizen on September 14 and 21; as well as mailed out to residents in the area. Display boards for the public meeting will be forwarded to you closer to the PIC date.

If you have any questions or concerns, please contact me. Information will continue to be updated on the project website including the display boards shortly after the PIC date. The link is here <https://www.peelregion.ca/pw/transportation/environ-assess/index-cal.htm>

Thank you,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Roads – Design and Construction
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:26 AM
To: trevor.bell@ontario.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Ministry of Environment, Conservation and Parks Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Mr. Trevor Bell
Environmental Resource Planner/EA Coordinator
Ministry of Environment, Conservation and Parks
5775 Yonge St., 8th Floor
Toronto, ON M2M 4J1

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



Engineering
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Best Employer
CANADA 2021

Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CANADA 2019

A-2

Appendix A-2-3: Ministry of Tourism, Culture and Sport (MTCS)

**Ministry of Tourism,
Culture and Sport**

Heritage Program Unit
Programs and Services Branch
401 Bay Street, Suite 1700
Toronto ON M7A 0A7
Tel: 416 314 3108
Fax: 416 212 1802

**Ministère du Tourisme,
de la Culture et du Sport**

Unité des programmes patrimoine
Direction des programmes et des services
401, rue Bay, Bureau 1700
Toronto ON M7A 0A7
Tél: 416 314 3108
Télééc: 416 212 1802



April 11, 2017 (EMAIL ONLY)

Mr. Gino Dela Cruz
Project Manager, Infrastructure Programming & Studies
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
E: gino.delacruz@peelregion.ca

RE: MTCS file #: 0006414
Proponent: Region of Peel
Subject: Notice of Commencement
Coleraine Drive Grade Separation
Location: Town of Caledon, Ontario

Dear Mr. Dela Cruz:

Thank you for providing the Ministry of Tourism, Culture and Sport (MTCS) with the Notice of Commencement for your project. MTCS's interest in this Environmental Assessment (EA) project relates to its mandate of conserving Ontario's cultural heritage, which includes:

- Archaeological resources, including land-based and marine;
- Built heritage resources, including bridges and monuments; and,
- Cultural heritage landscapes.

Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources. The recommendations below are for a Schedule C Municipal Class EA project, as described in the notice of study commencement.

It is my understanding that this study will consider a range of options for long term traffic improvements and a grade separation for the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon.

While some cultural heritage resources may have already been formally identified, others may be identified through screening and evaluation. Aboriginal communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Aboriginal communities includes a discussion about known or potential cultural heritage resources that are of value to these communities. Municipal Heritage Committees, historical societies and other local heritage organizations may also have knowledge that contributes to the identification of cultural heritage resources.

Archaeological Resources

Your EA project may impact archaeological resources and you should screen the project with the MTCS [Criteria for Evaluating Archaeological Potential](#) to determine if an archaeological assessment is needed. MTCS archaeological sites data are available at archaeology@ontario.ca. If your EA project area exhibits archaeological potential, then an archaeological assessment (AA) should be undertaken by an archaeologist licenced under the OHA, who is responsible for submitting the report directly to MTCS for review.

Built Heritage and Cultural Heritage Landscapes

The MTCS [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#) should be completed to help determine whether your EA project may impact cultural heritage

resources. The Clerk or the Heritage Resources Officer at the Town of Caledon can provide information on property registered or designated under the *Ontario Heritage Act*. The Heritage Resources Officer can also provide information that will assist you in completing the checklist.

If potential or known heritage resources exist, MTCS recommends that a Heritage Impact Assessment (HIA), prepared by a qualified consultant, should be completed to assess potential project impacts. Our Ministry's [Info Sheet #5: Heritage Impact Assessments and Conservation Plans](#) outlines the scope of HIAs. Please send the HIA to MTCS (and the local municipality as appropriate) for review, and make it available to local organizations or individuals who have expressed interest in review.

Environmental Assessment Reporting

All technical heritage studies and their recommendations are to be addressed and incorporated into EA projects. Please advise MTCS whether any technical heritage studies will be completed for your EA project, and provide them to MTCS before issuing a Notice of Completion. If your screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file.

Thank-you for consulting MTCS on this project: please continue to do so through the EA process, and contact me for any questions or clarification.

Sincerely,

Laura Hatcher
Heritage Planner
laura.e.hatcher@Ontario.ca

Copied to: Stephen Keen, Project Manager, CIMA Canada Inc.

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. MTCS makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MTCS be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Please notify MTCS if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out an archaeological assessment in accordance with the Ontario Heritage Act and the Standards and Guidelines for Consultant Archaeologists.

If human remains are encountered, all activities must cease immediately and the local police as well as the Cemeteries Regulation Unit of the Ministry of Government and Consumer Services must be contacted. In situations where human remains are associated with archaeological resources, MTCS should also be notified to ensure that the site is not subject to unlicensed alterations which would be a contravention of the Ontario Heritage Act.

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:29 AM
To: rosi.zirger@ontario.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Ministry of Tourism, Culture and Sport Letter(2).pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Ms. Rosi Zirger
Heritage Planner - Culture Services Unot
Ministry of Tourism, Culture and Sport
401 Bay Street, Suite 1700
Toronto, ON M7A 0A7

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: October 9, 2019 2:29 PM
To: Karla.Barboza@ontario.ca; dan.minkin@ontario.ca
Cc: Stephen Keen; Sonya Bubas (Kapusin) (sonya.bubas@peelregion.ca)
Subject: RE: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1

Good Afternoon Karla,

Thank you for confirming the appropriate contact information for the Ministry of Tourism, Culture and Sport for our study. Rosi Zirger has been removed from the study mailing list, as requested. Yourself and Dan Minkin will receive all future study notifications.

A copy of the Public Information Centre #1 material is available on the file transfer site below for download. Please let us know if you have any issues accessing the files.

[Access the file transfer site](#)

A Cultural Heritage Assessment and a Stage 1 Archaeological Assessment are being conducted as part of this study.

Thank you,

Jessica Dorgo, EIT
Transportation
T 289-288-0287 ext. 6819 F 289-288-0285

CIMA+

From: Barboza, Karla (MTCS) <Karla.Barboza@ontario.ca>
Sent: Wednesday, October 9, 2019 1:28 PM
To: sonya.bubas@peelregion.ca; Stephen Keen <Stephen.Keen@cima.ca>
Cc: Minkin, Dan (MTCS) <Dan.Minkin@ontario.ca>
Subject: FW: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1

Hi all,

Thanks for sending this PIC notice to the Ministry of Tourism, Culture and Sport, Culture Division.

Please note that there has been some changes in our unit (see full MTCS contact below). For this project (MTCS File 0006414), please continue to send any notices and/or information to Dan Minkin, MTCS Heritage Planner, and me. Dan will review and provide preliminary comments. You can remove Rosi Zirger from your list.

Could you please send us a copy of the PIC materials and let us know if any technical cultural heritage studies have been or will be undertaken for this project?

In the meantime, please let us know if you have any questions.

Regards,

Karla Barboza MCIP, RPP, CAHP | (A) Team Lead, Heritage
 Ministry of Tourism, Culture and Sport
 Culture Division | Programs and Services Branch | Heritage Planning Unit
 T. 416.314.7120 | Email: karla.barboza@ontario.ca

Government Review Team List (prepared by the Ministry of the Environment, Conservation and Parks)

Ministry of Tourism, Culture and Sport – Culture Division

Name, Position, Agency and Address	Document Form	Phone, Fax and Email	Types of EA Projects to be Circulated
Ministry of Tourism, Culture and Sport: Culture Division			
Ms. Karla Barboza, Team Lead(A), Heritage Heritage Planning Unit Programs and Services Branch Ministry of Tourism, Culture and Sport 401 Bay Street, Suite 1700 Toronto ON M7A 0A7	1 electronic/ email copy each (preferred)	T: 416-314 7120 karla.barboza@ontario.ca	Receives the initial circulations for all individual and site specific Class EAs for all regions of the province. The Team Lead will assign to a Heritage Planner for review. EA matters of province-wide significance (including Parent Class EAs and Environmental Assessment policies and guidelines).
Heritage Planners: Site-specific individual and Class EA projects – Heritage Planners review site specific EAs impacts on cultural heritage resources. They act as one-window for Culture Division and gather information on culture heritage resources from other staff, including Regional Offices.			
Ms. Katherine Kirzati, Heritage Planner Heritage Planning Unit Programs and Services Branch Ministry of Tourism, Culture and Sport 401 Bay Street, Suite 1700 Toronto ON M7A 0A7	1 electronic/ email copy each (preferred)	T: 416-314-7643 katherine.kirzati@ontario.ca	Contact Karla Barboza as initial step prior to circulating documents. All individual and site specific Class EAs for South-western Ontario which covers upper- and single-tier municipalities from Grey, Wellington, Waterloo, Brant and Norfolk, westward, plus Northern Ontario (Kenora, Rainy River, Thunder Bay, Cochrane, Algoma).
Ms. Laura Hatcher, Heritage Planner	1 electronic/ email copy each (preferred)	On parental leave	Contact Karla Barboza as initial step prior to circulating documents.
Mr. Dan Minkin, Heritage Planner	1 electronic/ email copy	T: 416-314-7147 dan.minkin@ontario.ca	Contact Karla Barboza as initial step prior to circulating documents.

Heritage Planning Unit Programs and Services Branch Ministry of Tourism, Culture and Sport 401 Bay Street, Suite 1700 Toronto ON M7A 0A7	each (preferred)		All individual and site specific Class EAs in Central Ontario, which covers upper- and single- tier municipalities of: Hamilton, Halton, Niagara, Peel, Dufferin; Durham, York, Toronto, Simcoe, Muskoka, Kawartha Lakes, Haliburton, Peterborough and Northumberland.
Ms. Kimberly Livingstone, Heritage Planner(A) Heritage Planning_Unit Programs and Services Branch Ministry of Tourism, Culture and Sport 401 Bay Street, Suite 1700 Toronto ON M7A 0A7	1 electronic/ email copy each (preferred)	T. 416-314-7133 kimberly.livingstone@ontario.ca	Contact Karla Barboza as initial step prior to circulating documents. All individual and site specific Class EAs in Eastern Ontario which covers upper- and single- tier municipalities from Hastings, east to the Quebec border, as well as Renfrew, Parry Sound, Nipissing, Sudbury, Timiskaming, Algoma and Cochrane, including Manitoulin.

From: Zirger, Rosi (MTCS) <Rosi.Zirger@ontario.ca>

Sent: September-25-19 10:40 AM

To: Barboza, Karla (MTCS) <Karla.Barboza@ontario.ca>

Subject: FW: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1

Hi Karla

RE: MTCS file #0006414 - This file is assigned to Laura; it needs to be re-assigned. I have logged and saved this notice to PastPort. The HP should request the PIC boards and find out what if any technical studies have been done.

Rosi

From: Jessica Dorgo <Jessica.Dorgo@cima.ca>

Sent: September 25, 2019 9:29 AM

To: Zirger, Rosi (MTCS) <Rosi.Zirger@ontario.ca>

Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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A-2

Appendix A-2-4: Ministry of Northern Development, Mines, Natural Resources, and Forestry (MNDMNR)

Kate Barclay

From: Jessica Dorgo
Sent: April 26, 2017 9:52 AM
To: 'steve.varga@ontario.ca'
Cc: Jennifer Haslett
Subject: Region of Peel EA Information Requests
Attachments: B000738-L-MNRF Info Request-170202.pdf; B000709-L-MNRF Info Request-170206.pdf

Good Morning Mr. Varga,

This correspondence is a follow up regarding two information requests that were sent to MNRF regarding two Environmental Assessments being conducted within the Region of Peel:

- Albion Vaughan Road and King Street, Town of Caledon
- Coleraine Drive Grade Separation, Town of Caledon

CIMA would like to inquire if you know of any areas of natural significance identified or designated by the Minister of Natural Resources and Forestry within 0.5km of the study areas, as well as any other environmental concerns that may be related to the study areas.

Two letters which were sent to MNRF are attached for further information.

Thank you,

Jessica Dorgo

EIT
Transportation Engineering

CIMA+
Partners in Excellence

3027 Harvester Road, Suite 400
Burlington Ontario L7N 3G7
CANADA
Tel: 289-288-0287 ext. 6819 / Fax: 289-288-0285



CONFIDENTIALITY WARNING This e-mail is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

February 02, 2017

Ministry of Natural Resources and Forestry

Aurora District
50 Bloomington Road
Aurora, ON L4G 0L8

Attention : Karen Golby, Resources Clerk (karen.golby@ontario.ca)

Dear Karen :

**RE: NATURAL ENVIRONMENT STUDY FOR COLERAINE DRIVE GRADE SEPARATION,
TOWN OF CALEDON**

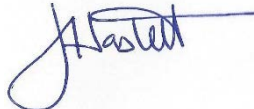
CIMA Canada Inc. ("CIMA") is currently conducting a Natural Environment Study for a proposed grade separation at Coleraine Drive within the Town of Caledon, Region of Peel. The study area extends from King Street to Holland Drive (refer to the attached site map).

CIMA would like to know of any areas of natural significance identified or designated by the Ministry of Natural Resources and Forestry within 0.5 km of the study area, as well as any other environmental concerns that may be related to the study area including occurrences of species at risk.

Should you have any questions, please do not hesitate to contact the undersigned directly.

Sincerely,

CIMA Canada Inc.

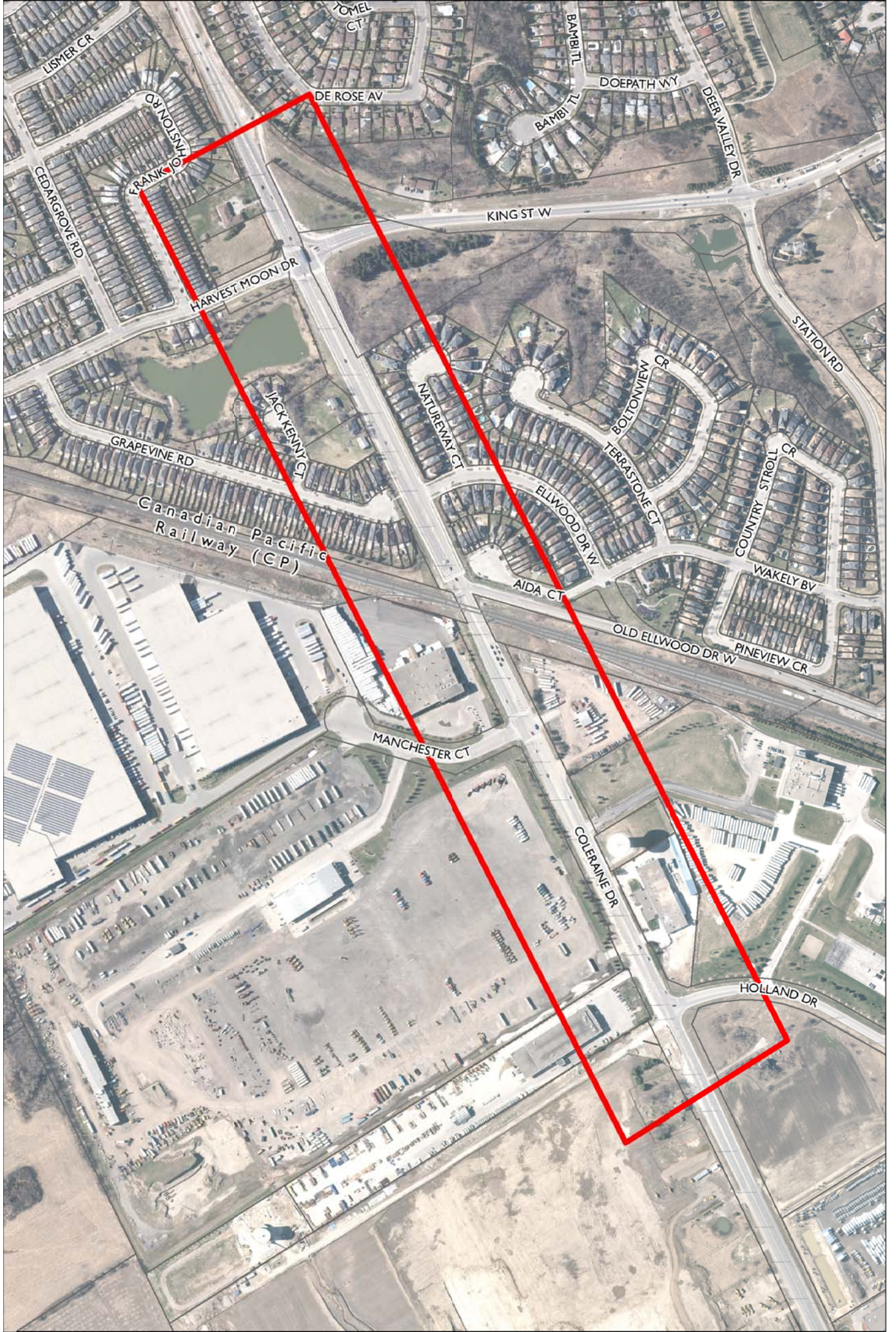


Jennifer Haslett, EP
Senior Environmental Professional
jennifer.haslett@cima.ca

JH:krs
Encl.: Site Map

cc: Region of Peel, P. Gino Dela Cruz, P.Eng. (gino.delacruz@peelregion.ca)
CIMA Canada Inc., Steve Keen, M.Sc., P.Eng. (stephen.keen@cima.ca)

B000738-L-MNRF Info Request-170202.docx



LISMER CR

ZOMEL CT

DE ROSE AV

BAMBI TL

DOEPATH WY

DER VALLEY DR

OV NOSTON RD

FRANK

KING ST W

CEDARGROVE RD

HARVEST MOON DR

JACK KENY CT

GRAPEVINE RD

NATUREWAY CT

BOLTONVIEW CR

STATION RD

Canadian Pacific Railway (CP)

ELLWOOD DR W

TERRASTONE CT

COUNTRY STROLL CR

AIDA CT

WAKELY BV

OLD ELLWOOD DR W

PINEVIEW CR

MANCHESTER CT

COLERAINE DR

HOLLAND DR

Kate Barclay

From: Ana-Laura Giacomel
Sent: August 9, 2017 12:12 PM
To: esa.aurora@ontario.ca
Cc: Jennifer Haslett
Subject: Natural Environment Study for Coleraine Drive Grade Separation
Attachments: Coleraine Drive Study Area.bmp; Aurora_Information_Request_Form_2015 Coleraine.pdf

Hello,

CIMA Canada Inc. has been retained by The Region of Peel to conduct a Schedule C Municipal Class Environmental Assessment for the Coleraine Drive Grade Separation.

We are requesting information on any areas of natural significance identified or designated by the Ministry of Natural Resources and Forestry within 0.5 km of the study area (refer to attached map), as well as any other environmental concerns that may be related to the study area including occurrences of species at risk.

Thank-you.

Ana-Laura Giacomel
Junior Ecologist

CIMA+
Partners in Excellence

55 King Street East
Bowmanville Ontario L1C 1N4
CANADA
Tel: 905-697-4464 ext. 6910
Cell: 416-835-2872



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Aurora MNRF Information Request Form

Name:

Company Name:

Proponent Name:

Phone Number:

Email Address:

Project Name:

Property Location:

Township:

Lot & Concession:

UTM Coordinates: Easting (X)

Northing (Y)

**Brief Description
of Undertaking**

Have you previously contacted someone at MNRØ for information on this site? Yes No

If yes, when and
who?

Provide a map of accurate scale to illustrate footprint/study area of the proposed activity in relation to the surrounding landscape (e.g. property boundaries, roads, waterbodies, natural features, towns, transmission corridors, and other human landmarks). Use of aerial photography is strongly encouraged. Include scale, north arrow and legend.

ATTACHMENTS - I have attached a:

Picture

Map

Other

REQUEST - I would like to request the following information for the property identified above:

**Requires an appointment and remittance of fees. See Information Request Guideline for details.*

*Fish Dot Information

(fish and other aquatic species found in a particular area of a watercourse)

Species at Risk

Other

For additional natural heritage information please visit [Land Information Ontario | Ontario.ca](http://LandInformationOntario.Ontario.ca)

Please forward the completed form to: esa.aurora@ontario.ca

Or send by mail:

**Aurora District, Ministry of Natural Resources
and Forestry**

50 Bloomington Rd Aurora, ON L4G 0L8

Kate Barclay

From: ESA Aurora (MNRF) <ESA.Aurora@ontario.ca>
Sent: December 14, 2017 3:33 PM
To: Ana-Laura Giacomel
Cc: Jennifer Haslett
Subject: RE: Natural Environment Study for Coleraine Drive Grade Separation
Attachments: 2017_12_14_CorelaineDr.pdf

Hi Ana-Laura

Please see attached for a Species at Risk screening. Apologies for the delay. I believe you have encapsulated the natural heritage features of note. Please refer to LIO for this information.

Melanie

Melanie Shapiera

Management Biologist | Ontario Ministry of Natural Resources and Forestry | Aurora District Office
50 Bloomington Road, Aurora, Ontario, L4G 0L8 | [Tel:905-713-7425](tel:905-713-7425) | Email: melanie.shapiera@ontario.ca

From: Ana-Laura Giacomel [mailto:Ana-Laura.Giacomel@cima.ca]
Sent: September-12-17 12:22 PM
To: ESA Aurora (MNRF)
Cc: Jennifer Haslett
Subject: Natural Environment Study for Coleraine Drive Grade Separation

Hello,

This is a follow-up email to a previous email sent on August 9th, 2017 regarding a request for information on natural heritage features within 0.5 km of the study area, as outlined in the attached map.

We have conducted a background review of available information through NHIC, LIO, and publically available atlases. From this review, we are aware of the following features in the study area:

- Tributary of the Humber River
- Woodlands

Please advise us of any other features that may be present in the study area, or any information that may be available within restricted data sets of LIO or NHIC that we cannot access.

Thank you,

Ana-Laura Giacomel
Junior Ecologist

CIMA+
Partners in Excellence

December 14, 2017

Ana-Laura Giacomel
55 King Street East
Bowmanville Ontario
L1C 1N4

Phone: 905-697-4464 ext. 6910

Re: Request for Information for Coleraine Drive south of Old Ellwood Drive, Town of Caledon, Regional Municipality of Peel

Dear Ms. Giacomel,

In your email dated September 12, 2017 you requested information on Species at Risk and rare species occurring on or adjacent to the above mentioned location. The species listed below have the potential to occur in your study area and may require further assessment or field studies to determine presence:

- Butternut (Endangered)
- Barn Swallow (Threatened)
- Chimney Swift (Threatened)
- Little Brown Myotis (Endangered)
- Snapping Turtle (Special Concern)

Additional natural heritage information including information on wetlands and Areas of Natural and Scientific Interest (ANSIs) can be obtained through Land Information Ontario (LIO).

The species listed above may receive protection under the *Endangered Species Act, 2007* (ESA) and thus, an approval from MNRF may be required if the work you are proposing could cause harm to these species or their habitats. If the Species at Risk in Ontario List is amended, additional species may be listed and protected under the ESA or the status and protection levels of currently listed species may change.

We require more detailed information on the proposed project in order to assess the impacts of the works on Species at Risk. When project details have been determined, please fill out an Information Gathering Form (IGF) for any *threatened* or *endangered* species listed in the provided letter and submit it to our office (to ESA.Aurora@ontario.ca). The IGF can be found [here](#) (along with its associated [guide](#)). Please include detailed descriptions of the undertakings such as proposed timing and phasing of the project and details on what is required at each phase.

All sections and tables should be filled out in their entirety – incomplete forms will be returned and may delay the review process. Any applicable supplemental information that will assist with

the review process should also be submitted with the IGF (e.g. field survey results, site plan/drawings, ELC mapping, etc.). Please note that forms are reviewed in the order in which they are received by MNRF and we will contact you with our response once the review is complete.

Absence of information provided by MNRF for a given geographic area, or lack of current information for a given area or element, does not categorically mean the absence of sensitive species or features. Many areas in Ontario have never been surveyed and new plant and animal species records are still being discovered for many localities. For these reasons, the MNRF cannot provide a definitive statement on the presence, absence or condition of biological elements in any part of Ontario. If development or site alteration is proposed, surveys by a qualified professional may need to be undertaken in the future to confirm presence or absence of sensitive species or features.

This Species at Risk information is highly sensitive and is not intended for any person or project unrelated to this undertaking. Please do not include any specific information in reports that will be available for public record. As you complete your fieldwork in these areas, please report all information related to any Species at Risk to our office. This will assist with updating our database and facilitate early consultation regarding your project.

If you have any questions or comments, please do not hesitate to contact ESA.aurora@ontario.ca.

Sincerely,



Melanie Shapiera
Management Biologist
Ontario Ministry of Natural Resources and Forestry, Aurora District

Kate Barclay

From: Dela Cruz, Gino <gino.delacruz@peelregion.ca>
Sent: September 26, 2017 3:04 PM
To: Lee, Thomas; Lapos, Natalie; Khan, Sanya; Masley, Mark; Lo, Arthur; Nieuwenhuysen, Bob; Simms, Joy; Detaramani, Tina; Kant Chawla; Sharon Lingertat; mark.heaton@ontario.ca; trevor.bell@ontario.ca; Jennifer Benedict; Hamdani, Hashim Ali; Thomsen, Lori-Ann; Stockman, Angela; Caughey, Rebecca; Ansari, Seema
Cc: Jessica Dorgo; Stephen Keen
Subject: RE: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

Good afternoon,

Please note that the PIC scheduled for tonight has been cancelled and will be re-scheduled for a later date.

Sorry for the inconvenience and we will keep you updated on the future date for this meeting.

Regards,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Roads – Design and Construction
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: Dela Cruz, Gino
Sent: September 19, 2017 4:38 PM
To: Lee, Thomas; Lapos, Natalie; Khan, Sanya; Masley, Mark; Lo, Arthur; Nieuwenhuysen, Bob; Simms, Joy; Detaramani, Tina; 'Kant Chawla'; 'Sharon Lingertat'; 'mark.heaton@ontario.ca'; 'trevor.bell@ontario.ca'; Jennifer Benedict; Hamdani, Hashim Ali; Thomsen, Lori-Ann; Stockman, Angela; Caughey, Rebecca; Ansari, Seema
Cc: 'Jessica Dorgo'; Stephen Keen
Subject: FW: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

FYI – please see attached notice and boards for the PIC #1 on September 26, 2017.

From: Dela Cruz, Gino
Sent: September 12, 2017 4:12 PM
To: Dale, Frank; Thompson, Allan; Groves, Annette; Innis, Jennifer
Cc: Smith, Janette; Kocialek, Gary; Saiyed, Sabbir; Rook, Sally
Subject: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

Good afternoon,

I am reaching out to let you know that the first Public Information Centre (PIC) for the Coleraine Drive Grade Separation Environmental Assessment (South of Old Ellwood Drive) is being held on September 26, 2017 at the Albion Bolton Community Centre. Additional details are noted in the attached notice which will be published in the Caledon Enterprise and Caledon Citizen on September 14 and 21; as well as mailed out to residents in the area. Display boards for the public meeting will be forwarded to you closer to the PIC date.

If you have any questions or concerns, please contact me. Information will continue to be updated on the project website including the display boards shortly after the PIC date. The link is here <https://www.peelregion.ca/pw/transportation/environ-assess/index-cal.htm>

Thank you,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Roads – Design and Construction
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:27 AM
To: steven.strong@ontario.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Ministry of Natural Resources and Forestry Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Mr. Steven Strong
Senior District Planner
Ministry of Natural Resources and Forestry
50 Bloomington Rd
Aurora, ON L4G 0L8

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:28 AM
To: mark.heaton@ontario.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Ministry of Natural Resources and Forestry (2).pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Mr. Mark Heaton
Area Biologist, Halton/Peel/Toronto
Ministry of Natural Resources and Forestry
50 Bloomington Rd
Aurora, ON L4G 0L8

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

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400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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A-2

Appendix A-2-5: Toronto and Region Conservation Authority (TRCA)

MINUTES OF MEETING

CLIENT : Peel Region

PROJECT : Class Environmental Assessment Study for Coleraine Drive Grade Separation (South Of Old Ellwood Drive), Town Of Caledon

MEETING : Project Team

DATE OF MEETING : February 16, 2017 at 9:30 am

LOCATION : Peel Region, 10 Peel Centre Drive, Suite A, 1st Floor Mississauga Room

ATTENDEES : Gino Dela Cruz, Joy Simms, Andrea Warren, Arthur Lo Zak Zakeria, Angela Stockman, Hashim Ali Hamdani, Mark Masley, Rebecca Caughey, Lori Ann Thomson, Tom Lee, Bob Nieuwenhuysen, Damian Jamroz, Miriam Polga (Peel Region)
Sharon Lingertart (TRCA)
Stephen Keen, Jennifer Haslett, Jessica Dorgo (CIMA)

C.C. TO : People invited and attending

Note: If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.

DISCUSSION TOPICS**ACTION BY**

1	WELCOME	
	<ul style="list-style-type: none"> • Roundtable introductions • An agenda was distributed prior to the meeting • Gino Dela Cruz will be Peel Region's Project Manager • Stephen Keen will be CIMA's Project Manager 	
2	PROJECT BACKGROUND AND SCOPE	
	<ul style="list-style-type: none"> • Region of Peel's Goods Movement Task Force analyzed all grade separation locations in Peel Region and Coleraine Drive was identified as a priority • In 2015, CIMA completed the Coleraine Drive Feasibility Study which reviewed the rail-over-road and road-over-rail options for a 6-lane cross section 	
3	WORK PLAN AND SCHEDULE	
	<ul style="list-style-type: none"> • EA is expected to continue into 2018 • Golder is the main subconsultant to this project • CIMA is currently working with the Region to identify properties that will require permission to enter requests for field investigations • First Public Information Centre is tentatively scheduled for June 2017 • Peel Region - Environmental Assessment - Health Criteria can be used a guidelines to assess the impact of the options on pedestrians and cyclists 	
4	DEVELOPMENT APPLICATIONS	
	<ul style="list-style-type: none"> • CIMA is currently working with the Region to review the plans from the 2015 Feasibility Study to identify impacts to businesses and proposed developments • Second meeting in the next few weeks to be scheduled with CIMA, Peel Region's Real Estate group and Town of Caledon to develop a plan for communication with the affected businesses and developments • Individual meetings with impacted parties to be further discussed at the meeting with Peel Region Real Estate Group and Town of Caledon • Community workshop with Town of Caledon is an option to present the design options to the public to review input and comments on community connectivity 	Peel Region
5	REQUEST FOR BACKGROUND INFORMATION	
	<ul style="list-style-type: none"> • TRCA to send CIMA hydraulic model used for King Street study if available • The north end of the study area is regulated as a valley, not a floodplain • TRCA will be interested in bank stability primarily 	TRCA Peel Region

DISCUSSION TOPICS

ACTION BY

	<ul style="list-style-type: none"> • Peel Region to send CIMA collision data and EMME model • Peel Region contact Town of Caledon and to forward CIMA SWM reports for adjacent subdivision if available 	Peel Region
6	OPEN DISCUSSION	
	<ul style="list-style-type: none"> • Holland Drive and Coleraine Drive is now a signalized intersection • Intersection with King Street is a problem for safety (i.e. speeding and pedestrians) • A reduced posted speed limit or narrow lanes may be warranted • A buffer could be included between the travel lanes and multi-use trail • Green walls or art on walls adjacent to residential subdivision to be considered. This issue can be a focus for consultation. • Peel Region to confirm if a 6-lane or 4-lane cross section will be implemented along Coleraine Drive (subsequently confirmed as a 4-lane cross-section only) • Butternut are thought to be in the study area, most likely located in the valley • Incorporation of LID measures and plantings are preferred for TRCA as are maintaining existing drainage patterns to existing watercourses including water quality and quantity. Soils may be conducive to infiltration. TRCA requires 80% TSS removal (which OGS do not achieve on their own). CB Shields and Jellyfish filters were mentioned as possibilities also. • Bolton Transportation Master Plan recommends buffered bicycle lanes for this area. CIMA to include rationale if a different solution is followed. • Walk and Roll Peel website shows up to date trail system for Humber Valley Trail • CIMA to consider guard rail treatment near King Street • Town of Caledon has established a Cycling Advisory Committee. Region to provide CIMA with a contact for stakeholder list • CIMA to determine if pre-consultation forms were received from Peel Region for developments that are pre-applications • PIC 1 will present preliminary rail-over-road and road-over-rail options (artistic presentation) to initiate discussion of key issues with public 	<p>Peel Region</p> <p>CIMA</p> <p>CIMA</p> <p>Peel Region</p> <p>CIMA</p>

DISCUSSION TOPICS**ACTION BY**

7	NEXT STEPS	
	<ul style="list-style-type: none">• CIMA to develop a consultation strategy and send to Peel Region• CIMA to create a land use/ development map for meeting with Real Estate Group and Town of Caledon. Peel Region to schedule meeting.• Public Information Centre #1 tentatively planned for June 2017	CIMA CIMA/ Peel Region

April 10, 2017

CFN 56294

BY E-MAIL ONLY (gino.delacruz@peelregion.ca)

Gino Dela Cruz
Project Manager
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9

Dear Mr. Dela Cruz:

**Re: Response to Notice of Commencement
Coleraine Drive Grade Separation
Municipal Class Environmental Assessment – Schedule C
Humber River Watershed;**

Toronto and Region Conservation Authority (TRCA) staff received the Notice of Commencement for the above noted Environmental Assessment (EA) on March 22, 2017.

It is our understanding that this undertaking involves examining a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive, south of Old Ellwood Drive.

TRCA Areas of Interest

Staff has identified the following Areas of Interest within the study area:

TRCA Regulated Areas

- Regulation Limit
- Crest of Slope
- Watercourses

TRCA Program and Policy Areas

- Aquatic Species and Habitat

- Living City Programs:
 - Living City Trails
- Stormwater Management
- Terrestrial Natural Heritage System Strategy
- Terrestrial Species and Habitat

Please contact the Ministry of Natural Resources to confirm if there are program interests related to this project for:

- Areas of Natural and Scientific Interest
- Provincially Significant Wetlands
- Provincially Endangered Species

Please contact the relevant federal agency to confirm if there are issues related to:

- Asian Long-horned Beetle Regulated Area
- Federally Endangered Species

There may be additional consultation with other federal and provincial agencies to ensure that the requirements of such legislation are met. This list is not inclusive and the onus is on the proponent and its consultants to consult with other agencies as required.

Available mapping and program information regarding these Areas of Interest were sent for your reference on August 10, 2016. Please ensure that the status, potential impacts and opportunities for enhancement related to these Areas of Interest are documented and assessed through a review of background material, technical study, field assessment and detailed evaluation, as appropriate.

Selection of Alternatives

In consideration of TRCA's *Living City Policies*, Ontario Regulation 166/06, and TRCA's other programs and policies, staff requires that the preferred alternative meets the following criteria:

1. Prevents the risk associated with flooding, erosion or slope instability.
2. Protects and rehabilitates existing landforms, features and functions.
3. Provides for aquatic, terrestrial and human access.
4. Minimizes water/energy consumption and pollution.
5. Addresses TRCA property and heritage resource concerns.

Staff recommends that the preferred alternative meets the policies of section 7, in particular section 7.4.4, of The Living City Policies for Planning and Development in the Watersheds of the Toronto and Region Conservation Authority. Furthermore, staff recommends that the preferred alternative allows the detailed design to meet the policies of section 8, including section 8.9, of The Living City Policies for Planning and Development in the Watersheds of the Toronto and Region Conservation Authority

TRCA Review

Prior to selecting the preferred alternative solution and design, please arrange a meeting to discuss issues that relate to TRCA Areas of Interest. In addition, please add TRCA's Humber River Project Manager Sonia Dhir to the project mailing list to receive any public information updates.

A copy of the TRCA Areas of Interest and the Service Delivery Standards is enclosed for your reference. We recommend you refer to these submission standards during the study to facilitate TRCA review.

Please provide the following submissions to expedite TRCA review:

- Notices of public meetings and display material and handouts
- Four hard copies of the Phases 1 and 2 Report
- Four hard copies of the Phase 3 Report
- Four hard copies of the Draft EA Document
- One hard copy of the Final EA Document.

Please include a digital copy of all submitted material. Materials must be submitted in PDF format, with drawings pre-scaled to print on 11"x17" pages. Materials may be submitted on discs, via e-mail (if less than 2.5 MB), or through file transfer protocol (FTP) sites (if posted for a minimum of two weeks).

Should you have any questions, please contact me at extension 5717 or at slingertat@trca.on.ca.

Yours truly,

Sharon Lingertat
Senior Planner, Environmental Assessment Planning
Planning and Development

Encl.: TRCA Areas of Interest Summary Table
Service Delivery Standards - Recommended TRCA Contact Points

BY E-MAIL

cc: Consultant: Stephen Keen (stephen.keen@cima.ca)
TRCA: Sonia Dhir, Humber River Watershed Project Manager

EA Requirements

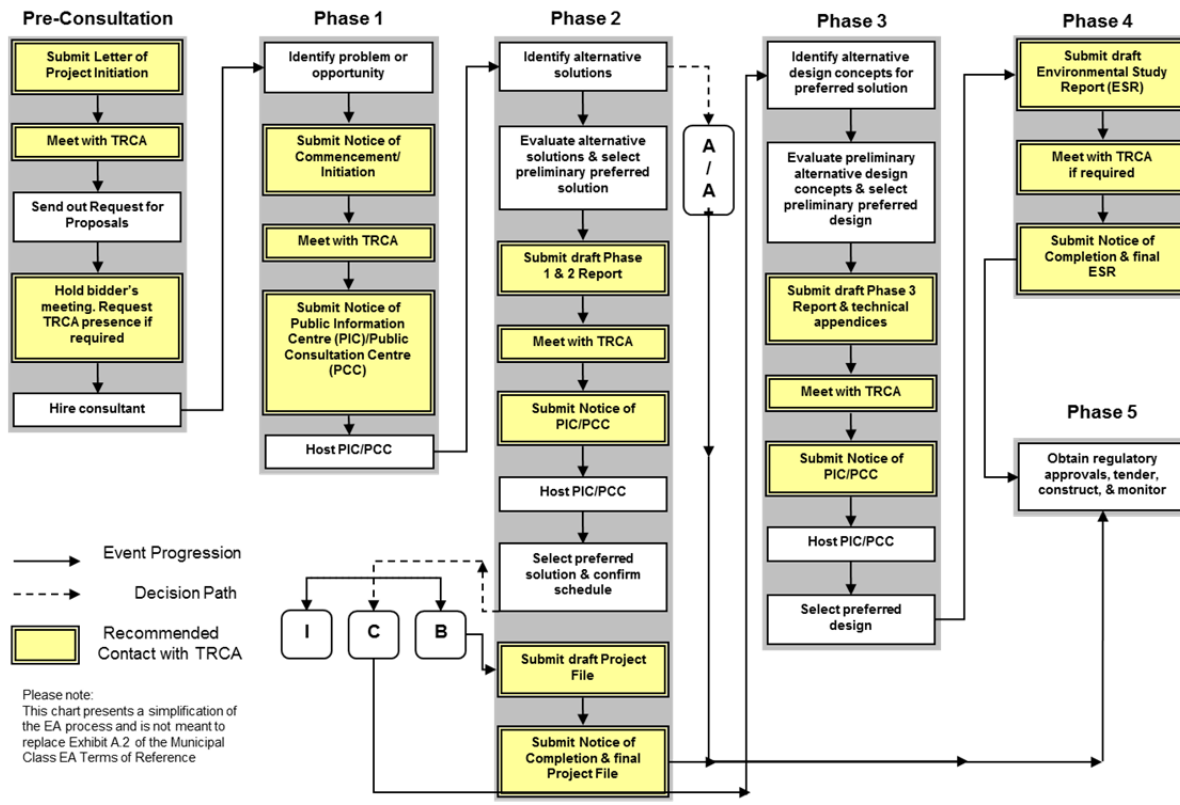
Document and assess the status, potential impacts and opportunities for enhancement that relate to the following Areas of Interest through a review of background material, technical study, field assessment and detailed evaluation, as appropriate. Make reference to the applicable Program and Policy documents. Include in the EA Document appendices any minutes, structure summary sheets for watercourses or wetlands, or other material collected through meetings with TRCA staff. Natural features may need to be confirmed on site by TRCA staff.

Area of Interest / Data Availability	Program and Policy Concerns
TRCA REGULATED AREAS	
Regulation Limit	<p>In accordance with Ontario Regulation 166/06 (Development, Interference with Wetlands and Alterations to Shorelines and Watercourses), a permit is required from the TRCA prior to any development (e.g. construction) if, in the opinion of TRCA, the control of flooding, erosion, dynamic beaches or pollution or the conservation of land may be affected. The Regulation Limit defines the greater of the natural hazards associated with Ontario Regulation 166/06 (listed below).</p> <p>NOTE: The Regulation Limit provides a geographical screening tool for determining if Ontario Regulation 166/06 will apply to a given proposal. Through site assessment or other investigation, it may be determined that areas outside of the defined Regulation Limit require permits under Ontario Regulation 166/06. In these instances, it is the text of the regulation that will prevail; modifications to the regulation line may be required.</p> <p>Any development within the Regulation Limit must comply with the applicable sections of TRCA's <i>Living City Policies</i>.</p>
Crest of Slope	<p>Valley and stream corridors are dynamic systems that provide important natural functions and linkages for the physical, chemical and biological processes of wildlife, watercourses, and other natural features. The Crest of Slope identifies the physical limit of these corridors; however, due to ecological sensitivities, development restrictions typically extend beyond the actual Crest of Slope.</p>
Watercourses	<p>Typically, watercourses are associated with aquatic species and habitat. Any alteration or interference to a watercourse (e.g. straightening, diverting, realigning, altering baseflow) has the potential to impact fish communities, but may also affect the Regulatory Flood Plain, erosion or other natural channel processes. TRCA may require an environmental study or site confirmation of watercourse locations.</p>
<p>TRCA PROGRAM AND POLICY AREAS <i>Note: Additional program and policy information may be available at www.trca.on.ca, or by request.</i></p>	
Aquatic Species and Habitat	<p>TRCA has prepared watershed plans or strategies, as well as watershed-based fisheries management plans for some of its watersheds in partnership with Aurora District MNR. TRCA may require an assessment of the existing aquatic system, together with an evaluation as to how the proposal will meet the objectives articulated in the watershed and watershed-based fisheries management plans, as well as prevent negative impacts to the aquatic system.</p> <p>If requested, TRCA will provide an opinion as to whether the project and its implementation will cause <i>serious harm</i> to fish. If <i>serious harm</i> to fish could result, then works will need to be reviewed and authorized by Fisheries and Oceans Canada (DFO).</p>
Living City Programs	<p>The Living City is a vision adopted by TRCA for a new kind of community, where human settlement can flourish forever as part of nature's beauty and diversity. The key objectives of the Living City are: healthy rivers and shorelines; regional biodiversity; sustainable communities; and business excellence.</p> <p>Programs associated with TRCA's Living City include: trails enhancement, renewable energy, sustainable communities, and the <i>Sustainable Technologies Evaluation Program (STEP)</i>.</p>
Terrestrial Natural	<p>TRCA has identified the need to improve both the quality and quantity of terrestrial habitat. TRCA's</p>

<p>Heritage System Strategy</p>	<p><i>Terrestrial Natural Heritage System Strategy</i> sets measurable targets for attaining a healthier natural system by creating an expanded and targeted land base. It includes strategic directions for stewardship and securement of the land base, a land use policy framework to help achieve the target system, and other implementation mechanisms.</p>
<p>Terrestrial Species and Habitat</p>	<p>The terrestrial system includes landscape features, vegetation communities and flora and fauna species. Terrestrial species and habitat should be assessed based on their conservation status according to sensitivity to disturbance and specialized ecological needs, as well as rarity.</p> <p>TRCA may require a site assessment and terrestrial inventory to confirm impacts to these resources. TRCA's <i>Terrestrial Natural Heritage Strategy</i> may be applicable to any work that impacts terrestrial species and habitat. In addition, relevant legislation (e.g. <i>Migratory Bird Convention Act</i>, <i>Species at Risk Act</i>) should be applied.</p>

Service Delivery Standards

Recommended TRCA Contact Points in the Municipal Class EA Planning & Design Process



Kate Barclay

From: Dela Cruz, Gino <gino.delacruz@peelregion.ca>
Sent: September 26, 2017 3:04 PM
To: Lee, Thomas; Lapos, Natalie; Khan, Sanya; Masley, Mark; Lo, Arthur; Nieuwenhuysen, Bob; Simms, Joy; Detaramani, Tina; Kant Chawla; Sharon Lingertat; mark.heaton@ontario.ca; trevor.bell@ontario.ca; Jennifer Benedict; Hamdani, Hashim Ali; Thomsen, Lori-Ann; Stockman, Angela; Caughey, Rebecca; Ansari, Seema
Cc: Jessica Dorgo; Stephen Keen
Subject: RE: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

Good afternoon,

Please note that the PIC scheduled for tonight has been cancelled and will be re-scheduled for a later date.

Sorry for the inconvenience and we will keep you updated on the future date for this meeting.

Regards,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Roads – Design and Construction
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: Dela Cruz, Gino
Sent: September 19, 2017 4:38 PM
To: Lee, Thomas; Lapos, Natalie; Khan, Sanya; Masley, Mark; Lo, Arthur; Nieuwenhuysen, Bob; Simms, Joy; Detaramani, Tina; 'Kant Chawla'; 'Sharon Lingertat'; 'mark.heaton@ontario.ca'; 'trevor.bell@ontario.ca'; Jennifer Benedict; Hamdani, Hashim Ali; Thomsen, Lori-Ann; Stockman, Angela; Caughey, Rebecca; Ansari, Seema
Cc: 'Jessica Dorgo'; Stephen Keen
Subject: FW: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

FYI – please see attached notice and boards for the PIC #1 on September 26, 2017.

From: Dela Cruz, Gino
Sent: September 12, 2017 4:12 PM
To: Dale, Frank; Thompson, Allan; Groves, Annette; Innis, Jennifer
Cc: Smith, Janette; Kocialek, Gary; Saiyed, Sabbir; Rook, Sally
Subject: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

Good afternoon,

I am reaching out to let you know that the first Public Information Centre (PIC) for the Coleraine Drive Grade Separation Environmental Assessment (South of Old Ellwood Drive) is being held on September 26, 2017 at the Albion Bolton Community Centre. Additional details are noted in the attached notice which will be published in the Caledon Enterprise and Caledon Citizen on September 14 and 21; as well as mailed out to residents in the area. Display boards for the public meeting will be forwarded to you closer to the PIC date.

If you have any questions or concerns, please contact me. Information will continue to be updated on the project website including the display boards shortly after the PIC date. The link is here <https://www.peelregion.ca/pw/transportation/envIRON-assess/index-cal.htm>

Thank you,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Roads – Design and Construction
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

Kate Barclay

From: Jessica Dorgo
Sent: October 7, 2019 9:01 AM
To: Caroline.Mugo@trca.ca
Cc: 'Sonya Bubas, MCIP (sonya.bubas@peelregion.ca)'; Stephen Keen
Subject: RE: Letter from TRCA concerning CFN 56254 - Coleraine Grade Separation PIC#1

Good Morning Caroline,

As requested, a copy of the Public Information Centre #1 display boards are available on the file transfer site below for download.

[Access the file transfer site](#)

Thank you,

Jessica Dorgo, EIT
Transportation
T 289-288-0287 ext. 6819 F 289-288-0285

CIMA+

From: Caroline Mugo <Caroline.Mugo@trca.ca>
Sent: Friday, October 4, 2019 11:53 AM
To: Bubas, Sonya <sonya.bubas@peelregion.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>
Subject: Letter from TRCA concerning CFN 56254 - Coleraine Grade Separation PIC#1

Caroline Mugo, Ph.D
Planner, Infrastructure Planning and Permits
Development and Engineering Services Division

T: [\(416\) 661-6600](tel:(416)661-6600) ext. 5689
E: caroline.mugo@trca.ca
A: [101 Exchange Avenue, Vaughan, ON, L4K 5R6](https://www.trca.ca) | [trca.ca](https://www.trca.ca)



October 4, 2019

CFN 56294

BY E-MAIL ONLY (sonya.bubas@peelregion.ca)

Sonya Bubas
Regional Municipality of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9

Dear Ms. Sonya Bubas:

**Re: Notice of Public Information Centre #1
Coleraine Drive Grade Separation EA
Municipal Class Environmental Assessment – Schedule C
Humber River Watershed; Town of Caledon; Regional Municipality of Peel**

Toronto and Region Conservation Authority (TRCA) staff received notice of the upcoming Public Information Centre (PIC) scheduled for October 8th, 2019-6:00 p.m. to 8:00 p.m.

Further to TRCA correspondence dated April 10, 2017, staff has expressed interest in this project. While staff will not be attending the meeting, staff will be reviewing the PIC#1 boards and providing comments. Please forward one copy of the handouts or display materials from this meeting for our files.

Should you have any questions or require any additional information please contact me at extension 5689 or at caroline.mugo@trca.ca.

Regards,

Caroline Mugo
Planner, Infrastructure Planning and Permits
Development and Engineering Services

CM

BY E-MAIL

cc: CIMA: Stephen Keen, (stephen.keen@cima.ca)

Kate Barclay

From: David Hiett
Sent: February 25, 2021 12:04 PM
To: Suzanne.Bevan@trca.ca; Emma.Benko@trca.ca; Kristen.Sullivan@trca.ca; Caroline Mugo
Cc: Mahmood, Tareq; Stephen Keen
Subject: P16-4315 (B738) Coleraine Rd Grade Separation EA - Draft SWM Report

Good Afternoon,

In support of the ongoing Coleraine Drive Grade Separation EA Study being undertaken by the Region of Peel, please find linked below the Draft Drainage and Stormwater Management Report for your review and comment. We have used our One Drive platform due to the size of file and so if you have any trouble accessing, please let me know.

https://cimeo365-my.sharepoint.com/:f/g/personal/david_hiett_cima_ca/EvSNPx0rtVtIrUVVtFohBsABfDRCOXjjKyqUbdd0ifi33g?e=n2fLDZ

Please let me know if any further information is required and more study background can be found on the project website: [Coleraine Drive Grade Separation - Transportation - Public Works - Region of Peel \(peelregion.ca\)](https://www.peelregion.ca/transportation/public-works/coleraine-drive-grade-separation)

Many Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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April 7, 2021

CFN 56294

BY E-MAIL ONLY (tareq.mahmood@peelregion.ca)

Tareq Mahmood
Region of Peel
10 Peel Centre Dr. Suite B, 4th Floor
Brampton, ON
L6T 4B9

Dear Mr. Mahmood:

Re: **Technical Report Review**
Coleraine Drive Grade Separation - South of Old Ellwood Drive
Municipal Class Environmental Assessment – Schedule C
Humber River Watershed; Town of Caledon; Regional Municipality of Peel

Toronto and Region Conservation Authority (TRCA) staff received supporting technical reports (Natural Heritage Report, Drainage and SWM Report, Fluvial Geomorphic Report and Hydrogeological Assessment Report) for the above noted Environmental Assessment (EA) on February 24, 2021 and March 5, 2021. Staff note that no draft Environmental Study Report has been received to date. Staff have completed their review and provide the following.

PROJECT OVERVIEW

TRCA staff understand that EA involves study of the long-term traffic improvements and grade separation of the CP Rail line for Coleraine Drive from north of King Street to Holland Drive in the Town of Caledon. Currently the project location represents a five-lane arterial road. The roadway crosses CPR corridor approximately 400 metres south of the intersection between Coleraine Drive and Harvest Moon Drive. A grade separation is proposed at Coleraine Drive and the CPR line as part of Peel Regional's 2019 Long Range Transportation Plan to mitigate future traffic congestion. The proposed roadway improvements include one watercourse crossing that is located 75 m south of the intersection of Coleraine Drive and Harvest Moon Drive.

PROJECT REVIEW

While staff has no objection in principle to the preferred alternative solution, the following concerns must be addressed in the draft EA document. Additional detailed comments are provided in Appendix A.

RESUBMISSION REQUIREMENTS

In order to facilitate the review of the next submission, please ensure that TRCA staff receives a digital copy of the draft ESR document. The draft EA document should be accompanied by a covering letter that uses the numbering scheme provided in this letter and identifies how these comments have been addressed.

Should you have any questions or require any additional information please contact me at extension 5759 or by email at suzanne.bevan@trca.ca

Regards,

A handwritten signature in blue ink that reads "Suzanne Bevan". The signature is written in a cursive, flowing style.

Suzanne Bevan
Senior Manager, Infrastructure Planning and Permits – Peel/York
Development and Engineering Services

SB/ks

Encl.: Appendix A

BY E-MAIL

cc: Consultant: CIMA+ David Hiett (David.Hiett@cima.ca)

APPENDIX A: TRCA COMMENTS AND PROPONENT RESPONSES

ITEM	TRCA COMMENTS (April 7, 2020)	PROponent RESPONSE (Date)
Stormwater Management		
1.	It appears that all considered alternatives for this project propose redirection of flow from the East Humber into the West Humber watershed. Typically, TRCA does not support the redirection of flows between watersheds. Therefore, further opportunities to avoid redirection of flows should be considered in the selection of the preferred alternative. If it is not possible, please provide staff with a detailed rationale and reasons. Please provide a digital copy of the Visual Otthymo model that was recreated to estimate the updated flows.	
2.	Enhanced quality control not only for the additional new paved but also for the entire area that will ultimately be discharged into the mentioned SWM facility is required. TRCA defers to the Region/Town to comment and confirm the existing 1350mm intercept storm sewer can handle the proposed flows and allow for the SWM Pond 5 to be retrofitted to achieve the required quality control.	
3.	Please note that the 5.0 mm onsite retention to maintain pre-development water balance is a minimum requirement set by the TRCA SWM criteria. However, it is the Region SWM Design criteria that will govern in this case. It is recommended that TRCA be provided with a preliminary design/sizing of the proposed LID measures at this stage, along with suitable locations and maintenance requirements/recommendations.	
4.	Please provide a discussion on the crossing's existing condition and whether upgrades or replacement will be required. Please refer to TRCA Crossing Guideline (2015) for further details and guidance. Staff will provide further comments at the next stage of EA review.	
5.	At the detailed design, please submit Erosion and Sediment Control drawings and a report which follow the Erosion and Sediment Control Guideline for Urban Construction, December 2019. The most up-to-date guideline can be downloaded from https://s3-ca-central-1.amazonaws.com/trcaca/app/uploads/2020/01/30145157/ESC-Guide-for-Urban-Construction_FINAL.pdf . As outlined in the guideline, an erosion and sediment control report and phased drawings are to be submitted. The two construction stages to consider and provide separate erosion and sediment control drawings for are: <ul style="list-style-type: none"> a) Potential Topsoil Stripping and Grading (Bulk Earth Works) b) Post Servicing Installation 	
6.	Please provide all subsequent reports stamped, signed, and dated by a qualified professional.	

Kate Barclay

From: David Hiett
Sent: September 23, 2021 11:27 AM
To: Emma Benko
Cc: Suzanne Bevan; Stephen Keen; Kevin Lukawiecki; Mahmood, Tareq
Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting
Attachments: B000738 - SWMP 9 Capacity Memo_e03.pdf; B000738 - LID Assessment Memo_e04.pdf; CFN 56294_Coleraine Drive Grade Separation_EA_Submission 2_Appendix A_CIMA Response Sept23-21.pdf; CFN 56294_Coleraine Drive Grade Separation_EA reponse letter_September 20 2021.pdf

Hi Emma and Suzanne,

Thanks very much for your previous comments and updates.

While awaiting comments on the model, we have reviewed and addressed other questions raised through your previous comments, and as a result, have updated our previous tech memos.

Please see attached updated versions of the SWM Pond Capacity Memo and also the LID Assessment Memo.

The memos were updated due to items 2 and 3 of the previous comment table - also attached is the same table with some summary clarifications/notes.

Following the recommendations of your letter, the above analysis and considerations will be included in our SWM Report, included in the draft ESR document, for resubmission with the TRCA. However, we also wanted to provide and inform now, as it would assist in the completion of your review of the model.

Please let me know if you have any questions,

Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CONFIDENTIALITY WARNING This email is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: Emma Benko <emma.benko@trca.ca>
Sent: Monday, September 20, 2021 11:39 AM
To: David Hiett <David.Hiett@cima.ca>

Cc: Suzanne Bevan <suzanne.bevan@trca.ca>; Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

EXTERNAL EMAIL

Hi David,

I hope you are doing well. Please find attached TRCA's comment response memo.

Please let me know if you have any questions. Thank you,

Emma Benko (she, her, hers)

Planner

Infrastructure Planning and Permits | Development and Engineering Services

T: [\(416\) 661-6600](tel:4166616600) ext. 5648

E: emma.benko@trca.ca

A: [101 Exchange Avenue, Vaughan, ON, L4K 5R6](https://www.trca.ca) | [trca.ca](https://www.trca.ca)



From: David Hiett <David.Hiett@cima.ca>

Sent: September 17, 2021 1:54 PM

To: Suzanne Bevan <Suzanne.Bevan@trca.ca>

Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Emma Benko <emma.benko@trca.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>

Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi Suzanne,

Following the previous correspondence and as requested, please see attached the digital copy of the hydrologic model and also a memo on the preliminary LID sizing and location for your review.

As below, we had also wondered if the TRCA were on board with the general approach of our improvements strategy, and you had mentioned a comment memo was being prepared – has that been completed?

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA

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CONFIDENTIALITY WARNING This email is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: Suzanne Bevan <Suzanne.Bevan@trca.ca>
Sent: Monday, September 13, 2021 9:33 AM
To: David Hiatt <David.Hiatt@cima.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Emma Benko <emma.benko@trca.ca>
Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

EXTERNAL EMAIL

Hi David,

Please note that Emma will provide a comment memo to you shortly.

In the mean time – please also note that the engineer is available to review the requested information this week should it be available.

Thank you,
Suzanne

Suzanne Bevan, B.Sc. (Hons), ENV SP
Senior Manager
Infrastructure Planning and Permits – Peel/York | Development and Engineering Services

T: (416) 661-6600 ext. 5759
C: (647) 924-5467
E: suzanne.bevan@trca.ca
A: 101 Exchange Avenue, Vaughan, ON, L4K 5R6 | trca.ca



From: David Hiatt <David.Hiatt@cima.ca>
Sent: September 9, 2021 9:33 AM
To: Suzanne Bevan <Suzanne.Bevan@trca.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi Suzanne,

Thanks for the update, I will postpone the meeting for now and will speak to the team to send you the information. However, before commencing with the additional analysis I just wanted to ensure that the TRCA is on board with the general approach as outlined in our response to comment 1 (attached). The TRCA noted that we are planning to redirect flow from the East Humber into the West Humber watershed. This is a redirection from existing conditions which were established in 2010 when Coleraine Drive was widened and storm sewers were installed, redirecting minor flow south to the East Humber watershed. Our proposed redirection will direct flows back north to the West Humber watershed, returning all flow paths to pre-2010 construction conditions.

Please let us know if you are comfortable with this approach?, and please can you confirm if a rescheduled meeting would be required should the analysis prove to be satisfactory?

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CONFIDENTIALITY WARNING This email is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: Suzanne Bevan <Suzanne.Bevan@trca.ca>

Sent: Thursday, September 9, 2021 9:01 AM

To: David Hiett <David.Hiett@cima.ca>

Subject: FW: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

EXTERNAL EMAIL

Hi David,

I followed up with water resources engineering staff this morning and they have noted to me that he requires a digital copy of the hydrologic model and the preliminary LID sizing and location as indicated in their latest comments. He will need to review that before he can provide further comments. As such, I am not certain today's meeting will be useful – and suggest a reschedule of today's meeting to a week after we receive the above requested information.

However if there are other items you wish to discuss we can meet but I thought I should relay this information to you in advance.

Please let me know your thoughts.

Thank you,
Suzanne

Suzanne Bevan, B.Sc. (Hons), ENV SP
Senior Manager
Infrastructure Planning and Permits – Peel/York | Development and Engineering Services

T: (416) 661-6600 ext. 5759
C: (647) 924-5467
E: suzanne.bevan@trca.ca
A: 101 Exchange Avenue, Vaughan, ON, L4K 5R6 | trca.ca



From: Suzanne Bevan
Sent: August 18, 2021 3:27 PM
To: David Hiatt <David.Hiatt@cima.ca>
Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi David,

You are welcome to remove Caroline from the cc's etc. She doesn't work on Peel files anymore.

Thank you,
Suzanne

Suzanne Bevan, B.Sc. (Hons), ENV SP
Senior Manager
Infrastructure Planning and Permits – Peel/York | Development and Engineering Services

T: (416) 661-6600 ext. 5759
C: (647) 924-5467
E: suzanne.bevan@trca.ca
A: 101 Exchange Avenue, Vaughan, ON, L4K 5R6 | trca.ca



-----Original Appointment-----

From: David Hiatt <David.Hiatt@cima.ca>
Sent: August 18, 2021 2:59 PM
To: David Hiatt; Suzanne Bevan; Emma Benko; Kristen Sullivan; Caroline Mugo; Stephen Keen; Kevin Lukawiecki; Mahmood, Tareq; Saini, Sakshi
Subject: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting
When: September 9, 2021 10:30 AM-12:00 PM (UTC-05:00) Eastern Time (US & Canada).
Where: Microsoft Teams Meeting

Hi All,

Per the emails, we are proposing this meeting with the TRCA to further discuss the Stormwater Management and Drainage strategy proposed as part of the improvements of the Coleraine Drive CP Grade Separation EA, undertaken by the Region of Peel.

We have previously consulted with TRCA, and this meeting will be to confirm understanding of the additional analysis conducted into Pond 9 / responses provided to the original SWM report (previous correspondence attached), and seek agreement with the proposed strategy.

Please let me know if you have any questions,

Thanks,

Microsoft Teams meeting

Join on your computer or mobile app

[Click here to join the meeting](#)

[Learn More](#) | [Meeting options](#)

From: David Hiatt

Sent: Wednesday, August 18, 2021 2:52 PM

To: Suzanne Bevan <Suzanne.Bevan@trca.ca>; Emma Benko <emma.benko@trca.ca>; Kristen Sullivan <kristen.sullivan@trca.ca>; Caroline Mugo <Caroline.Mugo@trca.ca>; Cassie Schembri <Cassie.Schembri@caledon.ca>; Margi Sheth <Margi.Sheth@caledon.ca>; Ryan Grodecki <Ryan.Grodecki@caledon.ca>

Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Saini, Sakshi <sakshi.saini@peelregion.ca>

Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi All,

Thanks for all feedback provided.

As an update - unfortunately, a date that both TRCA and Town are available does not lie in the near future and fit with the project schedule. Therefore, we will hold meetings separately.

Cassie – thanks for the further info. The Project Team already have other anticipated meetings with the Town coming up, and so opportunities will be sought to further discuss SWM matters with the Town at those points. We will confirm. Suzanne – thanks again for the availability, our schedules align best with September 9 in the AM and so I will shortly send out a meeting invite to TRCA staff.

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

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From: David Hiatt
Sent: Wednesday, August 18, 2021 9:34 AM
To: Suzanne Bevan <Suzanne.Bevan@trca.ca>; Emma Benko <emma.benko@trca.ca>; Kristen Sullivan <kristen.sullivan@trca.ca>; Caroline Mugo <Caroline.Mugo@trca.ca>; Cassie Schembri <Cassie.Schembri@caledon.ca>; Margi Sheth <Margi.Sheth@caledon.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Saini, Sakshi <sakshi.saini@peelregion.ca>
Subject: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Good Morning All,

In support of the Coleraine Drive CP Grade Separation EA undertaken by the Region of Peel, we would like to organize a meeting with the TRCA and Town of Caledon to further discuss the Stormwater Management and Drainage strategy proposed as part of the improvements.

We have previously consulted with both the Town and TRCA, and this meeting will be to confirm understanding of the additional analysis conducted into Pond 9 / responses provided to the original SWM report, and seek agreement with the proposed strategy.

We are hoping to have this meeting in the week of September 6th or 13th, and so can we kindly request your availability?

Please respond using table below

Also, for this email I have included all those that were involved in previous correspondence, and so please also confirm that those above should attend or if there should also be others.

Many thanks,

x ✓

	Mon Sept 6	Tues Sept 7	Wed Sept 8	Thurs Sept 9	Fri Sept 10
AM	N/A - Stat				
PM	N/A - Stat				
	Mon Sept 13	Tues Sept 14	Wed Sept 15	Thurs Sept 16	Fri Sept 17
AM					
PM					

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

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September 20, 2021

CFN 56294

BY E-MAIL ONLY (tareq.mahmood@peelregion.ca)

Tareq Mahmood
Region of Peel
10 Peel Centre Dr. Suite B, 4th Floor
Brampton, ON
L6T 4B9

Dear Tareq Mahmood:

Re: **Technical Report Review**
Coleraine Drive Grade Separation - South of Old Ellwood Drive
Municipal Class Environmental Assessment – Schedule C
Humber River Watershed; Town of Caledon; Regional Municipality of Peel

Toronto and Region Conservation Authority (TRCA) staff received the Draft Stormwater Management Report on July 11, 2021.

PROJECT OVERVIEW

TRCA staff understand that EA involves study of the long-term traffic improvements and grade separation of the CP Rail line for Coleraine Drive from north of King Street to Holland Drive in the Town of Caledon. Currently the project location represents a five-lane arterial road. The roadway crosses CPR corridor approximately 400 metres south of the intersection between Coleraine Drive and Harvest Moon Drive. A grade separation is proposed at Coleraine Drive and the CPR line as part of Peel Regional's 2019 Long Range Transportation Plan to mitigate future traffic congestion. The proposed roadway improvements include one watercourse crossing that is located 75 m south of the intersection of Coleraine Drive and Harvest Moon Drive.

PROJECT REVIEW

While staff has no objection in principle to the preferred alternative solution, the following concerns must be addressed in the draft EA document. Additional detailed comments are provided in Appendix A.

RESUBMISSION REQUIREMENTS

In order to facilitate the review of the next submission, please ensure that TRCA staff receives a digital copy of the draft ESR document. The draft EA document should be accompanied by a covering letter that uses the numbering scheme provided in this letter and identifies how these comments have been addressed.

Should you have any questions or require any additional information please contact me at extension 5759 or by email at suzanne.bevan@trca.ca

Regards,



Suzanne Bevan
Senior Manager, Infrastructure Planning and Permits – Peel/York
Development and Engineering Services

SB/eb

Encl.: Appendix A

BY E-MAIL

cc: Consultant: CIMA+ David Hiatt (David.Hiatt@cima.ca)

APPENDIX A: TRCA COMMENTS AND PROPONENT RESPONSES

ITEM	TRCA COMMENTS (April 7, 2021)	PROponent/CONSULTANT RESPONSE (July 11, 2021)	TRCA COMMENTS (September 20, 2021)
1.	<p>It appears that all considered alternatives for this project propose redirection of flow from the East Humber into the West Humber watershed. Typically, TRCA does not support the redirection of flows between watersheds. Therefore, further opportunities to avoid redirection of flows should be considered in the selection of the preferred alternative. If it is not possible, please provide staff with a detailed rationale and reasons. Please provide a digital copy of the Visual Otthymo model that was recreated to estimate the updated flows.</p>	<p>We have revisited the original drainage patterns before 2010 construction. As of 1997, all major and minor flows from the area north of Manchester Court, went into the East Humber Water shed.</p> <p>The widening of Coleraine Drive in 2010 diverted the minor flow south of the railway, into the West Humber Watershed - this was a change.</p> <p>Would it be acceptable if we were to devise a plan to return all flows to the pre-widening situation and send major and minor flows to the East Humber Watershed?</p> <p>We believe this may be possible through the introduction of a small SWM facility near Manchester Court which would hold the regional storm before discharging it to the north while also providing quality treatment.</p>	<p>It appears the diversion of flows into the west Humber river was completed back in 2010. If that is the case staff has no further objections. If the proponent would like to return all flow to pre-widening conditions, supporting information/study that demonstrates there will be no significant increases in flows (or flooding) in adjacent properties should be provided. . Please provide a digital copy of the Visual Otthymo model that was recreated to estimate the updated flows.</p>
2.	<p>Enhanced quality control not only for the additional new paved but also for the entire area that will ultimately be discharged into the mentioned SWM facility is required.</p> <p>TRCA defers to the Region/Town to comment and confirm the existing 1350mm intercept storm sewer can handle the proposed flows and allow for the SWM Pond 5 to be retrofitted to achieve the required quality control.</p>	<p>Entire study area will be directed to existing SWMP 9 (also documented as SWM Pond 5). A preliminary assessment has been completed that concludes the pond should be able to handle the extra flow and will provide quality control. The technical memo has been attached to these responses and submitted to the Town for review. The Town have been consulted with regarding this strategy and the use of the pond will be at the Town's approval, which will be confirmed at Detail Design and with agreements in place.</p>	<p>It should be noted that in addition to the required 11,300m³ for the permanent pool an approximate volume of 15,000 m³ is required for erosion control. Please confirm the outlet structure provides enough extended detention storage and would provide the required 48hr drawdown consistent with the TRCA SWM criteria.</p>

3.	Please note that the 5.0 mm onsite retention to maintain pre-development water balance is a minimum requirement set by the TRCA SWM criteria. However, it is the Region SWM Design criteria that will govern in this case. It is recommended that TRCA be provided with a preliminary design/sizing of the proposed LID measures at this stage, along with suitable locations and maintenance requirements/recommendations.	The Region 28mm water balance requirement will need to be met. Quantity required for each drainage area under all proposed alternatives has been calculated. If the flow redirection and the new SWM facility at Manchester Court (Item 1) suggested above is acceptable, then suitable locations have been found for SWM to address water balance. Maintenance requirements will be recommended.	TRCA staff consider it would be feasible to provide the required onsite retention (28mm) from the Region. However, preliminary sizing and LID locations should be provided at this stage. The proposed LID locations and maintenance plan will be deferred to the Region for review and approval.
4.	Please provide a discussion on the crossing's existing condition and whether upgrades or replacement will be required. Please refer to TRCA Crossing Guideline (2015) for further details and guidance. Staff will provide further comments at the next stage of EA review.	There are no crossings in the study area.	The SWM memo states that there are no crossings within the study limits. Comment addressed, no further comments.
5.	At the detailed design, please submit Erosion and Sediment Control drawings and a report which follow the Erosion and Sediment Control Guideline for Urban Construction, December 2019. The most up-to-date guideline can be downloaded from https://s3-ca-central-1.amazonaws.com/trcaca/app/uploads/2020/01/30145157/ESC-Guide-for-Urban-Construction_FINAL.pdf . As outlined in the guideline, an erosion and sediment control report and phased drawings are to be submitted. The two construction stages to consider and provide separate erosion and sediment control drawings for are: Potential Topsoil Stripping and Grading	Noted	Erosion and Sediment Control measures are to be submitted at the detailed design stage. Comment addressed, no further comments.

	(Bulk Earth Works) and Post Servicing Installation		
6.	Please provide all subsequent reports stamped, signed, and dated by a qualified professional	Subsequent reports to be stamped, signed and dated.	Comment addressed, no further comments.
7.			Staff noted that the redirection of flow from the East Humber watershed into the West Humber watershed may cause undesirable impact by diverting flow from existing natural features. Please clarify if existing natural features will be impacted. Mitigation may be required if features are impacted.

APPENDIX A: TRCA COMMENTS AND PROPONENT RESPONSES

ITEM	TRCA COMMENTS (April 7, 2021)	PROponent/CONSULTANT RESPONSE (July 11, 2021)	TRCA COMMENTS (September 20, 2021)	PROponent/CONSULTANT RESPONSE (Sept 23, 2021)
1.	<p>It appears that all considered alternatives for this project propose redirection of flow from the East Humber into the West Humber watershed. Typically, TRCA does not support the redirection of flows between watersheds. Therefore, further opportunities to avoid redirection of flows should be considered in the selection of the preferred alternative. If it is not possible, please provide staff with a detailed rationale and reasons. Please provide a digital copy of the Visual Otthymo model that was recreated to estimate the updated flows.</p>	<p>We have revisited the original drainage patterns before 2010 construction. As of 1997, all major and minor flows from the area north of Manchester Court, went into the East Humber Water shed. The widening of Coleraine Drive in 2010 diverted the minor flow south of the railway, into the West Humber Watershed - this was a change.</p> <p>Would it be acceptable if we were to devise a plan to return all flows to the pre-widening situation and send major and minor flows to the East Humber Watershed?</p> <p>We believe this may be possible through the introduction of a small SWM facility near Manchester Court which would hold the regional storm before discharging it to the north while also providing quality treatment.</p>	<p>It appears the diversion of flows into the west Humber river was completed back in 2010. If that is the case staff has no further objections. If the proponent would like to return all flow to pre-widening conditions, supporting information/study that demonstrates there will be no significant increases in flows (or flooding) in adjacent properties should be provided. . Please provide a digital copy of the Visual Otthymo model that was recreated to estimate the updated flows.</p>	<p>Noted. Model has been provided for TRCA review.</p>
2.	<p>Enhanced quality control not only for the additional new paved but also for the entire area that will ultimately be discharged into the mentioned SWM facility is required.</p> <p>TRCA defers to the Region/Town to comment and confirm the existing</p>	<p>Entire study area will be directed to existing SWMP 9 (also documented as SWM Pond 5). A preliminary assessment has been completed that concludes the pond should be able to handle the extra flow and will provide quality control. The technical memo has been attached</p>	<p>It should be noted that in addition to the required 11,300m³ for the permanent pool an approximate volume of 15,000 m³ is required for erosion control. Please confirm the outlet</p>	<p>SWM Pond 9 Capacity Memo has been updated to include erosion control, and the updated memo is enclosed with this response. The required additional volume will be</p>

	1350mm intercept storm sewer can handle the proposed flows and allow for the SWM Pond 5 to be retrofitted to achieve the required quality control.	to these responses and submitted to the Town for review. The Town have been consulted with regarding this strategy and the use of the pond will be at the Town's approval, which will be confirmed at Detail Design and with agreements in place.	structure provides enough extended detention storage and would provide the required 48hr drawdown consistent with the TRCA SWM criteria.	accommodated by a combination of the new LID facility at Manchester Court and the existing SWM Pond 9. Even though the proposed Coleraine Drive conditions will result in increased runoff into SWM Pond 9, the use of the additional LID facility means that the capacity of SWM Pond 9 will remain the same as existing. The LID Analysis Memo has been updated with these requirements.
3.	Please note that the 5.0 mm onsite retention to maintain pre-development water balance is a minimum requirement set by the TRCA SWM criteria. However, it is the Region SWM Design criteria that will govern in this case. It is recommended that TRCA be provided with a preliminary design/sizing of the proposed LID measures at this stage, along with suitable locations and maintenance requirements/recommendations.	The Region 28mm water balance requirement will need to be met. Quantity required for each drainage area under all proposed alternatives has been calculated. If the flow redirection and the new SWM facility at Manchester Court (Item 1) suggested above is acceptable, then suitable locations have been found for SWM to address water balance. Maintenance requirements will be recommended.	TRCA staff consider it would be feasible to provide the required onsite retention (28mm) from the Region. However, preliminary sizing and LID locations should be provided at this stage. The proposed LID locations and maintenance plan will be deferred to the Region for review and approval.	As above, in conjunction with the SWM Pond 9 memo, the LID Analysis memo has also been updated and enclosed with this response.
4.	Please provide a discussion on the crossing's existing condition and whether upgrades or replacement will be required. Please refer to	There are no crossings in the study area.	The SWM memo states that there are no crossings within the study limits. Comment	-

	TRCA Crossing Guideline (2015) for further details and guidance. Staff will provide further comments at the next stage of EA review.		addressed, no further comments.	
5.	At the detailed design, please submit Erosion and Sediment Control drawings and a report which follow the Erosion and Sediment Control Guideline for Urban Construction, December 2019. The most up-to-date guideline can be downloaded from https://s3-ca-central-1.amazonaws.com/trcaca/app/uploads/2020/01/30145157/ESC-Guide-for-Urban-Construction_FINAL.pdf . As outlined in the guideline, an erosion and sediment control report and phased drawings are to be submitted. The two construction stages to consider and provide separate erosion and sediment control drawings for are: Potential Topsoil Stripping and Grading (Bulk Earth Works) and Post Servicing Installation	Noted	Erosion and Sediment Control measures are to be submitted at the detailed design stage. Comment addressed, no further comments.	-
6.	Please provide all subsequent reports stamped, signed, and dated by a qualified professional	Subsequent reports to be stamped, signed and dated.	Comment addressed, no further comments.	-
7.	-	-	Staff noted that the redirection of flow from the	The redirection of flow is only the minor storm sewer

			<p>East Humber watershed into the West Humber watershed may cause undesirable impact by diverting flow from existing natural features. Please clarify if existing natural features will be impacted. Mitigation may be required if features are impacted.</p>	<p>drainage which drained area B1 south into the existing storm sewer system. As this runoff is direct from the roadway and did not first enter any existing natural features, no natural features will be impacted by the diversion</p>
--	--	--	---	--

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

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400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

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Kate Barclay

From: Suzanne Bevan <Suzanne.Bevan@trca.ca>
Sent: February 13, 2022 1:33 PM
To: David Hiatt
Cc: Stephen Keen; Kevin Lukawiecki; Mahmood, Tareq; Emma Benko; Madhav Baral
Subject: RE: CFN 56294 B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

EXTERNAL EMAIL

Hi Dave,

TRCA water resources engineering staff provides the following further to your email of January 31, 2022.

Please note that staff agree that the existing SWM pond (Pond 9) would be able to provide the required water quantity, quality and erosion storage (as per the as-built drawing). However, a detailed update to the pond stage-storage-discharge and further hydrologic modeling to confirm potential changes in the outflows will be required at the detailed design stage. A lot of sediments may accumulate over time. Therefore, staff will also require confirmation that the facility still provides adequate storage by the time the proposed works are implemented.

Thank you,
Suzanne

Suzanne Bevan, B.Sc. (Hons), ENV SP
Senior Manager
Infrastructure Planning and Permits – Peel/York | Development and Engineering Services

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E: suzanne.bevan@trca.ca
A: 101 Exchange Avenue, Vaughan, ON, L4K 5R6 | trca.ca



From: Suzanne Bevan
Sent: January 31, 2022 1:12 PM
To: David Hiatt <David.Hiatt@cima.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Emma Benko <emma.benko@trca.ca>; Madhav Baral <Madhav.Baral@cima.ca>
Subject: RE: CFN 56294 B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi Dave,

I hope you (all) did too.

Thank you for the follow up. I have circulated to water resources engineering staff. I will follow up with the team once staff have reviewed.

Perfect – thanks for the update on the ESR.

Take care,
Suzanne

Suzanne Bevan, B.Sc. (Hons), ENV SP
Senior Manager
Infrastructure Planning and Permits – Peel/York | Development and Engineering Services

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A: 101 Exchange Avenue, Vaughan, ON, L4K 5R6 | trca.ca



From: David Hiatt <David.Hiatt@cima.ca>
Sent: January 31, 2022 12:03 PM
To: Suzanne Bevan <Suzanne.Bevan@trca.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Emma Benko <emma.benko@trca.ca>; Madhav Baral <Madhav.Baral@cima.ca>
Subject: RE: CFN 56294 B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi Suzanne,

I hope you had a good Christmas and Happy New Year,
Regarding the Coleraine Drive Grade Separation EA, we thought to close the loop on the SWM Report and our past correspondence. Previously and as below, we had provided updated analysis and memos in support of our proposed strategy, that incorporated the existing SWM Pond and also additional LID Facilities (previous email attached just for reference). This analysis, with a slight update (see below), is now incorporated into the final draft SWM Report, with the final analysis included in appendix E, and available in the following shared folder for your review:

[P16-4315 \(B738\) Coleraine Rd EA - Files to TRCA](#)

Also, in answer to your questions below, please see responses in red to your last comments previously provided:

- The proposed water balance measures appear to be acceptable. We defer the Region to review the LID sizing calculations and approve the proposed locations. **Understood.**
- Based on the 1998 MOE Certificate of Approval, the SWMP 9 has a quantity control volume of 58,000m³. If this is the case, the facility would be able to accommodate the required 100-year storage of the original pre-2010 (42,253m³), existing post-2010 (42,790m³) and proposed (43,068m³). It should be noted that the erosion storage (25mm runoff volume) is part of the active storage of the pond and should not be added to the required quantity control. Please revise the values provided in Table 1 of the SWMP 9 Capacity Analysis Memo, the quantity and erosion control volume values appear to be interchanged. **Understood, values have been amended.**

- Is it possible to get a complete copy of the Falby Burnside and Associates Stormwater Management Report for the Heritage Hills Subdivision (Aug 1997) and associated as-built drawings. Staff would like to have a better understanding of how the outlet structure was set. Based on the route reservoir command information provided in Appendix B of the Capacity memo prepared by CIMA (September 22, 2021), the original pond would not be able to provide the required 100-year storage. If the pond has a capacity of 58,000m3 as indicated in the MOE letter, staff can infer there is plenty of storage. However, staff would like to confirm that the outlet structure was set properly and will continue to attenuate the redirected flow to the appropriate pre-development levels. **Please see attached for complete report, however we note that this report is not for the SWMP Pond 9 however a pond downstream. The report has a stage storage for SWMP included in its appended calculations, yet that is all the data. It is highlighted that this report actually had a different capacity (42,500 m3 noted on page 34, compared to the 58,000m3 noted in the MOE letter). Due to the lack of stage storage data from the CofA, the more conservative quantity storage volume of 42,500 m3 has been used for the analysis of the SWMP.**

Please let us know if you have any further comments,
We are aiming to include the Final SWM report in the Draft ESR submission, anticipated end of February.

Many Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

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CONFIDENTIALITY WARNING This email is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: Suzanne Bevan <Suzanne.Bevan@trca.ca>
Sent: Monday, September 27, 2021 6:36 PM
To: David Hiett <David.Hiett@cima.ca>; Emma Benko <emma.benko@trca.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Emma Benko <emma.benko@trca.ca>
Subject: CFN 56294 B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

EXTERNAL EMAIL

Hi David,

Thank you for the additional information.

Stormwater Management Comments

- The proposed water balance measures appear to be acceptable. We defer the Region to review the LID sizing calculations and approve the proposed locations.
- Based on the 1998 MOE Certificate of Approval, the SWMP 9 has a quantity control volume of 58,000m³. If this is the case, the facility would be able to accommodate the required 100-year storage of the original pre-2010 (42,253m³), existing post-2010 (42,790m³) and proposed (43,068m³). It should be noted that the erosion storage (25mm runoff volume) is part of the active storage of the pond and should not be added to the required quantity control. Please revise the values provided in Table 1 of the SWMP 9 Capacity Analysis Memo, the quantity and erosion control volume values appear to be interchanged.
- Is it possible to get a complete copy of the Falby Burnside and Associates Stormwater Management Report for the Heritage Hills Subdivision (Aug 1997) and associated as-built drawings. Staff would like to have a better understanding of how the outlet structure was set. Based on the route reservoir command information provided in Appendix B of the Capacity memo prepared by CIMA (September 22, 2021), the original pond would not be able to provide the required 100-year storage. If the pond has a capacity of 58,000m³ as indicated in the MOE letter, staff can infer there is plenty of storage. However, staff would like to confirm that the outlet structure was set properly and will continue to attenuate the redirected flow to the appropriate pre-development levels.

Should you have any questions please do not hesitate to contact me.

Thank you,
Suzanne

Suzanne Bevan, B.Sc. (Hons), ENV SP

Senior Manager

Infrastructure Planning and Permits – Peel/York | Development and Engineering Services

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From: David Hiatt <David.Hiatt@cima.ca>

Sent: September 23, 2021 11:27 AM

To: Emma Benko <emma.benko@trca.ca>

Cc: Suzanne Bevan <Suzanne.Bevan@trca.ca>; Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>

Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi Emma and Suzanne,

Thanks very much for your previous comments and updates.

While awaiting comments on the model, we have reviewed and addressed other questions raised through your previous comments, and as a result, have updated our previous tech memos.

Please see attached updated versions of the SWM Pond Capacity Memo and also the LID Assessment Memo.

The memos were updated due to items 2 and 3 of the previous comment table - also attached is the same table with some summary clarifications/notes.

Following the recommendations of your letter, the above analysis and considerations will be included in our SWM Report, included in the draft ESR document, for resubmission with the TRCA. However, we also wanted to provide and inform now, as it would assist in the completion of your review of the model.

Please let me know if you have any questions,

Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

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From: Emma Benko <emma.benko@trca.ca>
Sent: Monday, September 20, 2021 11:39 AM
To: David Hiett <David.Hiett@cima.ca>
Cc: Suzanne Bevan <suzanne.bevan@trca.ca>; Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

EXTERNAL EMAIL

Hi David,

I hope you are doing well. Please find attached TRCA's comment response memo.

Please let me know if you have any questions. Thank you,

Emma Benko (she, her, hers)
Planner
Infrastructure Planning and Permits | Development and Engineering Services

T: [\(416\) 661-6600](tel:(416)661-6600) ext. 5648

E: emma.benko@trca.ca

A: [101 Exchange Avenue, Vaughan, ON, L4K 5R6](https://www.trca.ca) | [trca.ca](https://www.trca.ca)



From: David Hiett <David.Hiett@cima.ca>

Sent: September 17, 2021 1:54 PM

To: Suzanne Bevan <Suzanne.Bevan@trca.ca>

Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Emma Benko <emma.benko@trca.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>

Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi Suzanne,

Following the previous correspondence and as requested, please see attached the digital copy of the hydrologic model and also a memo on the preliminary LID sizing and location for your review.

As below, we had also wondered if the TRCA were on board with the general approach of our improvements strategy, and you had mentioned a comment memo was being prepared – has that been completed?

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CONFIDENTIALITY WARNING This email is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: Suzanne Bevan <Suzanne.Bevan@trca.ca>

Sent: Monday, September 13, 2021 9:33 AM

To: David Hiett <David.Hiett@cima.ca>

Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Emma Benko <emma.benko@trca.ca>

Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

EXTERNAL EMAIL

Hi David,

Please note that Emma will provide a comment memo to you shortly.

In the mean time – please also note that the engineer is available to review the requested information this week should it be available.

Thank you,
Suzanne

Suzanne Bevan, B.Sc. (Hons), ENV SP
Senior Manager
Infrastructure Planning and Permits – Peel/York | Development and Engineering Services

T: (416) 661-6600 ext. 5759

C: (647) 924-5467

E: suzanne.bevan@trca.ca

A: 101 Exchange Avenue, Vaughan, ON, L4K 5R6 | trca.ca



From: David Hiatt <David.Hiatt@cima.ca>

Sent: September 9, 2021 9:33 AM

To: Suzanne Bevan <Suzanne.Bevan@trca.ca>

Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>

Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi Suzanne,

Thanks for the update, I will postpone the meeting for now and will speak to the team to send you the information. However, before commencing with the additional analysis I just wanted to ensure that the TRCA is on board with the general approach as outlined in our response to comment 1 (attached). The TRCA noted that we are planning to redirect flow from the East Humber into the West Humber watershed. This is a redirection from existing conditions which were established in 2010 when Coleraine Drive was widened and storm sewers were installed, redirecting minor flow south to the East Humber watershed. Our proposed redirection will direct flows back north to the West Humber watershed, returning all flow paths to pre-2010 construction conditions.

Please let us know if you are comfortable with this approach?, and please can you confirm if a rescheduled meeting would be required should the analysis prove to be satisfactory?

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CONFIDENTIALITY WARNING This email is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: Suzanne Bevan <Suzanne.Bevan@trca.ca>
Sent: Thursday, September 9, 2021 9:01 AM
To: David Hiatt <David.Hiatt@cima.ca>
Subject: FW: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

EXTERNAL EMAIL

Hi David,

I followed up with water resources engineering staff this morning and they have noted to me that he requires a digital copy of the hydrologic model and the preliminary LID sizing and location as indicated in their latest comments. He will need to review that before he can provide further comments. As such, I am not certain today's meeting will be useful – and suggest a reschedule of today's meeting to a week after we receive the above requested information.

However if there are other items you wish to discuss we can meet but I thought I should relay this information to you in advance.

Please let me know your thoughts.

Thank you,
Suzanne

Suzanne Bevan, B.Sc. (Hons), ENV SP
Senior Manager
Infrastructure Planning and Permits – Peel/York | Development and Engineering Services

T: (416) 661-6600 ext. 5759
C: (647) 924-5467
E: suzanne.bevan@trca.ca
A: 101 Exchange Avenue, Vaughan, ON, L4K 5R6 | trca.ca



From: Suzanne Bevan
Sent: August 18, 2021 3:27 PM
To: David Hiatt <David.Hiatt@cima.ca>
Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi David,

You are welcome to remove Caroline from the cc's etc. She doesn't work on Peel files anymore.

Thank you,
Suzanne

Suzanne Bevan, B.Sc. (Hons), ENV SP
Senior Manager
Infrastructure Planning and Permits – Peel/York | Development and Engineering Services

T: (416) 661-6600 ext. 5759
C: (647) 924-5467
E: suzanne.bevan@trca.ca
A: 101 Exchange Avenue, Vaughan, ON, L4K 5R6 | trca.ca



-----Original Appointment-----

From: David Hiett <David.Hiett@cima.ca>

Sent: August 18, 2021 2:59 PM

To: David Hiett; Suzanne Bevan; Emma Benko; Kristen Sullivan; Caroline Mugo; Stephen Keen; Kevin Lukawiecki; Mahmood, Tareq; Saini, Sakshi

Subject: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

When: September 9, 2021 10:30 AM-12:00 PM (UTC-05:00) Eastern Time (US & Canada).

Where: Microsoft Teams Meeting

Hi All,

Per the emails, we are proposing this meeting with the TRCA to further discuss the Stormwater Management and Drainage strategy proposed as part of the improvements of the Coleraine Drive CP Grade Separation EA, undertaken by the Region of Peel.

We have previously consulted with TRCA, and this meeting will be to confirm understanding of the additional analysis conducted into Pond 9 / responses provided to the original SWM report (previous correspondence attached), and seek agreement with the proposed strategy.

Please let me know if you have any questions,

Thanks,

Microsoft Teams meeting

Join on your computer or mobile app

[Click here to join the meeting](#)

[Learn More](#) | [Meeting options](#)

From: David Hiett
Sent: Wednesday, August 18, 2021 2:52 PM
To: Suzanne Bevan <Suzanne.Bevan@trca.ca>; Emma Benko <emma.benko@trca.ca>; Kristen Sullivan <kristen.sullivan@trca.ca>; Caroline Mugo <Caroline.Mugo@trca.ca>; Cassie Schembri <Cassie.Schembri@caledon.ca>; Margi Sheth <Margi.Sheth@caledon.ca>; Ryan Grodecki <Ryan.Grodecki@caledon.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Saini, Sakshi <sakshi.saini@peelregion.ca>
Subject: RE: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Hi All,

Thanks for all feedback provided.

As an update - unfortunately, a date that both TRCA and Town are available does not lie in the near future and fit with the project schedule. Therefore, we will hold meetings separately.

Cassie – thanks for the further info. The Project Team already have other anticipated meetings with the Town coming up, and so opportunities will be sought to further discuss SWM matters with the Town at those points. We will confirm.

Suzanne – thanks again for the availability, our schedules align best with September 9 in the AM and so I will shortly send out a meeting invite to TRCA staff.

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CONFIDENTIALITY WARNING This email is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: David Hiett
Sent: Wednesday, August 18, 2021 9:34 AM
To: Suzanne Bevan <Suzanne.Bevan@trca.ca>; Emma Benko <emma.benko@trca.ca>; Kristen Sullivan <kristen.sullivan@trca.ca>; Caroline Mugo <Caroline.Mugo@trca.ca>; Cassie Schembri <Cassie.Schembri@caledon.ca>; Margi Sheth <Margi.Sheth@caledon.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Saini, Sakshi <sakshi.saini@peelregion.ca>
Subject: B000738-Peel-Coleraine CP Grade Separation EA - SWM and Drainage Meeting

Good Morning All,

In support of the Coleraine Drive CP Grade Separation EA undertaken by the Region of Peel, we would like to organize a meeting with the TRCA and Town of Caledon to further discuss the Stormwater Management and Drainage strategy proposed as part of the improvements.

We have previously consulted with both the Town and TRCA, and this meeting will be to confirm understanding of the additional analysis conducted into Pond 9 / responses provided to the original SWM report, and seek agreement with the proposed strategy.

We are hoping to have this meeting in the week of September 6th or 13th, and so can we kindly request your availability? Please respond using table below

Also, for this email I have included all those that were involved in previous correspondence, and so please also confirm that those above should attend or if there should also be others.

Many thanks,

x ✓

	Mon Sept 6	Tues Sept 7	Wed Sept 8	Thurs Sept 9	Fri Sept 10
AM	N/A - Stat				
PM	N/A - Stat				
	Mon Sept 13	Tues Sept 14	Wed Sept 15	Thurs Sept 16	Fri Sept 17
AM					
PM					

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
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A-2

Appendix A-2-6: Transport Canada

Kate Barclay

From: Jessica Dorgo
Sent: January 26, 2017 9:17 AM
To: railsafety@tc.gc.ca
Cc: Jennifer Haslett
Subject: B738_Peel Coleraine Drive Grade Separation EA

Good morning,

The Region of Peel has initiated a Municipal Class Environmental Assessment (EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. A key map of the study area is provided below.

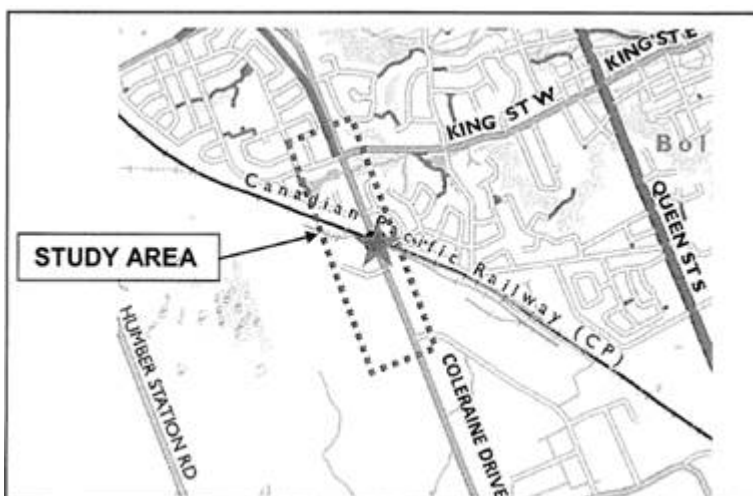
The Region of Peel Long Range Transportation Plan (LRTP) 2012 identifies significant growth expected in Peel Region over the next 20 years and the associated transportation issues including traffic congestion. The study will assess the impacts and benefits of proposed road improvements in this area on cultural, economic and natural environments as well as safety.

The study is being conducted in compliance with *Schedule C* of the *Municipal Class Environmental Assessment* process (October 2000, as amended in 2007, 2011 and 2015), which is approved under the *Ontario Environmental Assessment Act*.

The study will define the problem, identify alternative solutions, and determine a preferred solution. Consultation with the public and stakeholders is a key component of the study. Two (2) Public Information Centres will be held to review the study and obtain public input on issues related to alternative solutions and design, environmental impacts and mitigation measures.

We would like to request your assistance in identifying a project contact from Transport Canada who will be involved in this study. We currently have a representative from CP Rail included on our stakeholder list who will be included in study correspondence.

I can be contacted via email at Jessica.dorgo@cima.ca or by phone 289-2888-0287 ext. 6819.



Thank you,

Jessica Dorgo

EIT Transportation
Traffic Engineering, Transportation

CIMA+

Partners in Excellence

3027 Harvester Road, Suite 400
Burlington Ontario L7N 3G7
CANADA
Tel: 289-288-0287 ext. 6819 / Fax: 289-288-0285



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Kate Barclay

From: EnviroOnt <EnviroOnt@tc.gc.ca>
Sent: February 1, 2017 11:23 AM
To: Jessica Dorgo
Cc: Jennifer Haslett
Subject: NEATS 43891: Class EA - Coleraine Drive Grade Separation, Region of Peel ONT

Follow Up Flag: Follow up
Flag Status: Flagged

Greetings Jessica,

Thank you for your correspondence. Please remove RailSafety@tc.gc.ca from your mailing list and replace it with EnviroOnt@tc.gc.ca for all Municipal Class Environmental Assessment notifications.

In regards to your project: pursuant to the *Railway Safety Act*, the Ontario Region Rail Safety branch should only receive notification for this project through a Notice of Railway Works submission. To ensure your project will meet safety and operational requirements, please refer to the *Railway Safety Act* and supporting regulations for more detail.

If you have additional questions for Transport Canada at this time, please do not hesitate to submit them here and they will be directed to the appropriate group for review.

Thank you,

Environmental Assessment Program | Programme d'évaluation environnementale
Transport Canada, Ontario Region | Transports Canada, Région de l'Ontario
4900 Yonge St., Toronto, ON M2N 6A5 | 4900, rue Yonge, Toronto, ON, M2N 6A5
Email | Courriel: EnviroOnt@tc.gc.ca
Facsimile | télécopieur: (416) 952-0514
Government of Canada | Gouvernement du Canada

From: Jessica Dorgo [<mailto:Jessica.Dorgo@cima.ca>]
Sent: Thursday, January 26, 2017 9:17 AM
To: RailSafety <RailSafety@tc.gc.ca>
Cc: Jennifer Haslett <Jennifer.Haslett@cima.ca>
Subject: B738_Peel Coleraine Drive Grade Separation EA

Good morning,

The Region of Peel has initiated a Municipal Class Environmental Assessment (EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. A key map of the study area is provided below.

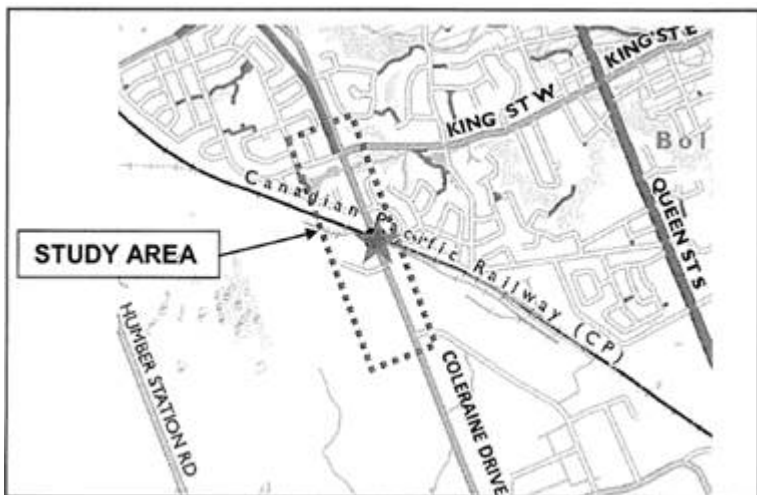
The Region of Peel Long Range Transportation Plan (LRTP) 2012 identifies significant growth expected in Peel Region over the next 20 years and the associated transportation issues including traffic congestion. The study will assess the impacts and benefits of proposed road improvements in this area on cultural, economic and natural environments as well as safety.

The study is being conducted in compliance with *Schedule C* of the *Municipal Class Environmental Assessment* process (October 2000, as amended in 2007, 2011 and 2015), which is approved under the *Ontario Environmental Assessment Act*.

The study will define the problem, identify alternative solutions, and determine a preferred solution. Consultation with the public and stakeholders is a key component of the study. Two (2) Public Information Centres will be held to review the study and obtain public input on issues related to alternative solutions and design, environmental impacts and mitigation measures.

We would like to request your assistance in identifying a project contact from Transport Canada who will be involved in this study. We currently have a representative from CP Rail included on our stakeholder list who will be included in study correspondence.

I can be contacted via email at Jessica.dorgo@cima.ca or by phone 289-2888-0287 ext. 6819.



Thank you,

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation

CIMA+
Partners in Excellence

3027 Harvester Road, Suite 400
Burlington Ontario L7N 3G7
CANADA
Tel: 289-288-0287 ext. 6819 / Fax: 289-288-0285



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Kate Barclay

From: Dela Cruz, Gino <gino.delacruz@peelregion.ca>
Sent: March 28, 2017 11:51 AM
To: Jessica Dorgo
Subject: Fwd: NEATS 43891: Class EA - Coleraine Drive Grade Separation, Region of Peel ONT
Attachments: NEATS 43891 Class EA - Coleraine Drive Grade Separation, Region of Peel ONT.pdf; ATT00001.htm

Follow Up Flag: Follow up
Flag Status: Flagged

FYI and response

Sent from my iPhone

Begin forwarded message:

From: "EnviroOnt" <EnviroOnt@tc.gc.ca>
To: "Dela Cruz, Gino" <gino.delacruz@peelregion.ca>, "stephen.keen@cima.ca" <stephen.keen@cima.ca>
Subject: **NEATS 43891: Class EA - Coleraine Drive Grade Separation, Region of Peel ONT**

Greetings,

Thank you for your correspondence.

Please note Transport Canada **does not** require receipt of all individual or Class EA related notifications. We are requesting project proponents to self-assess if their project:

1. Will interact with a federal property and/or waterway by reviewing the Directory of Federal Real Property, available at www.tbs-sct.gc.ca/dfpr-rbif/; **and**
2. Will require approval and/or authorization under any Acts administered by Transport Canada* available at <http://www.tc.gc.ca/eng/acts-regulations/menu.htm>.

Projects that will occur on federal property prior to exercising a power, performing a function or duty in relation to that project, will be subject to a determination of the likelihood of significant adverse environmental effects, per Section 67 of the *Canadian Environmental Assessment Act, 2012*.

If the aforementioned does not apply, the Environmental Assessment program should not be included in any further correspondence and will not receive a response. If there is a role under the program, correspondence should be forwarded *electronically* to: EnviroOnt@tc.gc.ca with a **brief description of Transport Canada's expected role**.

*Below is a summary of the most common Acts that have applied to projects in an Environmental Assessment context:

- **Navigation Protection Act (NPA)** – the Act applies primarily to works constructed or placed in, on, over, under, through, or across scheduled navigable waters set out under the Act. The Navigation Protection Program administers the NPA through the review and authorization of works affecting scheduled navigable waters. Information about the Program, NPA and approval

process is available at: <http://www.tc.gc.ca/eng/programs-621.html>. Enquiries can be directed to NPPONT-PPNONT@tc.gc.ca or by calling (519) 383-1863.

- **Railway Safety Act (RSA)** – the Act provides the regulatory framework for railway safety, security, and some of the environmental impacts of railway operations in Canada. The Rail Safety Program develops and enforces regulations, rules, standards and procedures governing safe railway operations. Additional information about the Program is available at: <https://www.tc.gc.ca/eng/railsafety/menu.htm>. Enquiries can be directed to RailSafety@tc.gc.ca or by calling (613) 998-2985.
- **Transportation of Dangerous Goods Act (TDGA)** – the transportation of dangerous goods by air, marine, rail and road is regulated under the TDGA. Transport Canada, based on risks, develops safety standards and regulations, provides oversight and gives expert advice on dangerous goods to promote public safety. Additional information about the transportation of dangerous goods is available at: <https://www.tc.gc.ca/eng/tdg/safety-menu.htm>. Enquiries can be directed to TDG-TMDOntario@tc.gc.ca or by calling (416) 973-1868.
- **Aeronautics Act** – Transport Canada has sole jurisdiction over aeronautics, which includes aerodromes and all related buildings or services used for aviation purposes. Aviation safety in Canada is regulated under this Act and the Canadian Aviation Regulations (CARs). Elevated Structures, such as wind turbines and communication towers, would be examples of projects that must be assessed for lighting and marking requirements in accordance with the CARs. Transport Canada also has an interest in projects that have the potential to cause interference between wildlife and aviation activities. One example would be waste facilities, which may attract birds into commercial and recreational flight paths. The *Land Use In The Vicinity of Aerodromes* publication recommends guidelines for and uses in the vicinity of aerodromes, available at: <https://www.tc.gc.ca/eng/civilaviation/publications/tp1247-menu-1418.htm>. Enquires can be directed to CASO-SACO@tc.gc.ca or by calling 1 (800) 305-2059 / (416) 952-0230.

Please advise if additional information is needed.

Thank you,

Environmental Assessment Program, Ontario Region

Transport Canada / Government of Canada / 4900 Yonge St., Toronto, ON M2N 6A5

EnviroOnt@tc.gc.ca / Facsimile : (416) 952-0514 / TTY: 1-888-675-6863

Programme d'évaluation environnementale, Région de l'Ontario

Transports Canada / Gouvernement du Canada / 4900, rue Yonge, Toronto, ON, M2N 6A5

EnviroOnt@tc.gc.ca / télécopieur: (416) 952-0514

March 21, 2017

Environmental Coordinator
Transport Canada
4900 Yonge Street, 4th Floor
North York, ON M2N 6A5

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

Public consultation is an integral component of the Municipal Class EA process and opportunities to comment are welcomed throughout the entire study. Consultation activities will be announced at key milestones in the project.

You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

A-2

Appendix A-2-7: Fisheries and Oceans Canada

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:36 AM
To: dan.thompson@dfo-mpo.gc.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Fisheries and Oceans Canada Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Mr. Dan Thompson
Team Leader-Triage and Planning
Fisheries and Oceans Canada
867 Lakeshore Road, P.O. Box 5050

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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A-2

Appendix A-2-8: Technical Agencies Committee

**CLASS ENVIRONMENTAL ASSESSMENT STUDY FOR
COLERAINE DRIVE GRADE SEPERATION (SOUTH OF OLD ELLWOOD DRIVE),
TOWN OF CALEDON
16-4315 / B000738**

Technical Agencies Committee Meeting No. 1

AGENDA

**August 28, 2017 at 1:00pm
10 Peel Centre Drive, Suite B, 1st Floor
Peel Conference Centre**

- 1. WELCOME (ALL)**
- 2. NOTICE OF STUDY COMMENCEMENT COMMENTS (CIMA+)**
- 3. STAKEHOLDER MEETINGS (CIMA+ AND REGION OF PEEL)**
- 4. UPDATE ON BACKGROUND STUDIES (CIMA+)**
- 5. UPDATED FUNCTIONAL DESIGN OPTIONS (CIMA+)**
- 6. PUBLIC INFORMATION CENTRE NO. 1 MATERIALS (CIMA+)**
- 7. NEXT STEPS (ALL)**

TECHNICAL AGENCIES COMMITTEE MEETING NO.1

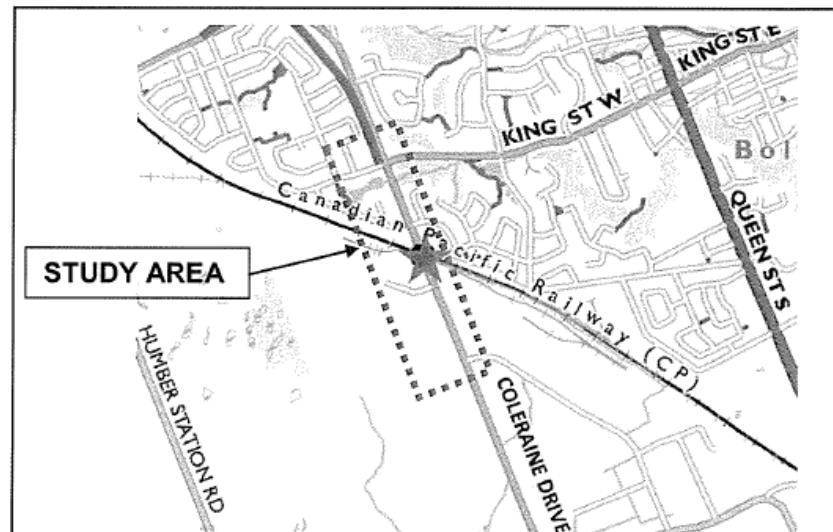
**Municipal Class Environmental Assessment
Coleraine Drive south of Old Ellwood Drive
Town of Caledon**



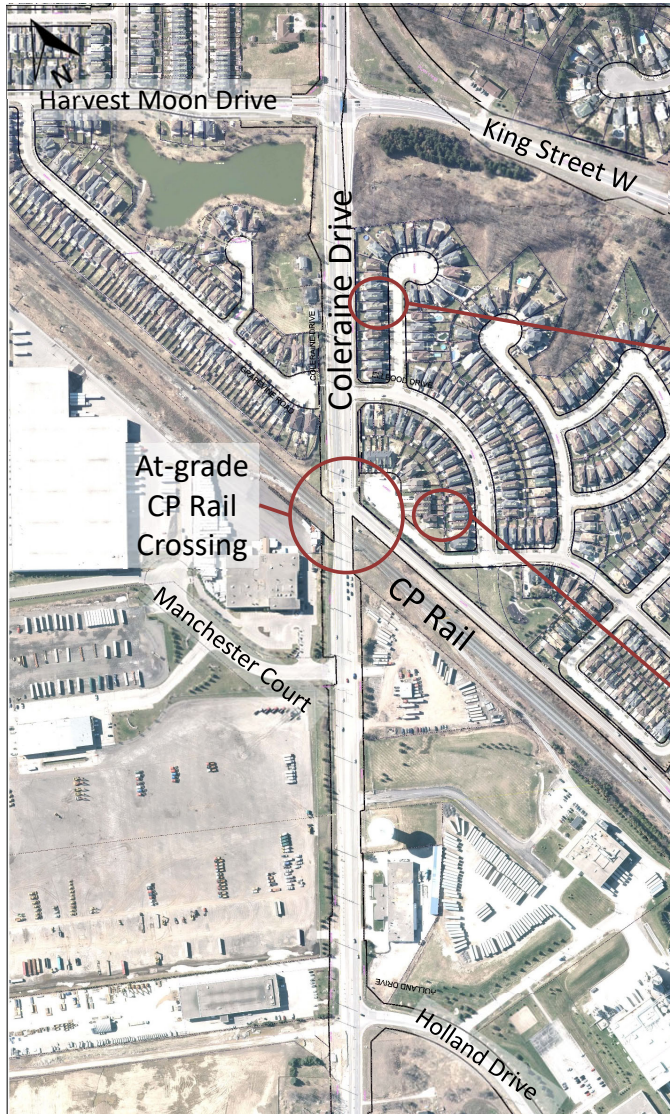
August 28, 2017

Agenda

1. Welcome and Introductions
2. Notice of Study Commencement Comments
3. Stakeholder Meetings
4. Update on Background Studies
5. Updated Functional Design Options
6. Public Information Centre No.1 Materials
7. Next Steps



Notice of Study Commencement Comments



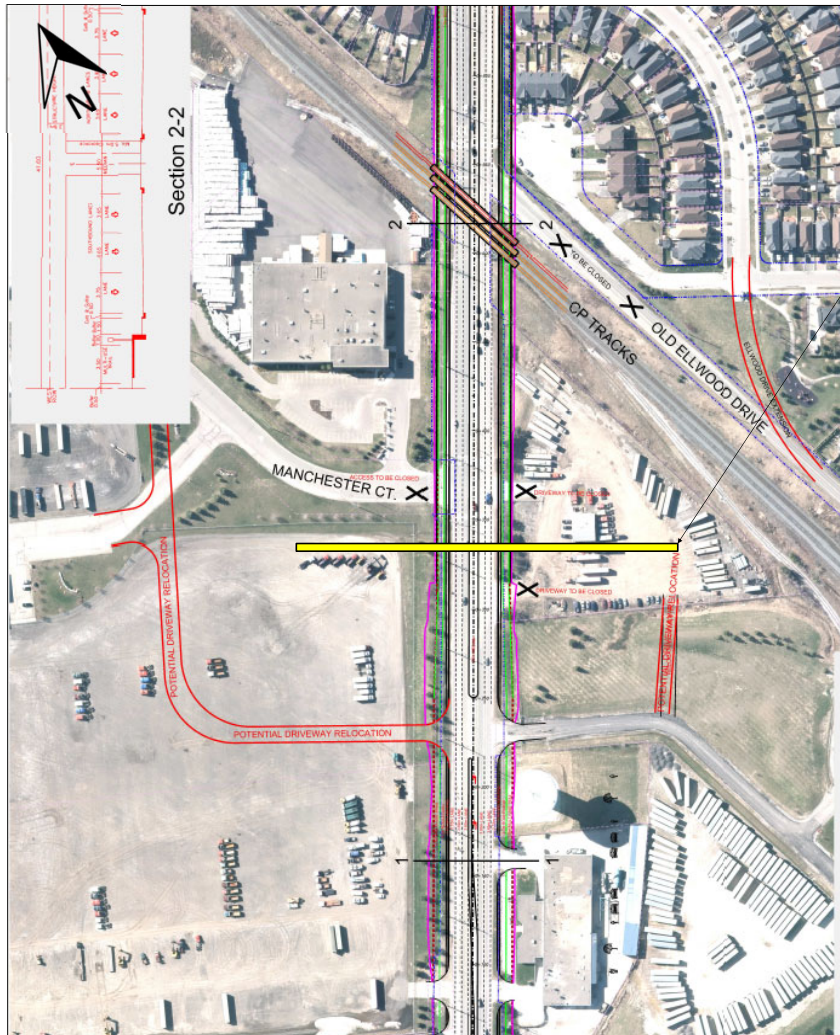
Property	Comments	Response
Property Owner 18 Natureway Court	<ul style="list-style-type: none"> Concerns regarding the previous widening project and the impacts to the berm and noise wall at the back of the property which abuts Coleraine Drive. Invited the project team to the property to discuss concerns. 	<ul style="list-style-type: none"> The study is in the initial phases and there will be opportunities to provide input and comments as we progress with more details of the study. The project team will schedule a meeting when a preliminary draft design is developed to review with the property owner.
Property Owner 12 Aida Court	<ul style="list-style-type: none"> Currently across from the train which is already a nuisance. When the homes were built there was never a proper berm built as it is merely a slight hill. The noise level from the train already exceeds the allowable decibel level. We have called to complain in the past. Would like more information regarding the study. Will you be considering a bridge overpass for cars, re-routing the train, etc.? 	<ul style="list-style-type: none"> Before the selection of the preferred solution for the grade separation, a Noise Impact Analysis will be conducted to evaluate the anticipated change in traffic sound levels. The results will be compared to applicable MTO/MOEC guidelines. Where applicable, mitigation measures will be recommended such as noise barriers. At this stage in the study, the main consideration is separating the car traffic from trains with a bridge overpass or underpass for vehicles. Both options and potentially others, will be considered as the study moves forward along with associated impacts and mitigation. Contact has been included on the study mailing list.

STAKEHOLDER MEETINGS



Stakeholder	Comments	Next Steps
Property Owner 13576 and 13584 Coleraine Drive	<ul style="list-style-type: none"> Development application submitted for the Jack Kenny Court proposed residential development. Potential access closure for the two houses from Coleraine Drive is a concern. Interest in obtaining additional lands from Region of Peel if surplus to requirements. 	<ul style="list-style-type: none"> Review to determine if the two accesses from Coleraine Drive can be maintained. Region of Peel reviewing if the traffic easement identified in the development application can be relocated. Follow-up meeting will be scheduled in Fall/Winter 2017 to review preliminary designs and provide further clarification on development impacts.
Ritchie Bros. Auctioneers 3 Manchester Court	<ul style="list-style-type: none"> The business would not be able to continue at the site were it to be severed as shown in the Feasibility Study where the Manchester Court intersection with Coleraine Drive is relocated south through the property. Two alternative mitigation options were suggested: <ul style="list-style-type: none"> For the road over rail option, extend Manchester Court under Coleraine to the east side of the road; and develop a new road parallel to Coleraine at the back of the Ritchie and Canadian Tire properties to connect with Healey Drive. 	<ul style="list-style-type: none"> The project team will look into these two options discussed as well as another option to keep a realigned Manchester Court as close to its current alignment as possible. Follow-up meeting will be scheduled in Fall/Winter 2017 to review preliminary designs and provide further clarification on development impacts.
MARS Canada 37 Holland Drive	<ul style="list-style-type: none"> MARS Canada has future developments including a parking lot expansion planned for the undeveloped property adjacent to Coleraine Drive. MARS access from Coleraine Drive is for employees only. In the future, this access may become a right-in right-out only. 	<ul style="list-style-type: none"> The project team will revisit the driveway relocation for the corner property to the north of MARS which is constrained once the grade separation is implemented. Follow-up meeting will be scheduled in Fall/Winter 2017 to review preliminary designs and provide further clarification on development impacts.

MANCHESTER COURT REALIGNMENT



The location shown in yellow is the northern most limit* for the realignment of Manchester Court associated with a grade separation of the CP Rail line.

*Subject to refinement during this study.

UPDATE ON BACKGROUND STUDIES

Background Study	Status
Natural Environment Assessment	Existing conditions review and field investigations completed
Drainage and Stormwater Management	Existing conditions review completed
Stage 1 Archaeological Assessment	Draft report under review
Cultural and Built Heritage Assessment	Draft report under review
Hydrogeological Investigation, Geotechnical Investigation and Pavement Design	Awaiting draft report
Noise Impact	Not initiated
Air Quality Assessment	Awaiting draft report
Contaminated Soils Assessment	Draft report under review
Culvert Inspections	Complete
Meander Belt and Fluvial Geomorphology Analysis	Draft report expected late September

A summary of existing conditions is provided in the PIC No.1 materials to follow.

UPDATED FUNCTIONAL DESIGN OPTIONS

ROLL PLAN

PUBLIC INFORMATION CENTRE NO.1

**Municipal Class Environmental Assessment
Coleraine Drive south of Old Ellwood Drive
Town of Caledon**



September 26, 2017
Bolton Community Centre

PURPOSE OF PIC NO.1

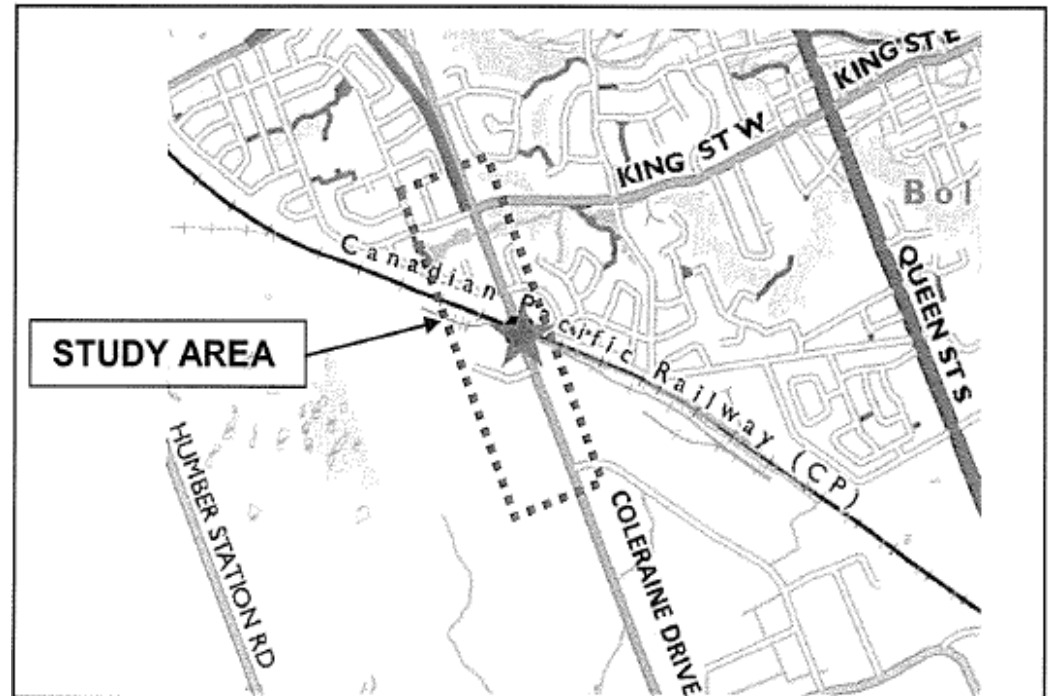
- You are invited to review the project information on display:
 - Background to the study
 - Municipal Class Environmental Assessment Process
 - Existing conditions of the study area
 - Traffic study
 - Alternative planning solutions and preliminary preferred solution
- Ask questions and discuss areas of interest with the study team
- Fill out and submit a comment sheet by DATE

Your feedback is important and will be considered and incorporated in the preferred alternative selection process

STUDY AREA

Peel Region is carrying out a Municipal Class Environmental Assessment (MCEA) Study to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon.

The study area extends approximate 150 metres north of Harvest Moon Drive in the north to Holland Drive in the south.



REGION OF PEEL CONTEXT

This Environmental Assessment supports the Region's 2015-2035 Strategic Plan and Council Priorities.

The Region of Peel Long Range Transportation Plan (LRTP) (2012)

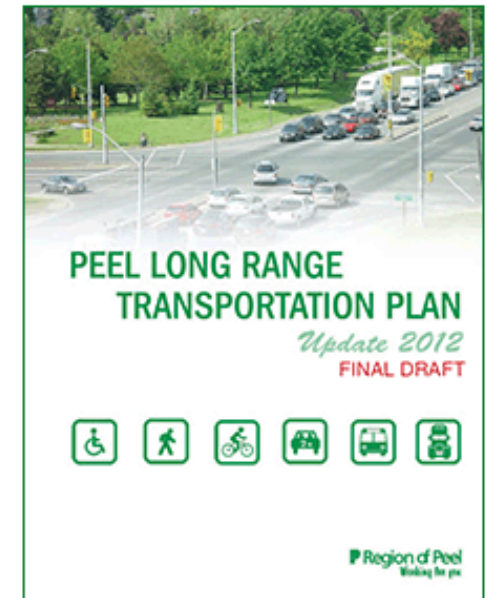
- The Region of Peel LRTP identifies significant growth expected in Peel Region over the next 20 years and the associated transportation issues including traffic congestion.

The Goods Movement Task Force Strategic Plan (2012-2016)

- Developed to identify Strategic Actions to improve Goods Movement in Peel
- Identifies Coleraine Drive as a critical crossing that should be prioritized for a grade separation.

The Bolton Commuter Rail Service Feasibility Study (2010)

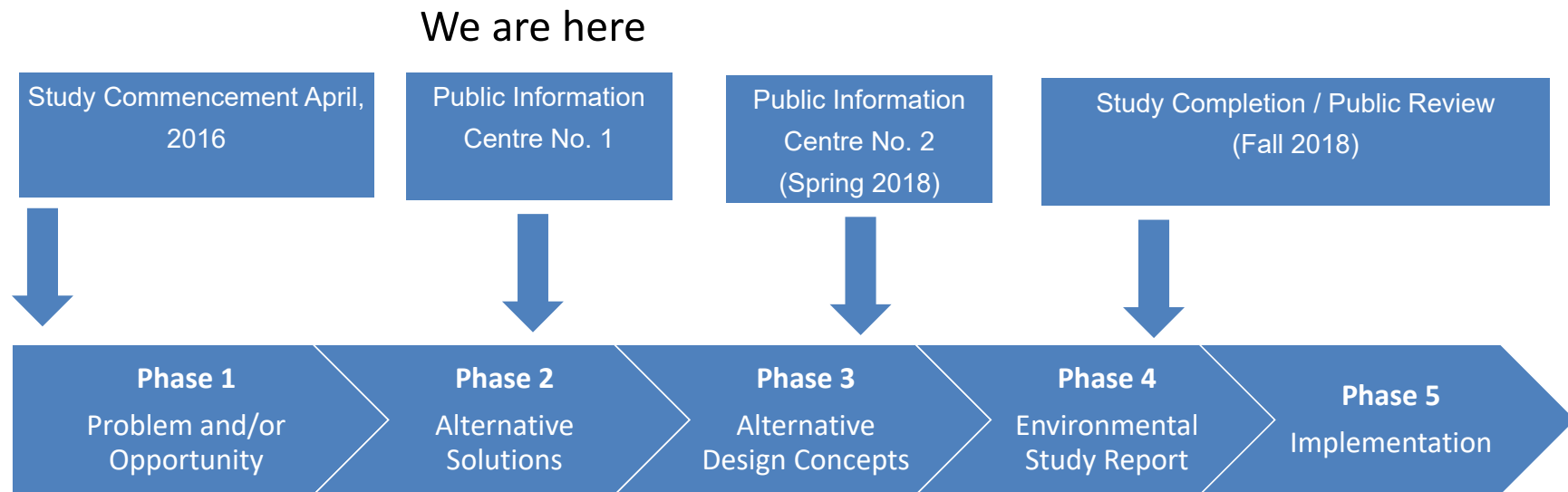
- Examined the feasibility of improving service on the Bolton rail line including a new Bolton GO Station near King Street and Humber Station Road in Caledon.



STUDY PROCESS

The Municipal Class EA is a planning and design process approved by the Ministry of Environment and Climate Change to meet the requirements of the Ontario *Environmental Assessment Act*.

This Study follows the Class EA process for **Schedule 'C'** projects and will complete Phases 1 to 4 as outlined below:



TRANSPORTATION NEEDS

Train Operations:

- Coleraine Drive at-grade rail crossing serves 18 freight trains per day
- Based on industry guidance established by Transport Canada, the need for grade separation is justified by the Rail/Road Crossing Exposure Index (EI) which is the product of daily bi-directional road traffic and train volumes.
- Typically, grade separation is considered where the EI value results 200,000 or higher.
- For Coleraine Drive, the current (2015) EI is 234,684 which exceeds the threshold of 200,000.

Bi-directional Daily Crossings	2015	2021	2031
Vehicular (A)	13,038	15,848	20,530
Train (B)	18	52*	63*
Exposure Index (AxB)	234,684	824,096	1,293,390

- **Based on the results of the EI, a grade separation is already justified in 2015 and the EI will increase significantly in the future.**

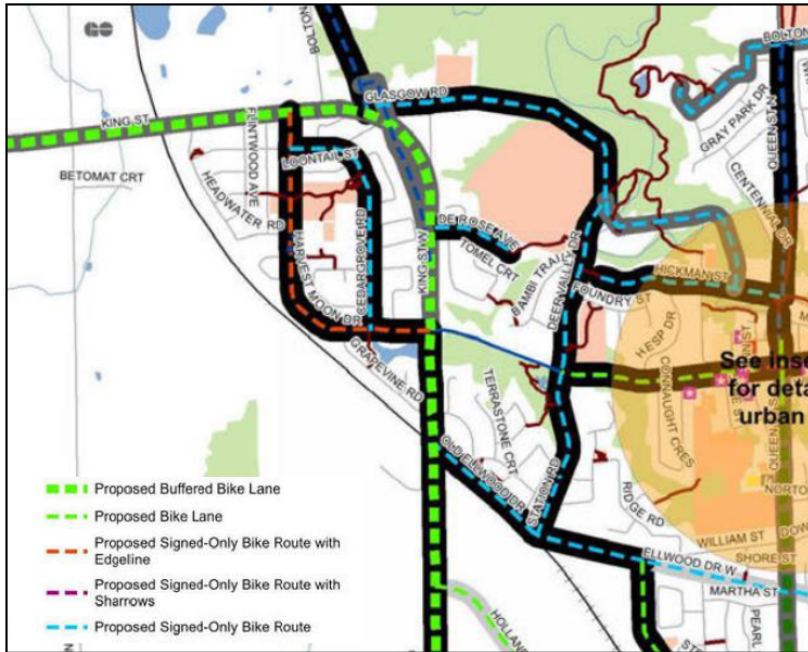
***Future train volumes are taken from the Bolton Commuter Rail Feasibility Study.**

FUTURE TRAFFIC CONDITIONS

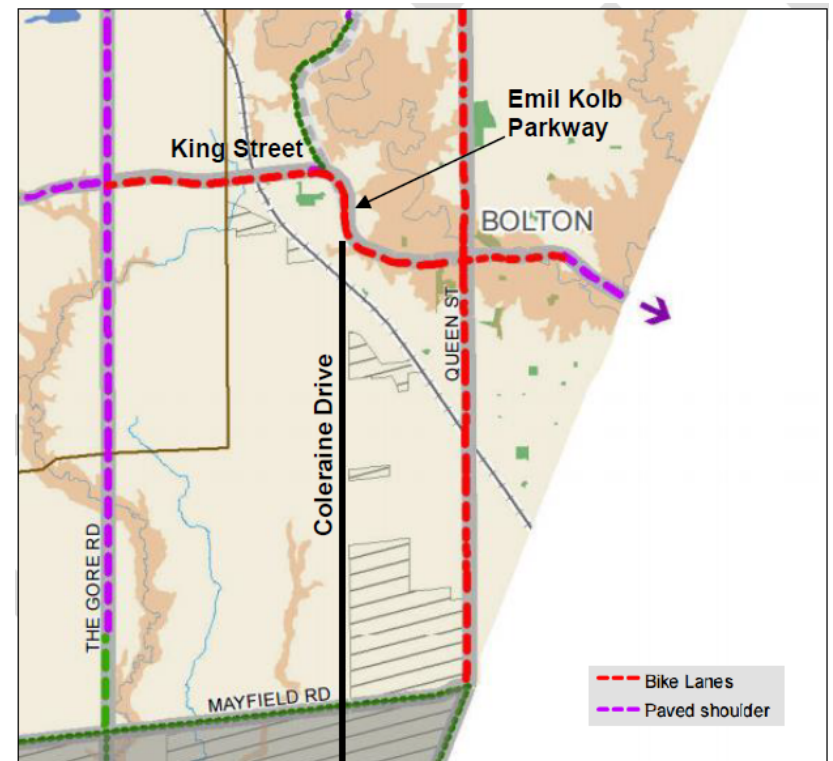
- With an increase in motor vehicle and train traffic projected for 2041, queues generated at the at-grade crossing are expected to extend beyond the intersection of Coleraine Drive & King Street West/Harvest Moon Drive.
- This issue is expected to be more intense as the frequency of trains during peak hours increases, particularly due to the planned extension of GO Train service to a new Bolton station.
- Expected benefits of providing grade separation on Coleraine Drive include:
 - Elimination of queues at the railway crossing;
 - Reduced exposure to collisions with trains;
 - Improved truck network and, consequently, increased reliability for the goods movement corridor (fewer delays and reduced exposure to collisions); and
 - The associated reconstruction of Coleraine Drive provides an opportunity to provide bicycle facilities and to upgrade pedestrian facilities to meet current accessibility standards.

Road Section	Direction	2017 Volumes (vph)		2041 Volumes (vph)	
		AM ¹¹	PM	AM ¹²	PM ¹³
Coleraine Drive south of King Street W	NB	249	1,070	349	1,500
	SB	933	336	1,824	657
King Street W between Coleraine Drive and Station Road	EB	294	433	486	716
	WB	422	338	982	787

ACTIVE TRANSPORTATION



Left: Bolton Transportation Master Plan identifies a proposed buffered bicycle lane along King Street and Coleraine Drive



Right: The proposed long-term Regional cycling network indicates that bicycle lanes are planned along King Street. This includes Emil Kolb Parkway, which is a direct continuation of Coleraine Drive north of King Street West.

EXISTING CONDITIONS PLAN

ROLL PLAN

ENVIRONMENTAL INVENTORIES

The following environmental inventories are being completed as part of this study:

- Natural Environment Assessment
- Drainage and Stormwater Management
- Stage 1 Archaeological Assessment
- Cultural and Built Heritage Assessment
- Hydrogeological Investigation
- Geotechnical Investigation and Pavement Design
- Noise Impact
- Air Quality Assessment
- Contaminated Soils Assessment
- Culvert Inspections
- Meander Belt and Fluvial Geomorphology Analysis

NATURAL ENVIRONMENT

- The project is located within the Humber River watershed.
- A small tributary of the Humber River originates at a stormwater pond on the west side of Coleraine Drive, north of the rail line, and flows under the road towards the east.
- TRCA identifies the valley along the watercourse as part of the Natural Heritage System, comprised of meadow and forest.
- CIMA field investigations did not find any rare species of flora or fauna; most of the species encountered were urban tolerant, and/or non-native species.
- Fish habitat is limited to the tributary east of Coleraine Drive, as the outlet of the stormwater pond is a barrier to fish.
- There are no natural features of provincial significance in the study area.
- There are no Core Areas of the Region of Peel Greenlands System in the study area, but the valley of the watercourse is considered an Environmental Policy Area in the Town of Caledon Official Plan.



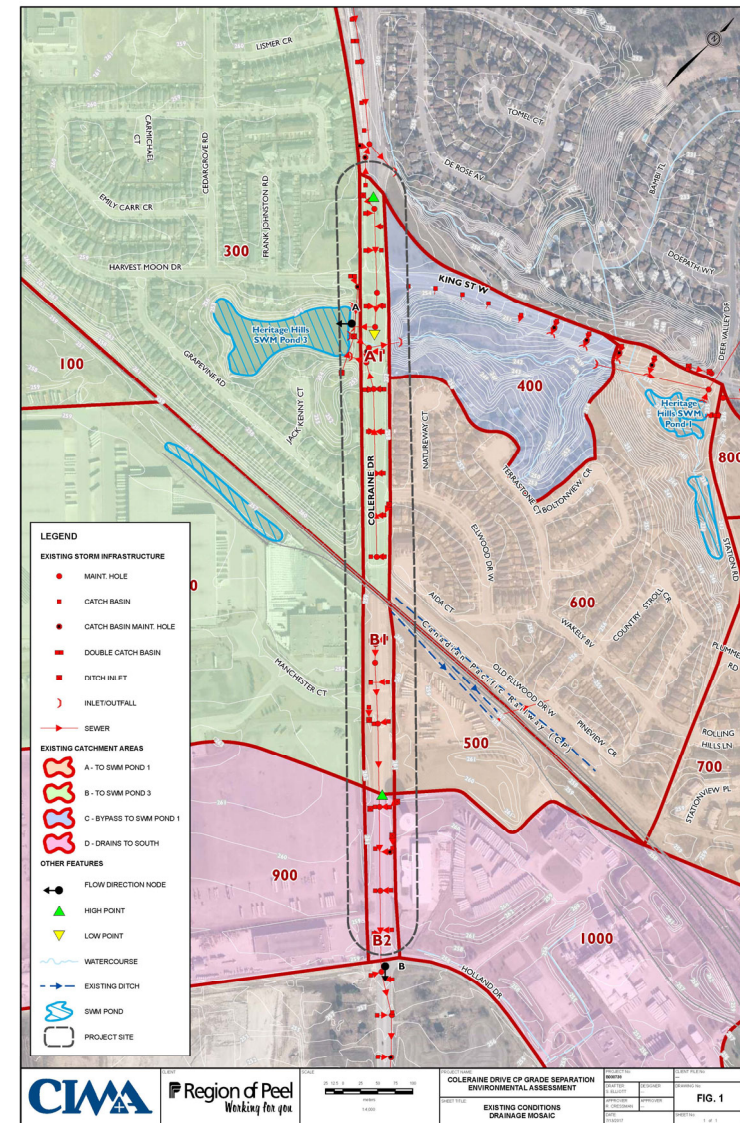
STORMWATER MANAGMENT

Existing Drainage

- Stormwater drainage from Coleraine Drive drains to the Humber River watershed.
- The soil within the study area consist primarily of clay loam.
- This type of soil is considered to have a moderate to poor drainage capability, meaning very little rainfall in the open areas will soak into the ground, leaving most of the water as runoff.

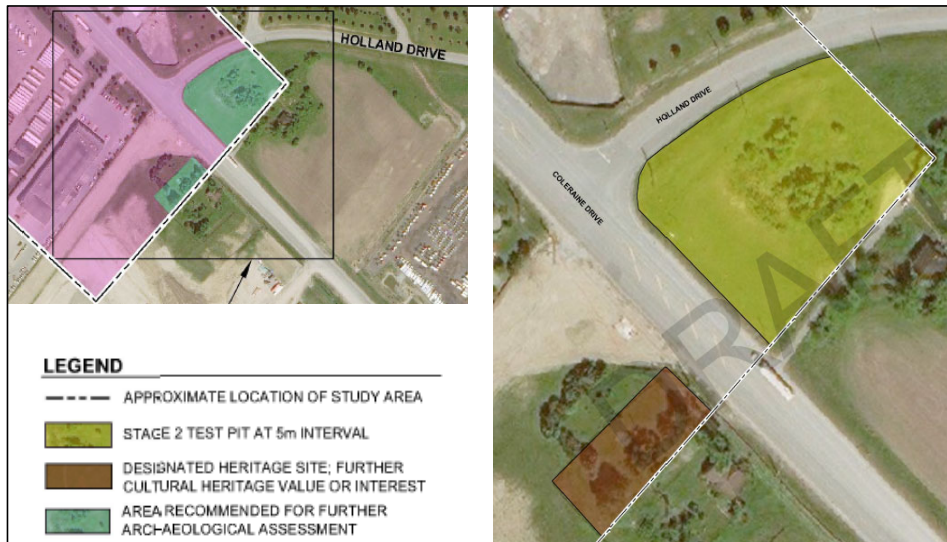
Existing Stormwater Infrastructure

- Coleraine Drive is an urban cross-section with curb and gutter, catch basins, and storm sewers which discharge into the Heritage Hills SWM Pond to the north and into roadside ditches to the south.
- Adjacent to the roadway there are two existing storm water management ponds constructed as part of the Heritage Hills Residential development that incorporate drainage from Coleraine Drive.



STAGE 1 ARCHAEOLOGICAL ASSESSMENT

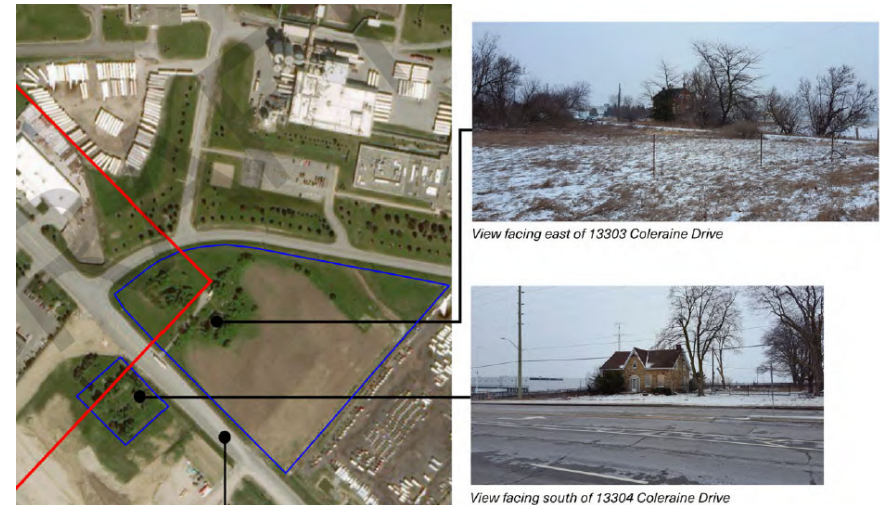
A Stage 1 Archaeological Assessment was completed for the study corridor, some locations are recommended for a Stage 2 Assessment.



BUILT HERITAGE

The Cultural Heritage Assessment determined that the following are present in the study area south of the anticipated limits of construction:

- One protected heritage property
 - Shore-Wakely House, 13304 Coleraine Drive
- One property of cultural heritage value or interest
 - 13303 Coleraine Drive



PROBLEM AND OPPORTUNITY STATEMENT

It is a Council priority to improve the efficiency in which goods move within and through Peel in order to manage congestion and enhance Peel's economic competitiveness. The Goods Movements Strategic Plan (2012-2016) was developed and identified Coleraine Drive as a critical crossing that should be prioritized for grade separation.

Based on industry guidance established by Transport Canada, the need for grade separation is justified by the Rail/Road Crossing Exposure Index (EI). Typically, grade separation is considered where the EI is 200,000 or higher. The EI in 2015 on Coleraine Drive exceeds the 200,000 threshold and the EI is projected to increase significantly in the future. Therefore, a grade separation is warranted.

A grade separation will allow for improved traffic operations, safety and visibility at the CP Rail crossing.

ALTERNATIVE PLANNING SOLUTIONS

Three alternative planning solutions were developed to address the problem statement:

1. Do Nothing

2. Change Grade of Rail

- 2A) Raise rail over road, or
- 2B) Depress rail under road

3. Change Grade of Road

- 3A) Raise road over rail, or
- 3B) Depress road under rail

(2A) Grade Separation: Raise Rail-over-Road Example



Langstaff Road, Vaughan ON



Parliament Street, Toronto



Woodbridge Avenue, Vaughan ON



Regional Road 25, Milton ON

(2B) Grade Separation: Depress Rail-under-Road Example



Camp Road, Australia



A1/N1 Dublin to Belfast motorway



A1/N1 Dublin to Belfast motorway

(3A) Grade Separation: Raise Road-over-Rail Example



Norwood Junction, Croydon, United Kingdom



Highway 50, Caledon ON

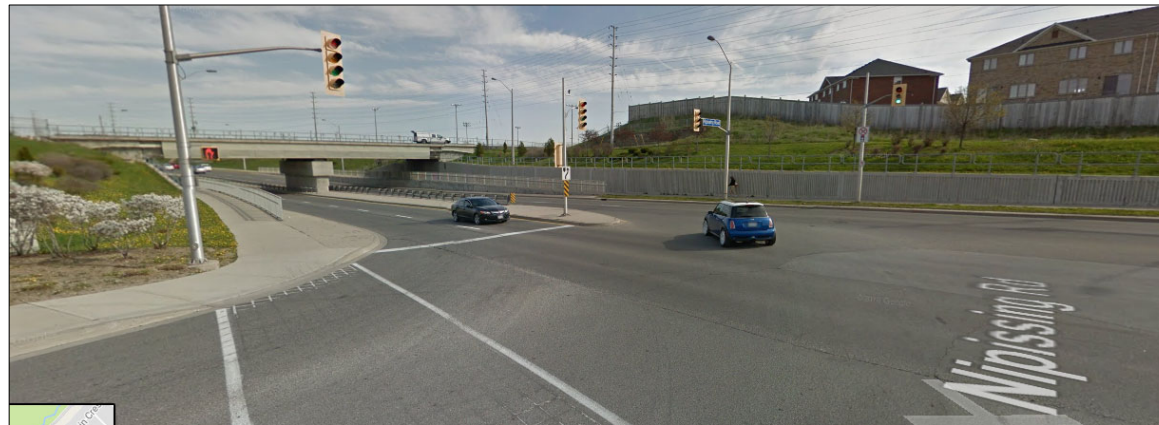
(3B) Grade Separation: Depress Road-under-Rail Example



Guelph Line, Burlington ON



Gardiner Expressway, Toronto ON



Thompson Road, Milton ON

Analysis of Alternative Planning Solutions

Criteria	Do Nothing	Change Grade of Rail	Change Grade of Road
Transportation	<ul style="list-style-type: none"> No changes to existing transportation system Does not accommodate future transportation needs 	<ul style="list-style-type: none"> Accommodates future transportation needs 	<ul style="list-style-type: none"> Accommodates future transportation needs
Socio-Economic Environment	<ul style="list-style-type: none"> No impact to adjacent properties 	<ul style="list-style-type: none"> Long distance required for grade change (1% max grade) Significant impact to properties adjacent to rail Temporary rail diversion needed 	<ul style="list-style-type: none"> Moderate distance required for grade change (6% max grade) Moderate impact to properties adjacent to Coleraine Drive
Natural Environment	<ul style="list-style-type: none"> No impact to natural environment 	<ul style="list-style-type: none"> Greater impact to meadow lands adjacent to rail Some impact to natural heritage feature 	<ul style="list-style-type: none"> Less impact to meadow lands adjacent to rail line No impact to natural heritage feature
Active Transportation	<ul style="list-style-type: none"> No opportunities for pedestrian or cycling facilities 	<ul style="list-style-type: none"> Opportunity for the provision of pedestrian or cycling facilities on Coleraine Drive 	<ul style="list-style-type: none"> Opportunity for the provision of pedestrian or cycling facilities on Coleraine Drive

PRELIMINARY PREFERRED SOLUTION

The preliminary preferred solution is to change the grade of the road on Coleraine Drive which will address the problem statement developed for the Coleraine Drive study area and is in support of the Region's Long Range Transportation Plan and Goods Movement Strategic Plan endorsed by Region of Peel Council.

This preliminary recommended solution will consider the following:

- Change grade of the road
 - Raise road-under-rail grade separation; OR
 - Depress road-over-rail grade separation
- Provision of pedestrian and bicycle facilities along Coleraine Drive

NEXT STEPS

- Review and confirm preferred planning solutions and assessment in light of comments received from the public and other stakeholders
- Develop alternative design concepts
- Complete detailed impact analysis
- Public Information Center No.2
- Prepare and File Environmental Study Report

Please complete a comment sheet and submit all comments by **October 17, 2017:**

Gino Dela Cruz, P. Eng.

Project Manager, Infrastructure
Programming & Studies
Region of Peel
10 Peel Centre Drive, Suite B, 4th Floor
Brampton, On L6T 4B9
905-791-7800 ext.7805
gino.delacruz@peelregion.ca

Stephen Keen, P. Eng.

Consultant Project Manager
CIMA Canada Inc.
3027 Harvester Road
Burlington ON L7N 3G7
289-288-0287
Stephen.keen@cima.ca

MINUTES OF MEETING

CLIENT	:	Peel Region
PROJECT	:	Class Environmental Assessment Study for Coleraine Drive Grade Separation (South Of Old Ellwood Drive), Town Of Caledon
MEETING	:	Technical Agencies Committee Meeting No. 1
DATE OF MEETING	:	August 28 th , 2017 at 1:00 PM
LOCATION	:	Peel Region, 10 Peel Centre Drive, Suite B, 1 st Floor Peel Conference Centre
ATTENDEES	:	Gino Dela Cruz (Peel), Lori-Ann Thomsen (Peel – Real Estate), Thomas Lee (Peel – Real Estate), Sanya Khan (Peel – Public Health), Seema Ansari (Peel), Mark Masley (Peel), Angela Stockman (Peel), Arthur Lo (Peel) Sharon Lingertat (Toronto and Region Conservation Authority) Stephen Keen, Jessica Dorgo (CIMA+)
C.C. TO	:	People invited and attendees

Note: If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.

DISCUSSION TOPICS**ACTION BY**

1 WELCOME

- Peel Region provided an overview of the study and led roundtable introductions.
- An agenda and copy of the meeting presentation was distributed to attendees prior to the meeting.

2 NOTICE OF STUDY COMMENCEMENT COMMENTS

- Two public comments were received in response to the Notice of Study Commencement. Both comments expressed concern for noise levels from the train. The project team has responded to these comments indicating that a Noise Assessment will be completed as part of this study. Both property owners are included on the study mailing list and will be kept informed as the study progresses.

3 STAKEHOLDER MEETINGS

- Individual meetings with three stakeholders were held to discuss potential impacts of the proposed grade separation on development plans. The stakeholders include:
 - Property owner of 13576 and 13584 Coleraine Drive
 - Ritchie Bros. Auctioneers at 3 Manchester Court
 - MARS Canada at 37 Holland Drive
- Follow-up meetings with each of these stakeholders will be held in fall/winter 2017 to review preliminary designs and review potential development impacts.
- The realignment of Manchester Court will be reviewed during the development of alternative design concepts in order to minimize impact to adjacent properties.

Peel Region/CIMA

CIMA

4 UPDATED FUNCTIONAL DESIGN OPTIONS

- The updated functional designs have been reduced to 4-lanes from 6-lanes and include a multi-use trail on both sides of the road.
- TRCA noted that if required, a culvert replacement could have impacts on the adjacent natural areas. The hydraulics study will determine if a culvert replacement is required. Where possible, the design alternatives can be modified to reduce impacts to the adjacent natural area.
- Peel Region noted that the Bolton Transportation Master Plan identifies a proposed buffered bicycle lane along Coleraine Drive north of King Street.

DISCUSSION TOPICS**ACTION BY**

- North of the study area, there is a multi-use trail (MUT) present on the east side of Coleraine Drive. A MUT on the east-side should be maintained for connectivity. The MUT proposed on the west side will be reviewed during the development of alternative design concepts.
- Connectivity to existing trails north of the study limits should also be considered.
- Peel Region to review the active transportation requirements for the study area to provide input for the development of the alternative design concepts.

Peel Region

5 PUBLIC INFORMATION CENTRE NO. 1 MATERIALS

- CIMA presented the PIC No. 1 materials and requested comments from attendees prior to the PIC.
- TRCA noted that there are no significant natural features in the study area with the exception of the SWM pond. It was also noted that the Sustainable Neighbourhood Group is currently conducting a study east of the study area, west of Station Road near the watercourse. The study is not adjacent to Coleraine Drive and is understood to be stormwater management related.
- Upon development of the alternative design concepts, any impact to natural features in the study area should be noted in the Environmental Study Report.

6 NEXT STEPS

- All comments on the updated functional design options and PIC materials should be provided within two weeks.
- PIC No. 1 is scheduled for September 26th, 2017 at the Albion Bolton Community Centre.

ALL

Tips for Effective Meetings!

- ✓ Set a meeting goal
- ✓ Send agenda ahead
- ✓ Invite right people
- ✓ Be flexible
- ✓ Respect each other
- ✓ Acknowledge each other
- ✓ Stick to agenda
- ✓ Stay on track
- ✓ Store other ideas for later
- ✓ Use time effectively
- ✓ Be punctual
- ✓ Consider a walking meeting

**COLERAINE DR. GRADE SEPARATION EA -
Meeting #2 with Technical Advisory Group (TAG #2)**

Date May 04th, 2021

Location Virtual Meeting via Microsoft Teams

Start Time 10.00 a.m.

End Time 12.00 p.m.

Agenda Items

Item No.	Description
1	Welcome / Introductions
2	Study Update
3	Public Information Centre No. 2 Materials
4	Evaluation of Alternative Design Concepts
5	Technical Reports
6	Q&A Session
7	Next Steps

TECHNICAL ADVISORY GROUP MEETING NO.2

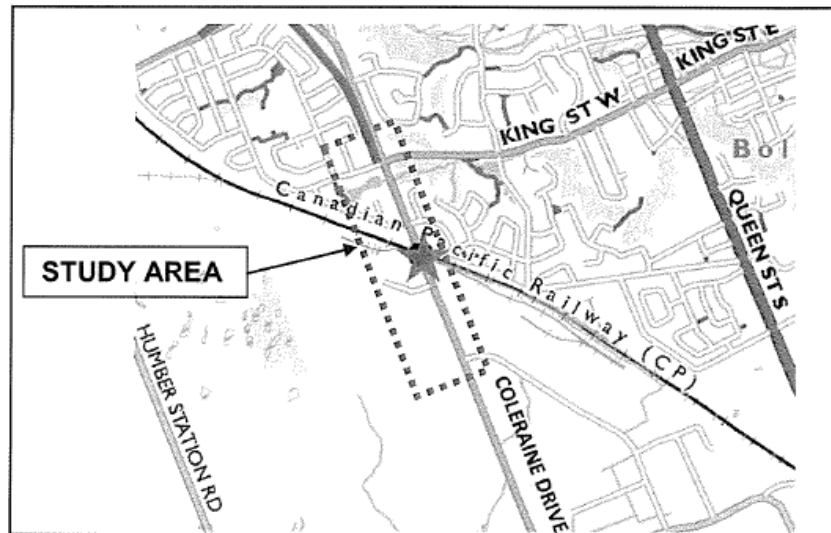
**Municipal Class Environmental Assessment
Coleraine Drive south of Old Ellwood Drive
Town of Caledon**



May 04, 2021

AGENDA

1. Technical Reports
2. Project Update Since Last Meeting
3. Alternative Design Concepts
4. Evaluation and Public Information Centre No.2
5. Project Schedule
6. Next Steps



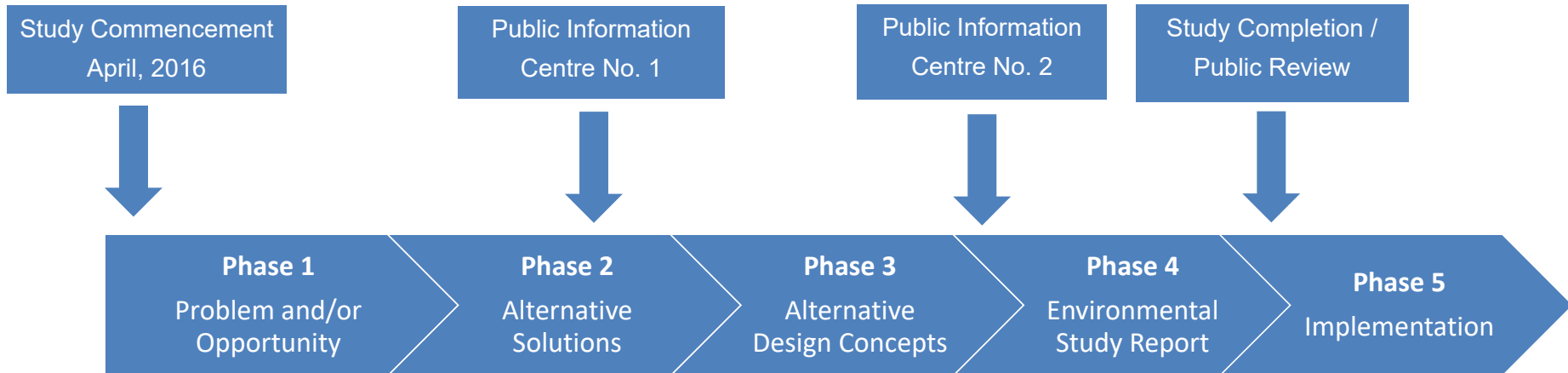
1. TECHNICAL REPORTS

Report	Status
Natural Environment Assessment	Final
Hydraulics and Stormwater Management	Draft – rec'd comments from TRCA
Fluvial Geomorphological Assessment	Final
Stage 1 Archaeological Assessment	Final
Cultural/Built Heritage Assessment	Final
Noise Impact Assessment	Draft – with Region for review
Geotechnical Investigation/Pavement Design	Final
Hydrogeological Assessment	Draft – being finalised
Phase 1 ESA	Draft – being finalised
Air Quality Assessment	Final
Culvert Inspections	Final
Transportation/Traffic Assessment	Final
Tree Inventory	Final
Structural Report	Final
Vibration Report	Draft – with Region for review
Shadow Analysis	Final

2. PROJECT UPDATE

- Held PIC #1 Oct 08, 2019 – Grade separation identified as preferred solution
- Completed technical investigations and assessments
- Developed and evaluated the alternative design concepts
- Preparation for PIC #2

We are here



3. ALTERNATIVE DESIGN CONCEPTS

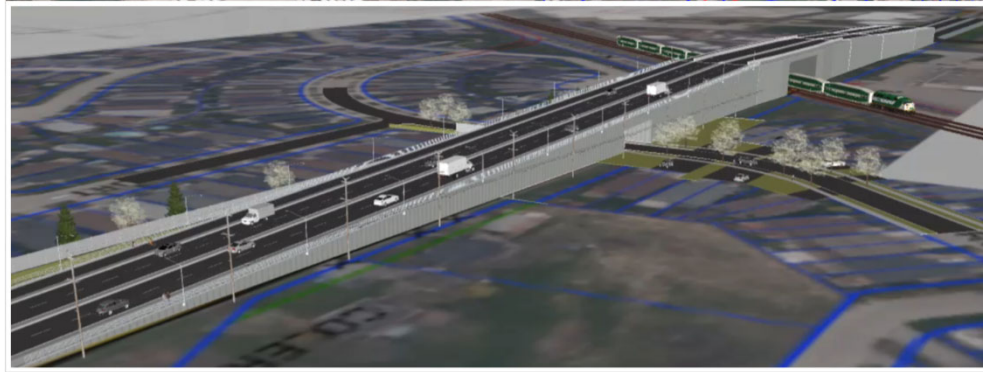
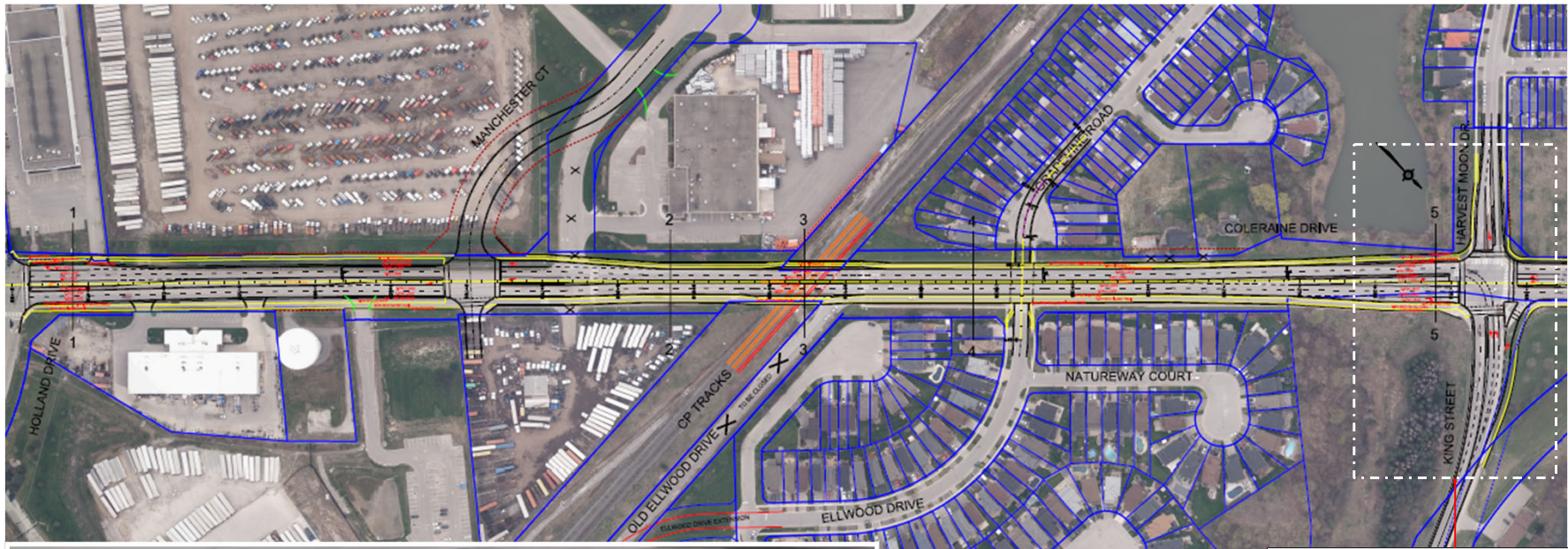
Two alternative design concepts were developed:

1. Road under Rail



3. ALTERNATIVE DESIGN CONCEPTS

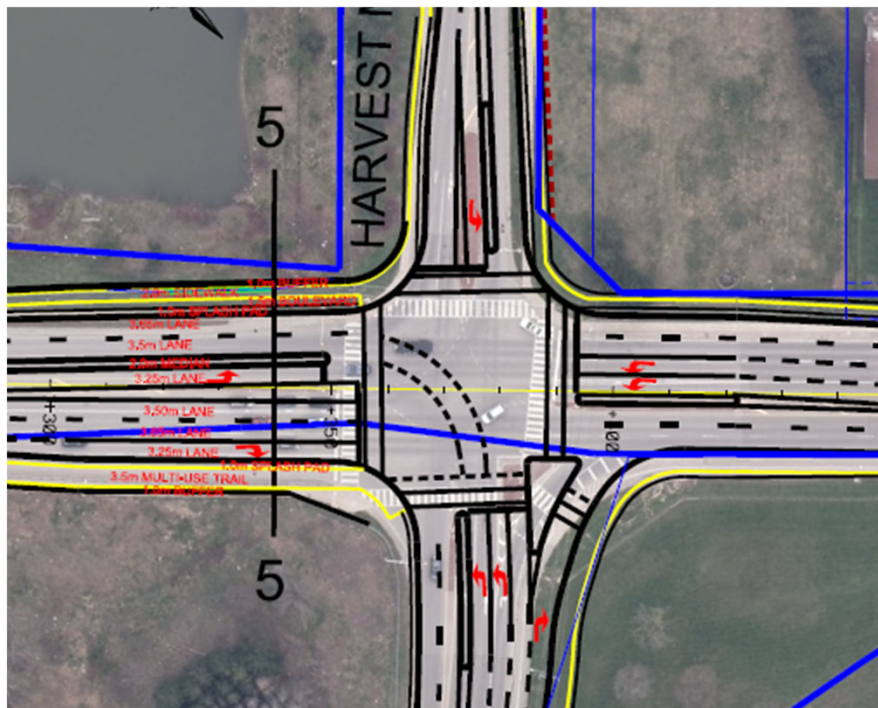
2. Road over Rail



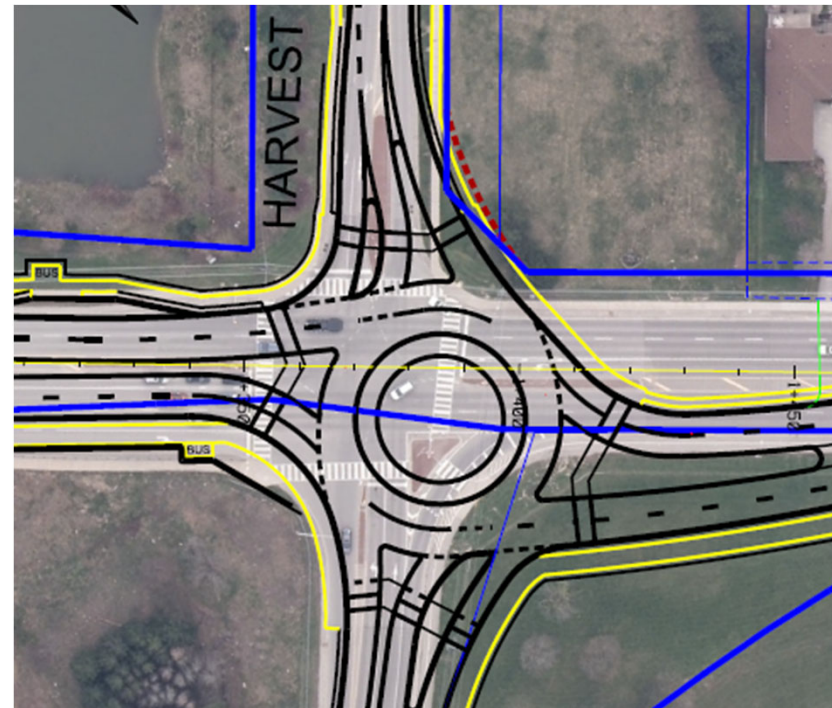
Intersection improvements considered separately

3. ALTERNATIVE DESIGN CONCEPTS FOR HARVEST MOON DR. / KING ST. & COLERAINE DR.

Two alternatives were considered for the intersection.

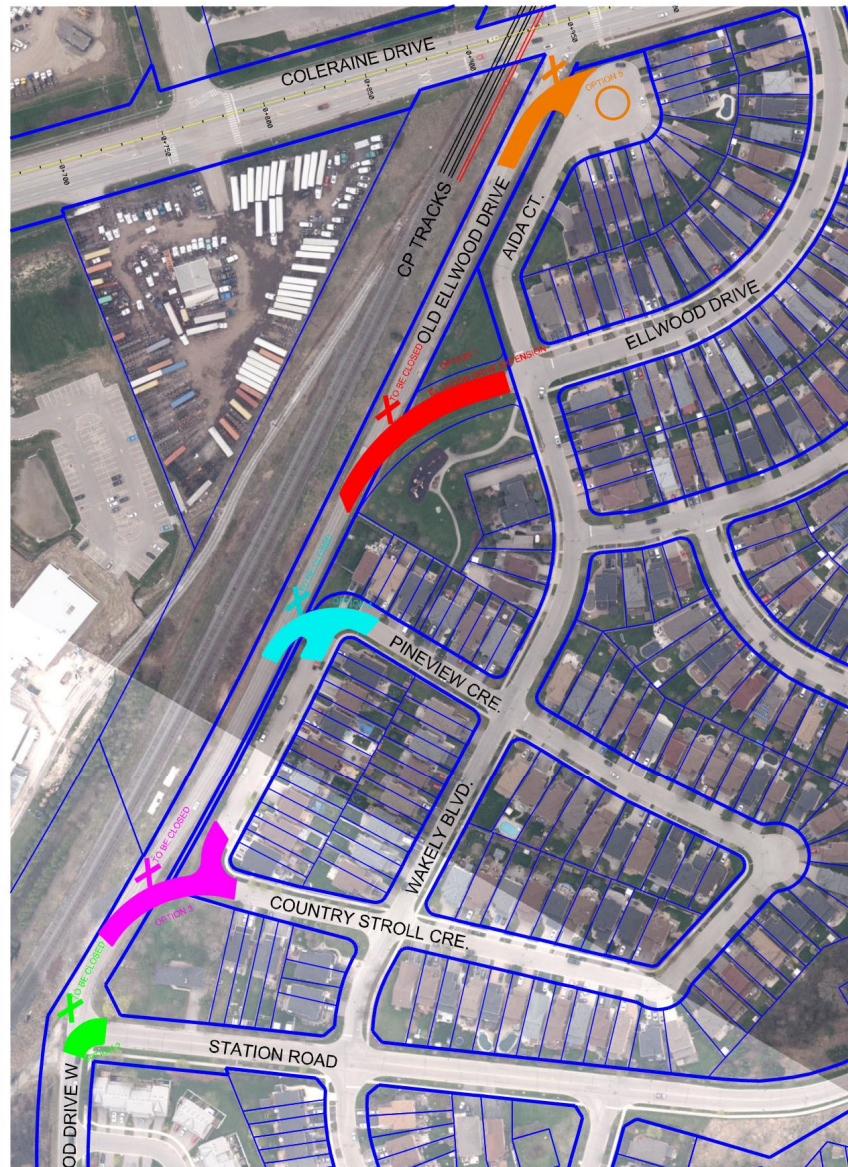


SIGNALIZED INTERSECTION IMPROVEMENTS



ROUNDBOUT

3. ALTERNATIVE DESIGN CONCEPTS



Closure of Ellwood Drive

Option 1: Connect Old Ellwood Drive to Ellwood Drive

Option 2: Connect to Station Road

Option 3: Connect to Stroll Crescent

Option 4: Connect to Pineview Crescent

Option 5: Connect to Aida Court Roundabout

4. EVALUATION AND PIC #2

- REFER TO PIC #2 BOARDS

5. SCHEDULE / 6. NEXT STEPS

- Hold Public Information Center No.2 June/July 2021
- Finalise outstanding technical reports
- Finalise preliminary design
- Complete draft Environmental Study Report (ESR)
- ESR and Notice of Study Completion End of August 2021

Kate Barclay

From: David Hiett
Sent: May 10, 2021 1:28 PM
To: Mahmood, Tareq; Rook, Sally; Nieuwenhuysen, Bob; Van Boxmeer, Kyle; Ansari, Seema; Hamdani, Hashim; Caughey, Rebecca; Saiyed, Sabbir; Lee, Arthur; Mannie, Sharon; Grzesiak, Ryan; Nevland, Erik; Fernandes, Trina; Smith, Neal; Hasselbacher, John; Gallagher, Joe; Saini, Sakshi; Eberhardt, Daniel; Koethe, Wayne; Thomsen, Jeanne; Bennington, Michael; Lotecki, Nancy; Woodtke, Trevor; Detaramani, Tina; ZZG-Peel Health & Built Environment; Peter Brocks; Stephen Keen; Suzanne Bevan; Emma Benko; Kristen Sullivan; Kant.chawla; Margi Sheth; Arash Olia; Ryan Grodecki; Tom_Twigge@cpr.ca; jennifer_benedict@cpr.ca; Andrew Pearce; Saddi, Asha; Cassie Schembri; Thomsen, Lori-Ann
Cc: Crawford, Heather; Lavhey, James; Kovach, Steven; Toy, William; Aymar, Matthew; Ursitti, Franca; Gardiner, Len; Michele Olliffe; Elysia Friedl
Subject: RE: Region of Peel - Coleraine Drive Grade Separation Class EA - Alternative Design (preliminary) Review Meeting (TAG-2)
Attachments: (B738) Coleraine Rd Grade Separation EA - TAG Meeting #2 Minutes_e02.pdf

Hi All,

Please see attached the minutes of last week's TAG #2 Meeting for the Coleraine Drive Grade Separation Class EA

As also noted in the minutes: If there are any errors or omissions, please contact myself and please provide any comments or concerns **no later than May 14, 2021**

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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-----Original Appointment-----

From: Mahmood, Tareq <tareq.mahmood@peelregion.ca>

Sent: Wednesday, April 7, 2021 11:46 AM

To: Mahmood, Tareq; Rook, Sally; Nieuwenhuysen, Bob; Van Boxmeer, Kyle; Ansari, Seema; Hamdani, Hashim; Caughey, Rebecca; Saiyed, Sabbir; Lee, Arthur; Mannie, Sharon; Grzesiak, Ryan; Nevland, Erik; Fernandes, Trina; Smith, Neal;

Hasselbacher, John; Gallagher, Joe; Saini, Sakshi; Eberhardt, Daniel; Koethe, Wayne; Thomsen, Jeanne; Bennington, Michael; Lotecki, Nancy; Woodtke, Trevor; Detaramani, Tina; ZZG-Peel Health & Built Environment; Peter Brocks; Stephen Keen; David Hiatt; Suzanne Bevan; Emma Benko; Kristen Sullivan; Kant.chawla; Margi Sheth; Arash Olia; Ryan Grodecki; Tom_Twigge@cpr.ca; jennifer_benedict@cpr.ca; Andrew Pearce; Saddi, Asha; Cassie Schembri; Thomsen, Lori-Ann

Cc: Crawford, Heather; Lavhey, James; Kovach, Steven; Toy, William; Aymar, Matthew; Ursitti, Franca; Gardiner, Len; Michele Olliffe; Elysia Friedl

Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Alternative Design (preliminary) Review Meeting (TAG-2)

When: Tuesday, May 4, 2021 10:00 AM-12:00 PM (UTC-05:00) Eastern Time (US & Canada).

Where: Microsoft Teams Meeting

EXTERNAL EMAIL

Hello

Please see attached the meeting agenda and PIC boards. It is proposed to have one main overview presentation that will be narrated (titled MAIN Overview Presentation).

Two additional slide decks are also be available for additional information, see the link below to access/download the slide decks.

The memo includes the evaluation tables and drawings of the improvements assessed at, see the link below to access/download the document.

- ❖ the Coleraine Drive / King Street and Harvest Moon Drive intersection,
- ❖ closure/connection concept design for Old Ellwood Drive, and
- ❖ grade separation concept design for the rail tracks

<https://www.dropbox.com/sh/ytxkt0mvowvm3a2/AAB6UAzgXBE8Jfq3dhDgeolFa?dl=0>

Please contact me if you require further information. I look forward to seeing you at TAG-2 meeting.

Thanks,

Tareq

Hi everyone,

As you may be aware, the Region of Peel is undertaking a Municipal Class Environmental Assessment Study for the grade separation of Coleraine Drive in the Town of Caledon. The study is considering options for the grade separation of Coleraine Drive and CP Rail. The project team would like to invite you to attend a virtual TAG-2 meeting to review the alternative preliminary design, the analysis and evaluation of alternative design and receive your comments and feedback on the preliminary preferred design for the Coleraine Drive Grade Separation Class EA.

Two options are being considered for the grade separation:

- Road-over-rail
- Road-under-rail

Additionally, two options for the intersection of King Street/Harvest Moon Drive and Coleraine Drive are being considered:

- Signalized intersection

- Roundabout

The meeting agenda, evaluation criteria, and preliminary design will be provided closer to the meeting. Please feel free to extend this invitation to the appropriate staff or any other staff that may be interested or impacted by this project.

Best regards,

Tareq Mahmood

Project Manager, Infrastructure Programming and Studies
Transportation, Public Works, Region of Peel
Cell:905-872-6475
Email: tareq.mahmood@peelregion.ca



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[\(844\) 589-6880,,435975801#](tel:(844)5896880435975801) Canada (Toll-free)

Phone Conference ID: 435 975 801#

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Meeting Minutes

Meeting: Technical Advisory Group (TAG) Meeting #2

Project: MCEA for the Coleraine Drive Grade Separation, Town of Caledon

Date and Time: May 4th, 2021 at 10:00am

Location: Microsoft Teams

Attendees: Tareq Mahmood, Region of Peel
Sally Rook, Region of Peel
Bob Nieuwenhuysen, Region of Peel
Sharon Mannie, Region of Peel
Rebecca Caughey, Region of Peel
John Hasselbacher, Region of Peel
Arthur Lee, Region of Peel
Sakshi Saini, Region of Peel
Ryan Grzesiak, Region of Peel
Heather Crawford, Region of Peel
Hashim Hamdani, Region of Peel
Seema Ansari, Region of Peel
Neil Smith, Region of Peel
Trevor Woodtke, Region of Peel
Joe Gallagher, Region of Peel
Sabbir Saiyed, Region of Peel
Kyle Van Boxmeer, Region of Peel
Jeanne Thomsen, Region of Peel
Kant Chawla, Region of Peel
Margi Sheth, Town of Caledon
Cassie Schembri, Town of Caledon
Ryan Crodecki, Town of Caledon
Olia Arash, Town of Caledon
Jennifer Benedict, CP Rail
Emma Benko, TRCA
Suzanne Bevan, TRCA
Steve Keen, CIMA+
David Hiett, CIMA+
Peter Brocks, CIMA+
Elysia Friedl, CIMA+

Regrets: Michele Olliffe, Town of Caledon
Kristen Sullivan, TRCA
Andrew Pearce, Town of Caledon
Daniel Eberhardt, Region of Peel
Dave Richa, Region of Peel

(B738) Coleraine Rd Grade Separation EA - TAG Meeting #2 Minutes_e02.docx

400-3027 Harvester Road, Burlington ON L7N 3G7 CANADA T 289-288-0287 F 289-288-0285

cima.ca

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CANADA 2019

Note: If there are any errors or omissions to these minutes, please contact David Hiatt. Please let us know if you have any comments or concerns **no later than May 14, 2021** as the focus of the Coleraine Grade separation EA study is to follow up on the action items as stated in these meeting minutes. Teams meeting chat transcript and sign-in sheet have also been attached at the end of these meeting minutes.

Discussion Topics		Action By
1. Welcome		
1.1	The Region welcomed everyone to the meeting and provided a brief overview / summary of the project.	Info
1.2	Everyone on the call introduced themselves and their roles.	Info
2. Project Update		
2.1	CIMA+ provided an overview of the studies that have been undertaken to date and the progress of the EA.	Info
2.2	CIMA+ went through the draft slides for PIC#2.	Info
3. Alternative Design Concepts – Old Ellwood Drive Options		
3.1	It will be important to have more of a consensus of the Old Ellwood Drive closure before approaching the public. CIMA to organize a meeting with the Town to further discuss.	CIMA+
3.2	In order to narrow down the options for Old Ellwood Drive, the Town would like to see some additional analysis from CIMA+/the Region in terms of traffic impacts, changes to local traffic patterns, etc. as part of the further discussions.	Info
3.3	The group discussed the possibility of Old Ellwood Drive being a good active transportation option.	Info
3.4	Old Ellwood Drive may be used by CP Rail for operational purposes. CIMA+ to request information from CP. <i>Post Meeting Note: CIMA+ sent an email to CP Rail.</i>	CIMA+
3.5	Station Road has a higher volume of traffic than the other options so this could be the preferred connection.	Info



Discussion Topics		Action By
4. Evaluation of the Grade Separation		
4.1	The Region and CIMA+ will need to consider the cost of the W/WW infrastructure – protection (vibration) & moving. CIMA+ to set up a meeting with the Region’s W/WW group to discuss the costs to ensure they are appropriately captured in the cost.	CIMA+
4.2	The Region’s Active Transportation guidelines provide a range for multi-use path widths, and it was noted that the multi-use path (4.25 m) and sidewalk (2.5 m) could be balanced to ~ 3 m width each side, with 2.4 m being an absolute minimum. Discussions revolved around AT configuration, including consideration of the barriers also required on structures, and further discussion is required with the Region. CIMA to draft an alternate cross-section for discussion purposes.	CIMA+
4.3	The team should consider adding a staged ramp(s) for a pedestrian / cycling connection between Coleraine drive and the streets below (Ellwood and Grapevine).	Info
4.4	The team discussed the Shadow analysis and how it should be presented to the public. Team believes it would be best to have time stamped images in a PDF so the public can review, instead of the video.	Info
5. Schedule / Next Steps		
5.1	The Project Team will be setting up meetings / further consult with the Town, the Region’s W/WW Group and the Region’s AT group to help finalize the closure of Ellwood Drive, costs and proposed cross-section.	Region / CIMA+
5.2	The Project Team will be setting up a meeting with the TRCA to discuss the SWM comments and potential solutions.	Region / CIMA+
5.3	Meetings will be held with the local Councillor and Cycling Committees before going to the public with PIC #2.	Region

End of Minutes



MCEA for the Coleraine Drive Grade Separation, Town of Caledon Technical Advisory Group (TAG) Meeting #2 – May 04, 2021

Meeting Minutes Attachments

Teams Meeting Chat Transcript:

Thomsen, Jeanne:

1. we have watermain along that entire section of Ellwood.
2. Significant feeder mains for the elevated water tower exist and these must be protected, also significant wastewater within the roadway exist.
3. Road under rail would be very challenging to rerouting.
4. I'm not seeing any sensitivity to W/WW costs of impacts here thus far.
5. Vibration is not a great this for our pipes either, I really need plan and profile possibilities.
6. what is maximum depth including road base?
7. we will need access and an easement in priority as well.
8. what are the development/redevelopment plans for the area to the south in the ICI area?
9. determine the # of properties affected and for how long - this way you know how many individual property concerns will come up and how they each could be dealt with - likely money will be involved (requests for tax deferral etc...)

Rook, Sally:

1. I can imagine traffic patterns and AADT will not be a factor between closing at Station Rd vs others between there and Coleraine but if the Station Rd option provides AT/greenspace/streetscape benefit, that should be noted and if TAC members support that option then show the public a graphic of what that space could be converted to.
2. This Construction slide should be updated to capture Jeanne's comments - does the \$20M in cost differential include the additional work described for Peel's existing W/WW infrastructure?
3. We will also need to get a lot more certainty about how to handle the closing of Old Ellwood before we take anything to the Public. We need to narrow down the options through further discussion with W/WW, CN & Town.
4. South of here is Amazon and Canadian Tire Distribution and other industrial type properties and zoning - not sure if anyone that works there can afford to live in this neighborhood but it is documented that workers in those types of places are heavy AT/Transit users

Lee, Arthur:

1. In regards to Old Ellwood alternatives, we could also consider a combination of alternatives such as having a short access from Aida Court for potential CP access/maintenance and a separate connection from Station court. This way, the section in between could be used for green space.

Sign-in Sheet:

Full Name	Email
Mahmood, Tareq	Tareq.Mahmood@peelregion.ca
Nieuwenhuysen, Bob	bob.nieuwenhuysen@peelregion.ca
Caughey, Rebecca	Rebecca.Caughey@peelregion.ca
Peter Brocks	peter.brocks@cima.ca
Jennifer Benedict	ben0301@cpr.ca
David Hiett	david.hiett@cima.ca
Crawford, Heather	heather.crawford@peelregion.ca
Elysia Friedl	elysia.friedl@cima.ca
Woodtke, Trevor	trevor.woodtke@peelregion.ca
Hasselbacher, John	John.Hasselbacher@peelregion.ca
Mannie, Sharon	sharon.mannie@peelregion.ca
Saini, Sakshi	sakshi.saini_peelregion.ca
Smith, Neal	neal.smith@peelregion.ca
Emma Benko	emma.benko@trca.ca
Lee, Arthur	arthur.lee@peelregion.ca
Grzesiak, Ryan	ryan.grzesiak@peelregion.ca
Hamdani, Hashim	HashimAli.Hamdani@peelregion.ca
Ansari, Seema	seema.ansari@peelregion.ca
Margi Sheth	Margi.Sheth@caledon.ca
Ryan Grodecki	Ryan.Grodecki@caledon.ca
Stephen Keen	stephen.keen@cima.ca
Cassie Schembri	Cassie.Schembri@caledon.ca
Gallagher, Joe	joe.gallagher@peelregion.ca
Suzanne Bevan	Suzanne.Bevan@trca.ca
Van Boxmeer, Kyle	Kyle.VanBoxmeer@peelregion.ca
Rook, Sally	sally.rook@peelregion.ca
Thomsen, Jeanne	jeanne.thomsen@peelregion.ca
Kant Chawla	Kant.Chawla@caledon.ca
Dave, Richa	richa.dave@peelregion.ca
Arash Olia	Arash.Olia@caledon.ca
Eberhardt, Daniel	daniel.eberhardt@peelregion.ca
Saiyed, Sabbir	sabbir.saiyed@peelregion.ca

A-3

Appendix A-3: Utility Consultation

A-3-1: Canadian Pacific Railway (CP Rail)

A-3-2: Enbridge

A-3-3: Hydro One

A-3-4: Rogers

A-3-5: Bell

A-3-6: TransCanada Pipelines

A-3

Appendix A-3-1: Canadian Pacific Railway (CP Rail)

Kate Barclay

From: Jessica Dorgo
Sent: March 2, 2017 10:18 AM
To: 'joy.simms@peelregion.ca'; 'andrea.warren@peelregion.ca'; 'hashimali.hamdani@peelregion.ca'; 'nishat.hassan@peelregion.ca'; 'mark.masley@peelregion.ca'; 'rebecca.caughey@peelregion.ca'; 'Lee, Thomas'; 'damian.jamroz@peelregion.ca'; 'bob.nieuwenhuysen@peelregion.ca'; 'miriam.polga@peelregion.ca'; 'eric.chan@peelregion.ca'; 'natalie.lapos@peelregion.ca'; 'Lorenzo.mele@peelregion.ca'; 'kim.mcadam@peelregion.ca'; 'lynne.germaine@peelregion.ca'; 'trevor.bell@ontario.ca'; 'mark.heaton@ontario.ca'; 'paul_kerry@cpr.ca'; 'sabbir.saiyed@peelregion.ca'; 'kant.chawla@caledon.ca'; 'slingertat@trca.on.ca'; 'arthur.lo@peelregion.ca'; 'zak.zakeria@peelregion.ca'; 'angela.stockman@peelregion.ca'; 'lori-ann.thomsen@peelregion.ca'
Cc: Jennifer Haslett; Stephen Keen; 'Dela Cruz, Gino'
Subject: Revision - Region of Peel - Coleraine Drive Grade Separation EA Kick Off Meeting Minutes
Attachments: B000738_MM_20170207_Project Initiation_e02.pdf

Good morning all,

Please find the revised meeting minutes for the Coleraine Drive Grade Separation EA Kick-off Meeting held on February 16th, 2017 at the Region of Peel.

An additional note has been added to Item 3 – Work Plan and Schedule.

Thank you,

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation

CIMA+
Partners in Excellence

3027 Harvester Road, Suite 400
Burlington Ontario L7N 3G7
CANADA
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MINUTES OF MEETING

CLIENT : Peel Region

PROJECT : Class Environmental Assessment Study for Coleraine Drive Grade Separation (South Of Old Ellwood Drive), Town Of Caledon

MEETING : Project Team

DATE OF MEETING : February 16, 2017 at 9:30 am

LOCATION : Peel Region, 10 Peel Centre Drive, Suite A, 1st Floor Mississauga Room

ATTENDEES : Gino Dela Cruz, Joy Simms, Andrea Warren, Arthur Lo Zak Zakeria, Angela Stockman, Hashim Ali Hamdani, Mark Masley, Rebecca Caughey, Lori Ann Thomson, Tom Lee, Bob Nieuwenhuysen, Damian Jamroz, Miriam Polga (Peel Region)
Sharon Lingertart (TRCA)
Stephen Keen, Jennifer Haslett, Jessica Dorgo (CIMA)

C.C. TO : People invited and attending

Note: If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.

DISCUSSION TOPICS**ACTION BY**

1	WELCOME	
	<ul style="list-style-type: none"> • Roundtable introductions • An agenda was distributed prior to the meeting • Gino Dela Cruz will be Peel Region's Project Manager • Stephen Keen will be CIMA's Project Manager 	
2	PROJECT BACKGROUND AND SCOPE	
	<ul style="list-style-type: none"> • Region of Peel's Goods Movement Task Force analyzed all grade separation locations in Peel Region and Coleraine Drive was identified as a priority • In 2015, CIMA completed the Coleraine Drive Feasibility Study which reviewed the rail-over-road and road-over-rail options for a 6-lane cross section 	
3	WORK PLAN AND SCHEDULE	
	<ul style="list-style-type: none"> • EA is expected to continue into 2018 • Golder is the main subconsultant to this project • CIMA is currently working with the Region to identify properties that will require permission to enter requests for field investigations • First Public Information Centre is tentatively scheduled for June 2017 • Peel Region - Environmental Assessment - Health Criteria can be used a guidelines to assess the impact of the options on pedestrians and cyclists 	
4	DEVELOPMENT APPLICATIONS	
	<ul style="list-style-type: none"> • CIMA is currently working with the Region to review the plans from the 2015 Feasibility Study to identify impacts to businesses and proposed developments • Second meeting in the next few weeks to be scheduled with CIMA, Peel Region's Real Estate group and Town of Caledon to develop a plan for communication with the affected businesses and developments • Individual meetings with impacted parties to be further discussed at the meeting with Peel Region Real Estate Group and Town of Caledon • Community workshop with Town of Caledon is an option to present the design options to the public to review input and comments on community connectivity 	Peel Region
5	REQUEST FOR BACKGROUND INFORMATION	
	<ul style="list-style-type: none"> • TRCA to send CIMA hydraulic model used for King Street study if available • The north end of the study area is regulated as a valley, not a floodplain • TRCA will be interested in bank stability primarily 	TRCA Peel Region

DISCUSSION TOPICS

ACTION BY

	<ul style="list-style-type: none"> • Peel Region to send CIMA collision data and EMME model • Peel Region contact Town of Caledon and to forward CIMA SWM reports for adjacent subdivision if available 	Peel Region
6	OPEN DISCUSSION	
	<ul style="list-style-type: none"> • Holland Drive and Coleraine Drive is now a signalized intersection • Intersection with King Street is a problem for safety (i.e. speeding and pedestrians) • A reduced posted speed limit or narrow lanes may be warranted • A buffer could be included between the travel lanes and multi-use trail • Green walls or art on walls adjacent to residential subdivision to be considered. This issue can be a focus for consultation. • Peel Region to confirm if a 6-lane or 4-lane cross section will be implemented along Coleraine Drive (subsequently confirmed as a 4-lane cross-section only) • Butternut are thought to be in the study area, most likely located in the valley • Incorporation of LID measures and plantings are preferred for TRCA as are maintaining existing drainage patterns to existing watercourses including water quality and quantity. Soils may be conducive to infiltration. TRCA requires 80% TSS removal (which OGS do not achieve on their own). CB Shields and Jellyfish filters were mentioned as possibilities also. • Bolton Transportation Master Plan recommends buffered bicycle lanes for this area. CIMA to include rationale if a different solution is followed. • Walk and Roll Peel website shows up to date trail system for Humber Valley Trail • CIMA to consider guard rail treatment near King Street • Town of Caledon has established a Cycling Advisory Committee. Region to provide CIMA with a contact for stakeholder list • CIMA to determine if pre-consultation forms were received from Peel Region for developments that are pre-applications • PIC 1 will present preliminary rail-over-road and road-over-rail options (artistic presentation) to initiate discussion of key issues with public 	<p>Peel Region</p> <p>CIMA</p> <p>CIMA</p> <p>Peel Region</p> <p>CIMA</p>

DISCUSSION TOPICS**ACTION BY**

7	NEXT STEPS	
	<ul style="list-style-type: none">• CIMA to develop a consultation strategy and send to Peel Region• CIMA to create a land use/ development map for meeting with Real Estate Group and Town of Caledon. Peel Region to schedule meeting.• Public Information Centre #1 tentatively planned for June 2017	CIMA CIMA/ Peel Region

Kate Barclay

From: Jessica Dorgo
Sent: March 21, 2017 11:28 AM
To: jennifer_benedict@cpr.ca
Cc: Stephen Keen; Dela Cruz, Gino
Subject: Region of Peel- Coleraine Drive Grade Separation EA Notice of Study Commencement
Attachments: B738_Peel Coleraine GS EA_Notice of Commencement Package_JBenedict_e01.pdf

Good Morning Ms. Benedict,

Please find the attached Notice of Study Commencement for the Region of Peel Municipal Class Environmental Assessment Study for Coleraine Drive Grade Separation. If you have any comments or questions please use the enclosed response form or contact the Project Manager on the attached Notice.

Thank you,

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation

CIMA+
Partners in Excellence

3027 Harvester Road, Suite 400
Burlington Ontario L7N 3G7
CANADA
Tel: 289-288-0287 ext. 6819 / Fax: 289-288-0285



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March 21, 2017

Ms. Jennifer Benedict
Canadian Pacific Rail

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

Public consultation is an integral component of the Municipal Class EA process and opportunities to comment are welcomed throughout the entire study. Consultation activities will be announced at key milestones in the project.

You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 5, 2017 9:18 AM
To: mark.heaton@ontario.ca; jennifer_benedict@cpr.ca
Cc: Stephen Keen; Dela Cruz, Gino
Subject: B738_Region of Peel Coleraine Dr Grade Separation EA_TAC No.1 Meeting Minutes
Attachments: B000738_MM_20170828_TAC1_e01.pdf

Good Morning,

Please find the attached minutes for the Coleraine Drive Grade Separation EA Technical Agencies Committee Meeting No. 1 held on August 28th, 2017. We ask that you kindly provide any comments regarding the PIC No. 1 materials by September 13th.

Thank you,

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation

CIMA+
Partners in Excellence

3027 Harvester Road, Suite 400
Burlington Ontario L7N 3G7
CANADA
Tel: 289-288-0287 ext. 6819 / Fax: 289-288-0285



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Kate Barclay

From: Dela Cruz, Gino <gino.delacruz@peelregion.ca>
Sent: September 19, 2017 4:39 PM
To: Lee, Thomas; Lapos, Natalie; Khan, Sanya; Masley, Mark; Lo, Arthur; Nieuwenhuysen, Bob; Simms, Joy; Detaramani, Tina; Kant Chawla; Sharon Lingertat; mark.heaton@ontario.ca; trevor.bell@ontario.ca; Jennifer Benedict; Hamdani, Hashim Ali; Thomsen, Lori-Ann; Stockman, Angela; Caughey, Rebecca; Ansari, Seema
Cc: Jessica Dorgo; Stephen Keen
Subject: FW: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)
Attachments: RegionofPeel_09_14_17[1]CITIZEN revised.pdf

FYI – please see attached notice and boards for the PIC #1 on September 26, 2017.

From: Dela Cruz, Gino
Sent: September 12, 2017 4:12 PM
To: Dale, Frank; Thompson, Allan; Groves, Annette; Innis, Jennifer
Cc: Smith, Janette; Kocialek, Gary; Saiyed, Sabbir; Rook, Sally
Subject: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

Good afternoon,

I am reaching out to let you know that the first Public Information Centre (PIC) for the Coleraine Drive Grade Separation Environmental Assessment (South of Old Ellwood Drive) is being held on September 26, 2017 at the Albion Bolton Community Centre. Additional details are noted in the attached notice which will be published in the Caledon Enterprise and Caledon Citizen on September 14 and 21; as well as mailed out to residents in the area. Display boards for the public meeting will be forwarded to you closer to the PIC date.

If you have any questions or concerns, please contact me. Information will continue to be updated on the project website including the display boards shortly after the PIC date. The link is here <https://www.peelregion.ca/pw/transportation/enviro-assess/index-cal.htm>

Thank you,
Gino

P. Gino Dela Cruz, P.Eng.
Project Manager, Roads – Design and Construction
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

Kate Barclay

From: Dela Cruz, Gino <gino.delacruz@peelregion.ca>
Sent: September 26, 2017 3:04 PM
To: Lee, Thomas; Lapos, Natalie; Khan, Sanya; Masley, Mark; Lo, Arthur; Nieuwenhuysen, Bob; Simms, Joy; Detaramani, Tina; Kant Chawla; Sharon Lingertat; mark.heaton@ontario.ca; trevor.bell@ontario.ca; Jennifer Benedict; Hamdani, Hashim Ali; Thomsen, Lori-Ann; Stockman, Angela; Caughey, Rebecca; Ansari, Seema
Cc: Jessica Dorgo; Stephen Keen
Subject: RE: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

Good afternoon,

Please note that the PIC scheduled for tonight has been cancelled and will be re-scheduled for a later date.

Sorry for the inconvenience and we will keep you updated on the future date for this meeting.

Regards,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Roads – Design and Construction
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: Dela Cruz, Gino
Sent: September 19, 2017 4:38 PM
To: Lee, Thomas; Lapos, Natalie; Khan, Sanya; Masley, Mark; Lo, Arthur; Nieuwenhuysen, Bob; Simms, Joy; Detaramani, Tina; 'Kant Chawla'; 'Sharon Lingertat'; 'mark.heaton@ontario.ca'; 'trevor.bell@ontario.ca'; Jennifer Benedict; Hamdani, Hashim Ali; Thomsen, Lori-Ann; Stockman, Angela; Caughey, Rebecca; Ansari, Seema
Cc: 'Jessica Dorgo'; Stephen Keen
Subject: FW: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

FYI – please see attached notice and boards for the PIC #1 on September 26, 2017.

From: Dela Cruz, Gino
Sent: September 12, 2017 4:12 PM
To: Dale, Frank; Thompson, Allan; Groves, Annette; Innis, Jennifer
Cc: Smith, Janette; Kocialek, Gary; Saiyed, Sabbir; Rook, Sally
Subject: Notice of Public Information Centre (PIC) 1 - Coleraine Dr Grade Separation (South of Old Ellwood Dr)

Good afternoon,

I am reaching out to let you know that the first Public Information Centre (PIC) for the Coleraine Drive Grade Separation Environmental Assessment (South of Old Ellwood Drive) is being held on September 26, 2017 at the Albion Bolton Community Centre. Additional details are noted in the attached notice which will be published in the Caledon Enterprise and Caledon Citizen on September 14 and 21; as well as mailed out to residents in the area. Display boards for the public meeting will be forwarded to you closer to the PIC date.

If you have any questions or concerns, please contact me. Information will continue to be updated on the project website including the display boards shortly after the PIC date. The link is here <https://www.peelregion.ca/pw/transportation/envIRON-assess/index-cal.htm>

Thank you,
Gino

P. Gino Dela Cruz, P.Eng.

Project Manager, Roads – Design and Construction
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

Kate Barclay

From: Jessica Dorgo
Sent: August 13, 2019 9:41 AM
To: Brian_Costigan@cpr.ca
Cc: Stephen Keen
Subject: RE: Coleraine Drive Grade Separation (S of Old Ellwood Dr) EA_Meeting with CP Rail

Good Morning Brian,

As a follow-up to our message below, we received your contact information through the auto-reply from Jennifer Benedict noting that she is currently on maternity leave. We had previously corresponded with Jennifer regarding CP Rail's involvement in a grade separation Class EA study being undertaken by the Region of Peel. The Region is undertaking an Environmental Assessment Study for the grade separation of Coleraine Drive and CP Rail, south of Old Ellwood Drive in the Town of Caledon.

We would greatly appreciate if you could kindly advise if you are the correct contact for our study as we would like to arrange a meeting with CP Rail to discuss the preliminary grade separation options and CP's requirements for this study.

Thank you,

Jessica Dorgo, EIT
Transportation
T 289-288-0287 ext. 6819 F 289-288-0285

CIMA+

From: Jessica Dorgo
Sent: Thursday, July 18, 2019 1:40 PM
To: Brian_Costigan@cpr.ca
Cc: Stephen Keen <Stephen.Keen@cima.ca>
Subject: FW: Coleraine Drive Grade Separation (S of Old Ellwood Dr) EA_Meeting with CP Rail

Good Afternoon Brian,

We received your contact information through the auto-reply from Jennifer Benedict noting that she is currently on maternity leave. We had previously corresponded with Jennifer regarding CP Rail's involvement in a grade separation Class EA study being undertaken by the Region of Peel. The Region is undertaking an Environmental Assessment Study for the grade separation of Coleraine Drive and CP Rail, south of Old Ellwood Drive in the Town of Caledon. I have attached the Notice of Study Commencement for your reference.

At this time, the project team would like to request a meeting with CP Rail to discuss the preliminary grade separation options and CP's requirements for this study. We would greatly appreciate if you could please advise if you are the correct contact for our study and if so, your availability over July/August for a meeting.

Thank you,

Jessica Dorgo, EIT
Transportation
T 289-288-0287 ext. 6819 F 289-288-0285

From: Dela Cruz, Gino <gino.delacruz@peelregion.ca>
Sent: Monday, February 6, 2017 10:50 AM
To: Jennifer Benedict <Jennifer_Benedict@cpr.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>
Subject: RE: Coleraine Drive Grade Separation (S of Old Ellwood Dr) EA Kick-Off Meeting

Thanks Jennifer – I will review the information you sent with our consultant and prepare a submission to work with CP on this project.

P. Gino Dela Cruz, P.Eng.
Project Manager, Infrastructure Programming & Studies
Transportation Division, Public Works, Region of Peel
10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9
Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

From: Jennifer Benedict [mailto:Jennifer_Benedict@cpr.ca]
Sent: February 6, 2017 10:43 AM
To: Dela Cruz, Gino
Subject: RE: Coleraine Drive Grade Separation (S of Old Ellwood Dr) EA Kick-Off Meeting

Good morning Gino,

I have another meeting to attend on February 16th and will not be able to make it to this meeting.

When you are ready for CP comments or consultation, please advise and we can setup a meeting to discuss in more detail.

I have attached some information about working with CP.
We will require a formal submission for the project (see the Application Guide attached). At that point, I will be able to assign a project manager to work with you on technical details.

If you have any questions, please let me know.

Thanks.

Jennifer R. Benedict, M.Eng., C.E.T. | Manager Public Works – Eastern Region | 1290 Central Parkway West, Suite 800, Mississauga ON L5C 4R3
O 905.803.5989 | C 416.995.0031 CP

-----Original Appointment-----

From: Dela Cruz, Gino [<mailto:gino.delacruz@peelregion.ca>]
Sent: Thursday, January 26, 2017 8:52 AM
To: Dela Cruz, Gino; Jennifer Benedict
Subject: FW: Coleraine Drive Grade Separation (S of Old Ellwood Dr) EA Kick-Off Meeting
When: Thursday, February 16, 2017 9:30 AM-11:00 AM (UTC-05:00) Eastern Time (US & Canada).
Where: Mississauga Room (10 Peel Centre Drive, Suite A, 1st Floor)

-----Original Appointment-----

From: Dela Cruz, Gino

Sent: January 23, 2017 11:28 AM

To: Dela Cruz, Gino; Lee, Thomas; Chan, Eric; Masley, Mark; Lapos, Natalie; Mele, Lorenzo; McAdam, Kim; Lo, Arthur; Nieuwenhuysen, Bob; Warren, Andrea; Germaine, Lynne; Sharon Lingertat; trevor.bell@ontario.ca; mark.heaton@ontario.ca; Stephen Keen; Kant Chawla; 'paul_kerry@cpr.ca'; Saiyed, Sabbir

Cc: Jennifer Haslett; Thomsen, Lori-Ann; Jessica Dorgo; Quigley, Brandon; Chan, Wayne; Laing, Rick; Toy, William; Carrick, Sean; Jamroz, Damian; rebbec.caughey@peelregion.ca; Dave, Richa (Richa.Dave@peelregion.ca); Zare, Mina

Subject: Coleraine Drive Grade Separation (S of Old Ellwood Dr) EA Kick-Off Meeting

When: February 16, 2017 9:30 AM-11:00 AM (UTC-05:00) Eastern Time (US & Canada).

Where: Mississauga Room (10 Peel Centre Drive, Suite A, 1st Floor)

Good afternoon,

The Region has commenced an Environmental Assessment Study for the grade separation of Coleraine Drive and CPR, south of Old Ellwood Drive in the Town of Caledon. Please hold the date and time for the project Kick Off meeting. The purpose of the kick-off meeting is to introduce the project, go over the scope of work, and receive information/input from stakeholders.

An agenda will be provided closer to the date of the meeting.

Regards,

P. Gino Dela Cruz, P.Eng.

Project Manager, Infrastructure Programming & Studies

Transportation Division, Public Works, Region of Peel

10 Peel Centre Dr., Suite B 4th Floor, Brampton, ON L6T 4B9

Tel: (905)791-7800, ext. 7805 Fax: (905)791-1442

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----- IMPORTANT NOTICE - AVIS IMPORTANT -----

Kate Barclay

From: Jessica Dorgo
Sent: August 14, 2019 2:00 PM
To: 'Brian_Costigan@cpr.ca'
Cc: Stephen Keen
Subject: Region of Peel Coleraine Dr Grade Separation EA

Hi Brian,

I hope this email find you well. As a follow up to our phone conversation, we had previously corresponded with Jennifer Benedict regarding CP Rail's involvement in a grade separation Class EA study being undertaken by the Region of Peel. The Region is undertaking an Environmental Assessment Study for the grade separation of Coleraine Drive and CP Rail, south of Old Ellwood Drive in the Town of Caledon.

At this time, the project team would like to request a meeting with CP Rail to discuss the preliminary grade separation options and CP's requirements for this study. We would greatly appreciate if you could please advise regarding your availability on the following dates for a meeting:

- September 3rd (available AM only)
- September 5th
- September 11th (available PM only)
- September 12th
- September 13th
- September 16th
- September 17th (available AM only)
- September 18th

If it is convenient for you, the meeting can be held at the CP Rail offices. Please let us know a time and date that work well for you and we can schedule the meeting accordingly.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



Kate Barclay

From: Jessica Dorgo
Sent: September 24, 2019 11:36 AM
To: Sonya Bubas (Kapusin) (sonya.bubas@peelregion.ca); 'Brian_Costigan@cpr.ca'; tom_twigge@cpr.ca
Cc: Stephen Keen
Subject: Region of Peel Coleraine Drive Grade Separation EA_Meeting with CP Rail
Attachments: B738_Coleraine Dr GS EA_CP Rail Meeting Presentation_e02.pdf; B738_CP Rail Meeting_MM_e02.pdf

Good Morning,

Please find the attached meeting minutes for the Region of Peel Coleraine Drive Grade Separation EA meeting with CP Rail. A copy of the presentation from the meeting is also attached for your reference.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



Meeting Minutes

- Meeting** : Meeting with CP Rail
- Project** : Region of Peel Class Environmental Assessment Study for Coleraine Drive Grade Separation (South Of Old Ellwood Drive), Town of Caledon
- Date / Time** : September 12, 2019, 10:00 am
- Location** : 1290 Central Parkway West, Mississauga, ON
- Attendees** :
- Tom Twigge, CP Rail
 - Brian Costigan, CP Rail
 - Sonya Bubas, Region of Peel
 - Bob Nieuwenhuysen, Region of Peel (by phone)
 - Stephen Keen, CIMA
 - Jessica Dorgo, CIMA

Note: please advise author immediately of any errors or omissions

Discussion Topics	Action By
1. Study Overview	
1.1. CIMA and the Region of Peel provided a study overview facilitated by a PowerPoint presentation.	
1.2. A Feasibility Study which considered grade separations at the Coleraine Drive and King Street rail crossings was completed in 2015.	
1.3. Rail diversion strategies were considered as part of this feasibility study. The preferred option in the feasibility study for Coleraine Drive includes two new shooflies south of the crossing. An additional future track north of the crossing was also considered in the grade separation.	
1.4. The feasibility study noted that grade separations were feasible at the three locations reviewed (Coleraine Dive, King Street and King Street bypass).	
1.5. This Class EA study is now being conducted for the Coleraine Drive Grade Separation.	
1.6. The preliminary recommended solution for the Class EA considers changing the grade of the road (i.e. depress road under rail or raise road over rail) along with the provision of pedestrian and bicycle facilities.	
2. Discussion	
2.1. CIMA presented two preliminary design plans for the road over rail and road under rail options. These plans are preliminary and do not represent the recent changes to the cross-section.	
2.2. It was noted that the area adjacent to the rail crossing is highly residential.	
2.3. The project team intends to use the rail diversion findings from the 2015 Feasibility Study for this Class EA if CP Rail is in agreement with the strategy presented.	
2.4. CP Rail noted that the Oldbright plant is no longer being serviced and this property could be utilized for the rail diversion.	
2.5. CP Rail noted that this is the last staging track before Vaughan terminal. CP's main goal is to ensure these trains are not disrupted. Capacity and speed reduction will be important considerations.	

Discussion Topics	Action By	
2.6.	The track speed at this location is 40km/h due to the super elevation.	
2.7.	It was noted that the road over rail option will likely be less costly and easier to build. CP Rail has no strong preference between the two options as long as capacity and track speed are maintained.	
2.8.	It was noted that road bridges are generally easier to maintain than rail bridges.	
2.9.	CP Rail noted that there are three customers serviced in this area (including MARS) and access to these parcels will need to be maintained during construction. The track going into MARS is on a slope and this may be a constraint during rail diversion.	
2.10.	CP Rail noted that one track on the west side may be surplus. This can be reviewed later in the study.	
2.11.	CP Rail noted that concrete tubs are typically the preferred structure type (easier to maintain).	
2.12.	CP Rail noted that there is likely no opportunity to adjust the rail elevation at this location. For the purpose of this study it will be assumed that the track elevation cannot change.	
2.13.	A pier could be considered to reduce the span for the underpass option and thereby reduce the depth of the structure.	
3. Next Steps		
3.1.	CP Rail requested to be kept as a stakeholder for this study and to be informed of the study progress.	
3.2.	CP Rail typically does not conduct detailed reviews during the EA stage but is available to review concept plans and provide comments.	
3.3.	CP Rail is available to review the structure design to comment on clearances/depth.	
3.4.	Construction is tentatively planned between 2027 and 2030.	

Jessica Dorgo, EIT

Coleraine Drive Grade Separation Municipal Class Environmental Assessment

Meeting with CP Rail

September 12, 2019

CIMA+

Presentation Summary

Study
Introduction

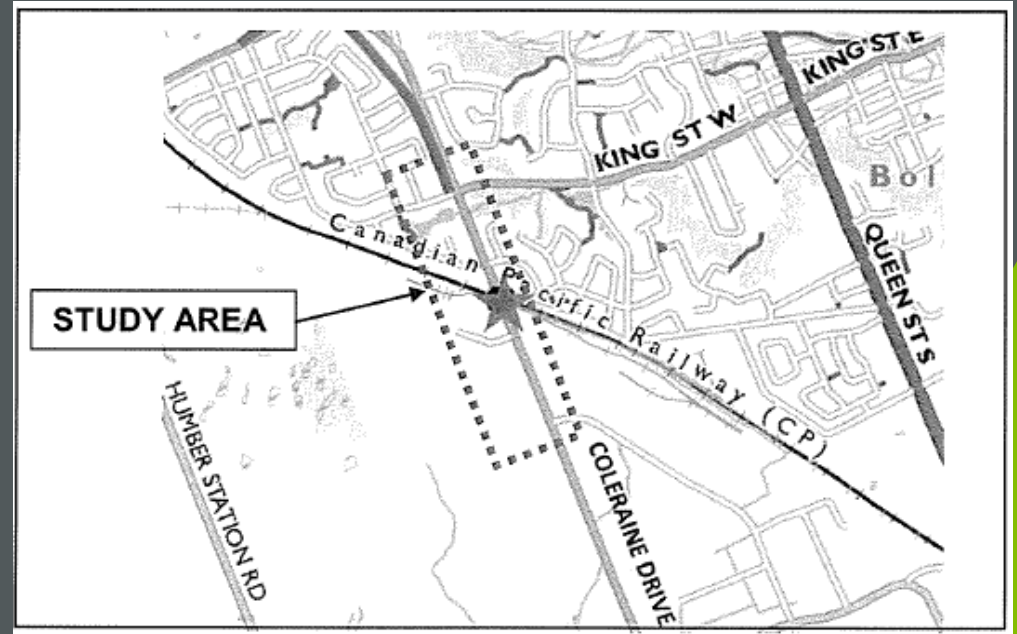
Feasibility
Study
(2015)

Preliminary
Options

Study
Schedule

Coleraine Drive Class EA Study Introduction

- The Region of Peel is conducting a Schedule 'C' Class EA to consider a range of options for a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon.
- The study area extends approximately 150 metres north of Harvest Moon Drive to Holland Drive.



Coleraine Drive Class EA Study Introduction

- The crossing at Coleraine Drive is located on the CP MacTier subdivision, a moderately trafficked portion of CP's transcontinental main line located northwest of Toronto.
- The line services minor industries, the nearby CP Vaughan Intermodal Terminal, and a Honda automobile plant in Alliston near Mile 44.

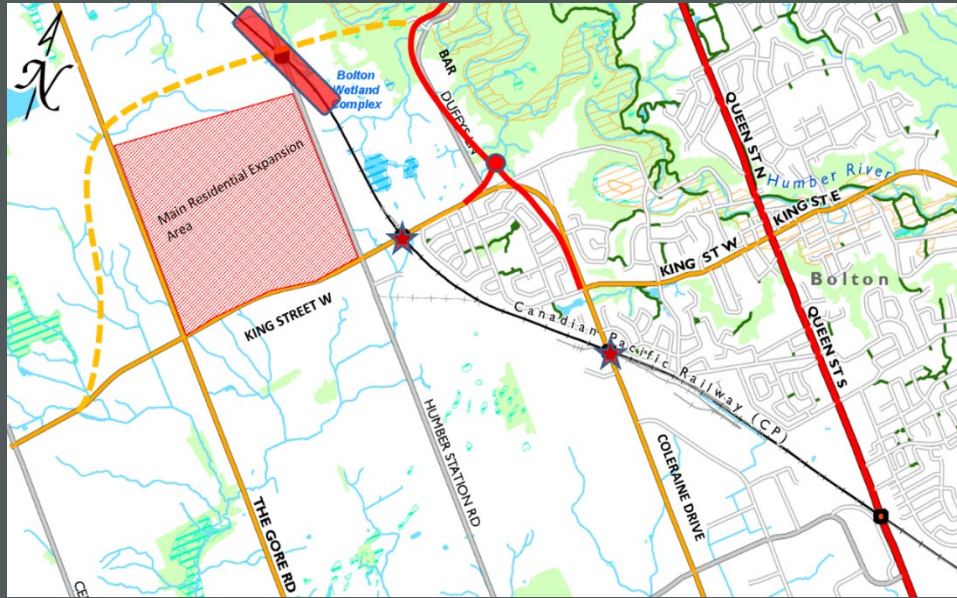
Table 1 - Details of CP Service at King Street and Coleraine Drive

	King Street	Coleraine Drive
Railway	CP	CP
Subdivision	MacTier	MacTier
Mileage	22.85	21.85
Track Speed	unknown	unknown
Siding [Clear Length]	none	Bolton [11,474 ft.]
Control	ABS/OCS	CTC
Track Signals Present At Crossing Location	no	yes
Fibre Present	likely	likely

**Feasibility Study
Separation of At-Grade Rail
Crossings at King Street and
Coleraine Drive (2015)**

2015 Feasibility Study

- Considered grade separations at the Coleraine Drive and King Street rail crossings.



LEGEND

- ★ King Street and Coleraine Drive at-grade crossings
- Red line Potential future rail crossing
- Yellow dashed line Future King Street realignment



Coleraine Drive At-Grade Crossing



King Street At-Grade Crossing

Feasibility Study (2015)

- Five options were considered for grade separations on Coleraine Drive/ King Street.

OPTION
King Street W. – Road over Rail
King Street W. – Road under Rail
Coleraine Drive – Road over Rail
Coleraine Drive – Road under Rail
King Street realignment – Road over Rail

- The feasibility study considered existing environmental features and traffic conditions (vehicle and railway).

Feasibility Study (2015)

Existing and Projected Traffic Volumes

Road Selection	Direction	2017 Volumes (vph)		2041 Volumes (vph)	
		AM	PM	AM	PM
Coleraine Drive south of King Street W	NB	249	1,070	349	1,500
	SB	933	336	1,824	657

Feasibility Study (2015)

Bolton Community Rail Feasibility Study (Metrolinx, 2010)

- Projected rail volumes 2021, 2031
- 2021: 52 trains/day
- 2031: 63 trains/day

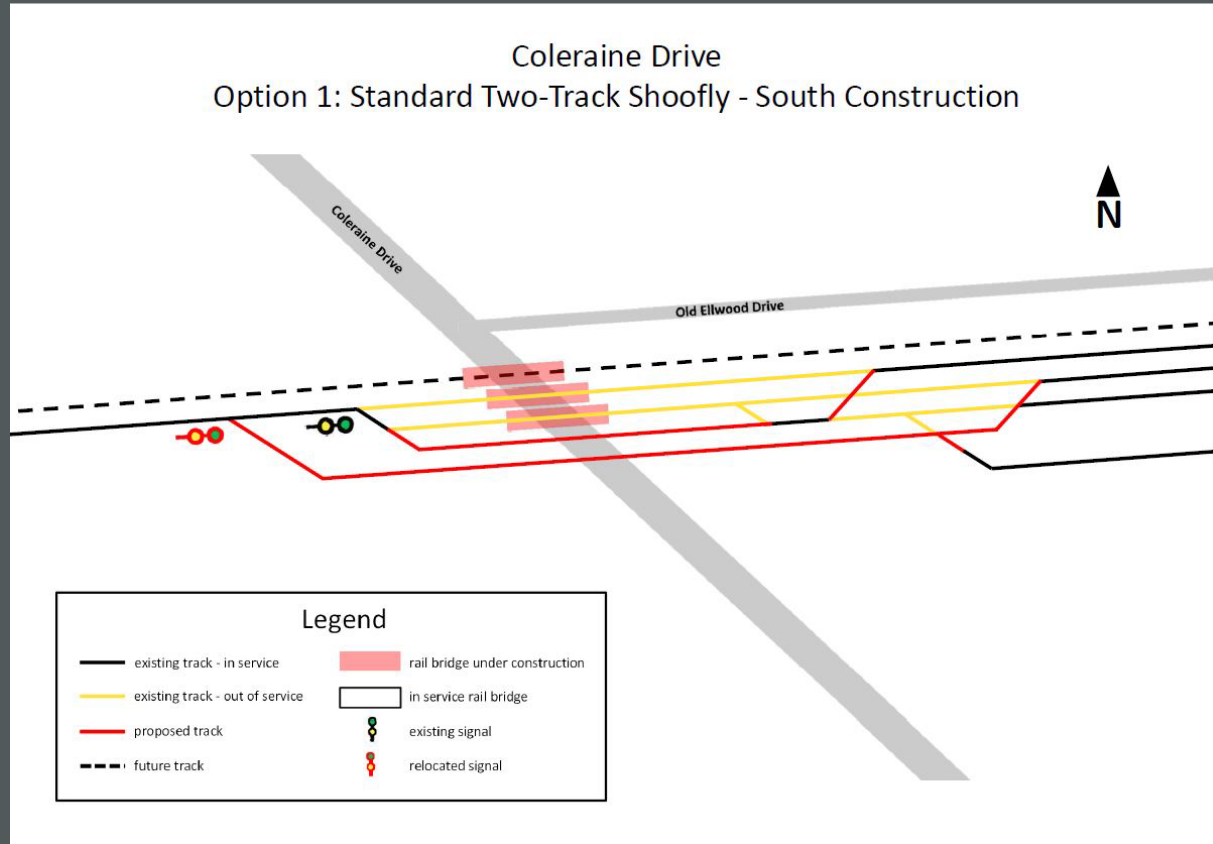
Regional Road	Daily GO Train Runs	GO Train Speeds (km/h)	GO Train – Max. Cars	GO Train- Number of Locomotives	GO Trains – Max. Length of Car (km)	Delay per GO Train (mins)
Coleraine Drive	10	40	10	1	0.026	0.42

Feasibility Study (2015)

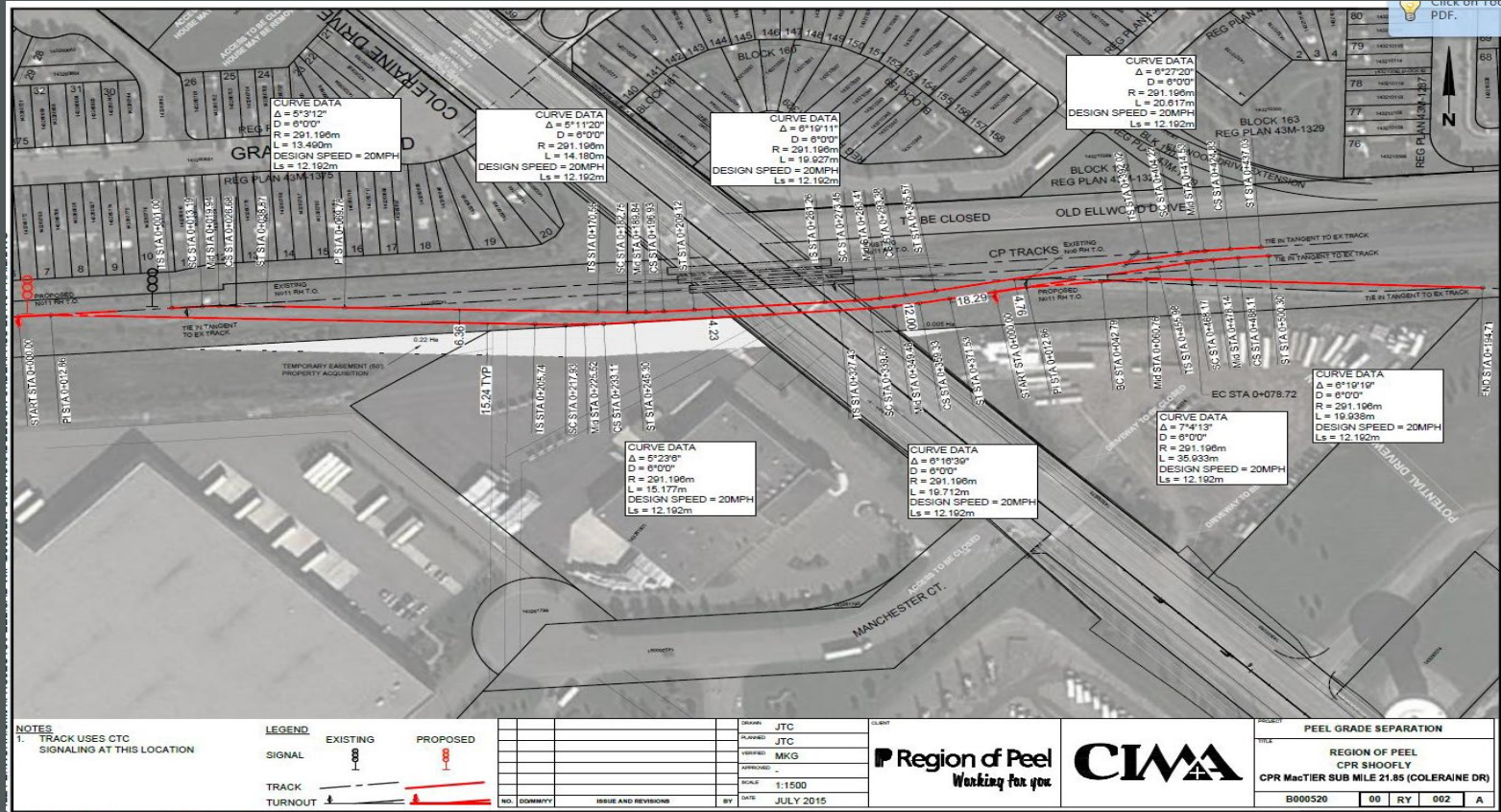
Rail Diversion (Road-under-Rail)

- Construction staging would require temporary rerouting
- Preferred option includes two new shooflies south of crossing
- Would require establishing temporary right-of-way on neighbouring industrial land
- Active spur into Mars Industrial plant will need to be maintained during construction
- The two existing tracks as well as a future track north of the crossing will be accommodated in the grade separation.

Feasibility Study (2015)



Feasibility Study (2015)



Feasibility Study (2015)

Recommendations from Feasibility Study

- Grade separated crossings are feasible at all three locations
- Further detailed studies required (i.e. Class EA studies)



Coleraine Drive Grade Separation EA

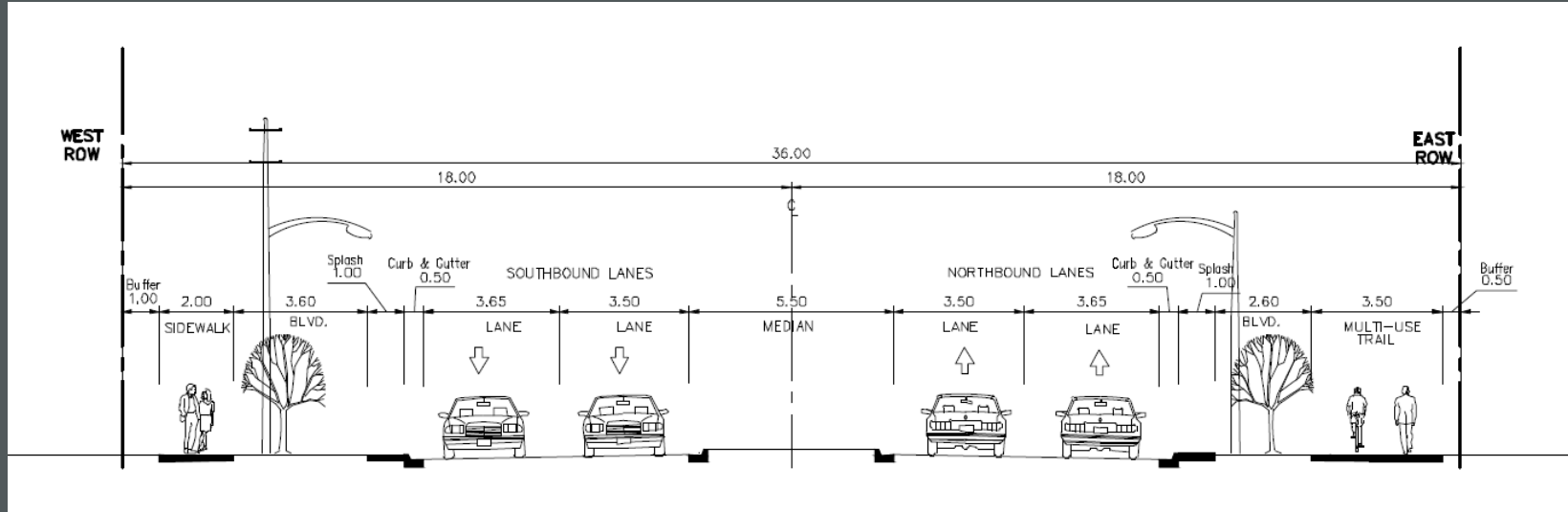
Preliminary Options

The preliminary recommended solution for this study considers the following:

- Change grade of the road
 - Depress road-under-rail grade separation; OR
 - Raise road-over-rail grade separation
- Provision of pedestrian and bicycle facilities along Coleraine Drive

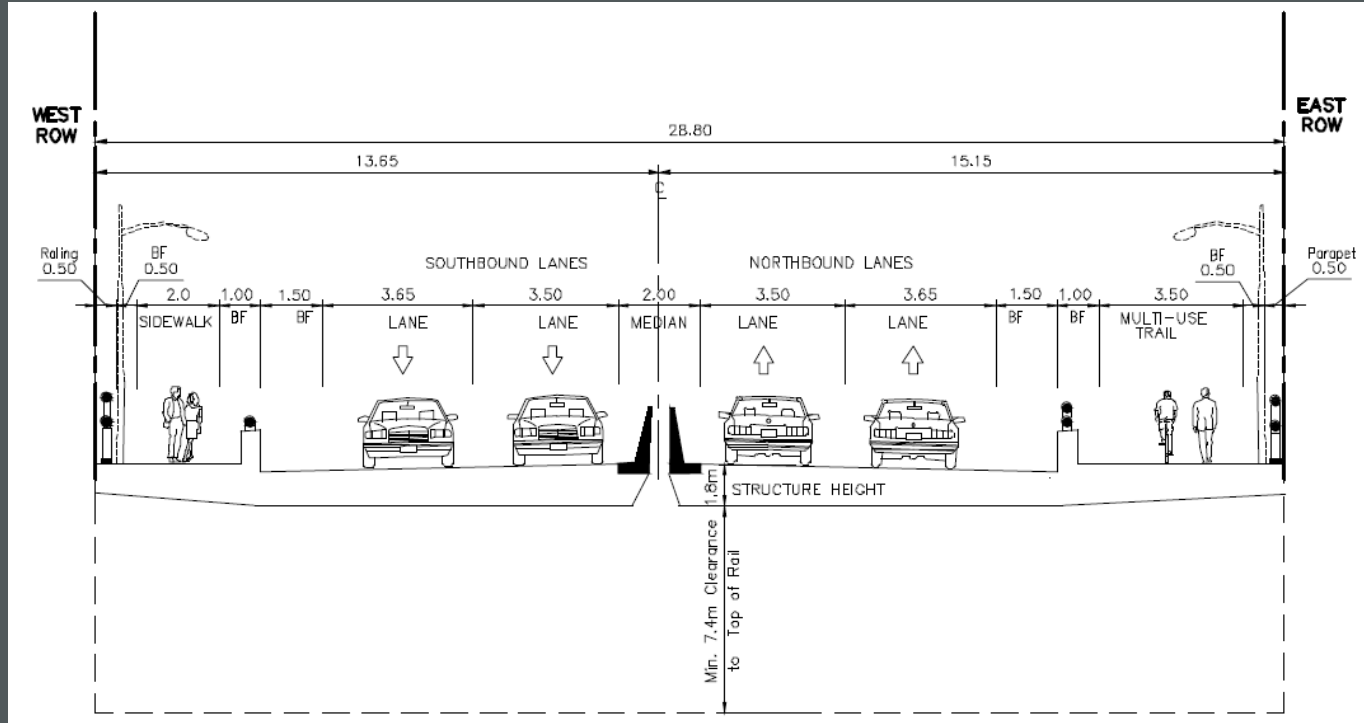
Preliminary Options

Typical Mid-Block Cross-Section



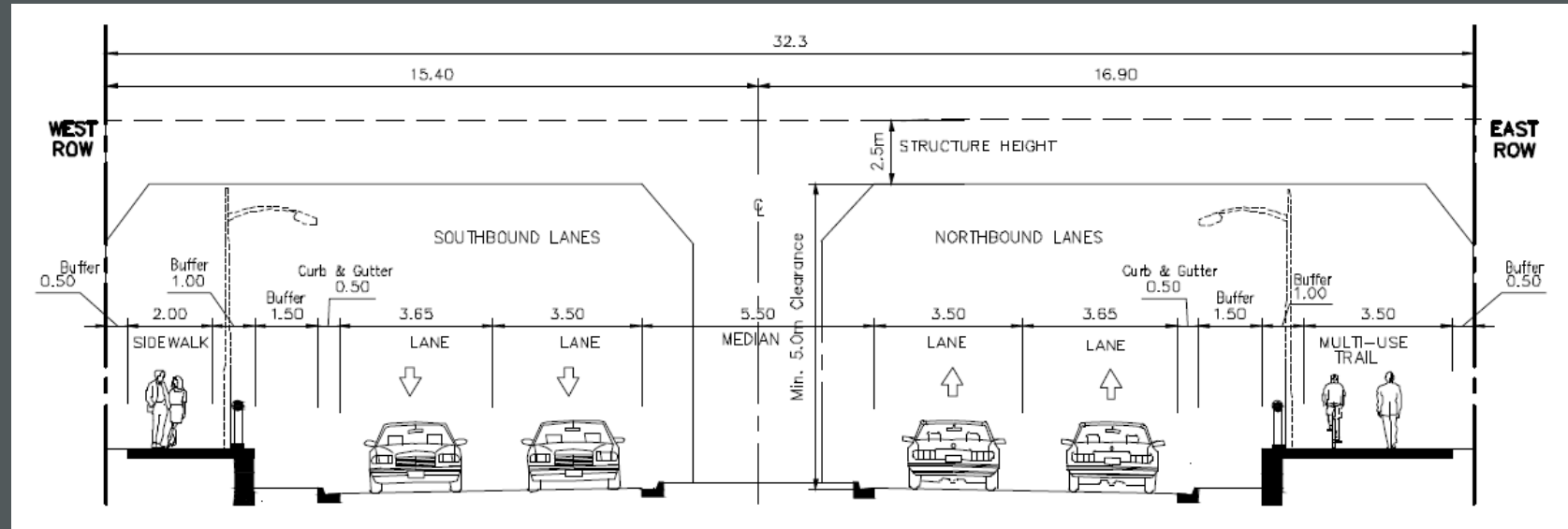
Preliminary Options

Road over Rail – Bridge Cross-Section



Preliminary Options

Road under Rail – Bridge Cross-Section



Study Schedule / Next Steps

Study Commencement	April 2016
Public Information Centre #1	October 8, 2019
Public Information Centre #2	Spring 2020
Study Completion	Summer 2020



Thank you!



Kate Barclay

From: Bubas, Sonya <sonya.bubas@peelregion.ca>
Sent: September 13, 2019 3:21 PM
To: Lee, Thomas; Lapos, Natalie; Ansari, Seema; Masley, Mark; Motala, Imran; Tatla, Manvir; Sharon Lingertat; Warren, Andrea; Zakeria, Zak; Hamdani, Hashim; Caughey, Rebecca; Nieuwenhuysen, Bob; Polga, Miriam; Van Boxmeer, Kyle; Paquette, Samantha; Saiyed, Sabbir; ZZG-Peel Health & Built Environment; Kant Chawla; Bell, Trevor (MOECC); Clark, Carol; Eberhardt, Daniel; Heaton, Mark (MNRF); Detaramani, Tina; Brian_Costigan@cpr.ca
Cc: Stephen Keen; Jessica Dorgo; Rook, Sally; Svirplys-Howe, Larissa; Jamroz, Damian; Gorman, Gayle; Jacyla, Orest; Crawford, Mark
Subject: Coleraine Dr Grade Separation EA - PIC Update
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; B738_Coleraine GS EA_PIC1 Boards_e02v01.pdf

To: Technical Advisory Committee

Good afternoon,

The first Public Information Centre (PIC) for the Coleraine Drive Grade Separation Environmental Assessment (South of Old Ellwood Drive) has been rescheduled to October 8, 2019 at the Albion Bolton Community Centre. You may recall this PIC was initially scheduled in September, 2017.

Attached are the Notice and Display for your information. Please respect the attachments as Draft for internal reference until final versions are presented at the PIC.

Please let me know by September 20 if you have any questions or concerns.

Regards,

Sonya Bubas, MCIP, RPP
Project Manager
Infrastructure Programming & Studies
Region of Peel
10 Peel Centre Dr., Suite B, 4th Floor
Brampton, ON L6T 4B9
905-791-7800 ext. 7801



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Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:37 AM
To: Brian_costigan@cpr.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Canadian Pacific Rail Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Mr. Brian Costigan
Engineer in Training – Public Works
Canadian Pacific Rail
1290 Central Parkway West, Suite 600
Mississauga, ON L5C 4R3

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Stephen Keen
Sent: May 4, 2021 10:31 AM
To: jennifer_benedict@cpr.ca
Cc: Mahmood, Tareq; David Hiatt; Elysia Friedl
Subject: B738 - Coleraine Grade Separation (Bolton) EA

Hi Jennifer

As discussed, can you let us know whether CP use Old Ellwood Drive (adjacent to the tracks) for maintenance/operations purposes?

As discussed, it is possible that we may recommend closing this road (flowing construction of the grade separation) unless we find a valid reason to keep it open.

Regards

STEPHEN KEEN, M.Sc., P.Eng.
Associate Partner / Senior Director Transportation



T 289-288-0287 ext. 6834 M 905-802-6523 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA

[Notice to our customers on the COVID-19](#)



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Kate Barclay

From: David Hiett
Sent: May 10, 2021 1:28 PM
To: Mahmood, Tareq; Rook, Sally; Nieuwenhuysen, Bob; Van Boxmeer, Kyle; Ansari, Seema; Hamdani, Hashim; Caughey, Rebecca; Saiyed, Sabbir; Lee, Arthur; Mannie, Sharon; Grzesiak, Ryan; Nevland, Erik; Fernandes, Trina; Smith, Neal; Hasselbacher, John; Gallagher, Joe; Saini, Sakshi; Eberhardt, Daniel; Koethe, Wayne; Thomsen, Jeanne; Bennington, Michael; Lotecki, Nancy; Woodtke, Trevor; Detaramani, Tina; ZZG-Peel Health & Built Environment; Peter Brocks; Stephen Keen; Suzanne Bevan; Emma Benko; Kristen Sullivan; Kant.chawla; Margi Sheth; Arash Olia; Ryan Grodecki; Tom_Twigge@cpr.ca; jennifer_benedict@cpr.ca; Andrew Pearce; Saddi, Asha; Cassie Schembri; Thomsen, Lori-Ann
Cc: Crawford, Heather; Lavhey, James; Kovach, Steven; Toy, William; Aymar, Matthew; Ursitti, Franca; Gardiner, Len; Michele Olliffe; Elysia Friedl
Subject: RE: Region of Peel - Coleraine Drive Grade Separation Class EA - Alternative Design (preliminary) Review Meeting (TAG-2)
Attachments: (B738) Coleraine Rd Grade Separation EA - TAG Meeting #2 Minutes_e02.pdf

Hi All,

Please see attached the minutes of last week's TAG #2 Meeting for the Coleraine Drive Grade Separation Class EA

As also noted in the minutes: If there are any errors or omissions, please contact myself and please provide any comments or concerns **no later than May 14, 2021**

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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-----Original Appointment-----

From: Mahmood, Tareq <tareq.mahmood@peelregion.ca>

Sent: Wednesday, April 7, 2021 11:46 AM

To: Mahmood, Tareq; Rook, Sally; Nieuwenhuysen, Bob; Van Boxmeer, Kyle; Ansari, Seema; Hamdani, Hashim; Caughey, Rebecca; Saiyed, Sabbir; Lee, Arthur; Mannie, Sharon; Grzesiak, Ryan; Nevland, Erik; Fernandes, Trina; Smith, Neal;

Hasselbacher, John; Gallagher, Joe; Saini, Sakshi; Eberhardt, Daniel; Koethe, Wayne; Thomsen, Jeanne; Bennington, Michael; Lotecki, Nancy; Woodtke, Trevor; Detaramani, Tina; ZZG-Peel Health & Built Environment; Peter Brocks; Stephen Keen; David Hiatt; Suzanne Bevan; Emma Benko; Kristen Sullivan; Kant.chawla; Margi Sheth; Arash Olia; Ryan Grodecki; Tom_Twigge@cpr.ca; jennifer_benedict@cpr.ca; Andrew Pearce; Saddi, Asha; Cassie Schembri; Thomsen, Lori-Ann

Cc: Crawford, Heather; Lavhey, James; Kovach, Steven; Toy, William; Aymar, Matthew; Ursitti, Franca; Gardiner, Len; Michele Olliffe; Elysia Friedl

Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Alternative Design (preliminary) Review Meeting (TAG-2)

When: Tuesday, May 4, 2021 10:00 AM-12:00 PM (UTC-05:00) Eastern Time (US & Canada).

Where: Microsoft Teams Meeting

EXTERNAL EMAIL

Hello

Please see attached the meeting agenda and PIC boards. It is proposed to have one main overview presentation that will be narrated (titled MAIN Overview Presentation).

Two additional slide decks are also be available for additional information, see the link below to access/download the slide decks.

The memo includes the evaluation tables and drawings of the improvements assessed at, see the link below to access/download the document.

- ❖ the Coleraine Drive / King Street and Harvest Moon Drive intersection,
- ❖ closure/connection concept design for Old Ellwood Drive, and
- ❖ grade separation concept design for the rail tracks

<https://www.dropbox.com/sh/ytxkt0mvowvm3a2/AAB6UAzgXBE8Jfq3dhDgeolFa?dl=0>

Please contact me if you require further information. I look forward to seeing you at TAG-2 meeting.

Thanks,

Tareq

Hi everyone,

As you may be aware, the Region of Peel is undertaking a Municipal Class Environmental Assessment Study for the grade separation of Coleraine Drive in the Town of Caledon. The study is considering options for the grade separation of Coleraine Drive and CP Rail. The project team would like to invite you to attend a virtual TAG-2 meeting to review the alternative preliminary design, the analysis and evaluation of alternative design and receive your comments and feedback on the preliminary preferred design for the Coleraine Drive Grade Separation Class EA.

Two options are being considered for the grade separation:

- Road-over-rail
- Road-under-rail

Additionally, two options for the intersection of King Street/Harvest Moon Drive and Coleraine Drive are being considered:

- Signalized intersection

- Roundabout

The meeting agenda, evaluation criteria, and preliminary design will be provided closer to the meeting. Please feel free to extend this invitation to the appropriate staff or any other staff that may be interested or impacted by this project.

Best regards,

Tareq Mahmood

Project Manager, Infrastructure Programming and Studies
Transportation, Public Works, Region of Peel
Cell:905-872-6475
Email: tareq.mahmood@peelregion.ca



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Microsoft Teams meeting

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Or call in (audio only)

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[\(844\) 589-6880,,435975801#](tel:(844)5896880435975801) Canada (Toll-free)

Phone Conference ID: 435 975 801#

[Find a local number](#) | [Reset PIN](#)

Region of Peel | Working with You

[Learn More](#) | [Meeting options](#) | [Legal](#)

Kate Barclay

From: Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Sent: December 16, 2021 9:22 AM
To: Rook, Sally; Van Boxmeer, Kyle; Ansari, Seema; Hamdani, Hashim <hashimali.hamdani@peelregion.ca>; Hamdani, Hashim; Caughey, Rebecca; Saiyed, Sabbir; Lee, Arthur; Mannie, Sharon; Grzesiak, Ryan; Nevland, Erik; Fernandes, Trina; Smith, Neal; Hasselbacher, John; Gallagher, Joe; Saini, Sakshi; Eberhardt, Daniel; Koethe, Wayne; Thomsen, Jeanne; Bennington, Michael; Lotecki, Nancy; Woodtke, Trevor; Detaramani, Tina; ZZG-Peel Health & Built Environment; Peter Brocks; Stephen Keen; David Hiett; Kate Barclay; Suzanne Bevan; Emma Benko; Kristen Sullivan; Kant Chawla; Margi Sheth; Arash Olia; Ryan Grodecki; Tom_Twigge@cpr.ca; jennifer_benedict@cpr.ca; Andrew Pearce; Saddi, Asha; Cassie Schembri; Thomsen, Lori-Ann; Crawford, Heather; Lavhey, James; Kovach, Steven; Toy, William; Aymar, Matthew; Ursitti, Franca; Gardiner, Len; Elysia Friedl; Evan Bearss; Sinthujan Navaratnavel; Jehan Zeb; Tatla, Manvir; Parm Chohan; Cristina Guido; Katelyn Tozer
Cc: Avsec, Joe; Ricketts, Terry
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

EXTERNAL EMAIL

Hello TAC Members,

The second PIC for the Coleraine Drive Grade Separation EA is being held [y1wxdo|#urp #Ghfhp ehu# 49/#354#r#0qxdu|#4/#355](#). Attached is the PIC 2 Notice which provides additional information on how you can provide your input and how you will be able to join the virtual event.

Please contact me if you require further information.

[How you can follow Coleraine Drive Grade Separation EA PIC-2:](#)

Visit the project website:

<https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp>

Link to PIC-2 presentation slides:

- <https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/pdf/coleraine/pic2-presentation.pdf>
- <https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/pdf/coleraine/pic2-technical-environmental-assessments.pdf>

Best regards,

Tareq Mahmood

Project Manager, Infrastructure Programming and Studies
Transportation, Public Works, Region of Peel
Cell:905-872-6475
Email: tareq.mahmood@peelregion.ca

A-3

Appendix A-3-2: Enbridge

March 21, 2017

Ms. Diana Beaulne
Mark-Up Administrator
Enbridge Gas Distribution
500 Consumers Road, 4th Floor
North York, ON M2J 1P8

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

Public consultation is an integral component of the Municipal Class EA process and opportunities to comment are welcomed throughout the entire study. Consultation activities will be announced at key milestones in the project.

You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

March 21, 2017

Jamie Rochford
Planning & Design Lead, Central Region East, Distribution Planning & Records
Enbridge Gas Distribution
101 Honda Blvd.
Markham, ON L6C 0M6

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

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Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:39 AM
To: Meetpal.Chhina@enbridge.com
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Enbridge Gas Distribution Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Meetpal Chhina
Planning Technical Specialist
Enbridge Gas Distribution
6 Colony Court
Brampton, ON L6T 4E4

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:39 AM
To: markups@enbridge.com
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Enbridge Gas Distribution Letter (2).pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Ms. Diana Beaulne
Mark-Up Administrator
Enbridge Gas Distribution
500 Consumers Road, 4th Floor
North York, ON M2J 1P8

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

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We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: David Hiett
Sent: February 1, 2022 11:43 AM
To: Meetpal.Chhina@enbridge.com
Cc: markups@enbridge.com; Stephen Keen; Mahmood, Tareq
Subject: B738 Coleraine Drive CP Grade Separation EA - Utilities - Enbridge
Attachments: Coleraine Drive GS EA - PIC Cross-section.jpg; B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02.pdf; B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02 with Image.pdf

Hi Meetpal,

The Region of Peel, supported by CIMA+, are completing a Municipal Class EA Schedule 'C' study for a Coleraine Drive Grade Separation, in Bolton. You would have previously received correspondence on the study, most recently regarding the PIC #2 that occurred at the end of last year. More information, including the PIC materials, can be found on the project website here: [Coleraine Drive grade separation Environmental assessment - Region of Peel \(peelregion.ca\)](https://www.peelregion.ca/coleraine-drive-grade-separation-environmental-assessment/)

Part of this preliminary design study is the identification of utility impacts and options for potential mitigation (i.e., relocation). Please see attached utility plans, with and without imagery.

We have yet to receive confirmation of any infrastructure within the study area and while infrastructure along the local roads would largely be unimpacted, any infrastructure along Coleraine Drive is likely to be impacted and would require relocation. This is mostly due to the large construction required – large fill and retaining walls will be used along Coleraine Drive to facilitate the raise over the CP rail lines (as seen in the attached cross-section). Also as detailed in the cross-section, there will be a buffer between the retaining wall and ROW boundary along Coleraine Drive, and it is within this area that we are proposing impacted utilities be relocated.

The impacts, final design and final relocation plans/agreements will be confirmed in Detailed Design, however we are reaching out to you at this time to confirm:

- That there is no Enbridge infrastructure along Coleraine Drive, or in the study area extents detailed in the attached. If there is, please provide data in PDF or CAD (preferred).
- If there is infrastructure, do you have any objection to the strategy of potentially relocating the impacted infrastructure on Coleraine Drive to the 'buffer' outside of the walls.
- If Enbridge are planning any future infrastructure improvements or future new infrastructure along Coleraine Drive

If you have any questions, please let us know.

If I can kindly have a response by February 9th, it will be greatly appreciated.

Many Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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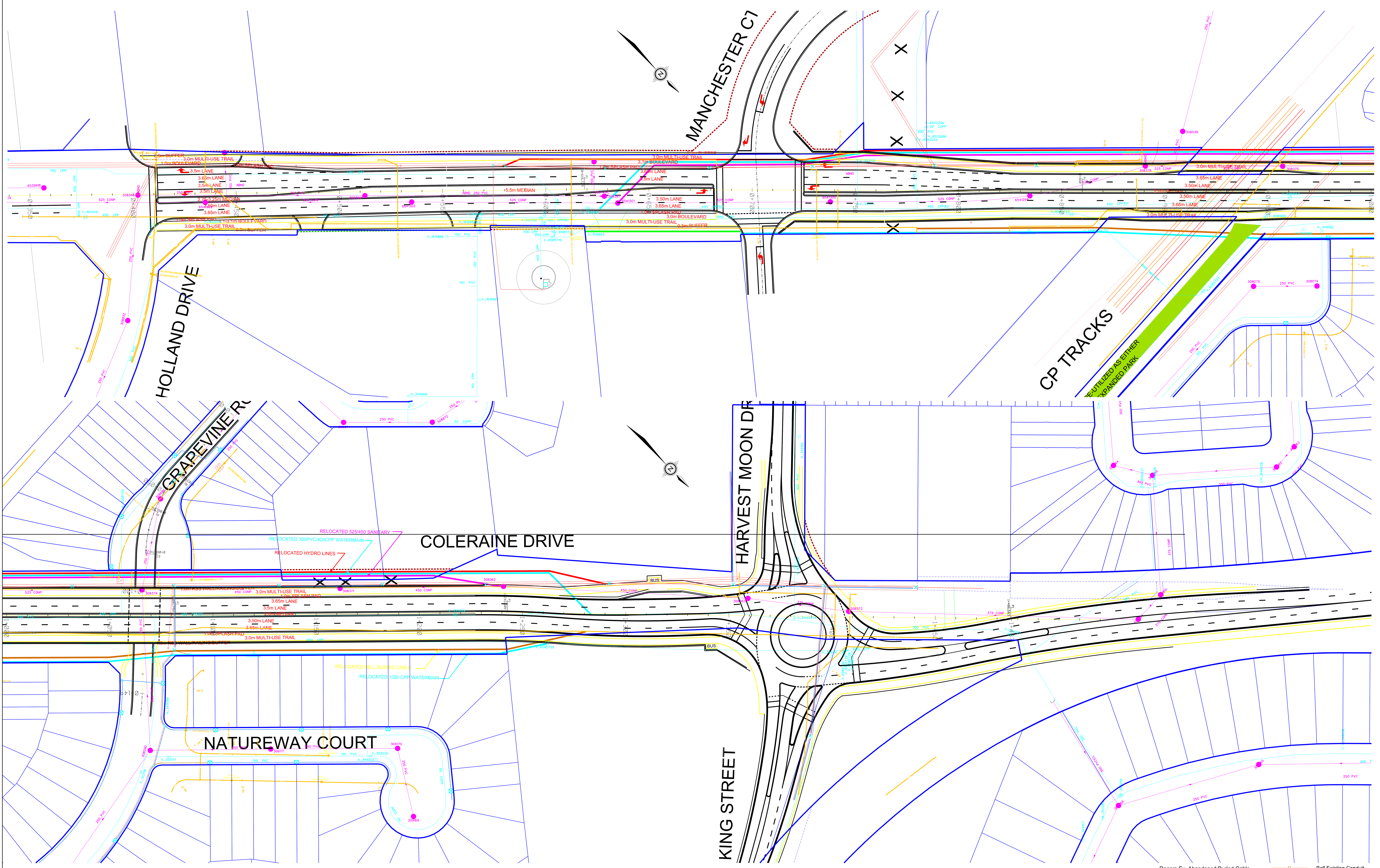


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CANADA 2019

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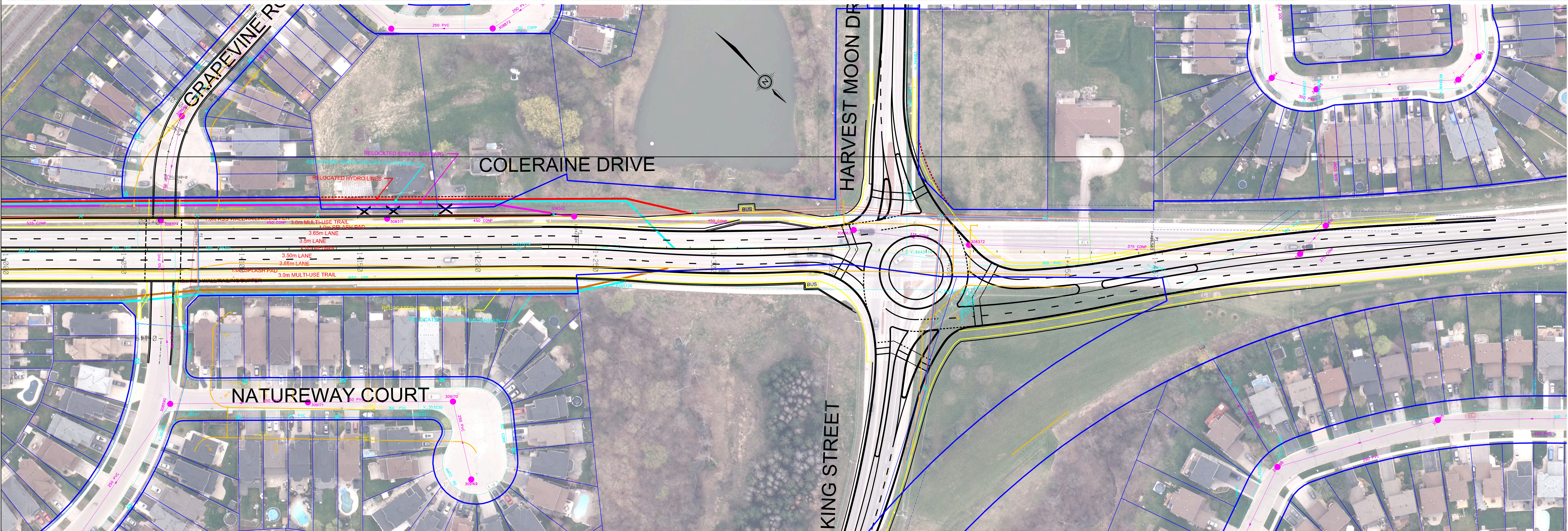
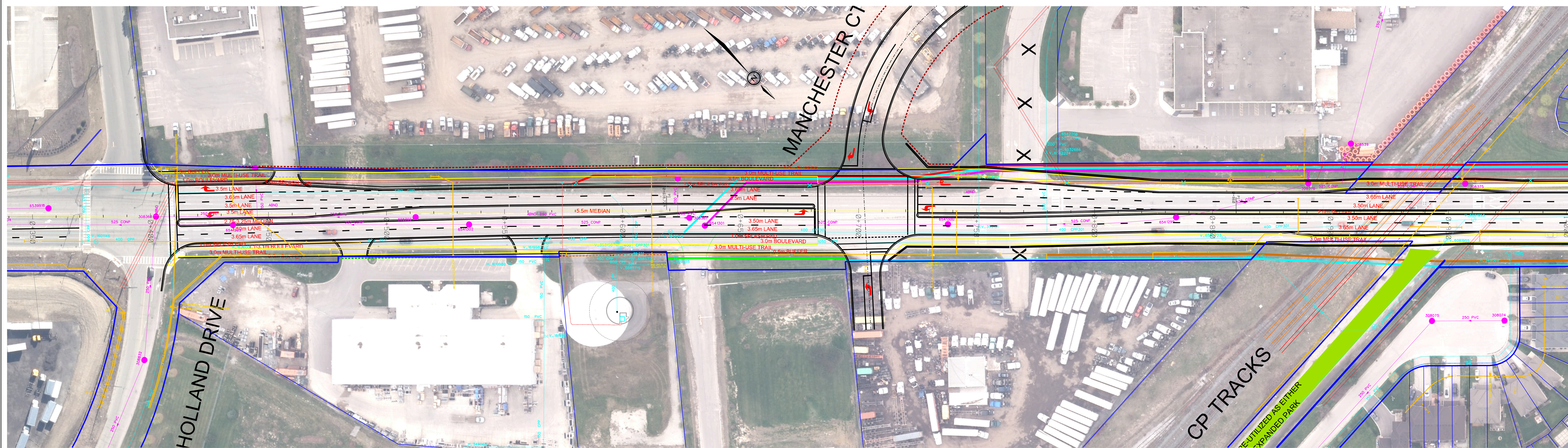
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COLERAINE DRIVE
(HOLLAND DRIVE TO HARVEST MOON DRIVE)





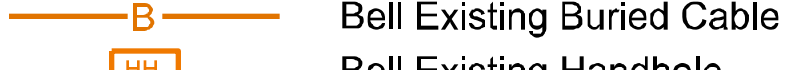




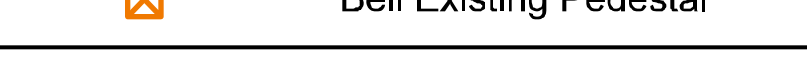
ROAD OVER RAIL OPTION
UTILITY PLAN

- | | | | |
|-----------------------|------------------|-----------------------------------|----------------------------|
| PROPOSED O/H HYDRO | EX. O/H HYDRO | Rogers Ex. Abandoned Buried Cable | Bell Existing Conduit |
| PROPOSED SANITARY | EX. SANITARY | Rogers Ex. Buried Coaxial Cable | Bell Existing Buried Cable |
| PROPOSED BURIED CABLE | EX. BURIED CABLE | Rogers Ex. Aerial Coaxial Cable | Bell Existing Handhole |
| PROPOSED WATER MAIN | EX. WATER MAIN | Rogers Ex. Buried Fibre Cable | Bell Existing Pedestal |
| | | Rogers Ex. Aerial Fibre Cable | |



COLERAINE DRIVE
(HOLLAND DRIVE TO HARVEST MOON DRIVE)

ROAD OVER RAIL OPTION
UTILITY PLAN

- | | | | |
|---|--|---|--|
|  PROPOSED O/H HYDRO |  EX. O/H HYDRO |  Rogers Ex. Abandoned Buried Cable |  Bell Existing Conduit |
|  PROPOSED SANITARY |  EX. SANITARY |  Rogers Ex. Buried Coaxial Cable |  Bell Existing Buried Cable |
|  PROPOSED BURIED CABLE |  EX. BURIED CABLE |  Rogers Ex. Aerial Coaxial Cable |  Bell Existing Handhole |
|  PROPOSED WATER MAIN |  EX. WATER MAIN |  Rogers Ex. Buried Fibre Cable |  Bell Existing Pedestal |
| | |  Rogers Ex. Aerial Fibre Cable | |

Kate Barclay

From: mark-ups <Mark-Ups@enbridge.com>
Sent: February 8, 2022 6:33 AM
To: David Hiatt
Subject: Re: EGD 34464010 - B738 Coleraine Drive CP Grade Separation EA - Utilities - Enbridge- General Location
Attachments: B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02 with Image.pdf; B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02.pdf; Cover EGD 34464010.pdf; Third Party Requirements in the Vicinity of Natural Gas ST-1E-30A8-8E30 V1.1.1.pdf

EXTERNAL EMAIL

Hello,

Attached is the information you had requested.

The information provided is for General Location Only. You must resubmit your detailed design for our review.

Should you require anything further please contact us at Mark-Ups@enbridge.com.

Kind Regards,

Kishore Sagar

From: mark-ups
Sent: Friday, February 4, 2022 6:00 PM
To: David Hiatt
Subject: RE: WO 34464010 - B738 Coleraine Drive CP Grade Separation EA - Utilities - Enbridge

Thank you for submitting your mark-up request. It has been assigned the following **EGD Number: 34464010**

Please reference this number on all status inquiries.

We are currently experiencing a very high number of submissions and may not be able to meet our normal 15 business day turnaround time for electronic submissions and 20 business day turnaround time for hard-copy submissions, especially for large or complex projects.

All emails should be forwarded to Mark-Ups@enbridge.com.



From: David Hiatt <David.Hiatt@cima.ca>
Sent: Tuesday, February 1, 2022 11:43 AM
To: Meetpal Chhina <Meetpal.Chhina@enbridge.com>
Cc: mark-ups <Mark-Ups@enbridge.com>; Stephen Keen <Stephen.Keen@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Subject: [External] WO 34464010 - B738 Coleraine Drive CP Grade Separation EA - Utilities - Enbridge

CAUTION: EXTERNAL EMAIL

This email originated from outside Enbridge and could be a phish. Criminals can pretend to be anyone. Do not interact with the email unless you are 100% certain it is legitimate. Report any suspicious emails.

Hi Meetpal,

The Region of Peel, supported by CIMA+, are completing a Municipal Class EA Schedule 'C' study for a Coleraine Drive Grade Separation, in Bolton. You would have previously received correspondence on the study, most recently regarding the PIC #2 that occurred at the end of last year. More information, including the PIC materials, can be found on the project website here: [Coleraine Drive grade separation Environmental assessment - Region of Peel \(peelregion.ca\)](https://www.peelregion.ca/coleraine-drive-grade-separation-environmental-assessment)

Part of this preliminary design study is the identification of utility impacts and options for potential mitigation (i.e., relocation). Please see attached utility plans, with and without imagery. We have yet to receive confirmation of any infrastructure within the study area and while infrastructure along the local roads would largely be unimpacted, any infrastructure along Coleraine Drive is likely to be impacted and would require relocation. This is mostly due to the large construction required – large fill and retaining walls will be used along Coleraine Drive to facilitate the raise over the CP rail lines (as seen in the attached cross-section). Also as detailed in the cross-section, there will be a buffer between the retaining wall and ROW boundary along Coleraine Drive, and it is within this area that we are proposing impacted utilities be relocated.

The impacts, final design and final relocation plans/agreements will be confirmed in Detailed Design, however we are reaching out to you at this time to confirm:

- That there is no Enbridge infrastructure along Coleraine Drive, or in the study area extents detailed in the attached. If there is, please provide data in PDF or CAD (preferred).
- If there is infrastructure, do you have any objection to the strategy of potentially relocating the impacted infrastructure on Coleraine Drive to the 'buffer' outside of the walls.
- If Enbridge are planning any future infrastructure improvements or future new infrastructure along Coleraine Drive

If you have any questions, please let us know.

If I can kindly have a response by February 9th, it will be greatly appreciated.

Many Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

EGD File Number:

Re:

- By law utility locations must be obtained prior to starting any excavation or ground disturbance activity, such as pile driving, boring, auguring or digging. Contact Ontario One Call at 1-800-400-2255 or www.on1call.com at least 5 business days before beginning work to obtain utility locates.
- Refer to the “Third Party Requirements in the Vicinity of Natural Gas Facilities” for requirements and precautions for working safety in the vicinity of natural gas pipelines. The most recent version of this document is available at: <https://enbridgegas.com/~media/extranet-pages/safety/before-you-dig/Third-Party-Requirements-in-the-Vicinity-of-Natural-Gas-Facilities>
- Enbridge’s responses are based on the information available and are valid for a period of 6 months from issue.

VITAL MAIN

- You are working within 3.0m of a Vital Main Pipeline. In order to accommodate Enbridge vital main standby requirements, our Damage Prevention department must be contacted a minimum of three business days prior to commencing any excavation at 1-866-922-3622 to schedule a site meeting.

CONFLICT

- We have an **OBJECTION** to your proposed plant as indicated. Please refer to attached drawings for information on our existing or proposed gas plant our existing and/or proposed gas plant within the road allowance.
- You must submit a revised design for our approval that meets the requirements detailed in the Third-Party Requirements guidelines before proceeding.
- If relocation of our plant is required, please contact:

Toronto Region	Janice Page	(416) 495-5373	janice.page@enbridge.com
Central Region West	Meetpal Chhina	(905) 458-2159	Meetpal.Chhina@enbridge.com
Central Region East	Ashu Kahol	(905) 927-3017	ashu.kahol@enbridge.com
Ottawa Region	James Arbuthnott	(613) 748-6840	james.arbuthnott@enbridge.com
Niagara Region	Rob D’Onofrio	(905) 641-4876	robert.donofrio@enbridge.com

EASEMENT REQUIRED

- Enbridge has no objection to this proposed application. Prior to Land Sale/Closure, we require an easement to protect our existing gas infrastructure. Please contact Charlene Parent at (416) 753-6641 or email charlene.parent@enbridge.com

















NO CONFLICT

- We have **NO OBJECTION** to your proposed plant as indicated. Please refer to the attached drawings for information on our existing and/or proposed gas plant within the road allowance.
- GAS MAINS MUST BE FIELD LOCATED. Before digging, please call ONTARIO ONE CALL at 1-800-400-2255 for free gas locates.

GENERAL LOCATION

- The information provided is for GENERAL LOCATION only and is not an approval. Detailed plans must be submitted for our review before an approval will be granted.
- Refer to the attached drawings for information on our existing and/or proposed gas plant within the road allowance.

Atlas Plate Legend

	Proposed
	Construct
	Pending Decomission
	Active, Low Pressure
	Active, Medium Pressure
	Active, Intermediate Pressure
	Active, High Pressure
	Active, High Pressure PE
	Active, Extra High Pressure
	Active, Unodourized Transmission Pressure, IMP;VITAL
	Proposed, IMP; NEB;VITAL
	Construct, IMP;VITAL
	Active, Intermediate Pressure, IMP;NEB
	Active, High Pressure, NEB; VITAL
	Active, Extra High Pressure, IMP;NEB; VITAL
	Pending Decomission, IMP;NEB;VITAL

Line Pressure Descriptors

LP	Low pressure
MP	Medium Pressure
IMP	Integrity Management Program (main)
HP	High pressure
XHP	Extra high pressure
VITAL	Vital Gas main
IP	Intermediate Pressure

Abbreviations

Pipe Descriptors

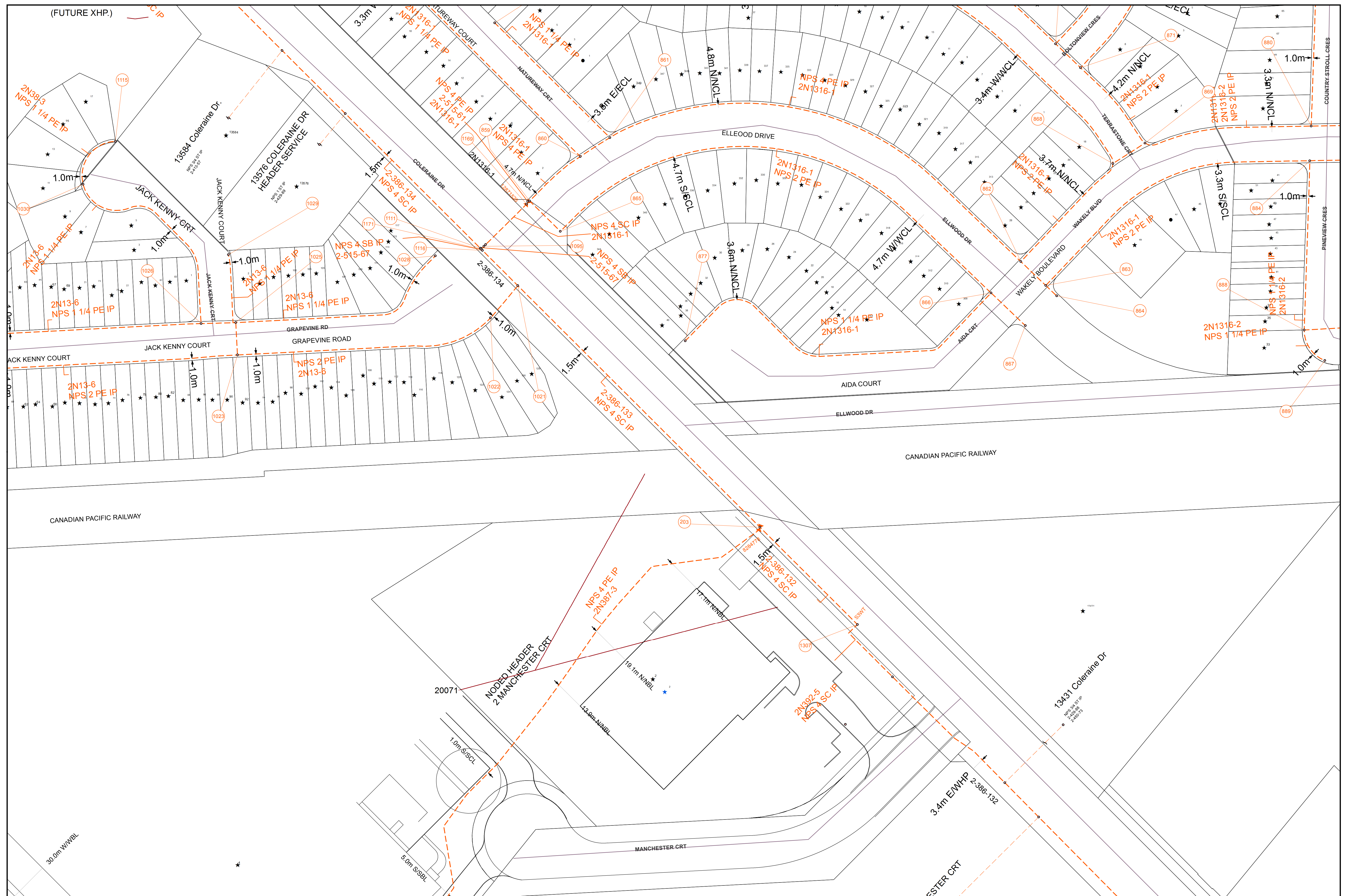
PS	Plastic main
SC	Steel coated main
ST	Steel main
NEB	National Energy Board
NPS	Nominal Pipe Size (<i>Number denotes pipe size in inches e.g. NPS 8 = Nominal Pipe Size 8 inches</i>)

Topographical Feature Descriptors:

AE	Asphalt edge
BL	Building Line
BP	Bell pole/pedestal
BPED	Bell pedestal
C/L	Centreline of road way/pavement
CB	Catch Basin
CL	Curb line/curb or edge of pavement
DCB	Double catch basin
E/P	Edge of Pavement
E/W SL	East of the west street line
FH	Fire Hydrant
FL	Fence line
HP	Hydro pole
HT	Hydro transformer or hydro box
LP	Light post
LS	Light Standard
MH	Manhole
N/S SL	North of the south street line
POLE L	Pole line
POLE(S)	Pole(s)
S/S SL	South of the south street line
SL	Street line or property line
SM	Sewer manhole
SWK	Sidewalk or walk
UW	Under sidewalk
W/E SL	West of the east street line
WVCH	Water valve chamber

Main Reference Descriptors

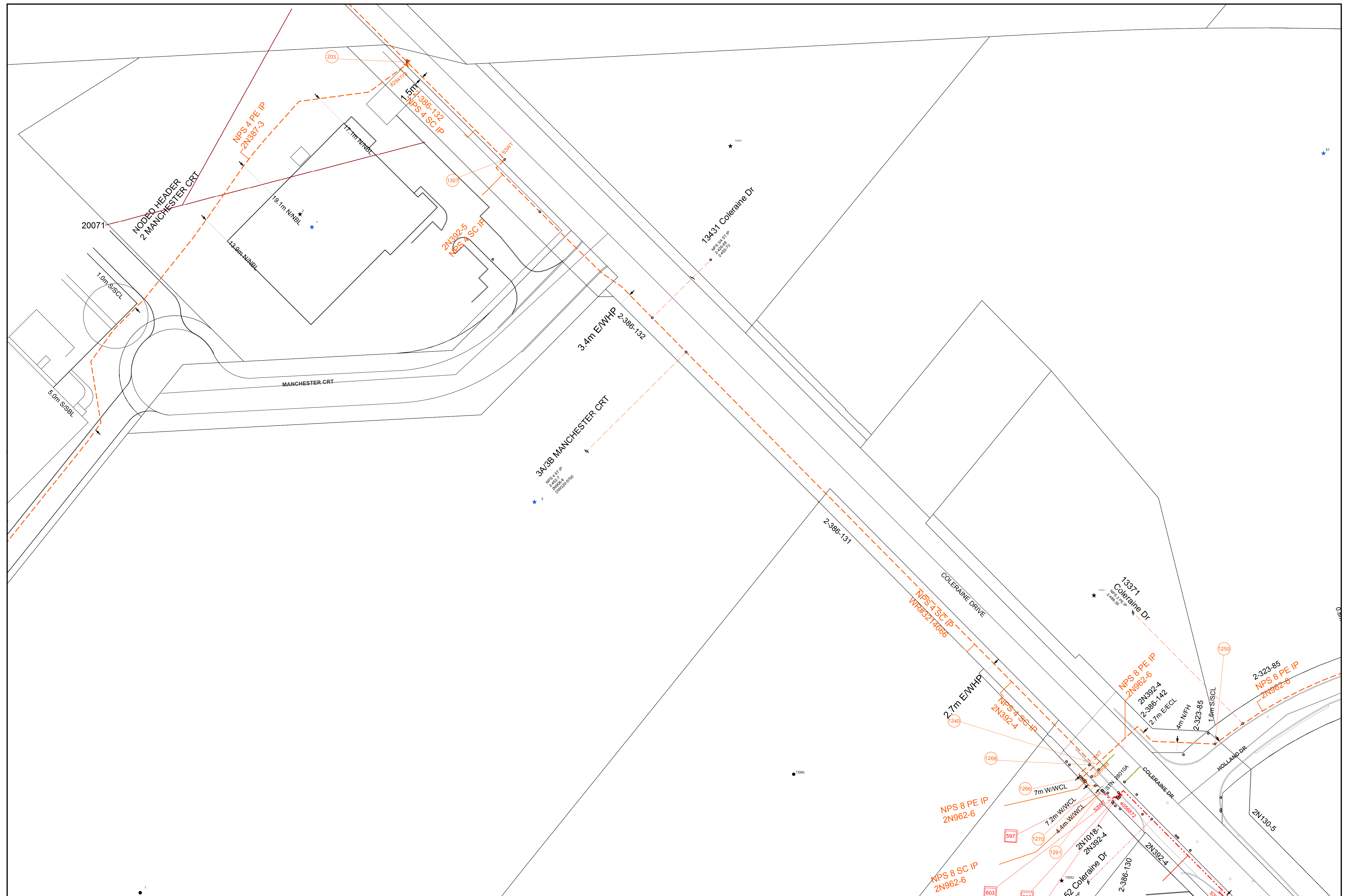
N/S SL	North of the south street line
S/S SL	South of the south street line
W/E SL	West of the west street line
E/W SL	East of the west street line
N/S CL	North of the south curb line
S/N CL	South of the north curb line
E/W CL	East of the west curb line
W/E CL	West of the east curb line
E/E CL	East of the east curb line
W/W CL	West of the west curb line
N/N CL	North of the north curb line
S/S CL	South of the south curb line
E/P	Edge of pavement
C/L	Centre line of road
W/WBL	West of west building line
W/EBL	West of east building line
E/EBL	East of east building line
N/NBL	North of north buliding line
N/SBL	North of south buliding line
S/SBL	South of south buliding line
S/NBL	South of north building line
Inside	Measured from centreline of street furniture towards streetline
Outside	Measured from centreline of street furniture towards road



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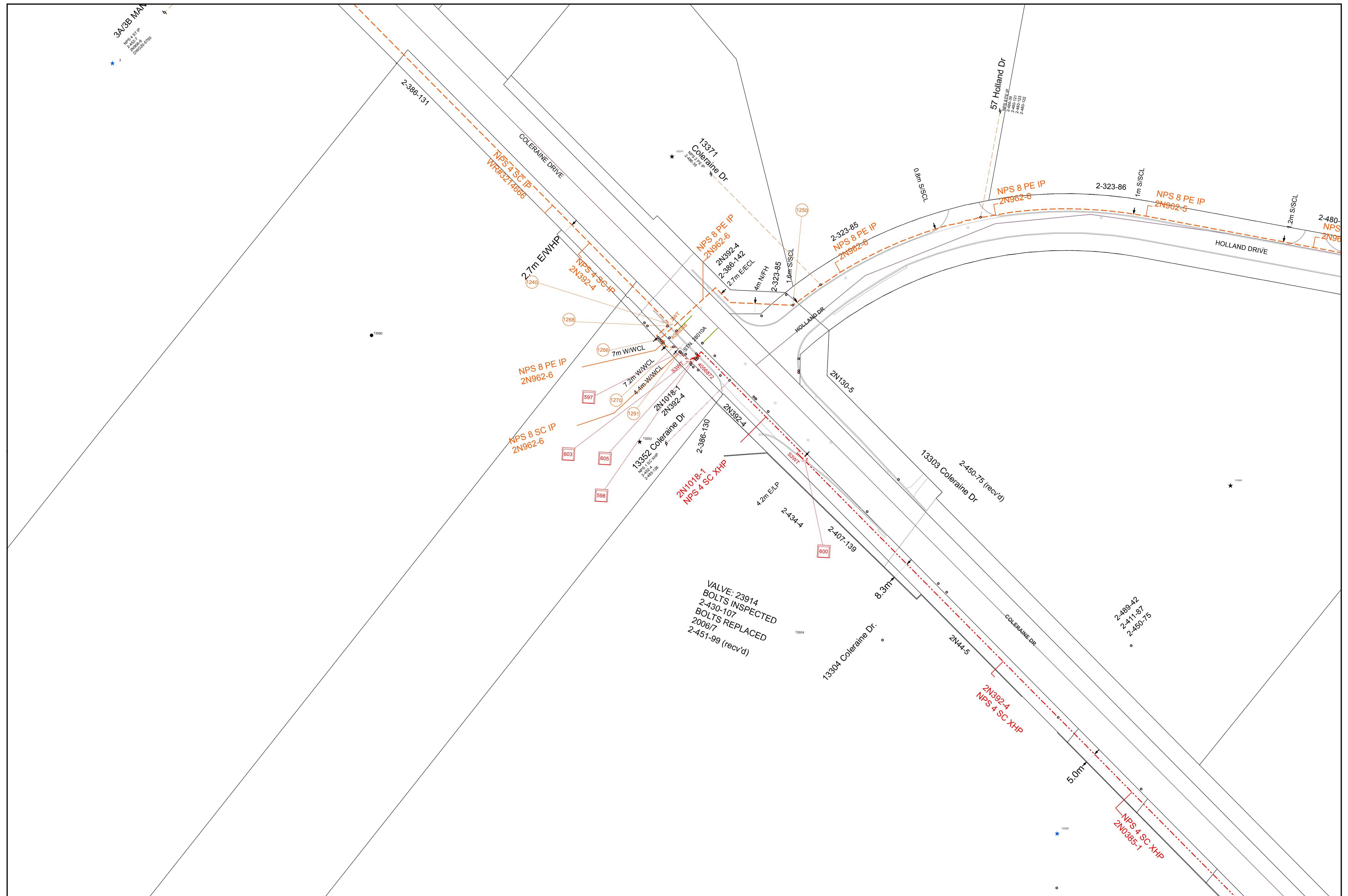
Note : Map is not to scale.



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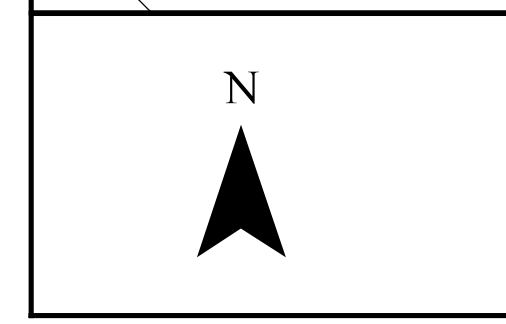
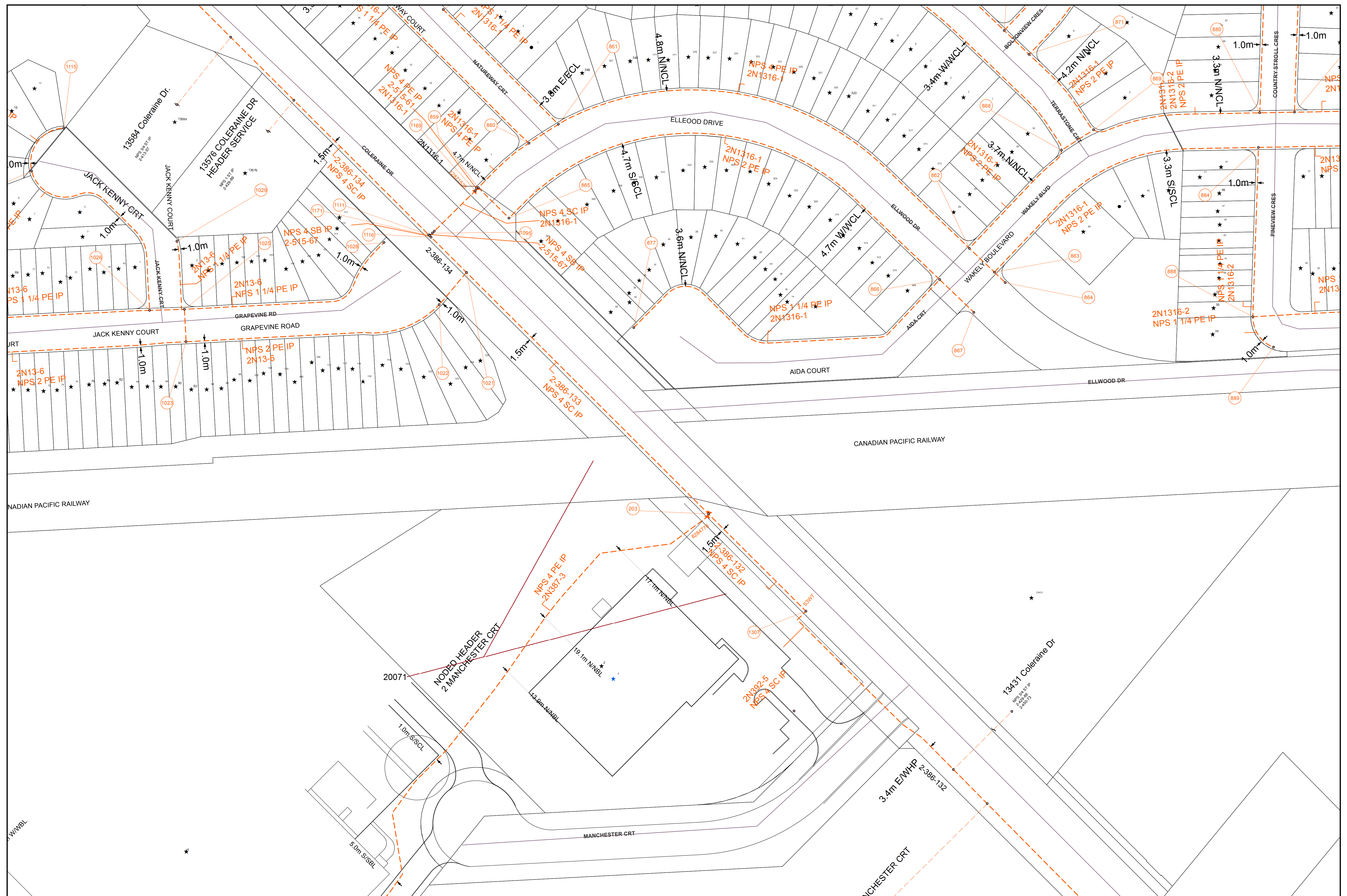
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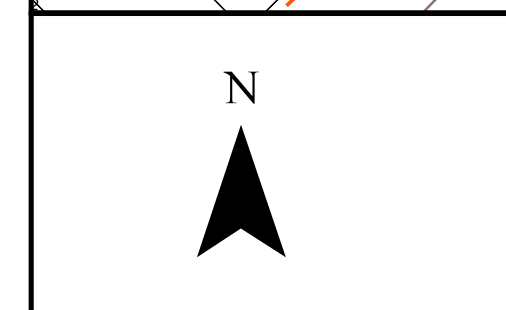
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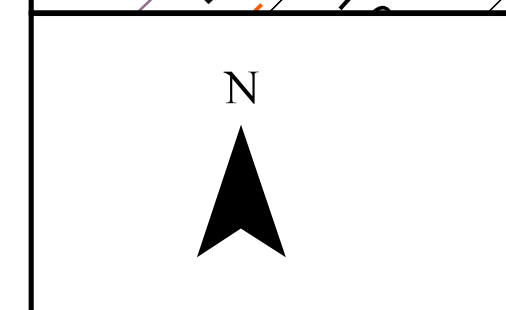
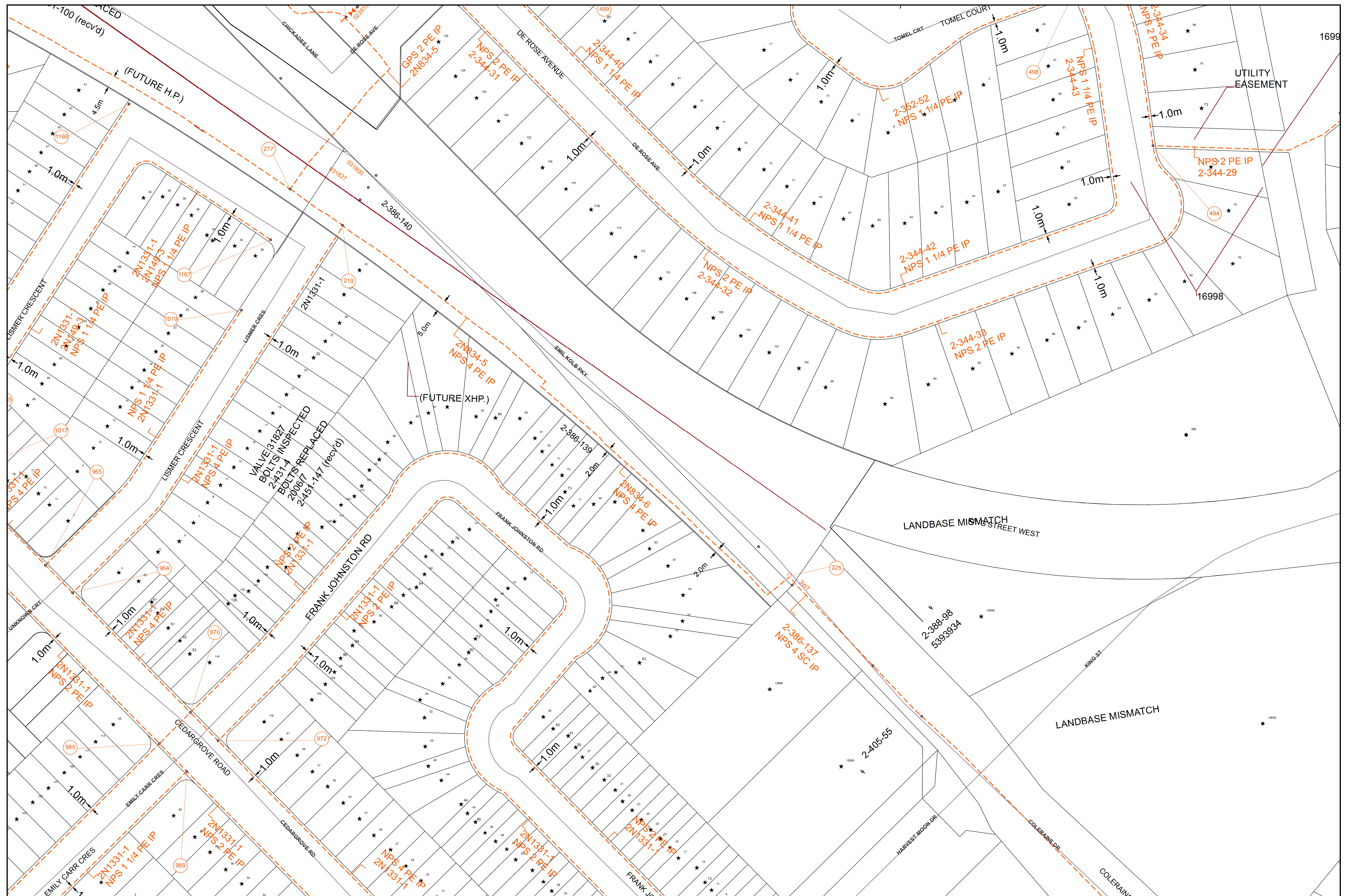
Note : Map is not to scale.



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Note : Map is not to scale.



Plotted By: Katte Raja Reddy

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Note : Map is not to scale.



Third-Party Requirements in the Vicinity of Natural Gas Facilities Standard

STANDARD

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Third-Party Requirements in the Vicinity of Natural Gas Facilities Standard

1 Introduction

This document is intended for anyone involved in planning or carrying out work in the vicinity of Enbridge Gas Distribution and Storage's (GDS) network. It summarizes the requirements to be followed and specifies the technical requirements aimed at protecting GDS's facilities, and by extension, ensuring public and worker safety.

Within this document, "third party" refers to an individual or organization that is not employed by, or performing work under, contract to GDS. These requirements are applicable to work done by individuals such as homeowners, landowners, other utility companies, excavators, constructors, and contractors.

Third parties must follow the regulations and legislation applicable to their work in addition to these requirements. It is understood that all legal provisions applicable to work carried out around natural gas facilities take precedence over this document.

The terms "gas lines", "gas pipelines", and "mains" used throughout this document apply equally to natural gas mains and service lines, as well as any other component of GDS's natural gas systems found on public or private land.

All work in the vicinity of gas facilities must adhere to the requirements set forth in this document. Work includes, but is not limited to, any ground disturbance in the vicinity of facilities or equipment crossing. Ground disturbance includes, but is not limited to, activities associated with excavation, directional drilling, blasting, piling, compaction, boring, ploughing, grading, backfilling, and hand digging.

A locate of the facilities must be requested at least five business days prior to beginning any work. Locates are required before ground disturbance takes place.

2 Terms and Definitions

The following is a list of terms found in this document and their definitions.

applicant: The owner of the proposed work.

blaster: The person or persons responsible for setting the charges and performing the blast.

blasting, surface: An operation involving the excavation of rock foundations for various types of structures, grade construction for highways or railroads, or canals (trenches) for water supply or collection purposes.

blasting, tunnel: Operations involving the piercing of below-ground (generally horizontal) opening in rock.

compaction: Any vibration-generating operation that will result in a potential increase of the density of soils or controlled backfill materials. The means to increase the density may be static or dynamic.

constructor: A person who undertakes a project for an owner and includes an owner who undertakes all or part of a project by himself or by more than one employer (as defined by Occupational Health & Safety Act).

contractor or excavator: Any individual, partnership, corporation, public agency, or other entity that intends to dig, bore, trench, grade, excavate, hammer into, or break ground with mechanical equipment or explosives in the vicinity of a gas pipeline or related facility.

EGI: Enbridge Gas Inc.

facility: Any Enbridge Gas Distribution, Transmission, Storage pipeline, main, service, regulator station or storage facility and its related components.

Gas Distribution and Storage (GDS): Enbridge Gas Distribution and Storage, Gazifère Inc., Niagara Gas Transmissions Limited, 2193914 Canada Limited.

ground disturbance: Any work, operation, or activity on or under the existing surface resulting in a disturbance or displacement of the soil or ground cover. Ground disturbance can include, but is not limited to: activities associated with excavation, directional drilling, blasting, piling, compaction, boring, ploughing, grading, backfilling, and hand digging.

hand dig: To excavate using either a shovel with a wooden or fiberglass handle, or using hydro vacuum excavation equipment. The use of picks, bars, stakes, or other earth piercing devices are not considered hand digging.

independent engineering consultant: A professional engineer who is registered with the provincial or state professional engineering association and a holder of a certificate of authorization (C of A).

locate service provider: Any entity that performs locates under the terms of a locate service agreement.

pile: Any vertical or slightly slanted structural member introduced or constructed in the soil in order to transmit loads and forces from the superstructure to the subsoil; the structural member can also be used as a component of a retaining wall system.

pile driving: The placement of piles carried out by gravity hammer, vibratory hammer, auger, pressing, screwing, or any combinations of the above methods.

positive identification: Visually locating (daylighting, exposing, digging test holes to determine) the location, depth, and size of a below-grade facility by using either vacuum excavating or hand digging. This includes elevation or alignment changes that can alter the depth or direction of the pipe (e.g., 45° and 90° elbows, fittings, plugs, weldolets, flanges, branch piping, known abandoned facilities, etc.).

pre-Engineering review: A process by which third parties can request a pre-engineering review for any potential conflict analysis.

professional engineer: An engineer registered and licensed with the provincial professional engineering association in the jurisdiction in which the engineer is practicing.

rural: All areas outside urban areas.

temporary support: The support of gas pipelines before or during an excavation to protect the pipeline from its own weight and to minimize deflection stresses.

third party: An individual or organization that is not employed by or performing work under contract to GDS (e.g., homeowners, other utility companies, contractor, excavators, constructors, etc.).

urban: An area with a population of at least 1,000 and a density of 400 or more people per square kilometer.

vital pipeline: A subset of pipelines that are critical to the safe and reliable operation of the natural gas system. Damages to vital mains could result in significant negative impact to public and worker safety or significant customer outages. This subset of mains consists of CER-regulated (Canada Energy Regulator) pipelines, transmission pipelines, and select distribution pipelines.

3 General Requirements

3.1 CER-Regulated Pipelines and Vital Pipelines

The CER regulates natural gas, oil, and commodity pipelines that extend beyond provincial, territorial, or national boundaries. All work in the prescribed area (within 30 m [100 ft] from each side of the CER-regulated pipeline) must be reviewed by the applicable CER-regulated operating company prior to commencing. This review is a regulatory requirement of the CER.

Mains are designated as vital pipelines by GDS. These include, but are not limited to, any pipeline NPS 16 or larger, transmission pipelines, CER-regulated pipelines, all pipelines operated by Storage and Transmission Operations (STO), and select distribution pipelines. The designation of a vital pipeline may change at the discretion of GDS. Vital Pipelines will be identified through locates. In these requirements, special considerations for CER-regulated pipelines and vital pipelines will be highlighted.

All work within 5 m (16 ft) from either side of lines operated by STO must be approved by GDS prior to commencing. For all other vital pipelines, all ground disturbance work within 3 m (10 ft) from either side of the vital pipeline must be approved by GDS prior to commencing. Approval by GDS may include specific conditions that third parties must follow. GDS may require representation on site for any ground disturbance work within the vicinity of vital pipelines and CER-regulated pipelines.

3.2 When Observation Is Required

A GDS representative is required to be on site to ensure the excavation or third-party activity is being safely completed near a pipeline when:

- Excavation with mechanical equipment will occur within 5 m (16 ft) of CER-regulated pipelines and all lines operated by STO.
- Excavation with mechanical equipment may take place within 3 m (10 ft) of vital pipelines and pipeline segments.
Once the pipeline is exposed, mechanical excavation is then permitted up to 1 m (3.3 ft) from the pipeline.
- It is anticipated that blasting will take place within 30 m (100 ft) of any pipeline.
- Any other situations which requires observation, as deemed necessary by EGI.

3.3 Safe Excavation

Mechanical excavation is not permitted within 5 m (16 ft) of CER-regulated pipelines and 3 m (10 ft) of vital pipelines, unless verified visually. After the exact location of the main is verified visually, mechanical excavation is allowed up to 1.0 m (3.3 ft) from the pipeline. Within 1 m (3.3 ft) of the CER-regulated or vital pipeline, only hand digging or hydro-excavation is allowed.

Mechanical excavation may not begin within 3 m (10 ft) of the pipe until:

- The pipe has been exposed by the excavator, under the supervision of GDS, by hand at the point of crossing, or the pipeline company has located the pipe and confirmed that it is at least 0.6 m deeper than the proposed excavation.
- The excavation is parallel, or the pipe has been exposed by hand to confirm the location of the pipe.

For all non-vital pipelines, mechanical excavation is not allowed within 1 m (3.3 ft) of the locate marks of the pipeline, until the exact location of the pipeline has been visually verified. The excavator must expose the pipeline by hand digging or hydro-excavation. Once the pipeline is exposed, mechanical excavation is then permitted up to 0.3 m (1 ft) from the pipeline. Within 0.3 m (1 ft) of any pipeline, only hand digging or hydro-excavation is permitted.

Only handheld compaction equipment may be used within 1 m (3.3 ft) of the sides or top of all gas pipelines. When ground conditions make hand excavation impractical (e.g., frost), the pipeline company may permit excavation to within 1 m (3.3 ft) of the pipeline if the pipeline company considers it safe to do so and directly supervises the excavation.

Spoil from excavation must not be piled on the pipeline or its easement.

3.4 Minimum Cover Requirements

[Table 3-1: Minimum Cover Requirements on page 8](#) defines mains and services cover requirements. In all cases where the depth of cover requirements cannot be met, contact GDS to review depth of the cover requirements.

Table 3-1: Minimum Cover Requirements

Pipeline	Location	Minum Cover m (ft)
Mains	Under traveled surfaces (roads), road crossings	1.2 m (4 ft)
	Right-of-ways	1 m (3.3 ft)
	Highways	1.5 m (5 ft)
	Water crossings, and below drainage and irrigation ditches	1.2 m (4 ft)
Services	Private property	0.5 m (1.6 ft)
	Road crossings	0.9 m (2.9 ft)

3.5 Points of Thrust

Additional precautions may need to be taken when working in the vicinity of points of thrust. Points of thrust occur at pipeline fittings such as elbows (45° or 90°), end caps, weld tees, reducers, closed valves, and reduced port valves. If a point of thrust is identified through the locate process, GDS may require additional time to review the proposed work area. In the event that the excavation involves exposing a point of thrust or exposing an area near a point of thrust, GDS may provide written specific instructions that are to be followed. Failure to follow these instructions can result in significant harm to persons, property, or the environment.

3.6 Repair of Damaged Pipe and Pipe Coating

In all cases where the pipeline or the pipeline coating is damaged by construction activities, GDS must be contacted immediately and the excavation left open until GDS personnel have made the necessary repairs.

3.7 Encroachment

Permanent awnings and roof structures are prohibited above GDS's facilities within public rights-of-way or GDS's rights-of-way. GDS will not accept responsibility for any damages resulting from maintenance or operation of its facilities to encroaching structures within the public or GDS rights-of-way. Examples of encroaching structures include: bus shelters, street benches, and garbage bins.

GDS requires approval for all permanent structures to be built within 7 m (22.9 ft) of GDS's vital pipelines. This requirement is in place to allow GDS sufficient access and working space should an inspection or repair be needed.

3.8 Tree Planting

When planting trees, the gas pipeline in and near the area of excavation must be located to ensure enough clearance is maintained between the pipeline and the tree.

For all vital pipelines (including CER and transmission pipelines), trees or large shrubs must maintain a horizontal clearance between the edge of the root ball or open bottom container to the adjacent edge of the existing pipelines of not less than 3.0 m (10 ft), or as specified in any applicable easement agreement.

For all other pipelines, a minimum horizontal clearance of 1.2 m (4 ft) is recommended between the edge of the root ball or open bottom container and adjacent edge of the existing gas pipeline.

In cases where the recommended clearance cannot be achieved, GDS may specify the installation of a root deflector.

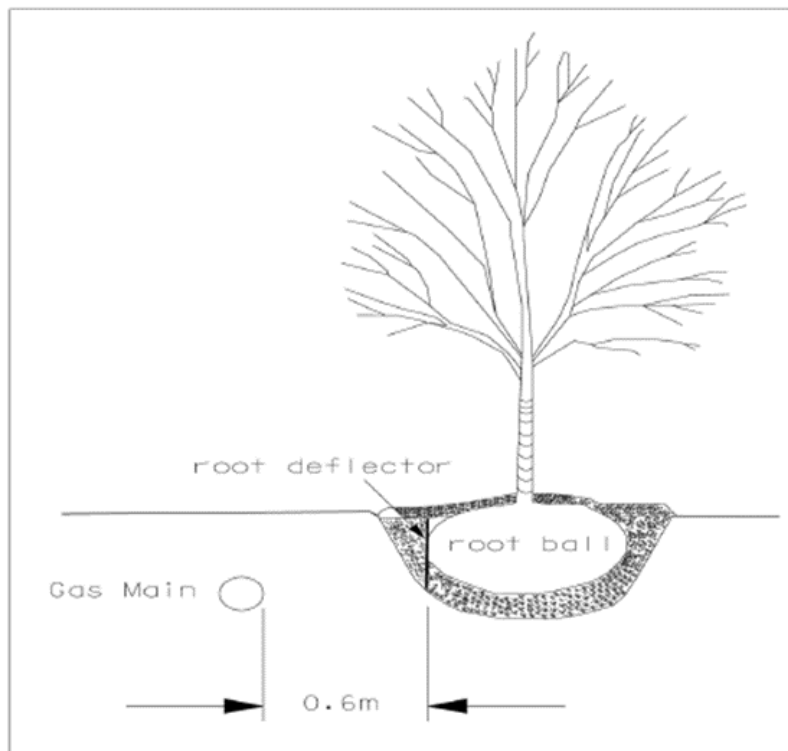
3.8.1 Root Deflectors

A root deflector is a physical barrier placed between tree roots and pipelines to prevent damage to the pipelines. A root deflector can be made from 1/4 in thick rigid plastic, fiberglass, or other non-degradable material. The root deflector is intended to prevent the root tips from attaching to the gas main.

Typically, root deflectors are straight barriers or encircle the tree. If installed as a straight barrier, the root deflector should be installed at a minimum 0.6 m (2 ft) from the pipeline on the tree-side of the pipeline. Also, it should extend parallel to the pipeline in both directions for 1.2 m (4 ft) measured from the centre of the tree trunk.

Root deflectors usually have a collar to keep the top of the deflector at ground level, and extend down to the bottom of the root-ball as shown in [Figure 1: Example of a Root Deflector](#).

Figure 3-1: Example of a Root Deflector



3.9 Sewer and Drain Cleaning

Prior to sewer clearing activity using mechanical cutting or high pressure jetting equipment, the third party should call into [Ontario One Call](#) at 1-800-400-2255 for a

cross bore sewer safety inspection. An EGI employee or contractor will attempt to attend the site within two hours to complete the inspection.

4 Minimum Clearance from Other Structures

The following clearances must be maintained between the circumference of the gas pipeline and other underground structures:

Table 4-1: Minimum Clearance Between Gas Pipelines (Less than NPS 16) and Other Underground Structures

Direction	Minimum Clearance m (ft)
Horizontal	0.6 m (2 ft)
Vertical	0.3 m (1 ft)

Table 4-2: Minimum Clearance Between CER-regulated Pipelines and Vital Pipelines and Other Underground Structures

Direction	Minimum Clearance m (ft)
Horizontal	1 m (3.3 ft)
Vertical	0.6 m (2 ft)

Additional clearance or mitigation may be required for installations (such as transit systems or power transformers) that will introduce DC stray current interference or AC fault hazards.

Note



For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required.

5 Pipeline Location Verification

5.1 Surface Road Work

Surface road work applies to ground disturbance on travelled roadways related to the removal of hard-surfaces only. For any ground disturbance work, locates must be obtained prior to commencing and the excavator must ensure accuracy of the locate by reviewing the locate paperwork with the physical locate markings. Surface road work can be completed without the requirement to positively identify EGI pipelines, provided no mechanical equipment will be used within 1 m (3.3 ft) horizontally of the located pipelines. If mechanical excavation is required within 1 m (3.3 ft) of the locate during any surface road work or work that will take place deeper than removal of the hard surface, the excavator must follow rules outlined in [5.2 Subgrade Road Work on page 11](#) for positive identification requirements.

5.2 Subgrade Road Work

Subgrade road work is any road work exceeding the depth required for removal of the hard surface that enters the sub-surface. The boundary area for the pipeline is the distance that is identified off the locate marks of the pipeline and applicable boundary areas are highlighted in [Table 5-1: Boundary Areas on page 11](#).

Table 5-1: Boundary Areas

Pipeline	Boundary Area
Vital pipelines (\geq NPS 24)	3 m (10 ft)
Vital pipelines ($<$ NPS 24)	2 m (6 ft)
Non-vital pipelines (all sizes)	1 m (3 ft)

Note



Work within the boundary areas must comply with the positive identification requirements set in [Table 8-2: Pipeline Location Verification Requirements for Vital Pipelines on page 22](#) and [Table 8-3: Pipeline Location Verification Requirements for All Other Pipelines on page 22](#).

If these guidelines cannot be complied with, the excavator must submit a variance request work package. No variance will be provided for work within 1 m (3.3 ft) of any pipeline. The variance work package must include, at a minimum, the following information:

- Pre-Engineering design.
- Location of EGI facilities with respect to proposed excavation area (vertical and horizontal offsets).
- Location of proposed excavation area (vertical and horizontal offsets off permanent landmarks).
- Pipeline protection plan.

If a variance is requested, the excavator must also provide a physical barrier (e.g., silt fence), which would denote the boundary of the pipeline, where possible.

[8.2 Drilling Parallel to Pipelines on page 21](#) and [Table 8-3: Pipeline Location Verification Requirements for All Other Pipelines on page 22](#) indicate GDS's minimum requirements for the verification of the pipeline location based on the nature of the work. The frequency and location of test holes may change at the

discretion of GDS. Additional test holes may be required to sufficiently confirm the location of the pipeline (e.g., regulator stations).



Note

Non-mechanical equipment must be used when working within 1 m (3.3 ft) of any pipeline. If mechanical equipment is required for use around non-vitals, the pipeline must be positively identified using hand tools or hydro-excavation. Once the non-vital pipeline location has been visually identified through positive identification requirements listed in the [8.2 Drilling Parallel to Pipelines on page 21](#) and [Table 8-3: Pipeline Location Verification Requirements for All Other Pipelines on page 22](#), mechanical equipment can be used up to 0.3 m (1 ft) of the non-vital pipeline and 1 m (3.3 ft) of a vital pipeline.

When using hydro-vacuum excavation as an alternative to hand digging, see [9 Hydro-Excavation on page 25](#) for safe operating practices.

6 Operation of Heavy Equipment

6.1 General

Additional precautions are necessary when equipment in excess of the weights listed in [Table 5: Vehicle Load Restrictions](#) is operated in the vicinity of buried facilities where no pavement exists or where grading operations are taking place.

Table 6-1: Vehicle Load Restrictions

Pipe Material	Weight/Axle Maximum Allowable Load kg (lb)
Plastic	7,000 kg (15,400 lb)
Steel	10,000 kg (22,046 lb)

Prior to any crossing, the location of the gas main must first be staked out by a GDS representative.

The excavator is responsible for confirming the location and depth of the main. Test hole spacing must not exceed 50 m (160 ft).

6.2 Equipment Moving Across the Pipeline

Crossing locations for heavy equipment must be kept to a minimum.

The crossing locations must be determined by GDS after reviewing:

- The nature of the construction operation.
- The types and number of equipment involved.
- The line and depth of the existing gas main.

The use of equipment is contingent upon the review by GDS. Once the crossing locations have been established, heavy equipment is restricted to crossing at these

locations only. It is the responsibility of the third party to inform their personnel of the crossing location restrictions.

Pipelines may require additional protection at crossing locations by constructing berms or installing steel plates over the pipeline.

Unless expressly allowed by the temporary crossing consent, equipment that crosses pipelines must be subject to the following conditions:

- The numbers of crossings back and forth must be kept to a minimum.
- Equipment must not remain stationary on top of a pipeline.
- Equipment must not cross with loaded side boom or other unbalanced loads.
- Equipment must cross perpendicular (not parallel) to the pipeline. The crossing angle for installations must be within 45° to 90° (with preference for as close to perpendicular as possible).
- Equipment must operate at slow speeds when crossing a pipeline in order to minimize loading impact.
- Existing cover over a pipeline must not be reduced; any loss of cover (e.g., due to rutting) must be promptly restored prior to crossing.
- Vibratory compaction equipment must not operate within 1.2 m (4 ft) of a pipeline.

6.3 Equipment Moving Along the Pipeline

Heavy equipment can be operated parallel to existing pipelines provided that a minimum offset of both:

- 1 m (3.3 ft) is maintained on pipeline sizes less than NPS 16.
- 2 m (6.6 ft) on pipeline sizes NPS 16 and larger, unless otherwise directed by GDS.

Only lightweight, rubber-tired equipment may be operated directly over the existing gas pipelines, unless a minimum pipe cover of twice the pipe diameter or 1 m (3.3 ft) (whichever is greater) can be verified. The use of all other equipment is contingent upon review and approval by GDS.

Unless expressly allowed by the temporary crossing consent, equipment moving along pipelines is subject to the following conditions:

- Equipment must operate at slow speeds when moving along a pipeline.
- Existing cover over a pipeline must not be reduced; any loss of cover (e.g., due to rutting) must be promptly restored prior to moving along the pipeline.

- Vibratory compaction equipment must not operate within 1.2 m (4 ft) of a pipeline.

Note



When crossing perpendicular to a pipeline that is smaller than NPS 16 (excluding vital pipelines), the vertical clearance outlined in Table 2: Minimum Clearance Between Gas Pipelines (Less than NPS 16) and Other Underground Structures may be used as long as all positive identification requirements are also followed.

Note



When crossing perpendicular to a pipeline that is NPS 16 or larger, or crossing any CER-regulated pipelines or vital pipelines, a minimum vertical clearance of 1 m (3.3 ft) is required; [8 Horizontal Directional Drilling on page 20](#).

7 Support of Gas Pipelines

7.1 General

The support requirements specified in this section are the minimum requirements. GDS must be notified regarding the support of any gas main. GDS has complete discretion in the approval of any support system. Additionally, if a pipeline is to be exposed for longer than one month, approval must be sought from GDS and work must follow the requirements outlined in [3 General Requirements on page 6](#). Third parties must not depart from these support requirements unless a professional engineer working for or on behalf of the third party has designed an alternative method. Any alternative method must be comparable to these specifications and be, in the opinion of the professional engineer, consistent with good engineering practices. The alternative specification must be documented, approved by a professional engineer and provided to GDS for review prior to the commencement of work. The third party is responsible for the adequate support of the buried gas pipelines exposed during excavation according to this section.

Prior to any crossing, the location of the gas main must first be staked out by a GDS representative.

7.2 Support of Gas Pipelines Perpendicular to Excavation

Temporary support refers to the support of gas pipelines prior to or at the time of excavation to protect the pipeline from deflection due to its own weight while it is exposed. Temporary support must remain in place until the backfill material underneath the pipeline is compacted adequately to restore support of the pipeline.

Before trenching beneath a main or service, temporary support must be erected for pipelines if the unsupported span of pipe in the trench exceeds the length indicated in [Table 7-1: Maximum Span without Support Beam on page 15](#).

Note



For pipelines larger than NPS 16, GDS must be contacted. Contact information can be found in the [12 Contact Information on page 32](#).

When temporary support is required, [Table 7-2: Support Beam Sizes and Maximum Span Between Beam Supports on page 15](#) indicates the required beam for a given span. The beam must be a continuous length grade No. 1 Spruce-Pine-Fir (S-P-F) or equivalent. For spans exceeding 4.5 m (15 ft), a continuous length timber beam may not be available. In that case, steel I-beams (or equivalents) can be used as the support beam. Steel beam selection must be certified by a professional engineer and submitted to GDS for review.

Table 7-1: Maximum Span without Support Beam

Pipe Size (NPS)	Steel m (ft)	PE (polyethylene) m (ft)
1/2	2 m (6.6 ft)	1 m (3.3 ft)
3/4 to 1-1/4	2.5 m (8.2 ft)	1.25 m (4.1 ft)
2	3 m (10 ft)	1.5 m (5 ft)
3 to 4	4.5 m (15 ft)	1.75 m (6 ft)
6	6 m (20 ft)	2 m (7 ft)
8	7 m (23 ft)	2 m (7ft)
10	8.5 m (28 ft)	-
12	10 m (33 ft)	-
16	11.5 m (38 ft)	-

Table 7-2: Support Beam Sizes and Maximum Span Between Beam Supports

Pipe Size (NPS)	Steel	Plastic	
	≤ 4.5 m	≤ 2 m	≤ 4.5 m
1/2 to 2	4 × 6	4 × 6	6 × 8
3 to 6	-	6 × 6	8 × 8

Note



In all cases where the support beam size requirements cannot be met, GDS must be contacted to review support beam requirements.

The beam must be placed above the pipe with the ends of the beam resting on firm undisturbed soil. The beam must not bear directly on the gas pipeline. The pipe must be supported from the beam with rope, canvas sling, or equivalent in a manner that will prevent damage to the pipe and coating and eliminate sag. The spacing between the ropes must not exceed 1 m (3.3 ft); see [Figure 7-1: Support of Gas Pipelines Crossing Excavations on page 18](#).

Backfill material underneath the exposed pipeline must be compacted to a minimum of 95% compaction. Sand padding must be placed to a level 150 mm (6 in) below and above the main. For additional details, see [10 Backfilling on page 26](#).

Perform compaction with the loose lift height not exceeding 200 mm (8 in) or one-quarter of the trench width, whichever is less. Injecting water into the backfill beneath the pipe is not an acceptable method of compaction.

All temporary support on pipelines must be removed before backfilling. Adequate support must remain in place until the backfill material has restored support.

7.3 Support of Pipelines Parallel to Excavation

Two cases exist for pipelines parallel to an excavation:

- Trench < 1.2 m deep
- Trench > 1.2 m deep

In either instance, the pipeline must not be exposed unless it is necessary to provide direct support.

Trench wall support may not be required for excavations provided the pipeline meets all of the following criteria:

- Depth is less than 1.2 m (4 ft).
- the pipeline is at least 0.6 m (2 ft) from the edge of the excavation or outside the 45° line projected upward from the trench bottom; see [Figure 7-3: Influence Lines for Gas Pipelines Adjacent to Excavations on page 20](#).
- Soil is stable (type 1 or 2, see [Table 15-1: Soil Types on page 34](#))

If the pipe does not meet these requirements and the soil is soft clay or sand (soil types 3 and 4), then the excavation must be suitably shored to prevent movement of the pipe. The shoring must remain in place until the backfill material has restored support.

Trench wall support is required for excavations if any one of the following conditions exist:

- Depth is \geq 1.2 m (4 ft).
- The pipeline is closer to the edge of the excavation than the minimum allowed distance indicated [Table 7-3: Minimum Allowed Distance from Main to Excavation on page 17](#).
- Depth is < 1.2 m (4 ft) and the soil is unstable (type 3 or 4, see [Table 15-1: Soil Types on page 34](#)).

Note

Adequate support must remain in place until the backfill material has restored support.

Minimum distances from the edge of the trench to the pipeline in which the excavation influences pipelines are shown in [Table 7-3: Minimum Allowed Distance from Main to Excavation on page 17](#). The pipeline must be supported if these minimum distances cannot be met.

Table 7-3: Minimum Allowed Distance from Main to Excavation

Trench Depth (m)	Soil ^a Type 1 and 2	Soil ^a Type 3 and 4
1.2 m (3.9 ft)	0.9 m (3 ft)	0.9 m (3 ft)
1.5 m (4.9 ft)	0.9 m (3 ft)	0.9 m (3 ft)
1.8 m (5.9 ft)	0.9 m (3 ft)	0.9 m (3 ft)
2.1 m (6.9 ft)	0.9 m (3 ft)	0.9 m (3 ft)
2.4 m (7.9 ft)	0.9 m (3 ft)	0.9 m (3 ft)
2.7 m (8.9 ft)	0.9 m (3 ft)	1 m (3.3 ft)
3 m (9.8 ft)	0.9 m (3 ft)	1.5 m (4.9 ft)
3.3 m (10.8 ft)	0.9 m (3 ft)	1.8 m (5.9 ft)
3.6 m (11.8 ft)	0.9 m (3 ft)	2.2 m (7.2 ft)
3.9 m (12.8 ft)	0.9 m (3 ft)	2.5 m (8.2 ft)
4.2 m (13.8 ft)	0.9 m (3 ft)	3 m (9.8 ft)
4.5 m (14.8 ft)	1 m (3.3 ft)	3.4 m (11.2 ft)
4.8 m (15.7 ft)	1.5 m (4.9 ft)	3.8 m (12.5 ft)
5.1 m (16.7 ft)	2 m (6.6 ft)	4.1 m (13.5 ft)
5.4 m (17.7 ft)	2.5 m (8.2 ft)	4.6 m (15.1 ft)
5.7 m (18.7 ft)	3 m (9.8 ft)	5 m (16.4 ft)
6 m (19.7 ft)	3.4 m (11.2 ft)	5.5 m (18 ft)

a. As defined in the Occupational Health and Safety Act.

For pipelines where the trench bottom is below the water table, the trench must be suitably shored as per the trench wall support requirements.

Any pipeline that is exposed for a length greater than indicated in [Table 7-1: Maximum Span without Support Beam on page 15](#) requires a field assessment.

For steel and polyethylene pipelines within the minimum distances given in [Table 7-3: Minimum Allowed Distance from Main to Excavation on page 17](#), support must remain in place until backfill material restores support.

Figure 7-1: Support of Gas Pipelines Crossing Excavations

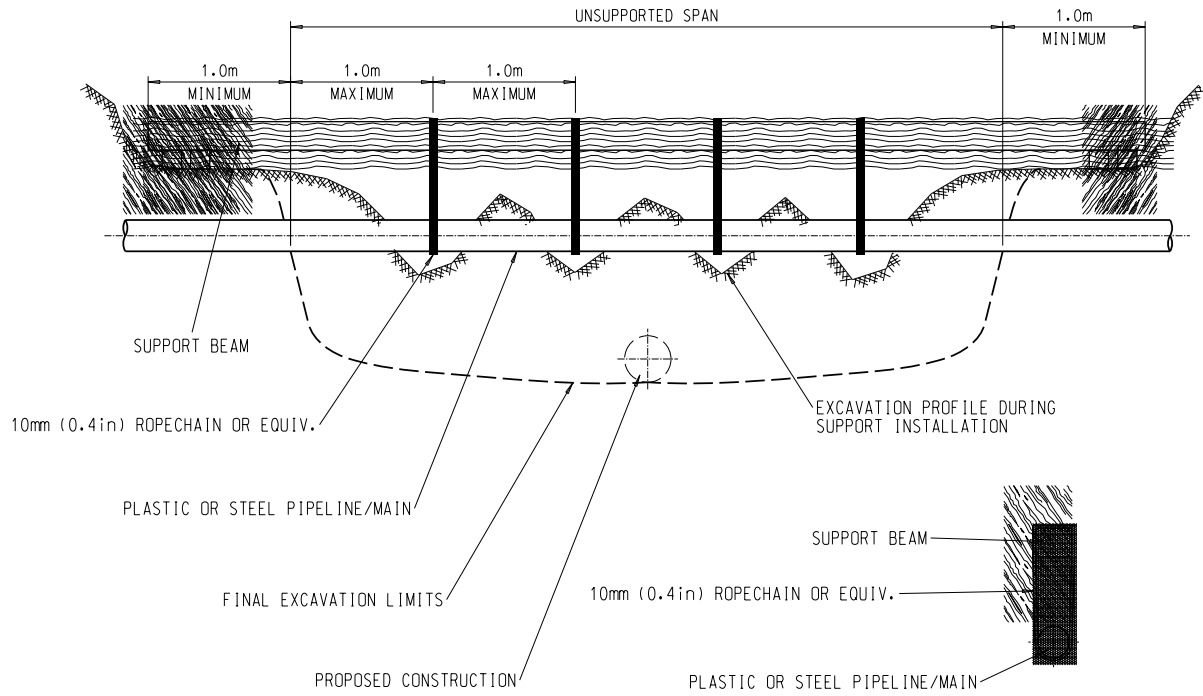
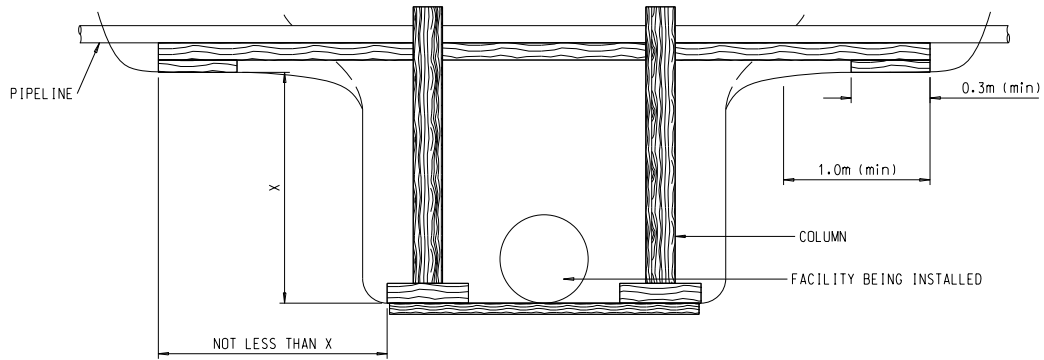


Figure 7-2: Typical Temporary Supports for Pipelines Crossing the Trench – Span Exceeds 4.5 m



NOTES:

1. LAMINATED 4X6 TIMBER BEAM REQUIRED BENEATH ALL NPS 1/2 - NPS 2.
2. LAMINATED 6X6 TIMBER BEAM REQUIRED BENEATH ALL NPS 3 - NPS 6.
3. LAMINATED 8X8 TIMBER BEAM REQUIRED BENEATH ALL NPS 8 - NPS 12.
4. COLUMN SIZE SHALL MATCH LAMINATED TIMBER BEAM REQUIREMENT.
5. COLUMN TO BE SPACED AS SPECIFIED BY PIPELINES AND STATIONS OPERATIONS ENGINEERING.
6. PLASTIC PIPE AND COATING ON STEEL PIPE TO BE PROTECTED FROM SUPPORTS AND STRAPPINGS WITH A PIECE OF RUBBER TIRE OR EQUIVALENT.
7. PLASTIC PIPE MUST BE SUITABLY STRAPPED TO PREVENT MOVEMENT OFF THE BEAM.
8. ADDITIONAL SUPPORTS WILL BE REQUIRED AT MECHANICAL COUPLINGS OR VALVES.

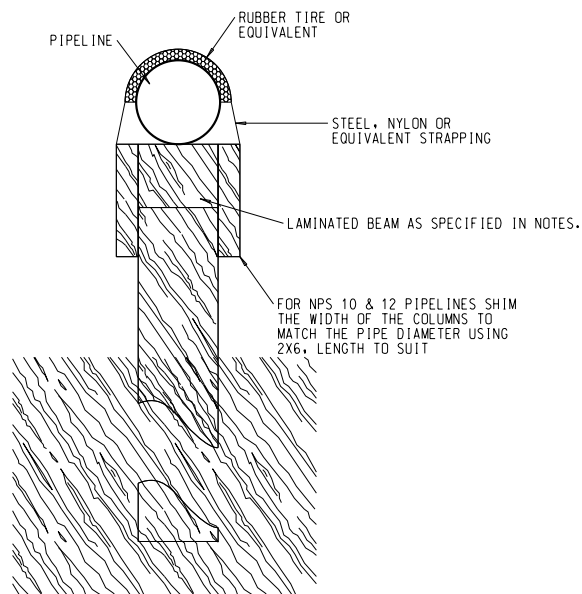
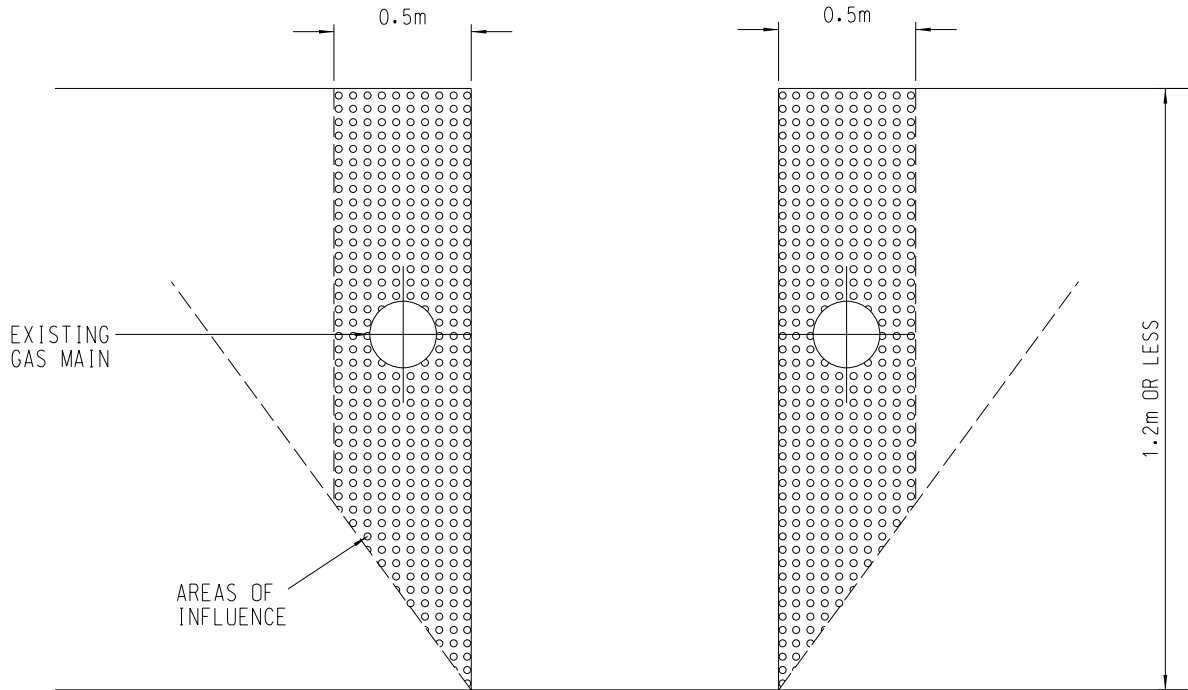


Figure 7-3: Influence Lines for Gas Pipelines Adjacent to Excavations



NOTE:
IF PIPE IS IN SHADED AREA AND SOIL IS TYPE 3 OR 4, THE TRENCH IS REQUIRED TO BE SHORED.

8 Horizontal Directional Drilling

8.1 General

Horizontal directional drilling (HDD) or directional boring is a steerable trenchless method of installing underground facilities. Trenchless technology is used where utilities being crossed are positively identified to confirm location.

For installations using any other type of drilling or augering equipment in the vicinity of gas facilities, GDS must be contacted.

In all cases, positive identification holes are required to visually verify the drill head's location (including depth) relative to the measurement of the tracking equipment. For positive identification hole requirements, see [Figure 8-2: Pipeline](#)

[Location Verification and Clearance Requirements for HDD for crossing all pipelines \(including Vital Pipelines\) on page 25](#). For pipeline location verification and clearance requirements for all horizontal directional drilling see [Table 8-1: Pipeline Location Verification and Clearance Requirements for HDD for all Pipelines \(including Vital Pipelines\) on page 21](#).

If these guidelines cannot be complied with, a variance request work package must be submitted. No variance will be provided for work within 1 m (3.3 ft) of any pipeline. The variance work package must include, at a minimum, the following information:

- Pre-Engineering design.
- Location of EGI facilities with respect to proposed installation area (vertical and horizontal offsets).
- Location of proposed installation area (vertical and horizontal offsets off permanent landmarks).
- Pipeline protection plan.

If a variance is requested, a physical barrier (e.g., silt fence) must also be provided, which would denote the boundary of the pipeline, where possible.

Table 8-1: Pipeline Location Verification and Clearance Requirements for HDD for all Pipelines (including Vital Pipelines)

Location of Work Relative to Pipeline ^a	Required Verification of Pipe Location by Hand Digging or Hydro-Excavation
Crossing below pipeline (HDD)	<p>All sides of pipeline (including below pipeline) exposed to 1.0 m (3.3 ft) from the pipeline's sidewalls.</p> <p>Additional positive identification hole at 2.0 m to 4.0 m (6.6 ft to 13.1 ft) prior to the daylight hole at the crossing, to verify depth and trajectory of drill head and backreamer.</p>
Crossing above pipeline (HDD)	<p>Top of pipeline and all sides exposed to 1.0 m (3.3 ft) or 1.0 m (3.3 ft) below the proposed installation.</p> <p>Additional positive identification hole at 2.0 m to 4.0 m (6.6 ft to 13.1 ft) prior to the positive identification hole at the crossing, to verify depth and trajectory of drill head and backreamer.</p>

a. See [Figure 8-2: Pipeline Location Verification and Clearance Requirements for HDD for crossing all pipelines \(including Vital Pipelines\) on page 25](#).

8.2 Drilling Parallel to Pipelines

When the proposed route is parallel to a natural gas pipeline at a perpendicular distance of 3 m (10 ft) or less, positive identification must be performed at intervals of no more than 10 m (33 ft) along the drilling path so that the precise location of the drilling head and backreamers (if any) can be verified visually. These

excavations must be sufficiently wide to see the entire width of the drilling head, backreamers, and structures from entry point to exit point.



Note

The location of the pipeline must be visually confirmed as per the requirements set out in [Table 8-2: Pipeline Location Verification Requirements for Vital Pipelines on page 22](#) and [Table 8-3: Pipeline Location Verification Requirements for All Other Pipelines on page 22](#).



Note

For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance of 1 m (3.3 ft) is required.

Table 8-2: Pipeline Location Verification Requirements for Vital Pipelines

Location of Work Relative to Pipeline ^a	Required Verification of Pipe Location by Hand Digging or Hydro-excavation
Work parallel to pipe, within 1 m (3.3 ft)	Spacing of test holes must not exceed 4.5 m (15 ft)
Work parallel to pipe, between 1 m (3.3 ft) and boundary area of pipeline based on size	Spacing of test holes must not exceed 4.5 m (15 ft) ^b
Crossing below pipeline (open excavation)	Top and sides of pipeline, and 0.6 m (2 ft) below the pipeline
Crossing above pipeline (open excavation)	Top and sides of pipeline, or 0.6 m (2 ft) below the proposed installation

a. Test holes must expose top and sides of pipeline

b. For work parallel to pipe, between 1 m (3.3 ft) and boundary area of pipeline based on size, for rural applications, test holes must be completed for any change in direction of the pipeline every 23 m (75 ft).

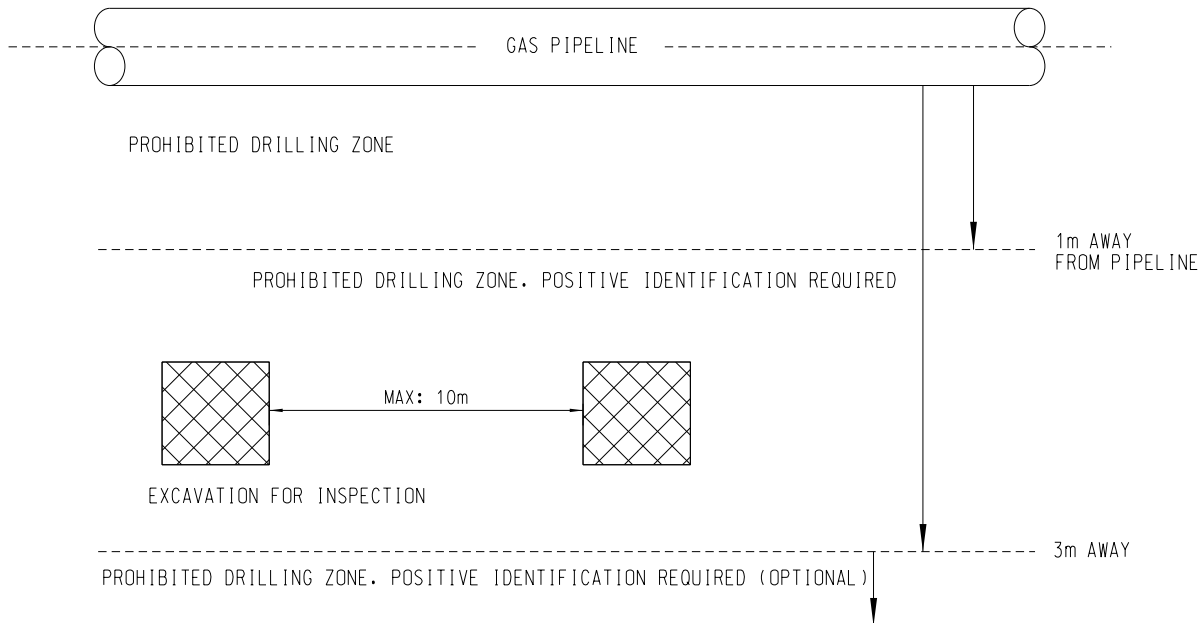
Table 8-3: Pipeline Location Verification Requirements for All Other Pipelines

Location of Work Relative to Pipeline	Required Verification of Pipe location by hand digging or hydro-excavation
Work parallel to pipe, inside of boundary area (1 m [3.3 ft])	Spacing of test holes must not exceed 4.5 m (15 ft)
Crossing below pipeline (open excavation)	For less than NPS 12: Top of pipeline and all sides of the pipeline, or 0.3 m (1 ft) below the pipeline For NPS 12 and larger: Top of pipeline and all sides of the pipeline, or 0.6 m (2 ft) below the pipeline
Crossing above pipeline (open excavation)	For less than NPS 12: Top of pipeline and all sides of the pipeline, or 0.3 m (1 ft) below the proposed installation For NPS 12 and larger: Top of pipeline and all sides of the pipeline, or 0.6 m (2 ft) below the proposed installation

No drilling installation may be performed within a distance of 1 m (3.3 ft) or less from either side of the pipeline. This buffer zone must be clearly designated and

marked off around the work area. This prohibited zone may be widened in some cases.

Figure 8-1: Drilling Parallel to Pipelines



8.3 Drilling Across Pipelines

When the proposed drill path crosses a GDS pipeline, the pipeline must be exposed to the desired depth of the crossing to ensure that the natural gas pipeline is not affected and that the required clearance is maintained during all drilling operations. All minimum clearances must be measured from the outer edge of the drill, including backreamers (if any), to the outer circumference of the pipeline.

To ensure that the directional drilling operation will not result in damage to the pipeline, the following positive identification hole requirements must be followed:

- A positive identification hole must be created that is sufficiently wide enough to see the drill head and backreamer entering the excavation at a minimum

of 1 m (3.3 ft) before crossing the pipeline. See [Figure 8-2: Pipeline Location Verification and Clearance Requirements for HDD for crossing all pipelines \(including Vital Pipelines\) on page 25](#) positive identification hole 1.

- A second positive identification hole must be created prior to reaching the pipeline such that the precise location of the drill head and backreamer (if any) can be verified visually. The positive identification hole must be sufficiently wide to measure the depth and trajectory of the drill head and backreamer. See [Figure 8-2: Pipeline Location Verification and Clearance Requirements for HDD for crossing all pipelines \(including Vital Pipelines\) on page 25](#) positive identification hole 2.

When drilling across pipelines that are smaller than NPS 16 (excluding vital pipelines), the vertical clearance, measured from the edge of the pipeline to the edge of the final bore hole, may follow the vertical clearance outlined in [Table 4-1: Minimum Clearance Between Gas Pipelines \(Less than NPS 16\) and Other Underground Structures on page 10](#) as long as all positive identification requirements are also followed.

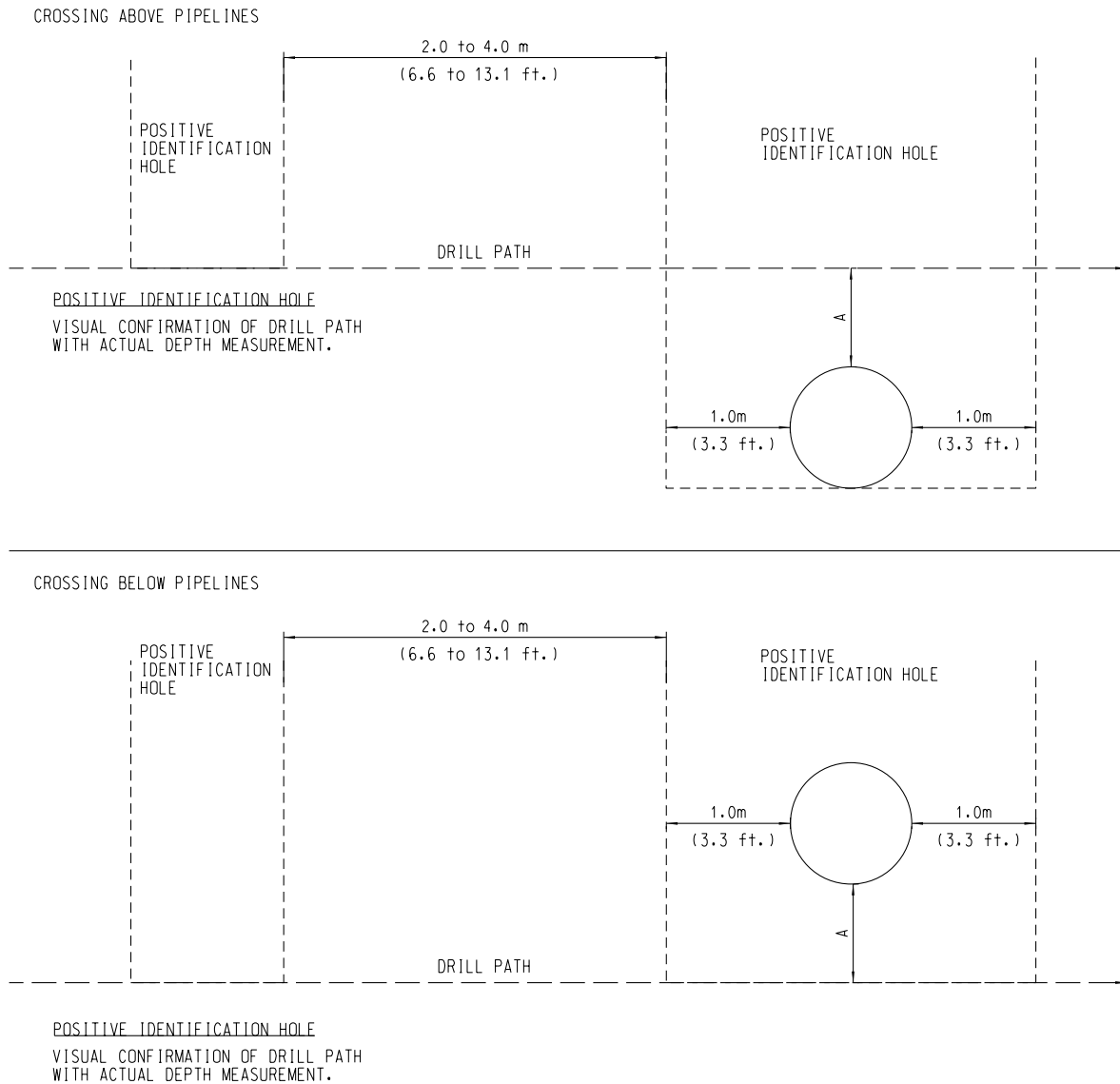
When drilling across pipelines that are NPS 16 or larger, or crossing any CER-regulated pipelines or vital pipelines, a minimum vertical clearance, measured from the edge of the pipeline to the edge of the final bore hole, of 1 m (3.3 ft.) is required.

Note



The location of the pipeline must be visually confirmed as per the requirements set out in [Table 8-2: Pipeline Location Verification Requirements for Vital Pipelines on page 22](#) and [Table 8-3: Pipeline Location Verification Requirements for All Other Pipelines on page 22](#). For specified minimum clearances, see [4 Minimum Clearance from Other Structures on page 10](#).

Figure 8-2: Pipeline Location Verification and Clearance Requirements for HDD for crossing all pipelines (including Vital Pipelines)



9 Hydro-Excavation

9.1 General

Hydro-excavation, also known as hydrovac, is the non-destructive process in which pressurized water is utilized as a method of excavation through loosening and suction of soil, rocks, and other earth materials. Hydro-excavation machines are an alternative to hand digging to locate and expose pipelines.

9.2 Hydro-Excavation Requirements

The following requirements must be met at all times when excavating with hydro-excavation technology:

- Spinning tip nozzles must be used for hydrovac excavations with water pressures that must not exceed the maximum water pressure of 17,236 kPa (2,500 psi) during excavation. Pressure measures must be permanently monitored using a calibrated device mounted on either the hydro-excavation machine (truck and pump), or the wand when using a spinning tip nozzle.
- The wand must never remain motionless during excavation. The wand must never point to the plant at any time.
- A distance of 20 cm (8 in) between the end of the pressure wand nozzle and the plant or subsoil must be maintained. The nozzle must never be inserted into the subsoil while excavating above the plant.
- Hydro-excavation equipment and nozzles must have been specifically designed for use above buried gas lines or other reasonably expected underground gas plants.
- A device capable of stopping the excavation on demand must be installed, such as an approved automatic electronic shut-off or valve on the wand.
- If heated water is used during excavation, the temperature and pressure of the water must not exceed 100 °F (38 °C) and 17,250 kPa (2,500 psi), respectively.
- The excavator must contact the gas utility if any damage to a gas plant occurs while using hydro-excavation technology or any other method of excavation.

10 Backfilling

The gas pipeline must be inspected by GDS for damages before backfilling the excavation. It is the third party's responsibility to ensure that the gas pipeline is not undermined or endangered in any way. If any damage occurs, GDS must be contacted immediately.

The following principles must be followed:

- The backfill does not harm the pipe or coating throughout the installation process and while in service.
- The use of native material (especially with respect to anode installation) and minimize haul out must be maximized.
- A reliable and stable installation must be created and the use of dams included when appropriate.

The Company permits the use of any compacting device that:

- Will compact backfill sufficiently to eliminate any settlement of the pipe or ground surface.
- Will not cause any deformation or damage to the pipe or coating.
- Will not cause any damage to any adjacent building, structure or utility.
- Will not cause any damage to any tree, shrub, tended lawn, or ground cover.

When backfilling where the finished grade has not been established, sufficient soil must be placed over the trench to allow for settlement.

Backfilling must be done in such a manner as to prevent any rocks from being placed at or near the surface of the pipe. Native excavated material must be used as backfill unless otherwise directed by GDS. Where native material is unsuitable, 150 mm (6 in) of approved earth or sand padding must be placed over the pipe for protection, to a minimum depth of 300 mm (12 in). Each layer must be compacted thoroughly by manual tamping. Topsoil must not be used for backfilling.

Aggregate backfill must be replaced in 200 mm (8 in) layers. Each layer must be thoroughly compacted by pneumatic tampers or an equivalent method acceptable to GDS to ensure no settlement. The final layer must be smoothed down with a grader (or a rake for small scale projects) and must be tamped flush or slightly higher than the surrounding ground surface in order to prevent ponding of water and accommodate any future soil subsidence over the trench line.

Backfilling a flooded trench is not allowed. The third party is responsible for the removal of water from the trench, before backfilling. If backfilling on a slope, the backfill must first be placed from the bottom of the slope, then the filling should continue by building upwards. This prevents large voids in the backfill that can occur when the backfill is dumped from the top of a slope.

Backfill and compaction within road allowances must be completed in accordance with the local governing authority.

Unshrinkable fill or other engineered backfill material must be installed only when requested by the municipalities, local governing authority, or as directed by GDS. The approved unshrinkable fill must be batched at a ready-mix plant with a specified maximum compressive strength of 0.7 MPa at 28 days and minimum slump of 150 mm (6 in). After curing, it must be excavatable using hand tools and must meet any governing agency requirements. The pipe and valve assemblies must be sand padded before placement of unshrinkable fill. The third party must ensure that placement of the unshrinkable fill does not displace sand padding or directly contact the pipeline.

If the bulk backfill material contains rocks, stones, or frozen material, pipelines must be padded with padding material to a minimum depth of 150 mm (6 in) over the pipe and fittings. If the location requires the backfill material to be tamped, the padding material must also be tamped.

The final covering of gas pipelines must adhere to municipal requirements.

11 Blasting and Pile Driving

11.1 General

Blasting and pile driving activities in the vicinity of GDS facilities require prior approval by GDS. The [Blasting and Pile Driving Form](#), provided by GDS, must be submitted by the owner of the proposed work for all blasting and pile-driving operations. The request must be submitted a minimum of four weeks prior to the beginning work to allow sufficient time for review.

11.2 Blasting

Before any blasting operation in the vicinity of a gas pipeline can occur, the hazards to the GDS facility must be evaluated. Responsibility for the design of the blast and any resultant damage is borne entirely by the party using the explosives.

A recognized independent blasting consultant must be retained at the applicant's expense to perform an evaluation of the blast design. The independent blasting consultant must be an independent engineering consultant specialized in blasting. A copy of the stamped consultant's validation report must be submitted to GDS for review if blasting is to occur within 30 m (100 ft) of GDS facilities.

If in the opinion of GDS or an independent blasting consultant, blasting cannot be carried out without affecting the facility's integrity, alternatives must be considered, including the replacement or relocation of the affected facility at the applicant's expense. In these situations, additional time must be allowed to obtain the necessary permits and to complete the necessary construction work. In the event a third party is affected as a result of the blasting operations, all expenses associated therewith incurred by GDS must also be at the applicant's expense.

Ontario: The third party must comply with the Ontario Provincial Standard Specification (OPSS 120 – General Specification for the Use of Explosives) in addition to GDS's blasting requirements.

Quebec: The third party must comply with Quebec's Acts regarding explosives (CQLR c E-22 and CQLR c E-22, r 1) and Safety Code (CQLR c S-2.1, r 4), in addition to GDS's blasting requirements.

11.2.1 Surface and Tunnel Blasting Application Process

For subsurface blasting application requirements, refer to the Surface Blasting section of the [Blasting and Pile Driving Form](#).

For tunnel blasting application requirements, refer to the Surface Blasting section of the [Blasting and Pile Driving Form](#) in addition to the Tunnel Blasting section.

To assist with the preparation of the form, locates must be requested to determine the location of the facilities.

11.2.2 Guidelines for Blasting

The information provided in this section is not to be construed as an exhaustive list of performance specifications, but rather a guide for conducting blasting in the vicinity of GDS's facilities. The third party is responsible for ensuring that all blasting work is performed in a good and workmanlike manner in accordance with all applicable laws, codes, by-laws, and regulations.

The third party will be held liable for and indemnify GDS in relation to any and all damage directly or indirectly caused or arising as a result of blasting operations carried out by the applicant, its employees, contractors, or those for whom the applicant is responsible by law. Prior to blasting operations, a site meeting must be arranged with an authorized representative of the applicant and a GDS representative to confirm the location of GDS's facilities and details of the proposed blast.

GDS's pipelines must not be excavated prior to blasting. If excavation is unavoidable, then the pipeline must be properly supported according to GDS's requirements as stated in [7 Support of Gas Pipelines on page 14](#).

The third party must take suitable precautions to protect the exposed pipeline from fly-rock .

Explosives must be of a type that cannot propagate between holes or be desensitized due to compression pressures. Explosives must not be left in the drill hole overnight.

If a surface blast is located less than 10 m (33 ft) from pipeline; creates its first blast hole at a depth equal to the top of the pipeline; and the depth of subsequent blast holes exceeds one half of the horizontal distance to the closest portion of the pipeline, then the required independent blasting consultant's report must specifically address the impact of these conditions. This is not applicable for tunnel blasting operations. The blasting consultant is responsible for the monitoring of blasting vibrations with a portable seismograph capable of transmitting data instantaneously (e.g., via email or cellular) to the required reviewer in the vicinity of GDS's facilities is mandatory to confirm that predicted vibration levels are respected. On a daily basis, a copy of the seismographic report must be provided to GDS.

Peak particle velocity (PPV) must be limited to 50 mm/s (2 in/s) and maximum amplitude must be limited to 0.15 mm (0.006 in).

11.2.3 Post Blasting

A leak survey must be completed at the end of each day of blasting. Upon completion of daily blasting operations and within 30 days after the final blasting, GDS will conduct a leak survey of the pipeline at the third party's expense. Leak surveys will also be completed at the end of each day of blasting. Damage that has resulted from the blasting will be repaired at the third party's expense. A summary of all blasting operations including blasting logs, vibration control, seismograph reports, and other pertinent information must be provided to GDS by the third party daily and at the completion of blasting operations.

11.3 Pile Driving

General pile installation or compaction activities in the vicinity of GDS's facilities must be evaluated by GDS prior to beginning. Any resultant damage as a result of these activities will be borne entirely by the third party undertaking the proposed work.

If in the opinion of GDS, the particular pile installation or compaction operation cannot be carried out without affecting the pipeline or facility integrity, the following must be considered:

- Risk analysis or mitigation program for the proposed operation.
- Alternative construction methods.
- Relocation or replacement of the facility.

All costs incurred will be covered by the third party undertaking the proposed work and final approval for the work will be granted by GDS.

Piles installed using an auger must satisfy the locating and clearance requirements listed in [5 Pipeline Location Verification on page 10](#) and [4 Minimum Clearance from Other Structures on page 10](#), respectively. GDS must provide approval for the installation of piles within 3 m (10 ft) of a vital pipeline.

The third party is responsible for all costs related to customer interruption as well as costs incurred because of work delays. In the event a third party is affected as a result of the pile installation or compaction operations, all expenses associated therewith incurred by GDS will be passed to the third party.

11.3.1 Pile Driving Application Process

The application to pile drive or do compaction work must be sent to GDS via the [Blasting and Pile Driving Form](#).

This work must be completed under the supervisor of qualified personnel. Vibration results must be provided to GDS on a daily basis.

11.3.2 Pile Installation and Compaction Work

The information provided in this section is not to be construed as an exhaustive list of performance specifications, but rather a guide for conducting pile installation and compaction work in the vicinity of GDS's facilities. The third party is responsible for ensuring that all pile installation and compaction work is performed in accordance with all applicable laws, codes, by-laws, and regulations.

Operations must not be permitted within a standoff distance of 3.0 m (10 ft) from the pipeline or other natural gas facility, unless approved by GDS.

Prior to pile installation or compaction work, a site meeting with an authorized representative of the third party and a GDS representative (for the Damage Prevention contact, see [12 Contact Information on page 32](#)) must be arranged by the third party, to confirm the location of GDS's facilities and the details of the proposed work.

It is recommended that during the design phase, pile installation or compaction work drawings be sent to Markups for review (see [12 Contact Information on page 32](#)).

The pipeline should not be excavated prior to the piling or compaction operation. If excavation of the pipeline is necessary, then it must be properly supported in accordance with [7 Support of Gas Pipelines on page 14](#).

The following situations require the opinion of an independent professional engineer:

- Compaction of soils or backfill rated at 10,000 ft-lbs (13,600 Nm) or higher at a stand-off distance of 6 m (20 ft) or less from the pipeline.
- Pile driving at a stand-off distance of 10 m (33 ft) or less from the pipeline facility.
- High-energy dynamic compaction for the rehabilitation of soils at a stand-off distance of 30 m (100 ft) or less from the pipeline.

- Type 4 soil as defined in Article 226 of the Occupational Health and Safety Act and Regulations for Construction Projects (see [Table 15-1: Soil Types on page 34](#)).

For these situations, the appropriate number of seismographs to monitor vibrations is mandatory. The seismographs must be portable with the capability of transmitting data instantaneously (e.g., via email or cellular). This control will confirm the intensity of the vibrations generated by the pile installation or compaction work as projected. Furthermore, reports of recorded intensities must be provided on a regular basis or at the request of GDS.

The peak particle velocity (PPV) measured on the pipeline, or at the closest point of the related structure with respect to the work, must not exceed 50 mm/s (2 in/s). Furthermore, the maximum displacement for the vertical or horizontal component corresponding to the above stated vibration intensity must not exceed 50 mm (2 in) at any given length of the pipeline in question.

If the PPV or displacement limit is surpassed, all operations must stop notwithstanding any delays or costs incurred by the third party or owner of the proposed work. GDS requires that the cause of these higher vibrations or displacements be investigated. GDS may arrange for a leak survey to be conducted. GDS Engineering must approve resumption of operations. Should a situation with low energy compaction operations with a soil cover of less than 1.5 m (5 ft) above the pipeline at a stand-off distance of 3 m (10 ft) or less from a pipeline be encountered, GDS may require the opinion of an independent engineering consultant.

In addition, if a Type 3 soil (see [Table 15-1: Soil Types on page 34](#)) is present on site, GDS may require the opinion of an independent engineering consultant.

The use of an auger may be required in order to avoid the use of piles.

All operations must comply with the Provincial Occupational Health and Safety Act and Regulations for Construction Projects, other applicable laws and regulations, as well as all applicable GDS specifications, standards, and guidelines.

11.3.3 Post Pile Driving Process

The third party must send GDS the items that follow within five business days of the completion of the pile installation via pile driving or compaction operations:

- A summary of all operations.
- Pile driving and compaction logs.
- Vibration control records.
- Seismograph records.

On completion of each day's work, and approximately 30 days after all work is completed, GDS will arrange to conduct a leak survey of the facility. If damage to GDS's facilities is found, it will be repaired by the third party. An invoice will be sent to the third party responsible for the work.

12 Contact Information

Location	Contact
Enbridge Gas Inc 500 Consumers Road North York, ON M2J 1P8	Markups: Mark-Ups@enbridge.com Ontario One Call Locates: 1-800-400-2255 Damage Prevention: 1-866-922-3622 Emergency: 1-866-763-5427 and 1-877-969-0999
Enbridge Gas Inc Storage and Transmission Operations Locates (Dawn) 3332 Bentpath Line P.O. Box 1180 Dresden, ON N0P 1M0	Ontario One Call Locates: 1 (800) 400-2255 Locates: 1-800-265-5260 ext 5102236 Stacey.Smith@enbridge.com Locates: 1-800-265-5260 ext 5102184 Janice.Langstaff@enbridge.com
Enbridge Gas Inc Storage and Transmission Operations Locates (Tecumseh) 3501 Tecumseh Road, Mooretown, Ontario N0N 1M0	Field Operations: 519-312-0176 jay.moore@enbridge.com Field Operations: 519-862- 6004 jason.japp@enbridge.com Tecumseh Control Room: 519-862-6012 Emergency: 1-800-255-1431
Gazifère 706 Boulevard Greber Gatineau, QC J8V 3P8	Locates: 1-800-663-9228 Planning Dept.: 1-819-776-8804 Emergency: 1-819-771- 8321, press 1

Note



The website www.clickbeforeyoudig.com gives access to the damage prevention centres in Canada, and allows locate requests to be made for each province.

13 References

- [IS_F_172 Blasting and Pile Driving Form](#)

14 Document Governance

For document control and maintenance purposes, the following tables capture important information related to this document.

Control and Maintenance

Category	Value
Owned By	Pipeline Engineering
Review Interval	Every three years
MOC-Related	No

Revision History

Table 14-1: September 29, 2021 Release

Release Date	Version	Project Number	RFC Number	Prepared By	Approved By
2021-09-29	1.1.1	n/a	4983	Hooman Zahedi, Supervisor, Pipeline Engineering	Todd Piercey, Manager, Pipeline Engineering
Doc ID	Scope	Document & Section		Summary of Changes	
ST-1E-30A8-8E30	GDS	Third-Party Requirements in the Vicinity of Natural Gas Facilities Standard		Corrected tyop in 11.2 Blasting	

Table 14-2: June 30, 2021 Release

Release Date	Version	Project Number	RFC Number	Prepared By	Approved By
2021-06-30	1.1	n/a	4922	Hooman Zahedi, Supervisor, Pipeline Engineering	Todd Piercey, Manager, Pipeline Engineering
Doc ID	Scope	Document & Section		Summary of Changes	
ST-1E-30A8-8E30	GDS	Third-Party Requirements in the Vicinity of Natural Gas Facilities Standard		Revise tree clearance restrictions in section 3.8.	

Table 14-3: April 28, 2021 Release

Release Date	Version	Project Number	RFC Number	Prepared By	Approved By
April 28, 2021	1.0	6513-20	None	Emily Varga, EIT I, Pipeline Engineering	Todd Piercey, Manager Pipeline Engineering
Doc ID	Scope	Document & Section		Summary of Changes	
ST-1E-30A8-8E30	GDS	Third-Party Requirements in the Vicinity of Natural Gas Facilities Standard		Initial version.	

15 Soil Types

Table 15-1: Soil Types

Type	Definition
Type 1	<ul style="list-style-type: none"> • Hard, very dense, and only able to be penetrated with difficulty by a small sharp object. • Low natural moisture content and a high degree of internal strength. • No signs of water seepage. • Can be excavated only by mechanical equipment.
Type 2	<ul style="list-style-type: none"> • Very stiff, dense, and can be penetrated with moderate difficulty by a small sharp object. • Low to medium natural moisture content and a medium degree of internal strength. • Damp appearance after it is excavated.
Type 3	<ul style="list-style-type: none"> • Stiff-to-firm and compact-to-loose in consistency or is previously-excavated soil. • Exhibits signs of surface cracking. • Exhibits signs of water seepage. • If dry, may run easily into a well-defined conical pile. • Low degree of internal strength.
Type 4	<ul style="list-style-type: none"> • Soft to very soft and very loose in consistency, very sensitive, and upon disturbance is significantly reduced in natural strength. • Runs easily or flows, unless it is completely supported before excavating procedures. • Almost no internal strength. • Wet or muddy. • Exerts substantial fluid pressure on its supporting system.

Kate Barclay

From: Emilio Labra <Emilio.Labra@enbridge.com>
Sent: July 14, 2022 8:28 AM
To: David Hiett; Victoria Vu
Cc: Zia, Solmaz; Stephen Keen; Mahmood, Tareq; Kate Barclay
Subject: RE: Coleraine Drive grade separation and RDC Project Update

EXTERNAL EMAIL

Thanks David for the additional comments.

Just a quick note for your detailed design consideration;

1. Proposed gas running line should be nearest to P/L
2. Possibly gas will require TOPO level B for Manchester Ct., since obviously the existing header to Brand Energy & Infrastructure Services will have to be rerouted since the header connection is very close to CP Rail ROW
3. Buffer zone for Utilities should be wide enough to accommodate Service Trucks for servicing gas and attending to emergencies
4. If there will be noise walls, there should be a minimum of 0.6m horizontal clearance from proposed gas running line

Regards,

Emilio Labra

Sr. Advisor Construction Project Management
Planning – GTA-W
Brampton, ON
Tel. 905-458-3811
Cell. 416-427-4386

From: David Hiett <David.Hiett@cima.ca>
Sent: Wednesday, July 13, 2022 4:49 PM
To: Emilio Labra <Emilio.Labra@enbridge.com>; Victoria Vu <Victoria.Vu2@enbridge.com>
Cc: Zia, Solmaz <solmaz.zia@peelregion.ca>; Stephen Keen <Stephen.Keen@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Kate Barclay <Katherine.Barclay@cima.ca>
Subject: [External] RE: Coleraine Drive grade separation and RDC Project Update

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Were you expecting this email? TAKE A CLOSER LOOK. Is the sender legitimate?
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Hi Emilio and Victoria,

I will add to Tareq's email below that this preliminary design study and proposed improvements were previously presented to Enbridge and we received feedback from mark-ups@enbridge.com – attached email. Per your emails and past emails from Enbridge, the ESR will make the commitment that Enbridge will be further consulted with during the Detailed Design process.

We will add you to the contact list for notification of when the ESR is filed, however if you are updating your Gas Relocation plans I will add that the provided information of the gas main along Coleraine Drive has been incorporated and the intent remains the same as presented previously: that utilities are relocated to the 'buffer' between the grade separation construction and the ROW boundaries (please see attached preliminary conflict plan that will be submitted with the ESR). It appears the gas main will be one of these utilities and further details will need to be confirmed in Detailed Design.

Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CONFIDENTIALITY WARNING This email is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: Emilio Labra <Emilio.Labra@enbridge.com>
Sent: Wednesday, July 13, 2022 2:15 PM
To: Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Cc: Zia, Solmaz <solmaz.zia@peelregion.ca>; David Hiett <David.Hiett@cima.ca>; Stephen Keen <Stephen.Keen@cima.ca>; Victoria Vu <Victoria.Vu2@enbridge.com>
Subject: RE: Coleraine Drive grade separation and RDC Project Update

EXTERNAL EMAIL

Thank you Tareq! For future communications on Coleraine's CP Rail Separation project, please include Victoria Vu.

Once detailed design has started, submission will be to mark-ups@enbridge.com including request for gas assets general location information .

Regards,

Emilio Labra

Sr. Advisor Construction Project Management
Planning – GTA-W
Brampton, ON
Tel. 905-458-3811
Cell. 416-427-4386

From: Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Sent: Wednesday, July 13, 2022 12:53 PM
To: Emilio Labra <Emilio.Labra@enbridge.com>
Cc: Zia, Solmaz <solmaz.zia@peelregion.ca>; David Hiett <David.Hiett@cima.ca>; Stephen Keen <Stephen.Keen@cima.ca>
Subject: [External] RE: Coleraine Drive grade separation and RDC Project Update

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Hello Emilio Labra,

The consultant has submitted the draft Environmental Study report and Utility Conflict plan, which will be circulated to stakeholders for their review, including Enbridge, next week. We plan to file the EA later this year. Tentatively, the detailed design can be started sometime next year.

I have included David from CIMA+ (Consultant) on this email, as in the past, you have received communication from him regarding utility conflicts.

Thanks,

Tareq Mahmood

PM, Infrastructure Programming
and Studies, Transportation
Tel: 905-791-7800 X 7828
Cell: 905-872-6475

From: Emilio Labra <Emilio.Labra@enbridge.com>
Sent: July 13, 2022 10:03 AM
To: Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Cc: Zia, Solmaz <solmaz.zia@peelregion.ca>
Subject: Coleraine Drive grade separation and RDC Project Update

CAUTION: EXTERNAL MAIL. DO NOT CLICK ON LINKS OR OPEN ATTACHMENTS YOU DO NOT TRUST.

Hello Tareq,

Enbridge will be updating our 2023-24 Gas Relocation Plans and I noticed that a communication on Coleraine Dr. CP Rail Separation was communicated to us in the past. Can you please update me on the status of the project (status of design / project timelines, etc.)? If the project have already progressed to detailed design, please confirm back if submissions were already submitted to mark-ups@enbridge.com . If a submission was already made please advise EGD# that was provided back when the submission was made (either for general mark-ups or 30% review, etc.). I have also attached our Gas Relocation Overview for your information and file.

By copy to Solmaz, can you also please provide me an update on the attached RDC Project sheet, so as we can adjust priority on our 2023-24 plans.

Regards,

Emilio Labra

Sr. Advisor Construction Project Management

Planning – GTA-W

Brampton, ON

Tel. 905-458-3811

Cell. 416-427-4386

A-3

Appendix A-3-3: Hydro One

March 21, 2017

Mr. Ian Mitchell
EA Coordinator
Hydro One Networks Inc.
65 Kelfield Street
Rexdale, ON M9W 5A3

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

Public consultation is an integral component of the Municipal Class EA process and opportunities to comment are welcomed throughout the entire study. Consultation activities will be announced at key milestones in the project.

You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

March 21, 2017

Planning and Scheduling Department
Hydro One Networks Inc.
40 Olympic Drive
Dundas, ON L9H 7P5

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

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You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Dela Cruz, Gino <gino.delacruz@peelregion.ca>
Sent: April 4, 2017 9:30 AM
To: Stephen Keen; Jessica Dorgo
Subject: FW: Region of Peel - Coleraine Drive Grade Separation
Attachments: 20170403152840163.pdf

FYI and file

Gino

From: Lindsay.Lawrence@HydroOne.com [mailto:Lindsay.Lawrence@HydroOne.com]
Sent: April 4, 2017 9:28 AM
To: Dela Cruz, Gino
Cc: WesternFBCPlanning@HydroOne.com; Lindsay.Lawrence@HydroOne.com
Subject: Region of Peel - Coleraine Drive Grade Separation

Good Morning,

Our tech had the opportunity to review your Municipal Class Environmental Assessment request for the Coleraine Drive Grade Separation. Unfortunately, as this request is just informing all relevant parties that the Region has initiated a Municipal Class EA with no scope of work outlined, maps to mark up or formal PUCC to review, Hydro One has no comment at this time until those items are provided. Once those items are provided by the Region of Peel, Hydro One will review them and provide comments and feedback.

Thank you,

Lindsay Lawrence

Zone Distribution Planning Tech, Western Field Business Centre
Hydro One Networks Inc
16 Graham St., Woodstock, ON, N4S 6J6
Tel: 519-537-7172 x2226
Fax: 519-537-5081
Email: Lindsay.Lawrence@HydroOne.com

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Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:40 AM
To: westcentralzonescheduling@hydroone.com
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Hydro One Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Planning and Scheduling Department
Hydro One Networks Inc.
40 Olympic Drive
Dundas, ON L9H 7P5

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:41 AM
To: ian.mitchell@hydroone.com
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Hydro One Letter (2).pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Mr. Ian Mitchell
EA Coordinator
Hydro One Networks Inc.
65 Kelfield Street
Rexdale, ON M9W 5A3

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: David Hiett
Sent: February 1, 2022 11:44 AM
To: ian.mitchell@hydroone.com
Cc: westcentralzonescheduling@hydroone.com; Stephen Keen; Mahmood, Tareq
Subject: B738 Coleraine Drive CP Grade Separation EA - Utilities - Hydro One
Attachments: B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02 with Image.pdf; Coleraine Drive GS EA - PIC Cross-section.jpg; B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02.pdf

Hi Ian,

The Region of Peel, supported by CIMA+, are completing a Municipal Class EA Schedule 'C' study for a Coleraine Drive Grade Separation, in Bolton. You would have previously received correspondence on the study, most recently regarding the PIC #2 that occurred at the end of last year. More information, including the PIC materials, can be found on the project website here: [Coleraine Drive grade separation Environmental assessment - Region of Peel \(peelregion.ca\)](https://www.peelregion.ca/coleraine-drive-grade-separation-environmental-assessment)

Part of this preliminary design study is the identification of utility impacts and options for potential mitigation (i.e., relocation). Please see attached utility plans, with and without imagery.

As seen, Hydro One has some infrastructure within the study area and while infrastructure along the local roads would largely be unimpacted, any infrastructure along Coleraine Drive is likely to be impacted and would require relocation. This is mostly due to the large construction required – large fill and retaining walls will be used along Coleraine Drive to facilitate the raise over the CP rail lines (as seen in the attached cross-section). Also as detailed in the cross-section, there will be a buffer between the retaining wall and ROW boundary along Coleraine Drive, and it is within this area that we are proposing impacted utilities be relocated.

The impacts, final design and final relocation plans/agreements will be confirmed in Detailed Design, however we are reaching out to you at this time to confirm:

- If you have any objection to the strategy of potentially relocating the impacted infrastructure on Coleraine Drive to the 'buffer' outside of the walls.
- If Hydro One are planning any future infrastructure improvements or future new infrastructure along Coleraine Drive

If you have any questions, please let us know.

If I can kindly have a response by February 9th, it will be greatly appreciated.

Many Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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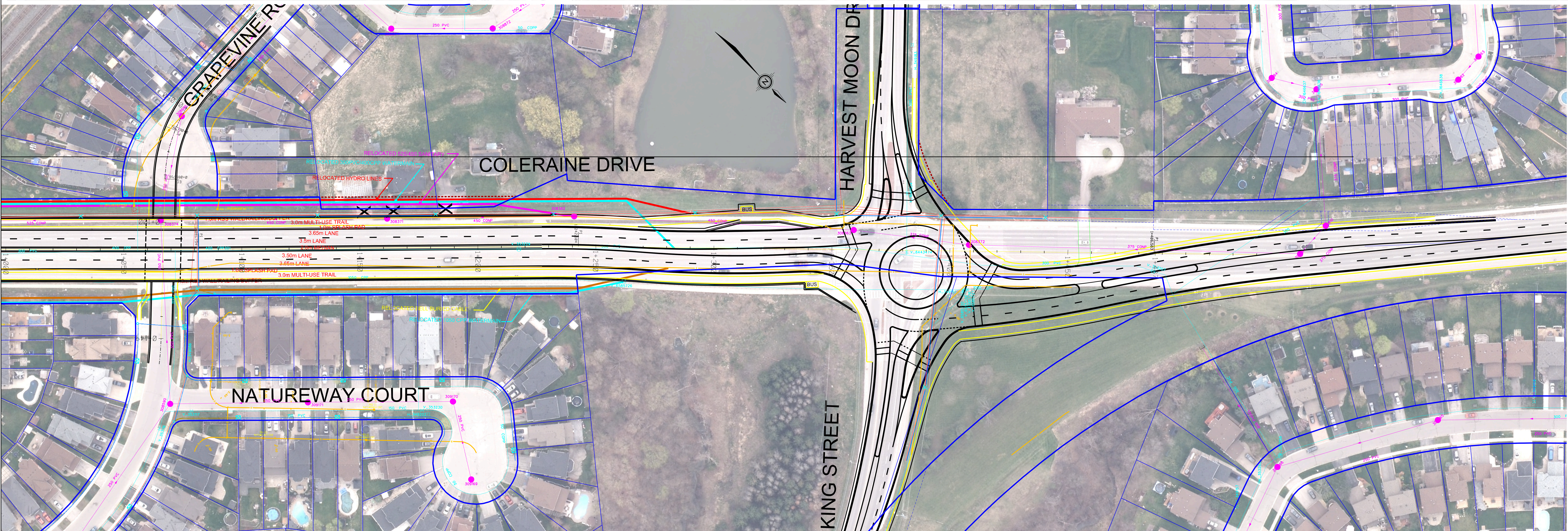
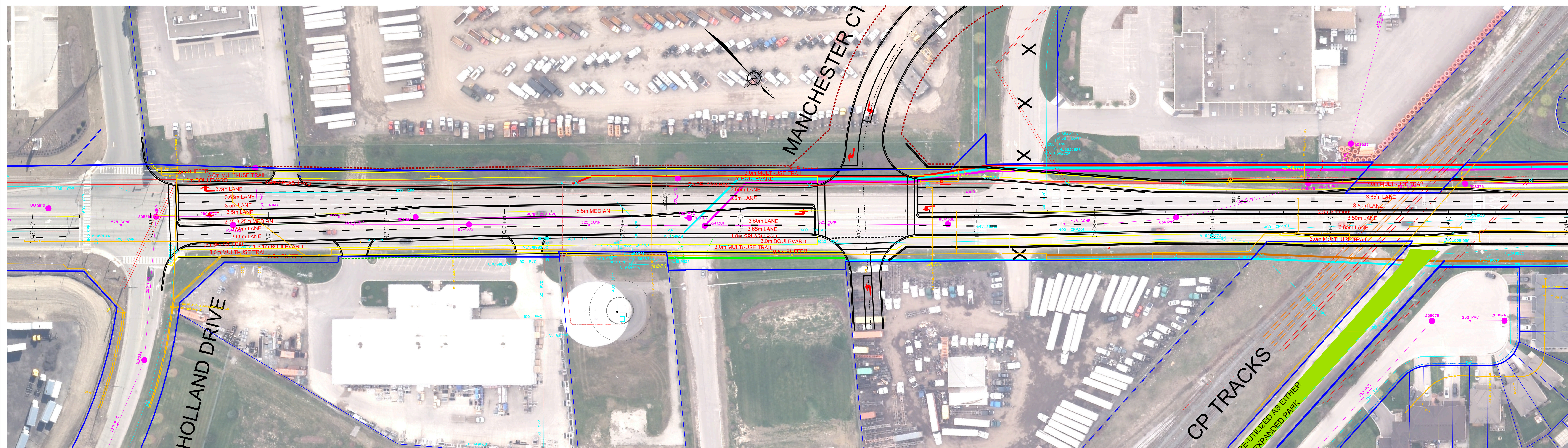


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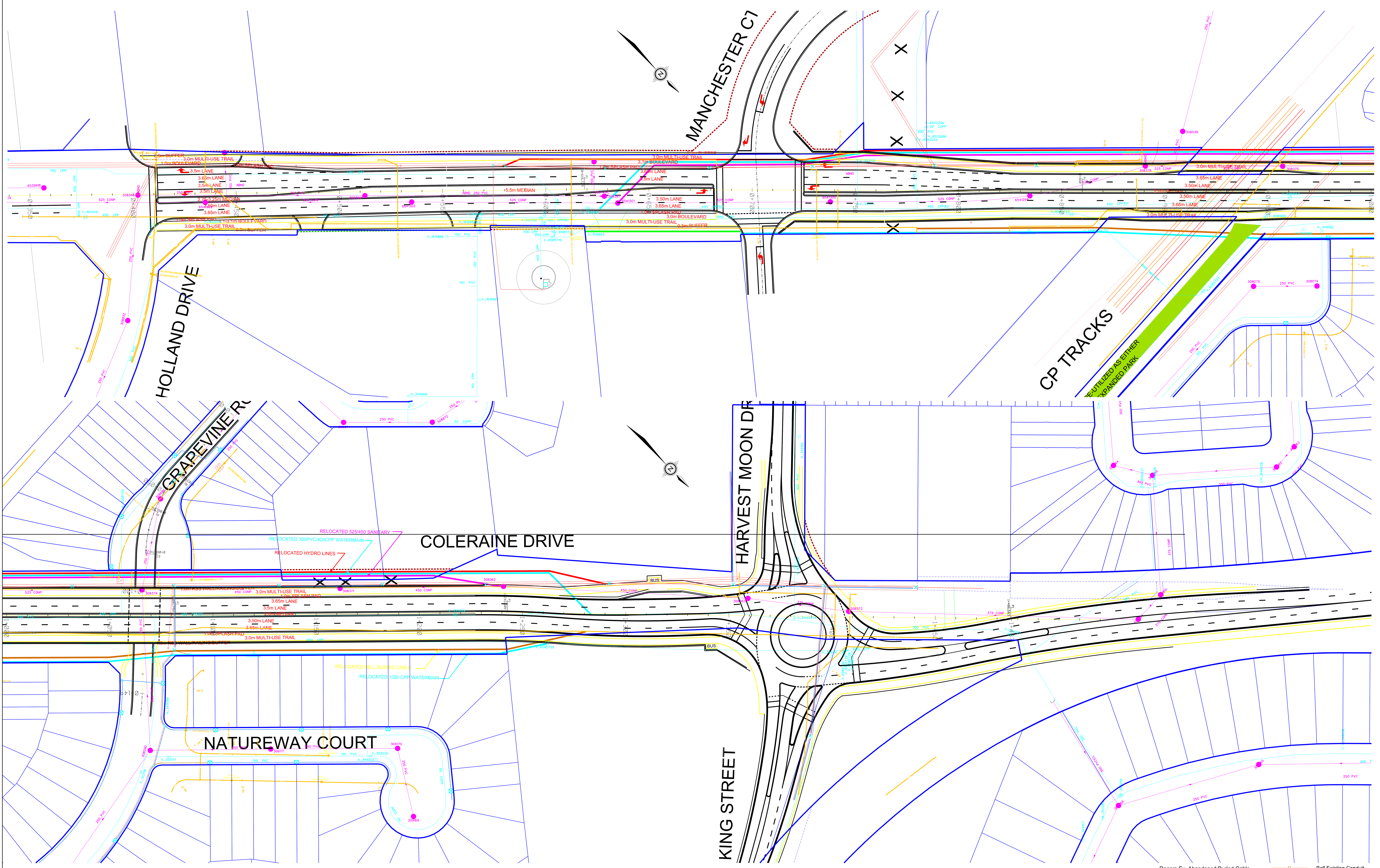
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COLERAINE DRIVE
(HOLLAND DRIVE TO HARVEST MOON DRIVE)

ROAD OVER RAIL OPTION
UTILITY PLAN

- | | | | | | | | |
|---|-----------------------|---|------------------|---|-----------------------------------|---|----------------------------|
|  | PROPOSED O/H HYDRO |  | EX. O/H HYDRO |  | Rogers Ex. Abandoned Buried Cable |  | Bell Existing Conduit |
|  | PROPOSED SANITARY |  | EX. SANITARY |  | Rogers Ex. Buried Coaxial Cable |  | Bell Existing Buried Cable |
|  | PROPOSED BURIED CABLE |  | EX. BURIED CABLE |  | Rogers Ex. Aerial Coaxial Cable |  | Bell Existing Handhole |
|  | PROPOSED WATER MAIN |  | EX. WATER MAIN |  | Rogers Ex. Buried Fibre Cable |  | Bell Existing Pedestal |
| | | | |  | Rogers Ex. Aerial Fibre Cable | | |



COLERAINE DRIVE
(HOLLAND DRIVE TO HARVEST MOON DRIVE)

ROAD OVER RAIL OPTION
UTILITY PLAN

- | | | | |
|-----------------------|------------------|-----------------------------------|----------------------------|
| PROPOSED O/H HYDRO | EX. O/H HYDRO | Rogers Ex. Abandoned Buried Cable | Bell Existing Conduit |
| PROPOSED SANITARY | EX. SANITARY | Rogers Ex. Buried Coaxial Cable | Bell Existing Buried Cable |
| PROPOSED BURIED CABLE | EX. BURIED CABLE | Rogers Ex. Aerial Coaxial Cable | Bell Existing Handhole |
| PROPOSED WATER MAIN | EX. WATER MAIN | Rogers Ex. Buried Fibre Cable | Bell Existing Pedestal |
| | | Rogers Ex. Aerial Fibre Cable | |

Kate Barclay

From: HOTOOutsidePlant@HydroOne.com
Sent: February 8, 2022 9:35 AM
To: David Hiatt
Cc: eddison.mungal@HydroOne.com; Nadeen.Wajdi-Houjeily@HydroOne.com; Bryan.Brennan@HydroOne.com; HOTOOutsidePlant@HydroOne.com; Zone2Scheduling@HydroOne.com; Stephen Keen; Mahmood, Tareq; HOTOOutsidePlant@HydroOne.com
Subject: FW: B738 Coleraine Drive CP Grade Separation EA - Utilities - Hydro One
Attachments: B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02 with Image.pdf; Coleraine Drive GS EA - PIC Cross-section.jpg; B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02.pdf

EXTERNAL EMAIL

Hi David,

Please find the requested information below.

The impacts, final design and final relocation plans/agreements will be confirmed in Detailed Design, however we are reaching out to you at this time to confirm:

- If you have any objection to the strategy of potentially relocating the impacted infrastructure on Coleraine Drive to the 'buffer' outside of the walls.
 - Acronym does not object to the strategy of relocation to the 'buffer' outside of the walls, provided accommodations are made to allow Acronym to be able to transfer the fiber optic cable with minimal disruption to our services. The duct under the railway tracks utilized by Acronym is owned by PSN who should be consulted in this matter. (Image A)
- If Hydro One are planning any future infrastructure improvements or future new infrastructure along Coleraine Drive
 - Acronym recently installed an underground cable utilizing a PSN duct at the rail crossing. (Image A)
 - Acronym's fiber cable is aerial along the west side of Coleraine Dr up to the NW corner of Coleraine and Harvest Moon Dr. From there Acronym's fiber cable dips and is in PSN owned conduit heading east along King St W. (Image B)
 - Any relocation of the Hydro pole line, will affect Acronym's aerial fiber optic cables and will need to be taken into consideration.

(Image A)

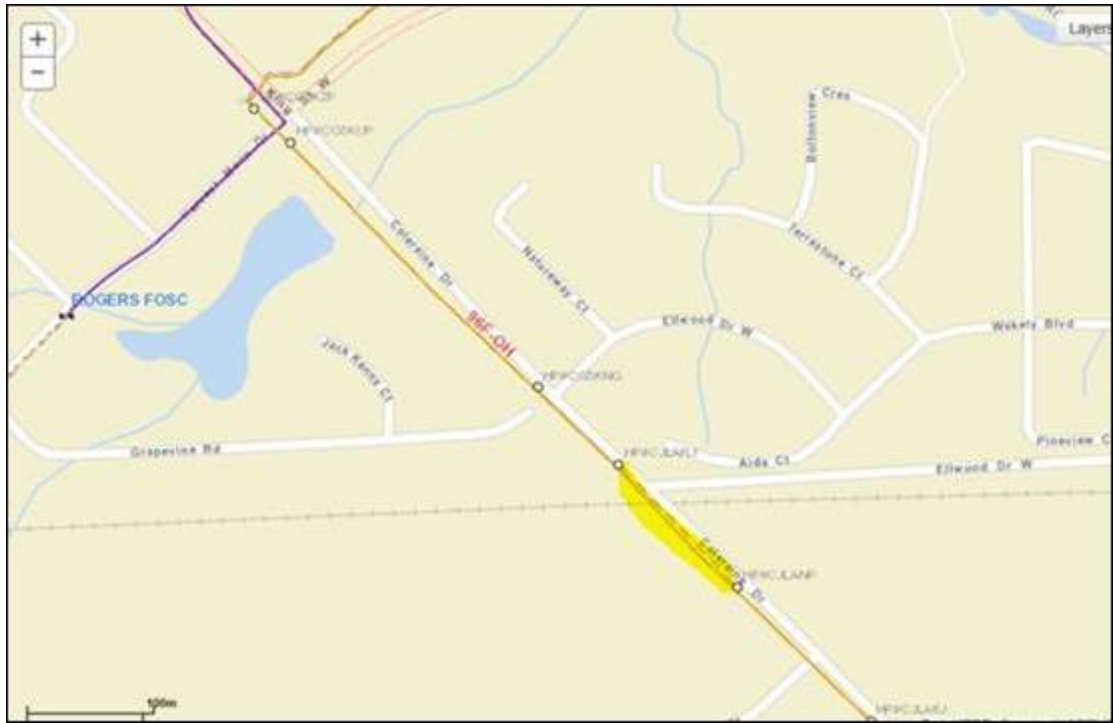


Image (B)



Regards,



Luka Baskot
 Group Lead – Telecom Operations
 Acronym Solutions Inc.
HydroOneTelecom.com/Acronym
 M: 416.700.4290

From: MITCHELL Ian
Sent: Tuesday, February 01, 2022 12:23 PM
To: David.Hiett@cima.ca; MUNGAL Eddison
Cc: ZONE 2 SCHEDULING; Stephen.Keen@cima.ca; tareq.mahmood@peelregion.ca; BRENNAN Bryan
Subject: FW: B738 Coleraine Drive CP Grade Separation EA - Utilities - Hydro One

David,

Thank you for the information. Unfortunately I am not the contact for relocations of existing plant, that is managed through our Operations group.

I have included both Bryan Brennan, Manager Operations Services and Kevin Mungal, OSP Specialist on this email. Please contact them directly for any future needs or requirements for this project. They will be more than happy to work with you on the relocations of the Acronym plant.

Thank you,



Ian Mitchell
Outside Plant (Engineering) Manager /
HSEMS Rep
Acronym Solutions Inc.
HydroOneTelecom.com/Acronym
M: 647-287-3007
65 Kelfield St, Toronto, ON M9W 5A3

From: David Hiett <David.Hiett@cima.ca>
Sent: Tuesday, February 01, 2022 11:44 AM
To: MITCHELL Ian <ian.mitchell@HydroOne.com>
Cc: ZONE 2 SCHEDULING <Zone2Scheduling@HydroOne.com>; Stephen Keen <Stephen.Keen@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Subject: B738 Coleraine Drive CP Grade Separation EA - Utilities - Hydro One

***** Exercise caution. This is an EXTERNAL email. DO NOT open attachments or click links from unknown senders or unexpected email. *****

Hi Ian,

The Region of Peel, supported by CIMA+, are completing a Municipal Class EA Schedule 'C' study for a Coleraine Drive Grade Separation, in Bolton. You would have previously received correspondence on the study, most recently regarding the PIC #2 that occurred at the end of last year. More information, including the PIC materials, can be found on the project website here: [Coleraine Drive grade separation Environmental assessment - Region of Peel \(peelregion.ca\)](http://Coleraine Drive grade separation Environmental assessment - Region of Peel (peelregion.ca))

Part of this preliminary design study is the identification of utility impacts and options for potential mitigation (i.e., relocation). Please see attached utility plans, with and without imagery.

As seen, Hydro One has some infrastructure within the study area and while infrastructure along the local roads would largely be unimpacted, any infrastructure along Coleraine Drive is likely to be impacted and would require relocation. This is mostly due to the large construction required – large fill and retaining walls will be used along Coleraine Drive to facilitate the raise over the CP rail lines (as seen in the attached cross-section). Also as detailed in the cross-section,

there will be a buffer between the retaining wall and ROW boundary along Coleraine Drive, and it is within this area that we are proposing impacted utilities be relocated.

The impacts, final design and final relocation plans/agreements will be confirmed in Detailed Design, however we are reaching out to you at this time to confirm:

- If you have any objection to the strategy of potentially relocating the impacted infrastructure on Coleraine Drive to the 'buffer' outside of the walls.
- If Hydro One are planning any future infrastructure improvements or future new infrastructure along Coleraine Drive

If you have any questions, please let us know.

If I can kindly have a response by February 9th, it will be greatly appreciated.

Many Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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A-3

Appendix A-3-4: Rogers

March 21, 2017

Mr. Richard Humpage
EA Coordinator
Rogers Cable
244 Newkirk Road
Richmond Hill, ON L4C 3S5

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

Public consultation is an integral component of the Municipal Class EA process and opportunities to comment are welcomed throughout the entire study. Consultation activities will be announced at key milestones in the project.

You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Comment Sheet

March 21, 2017

Please Return the Completed Comment Sheet to:

P. Gino Dela Cruz, P.Eng

Project Manager, Infrastructure Programming & Studies, Transportation Division, Public Works

Region of Peel

10 Peel Centre Drive, Suite B

Brampton, ON L6T 4B9

Tel: 905-791-7800 Fax: 905-791-1442

Email: gino.delacruz@peelregion.ca

FROM:

ROGERS COMMUNICATIONS CANADA INC.
ATTN: DENNIS BAIRDON

Re:

Class Environmental Assessment

Coleraine Drive Grade Separation

- We have no concerns and do not need to be involved in this study.
- We have no interest/concerns at this time, but wish to remain on the contact list for this study.
- We have the following comment(s) and/or information requirements.

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Dayalani, Deepak <Deepak.Dayalani@Telecon.ca>
Sent: August 21, 2019 4:15 PM
To: Jessica Dorgo
Cc: GTAW.Markups@rci.rogers.com
Subject: M193614_20193314231_COLERAINE DR
Attachments: M193614_20193314231_COLERAINE DR.dwg; M193614_20193314231_COLERAINE DR_Rogers Markup Response Letter .pdf

Hi,

Please find attached completed markup request.

***** Please use "REGEN" command for proper line style *****

Thank you.

Regards,

Deepak Dayalani

CAD Technician, Engineering - Central Canada
Technicien CAO, Ingénierie - Centre du Canada

T 289-657-8160
7777 Weston Rd, Woodbridge (Ontario) L4L 0G9



telecon.ca

From: GTA Mark-ups <GTA.Markups@rci.rogers.com>
Sent: Wednesday, August 14, 2019 9:48 AM
To: Rogers.MOC <Rogers.MOC@telecon.ca>
Subject: Fw: Request 20193314231

From: Solutions@on1call.com <Solutions@on1call.com>
Sent: Wednesday, August 14, 2019 9:46 AM
To: GTA Mark-ups <GTA.Markups@rci.rogers.com>
Subject: Request 20193314231

ONTARIO ONE CALL

NOTICE OF INTENT TO EXCAVATE Header Code:PLANNING UPDATE
Ticket No: 20193314231 Seq. No: 2680
Update of: 2019338719

Send To: ROGPEL01 Seq No: 2680 Map Ref: 905 857

Original Call Date: 08/14/2019 Time: 09:46:42 AM OP: 1806
Transmit Date: 08/14/2019 Time: 09:46:42 AM
Work to Begin Date: 08/21/2019 Time: 08:00:00 AM

Company: CIMA Canada Inc
Contact Name: Jessica Dorgo Contact Phone: (289)288-0287
Alternate Contact: Altern. Phone:
Best Time to Call: 8-5 Fax No:
Cell Phone: Pager No:
Caller Address: 3027 Harvester Road
Burlington, ON L7N3G7
Email Address: jessica.dorgo@cima.ca

Reg/County: PEEL City: CALEDON
Address: , COLERAINE DR
Lot/Unit#:
To Address:
Nearest Intersecting Street: KING ST W (REGIONAL ROAD 9)
2nd Intersecting Street: HOLLAND DR
Community: BOLTON
Nb of Segments: 5
WAP No:
Latitude: 43.86890600 Longitude: -79.74706750

Work Extent/Locn: CORLOT=U Planning an design request only - not a dig request.

Remarks: DEPTH UNKNOWN

Type of Work: DESIGN AND PLANNING Depth: 0.00 FT
Public property: NO Mark & Fax: NO Area is Not Marked: NO Machine Dig: NO
Private property: NO Site Meet Req.: NO Premarked: NO Hand Dig: NO
Directional Drilling: NO

Work Being Done For: REGION OF PEEL

Sending to: (listing of utilities tkt sent to)
BCPRE BELL CANADA - PLANNI H2GW02 G-TEL FOR HYDRO ONE
ROGPEL01 CCS FOR ROGERS (ROGP

Note: -C = Cleared, -S = Supressed, -L = Lookup center cleared, -A = Alternate Locate
-R = Existing locate valid - maintain marks

This communication is confidential. We only send and receive email on the basis of the terms set out at
www.rogers.com/web/content/emailnotice

Ce message est confidentiel. Notre transmission et réception de courriels se fait strictement suivant les modalités
énoncées dans l'avis publié à www.rogers.com/aviscourriel

Markup Response Form

Application Date: August 14, 2019 **Applicant:** CIMA Canada Inc
Date Returned: August 21, 2019
Rogers Ref. No.: M193416 **Applicant Job No.:** 20193314231
Location: Coleraine Dr

Rogers Communications has reviewed your drawing(s) as requested and returns one marked-up copy. Our comments follow below with an "X" indicating Rogers' stance on your proposed plan.

Comments:

- No Conflict** Rogers Communications currently does not possess existing plant in the area indicated on your attached plans.

- No Conflict** Rogers Communications currently has existing plant as marked on your drawing. Our standard depth in this
For your Reference municipality is: **1m**.
Please ensure you maintain clearances of 0.3m vertically and 0.6m horizontally.

- EXTREME CAUTION** Use vactruck and expose ducts, maintain minimum of 0.6m clearance.

- CONFLICT** Your proposed construction appears to encroach within existing Rogers Communications plant.
Please relocate your proposed construction to allow adequate clearance of 0.3 m vertically and 1 m horizontally.

- CAUTION** Rogers Communications has aerial plant in this area, as it is indicated on the attached plans.
- CAUTION** Fiber Optic Cable is present in the area of your proposed construction.

- Note** Proposed Fiber Optic Cable in a joint use duct structure .
- Note** Plant currently under construction.
- Note** Please inform Rogers Communications well in advance of the proposed construction schedule in order to coordinate our plant relocation.

- Note** Locates are still required. Call for locates at 1-800-738-7893
- Note** Hand dig when crossing, or within 1.0m of existing Rogers plant.
- Note** Plant is to Approximation.

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:41 AM
To: richard.humpage@rci.rogers.com
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Rogers Cable Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Mr. Richard Humpage
EA Coordinator
Rogers Cable
244 Newkirk Road
Richmond Hill, ON L4C 3S5

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CANADA 2021

Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: David Hiett
Sent: February 1, 2022 11:43 AM
To: richard.humpage@rci.rogers.com
Cc: Stephen Keen; Mahmood, Tareq
Subject: B738 Coleraine Drive CP Grade Separation EA - Utilities - Rogers
Attachments: B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02.pdf; B000738 Road over Rail Utility Conflicts and Potential Relocation - DRAFT e04v02 with Image.pdf; Coleraine Drive GS EA - PIC Cross-section.jpg

Hi Richard,

The Region of Peel, supported by CIMA+, are completing a Municipal Class EA Schedule 'C' study for a Coleraine Drive Grade Separation, in Bolton. You would have previously received correspondence on the study, most recently regarding the PIC #2 that occurred at the end of last year. More information, including the PIC materials, can be found on the project website here: [Coleraine Drive grade separation Environmental assessment - Region of Peel \(peelregion.ca\)](https://www.peelregion.ca/coleraine-drive-grade-separation-environmental-assessment)

Part of this preliminary design study is the identification of utility impacts and options for potential mitigation (i.e., relocation). Please see attached utility plans, with and without imagery.

As seen, Rogers has some infrastructure within the study area, and while infrastructure along the local roads would largely be unimpacted, the infrastructure along Coleraine Drive is likely to be impacted and would require relocation. This is mostly due to the large construction required – large fill and retaining walls will be used along Coleraine Drive to facilitate the raise over the CP rail lines (as seen in the attached cross-section). Also as detailed in the cross-section, there will be a buffer between the retaining wall and ROW boundary along Coleraine Drive, and it is within this area that we are proposing impacted utilities be relocated.

The impacts, final design and final relocation plans/agreements will be confirmed in Detailed Design, however we are reaching out to you at this time to confirm:

- If you have any objection to the strategy of potentially relocating the impacted infrastructure on Coleraine Drive to the 'buffer' outside of the walls.
- If Rogers are planning any future infrastructure improvements or future new infrastructure along Coleraine Drive

If you have any questions, please let us know.

If I can kindly have a response by February 9th, it will be greatly appreciated.

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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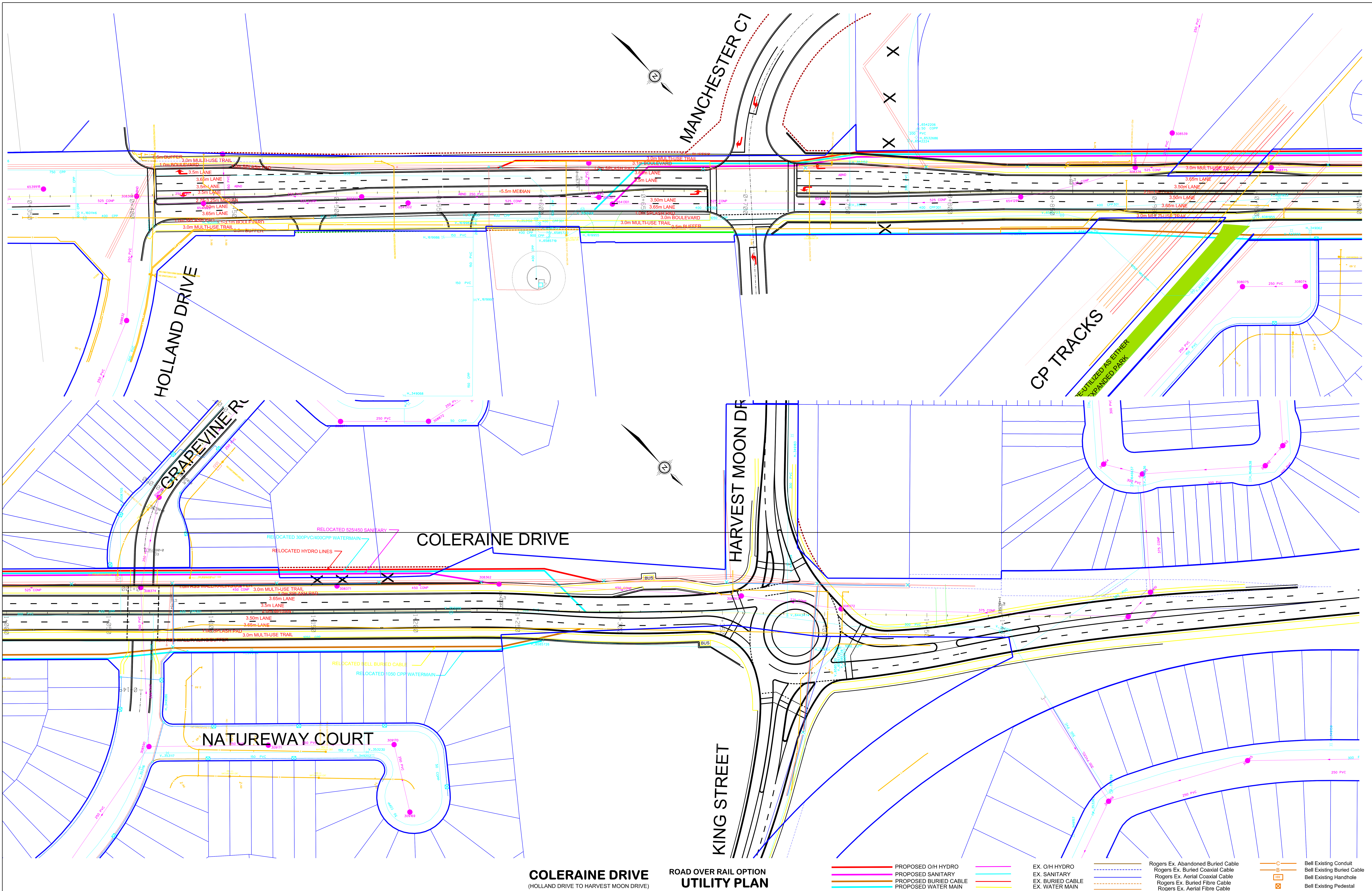


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CANADA 2019

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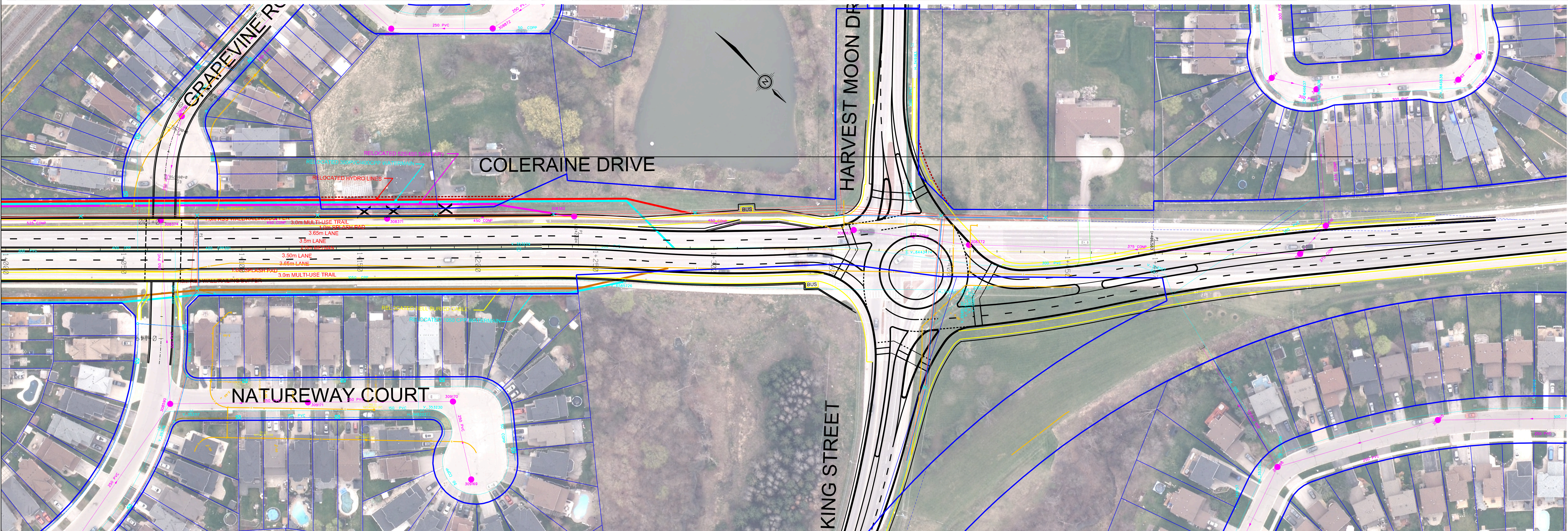
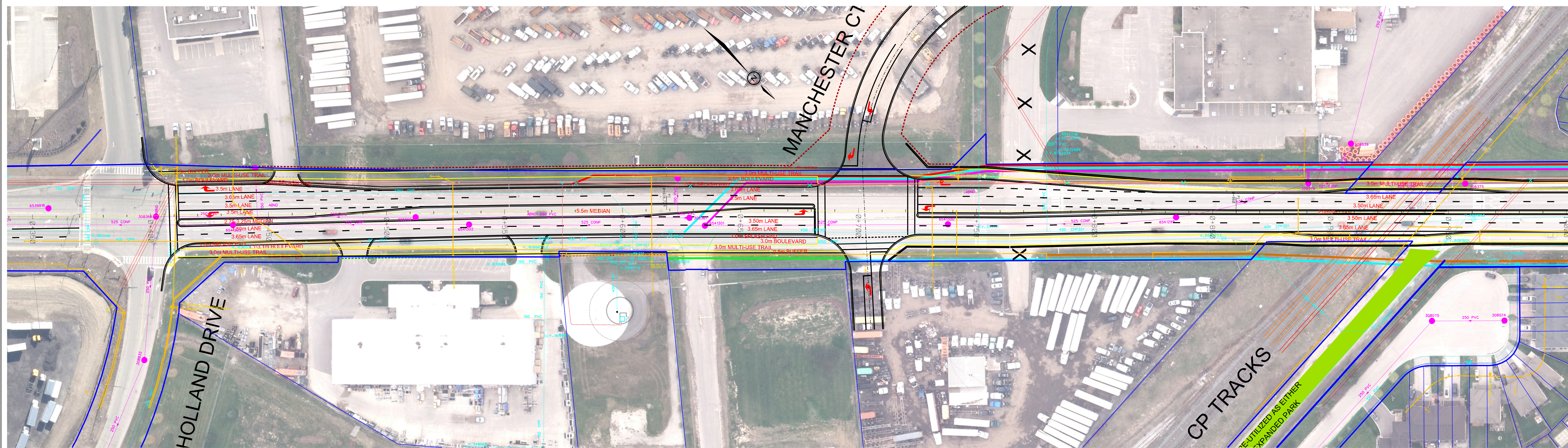
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COLERAINE DRIVE
(HOLLAND DRIVE TO HARVEST MOON DRIVE)






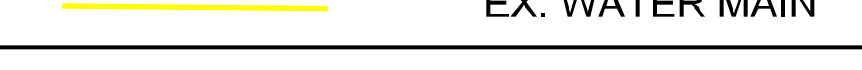

ROAD OVER RAIL OPTION
UTILITY PLAN

- | | | | | | | | | | | | | | | | | |
|--------------------|-------------------|-----------------------|---------------------|---------------|--------------|------------------|----------------|-----------------------------------|---------------------------------|---------------------------------|-------------------------------|-------------------------------|-----------------------|----------------------------|------------------------|------------------------|
| PROPOSED O/H HYDRO | PROPOSED SANITARY | PROPOSED BURIED CABLE | PROPOSED WATER MAIN | EX. O/H HYDRO | EX. SANITARY | EX. BURIED CABLE | EX. WATER MAIN | Rogers Ex. Abandoned Buried Cable | Rogers Ex. Buried Coaxial Cable | Rogers Ex. Aerial Coaxial Cable | Rogers Ex. Buried Fibre Cable | Rogers Ex. Aerial Fibre Cable | Bell Existing Conduit | Bell Existing Buried Cable | Bell Existing Handhole | Bell Existing Pedestal |
| (Red line) | (Magenta line) | (Orange line) | (Cyan line) | (Yellow line) | (Green line) | (Blue line) | (Purple line) | (Dashed red line) | (Dashed magenta line) | (Dashed orange line) | (Dashed cyan line) | (Dashed yellow line) | (Orange circle) | (Magenta circle) | (Blue square) | (Purple square) |



COLERAINE DRIVE
(HOLLAND DRIVE TO HARVEST MOON DRIVE)

ROAD OVER RAIL OPTION
UTILITY PLAN

- | | | | |
|---|--|---|--|
|  PROPOSED O/H HYDRO |  EX. O/H HYDRO |  Rogers Ex. Abandoned Buried Cable |  Bell Existing Conduit |
|  PROPOSED SANITARY |  EX. SANITARY |  Rogers Ex. Buried Coaxial Cable |  Bell Existing Buried Cable |
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|  PROPOSED WATER MAIN |  EX. WATER MAIN |  Rogers Ex. Buried Fibre Cable |  Bell Existing Pedestal |
| | |  Rogers Ex. Aerial Fibre Cable | |

Kate Barclay

From: Emad Eltowwi <Emad.Eltowwi@rci.rogers.com>
Sent: February 20, 2022 10:38 PM
To: Jim Washburn; David Hiett; Mike Coombe
Cc: Stephen Keen; Mahmood, Tareq
Subject: RE: B738 Coleraine Drive CP Grade Separation EA - Utilities - Rogers

EXTERNAL EMAIL

Hi All,
Mike will be the planner for this project.

Best Regards,

Emad Eltowwi

Mgr Outside Plant Engineering - GTAW,

Interim OPE Manager - SWO

Wireline Implementation.

3573 Wolfedale Rd

Mississauga, ON L5C 3T6

Emad.eltowwi@rci.rogers.com

m (647) 294 4609



From: Jim Washburn <Jim.Washburn@rci.rogers.com>
Sent: February 1, 2022 1:21 PM
To: David Hiett <David.Hiett@cima.ca>; Emad Eltowwi <Emad.Eltowwi@rci.rogers.com>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Subject: RE: B738 Coleraine Drive CP Grade Separation EA - Utilities - Rogers

+ Emad Eltowwi

Hello David.

This would need to go to one of our forced planners. Currently I'm engulfed in Metrolinx transportation initiatives. Emad is the Manager who will assign the appropriate personnel and have them contact you promptly.

Thanks

Jim Washburn
System Planner
Rogers Communications Inc.
3573 Wolfedale Road
Mississauga Ontario
905.724.0531

From: David Hiett <David.Hiett@cima.ca>
Sent: February 1, 2022 11:56 AM

To: Jim Washburn <Jim.Washburn@rci.rogers.com>

Cc: Stephen Keen <Stephen.Keen@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>

Subject: B738 Coleraine Drive CP Grade Separation EA - Utilities - Rogers

Hi Jim,

Your contact details were passed on to us by Richard Humpage, as the best contact for the below request.

The Region of Peel, supported by CIMA+, are completing a Municipal Class EA Schedule 'C' study for a Coleraine Drive Grade Separation, in Bolton. Our previous notifications had been sent to Richard, however it is now our understanding that he no longer looks after Peel. The most recent correspondence sent on the study was regarding the PIC #2 that occurred at the end of last year. More information, including the PIC materials, can be found on the project website here: [Coleraine Drive grade separation Environmental assessment - Region of Peel \(peelregion.ca\)](#)

Part of this preliminary design study is the identification of utility impacts and options for potential mitigation (i.e., relocation). Please see attached utility plans, with and without imagery.

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- If Rogers are planning any future infrastructure improvements or future new infrastructure along Coleraine Drive

If you have any questions, please let us know.

If I can kindly have a response by February 9th, it will be greatly appreciated.

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Appendix A-3-5: TransCanada Pipelines

March 21, 2017

Ms. Darlene Presley
C/O Lehman & Associated - Urban Planning Consultants
TransCanada Pipelines Limited
97 Collier Street
Barrie, ON L4M 1H2

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

Public consultation is an integral component of the Municipal Class EA process and opportunities to comment are welcomed throughout the entire study. Consultation activities will be announced at key milestones in the project.

You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:38 AM
To: dqilty@mhbcplan.com
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; TransCanada Pipelines Limited Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Ms. Darlene Quilty
MHBC Planning, Urban Design & Landscape Architecture
On Behalf of TransCanada Pipelines Limited
442 Brant Street, Suite 204
Burlington, ON L7R 2G4

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Sonya Bubas, MCIP, RPP
Project Manager
Infrastructure Programming & Studies
Tel: 905-791-7800 ext. 7801
Email: sonya.bubas@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CANADA 2019

A-3

Appendix A-3-6: Bell

March 21, 2017

Ms. Diana Velez
c/o Netricom
Bell Canada Municipal Operations Centre
200 Town Centre Boulevard, Suite 300
Markham, ON L3R 8G5

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

Public consultation is an integral component of the Municipal Class EA process and opportunities to comment are welcomed throughout the entire study. Consultation activities will be announced at key milestones in the project.

You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:40 AM
To: bell.moc@netricom.com
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Bell Canada Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Ms. Diana Velez
c/o Netricom
Bell Canada Municipal Operations Centre
200 Town Centre Boulevard, Suite 300
Markham, ON L3R 8G5

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CANADA 2021

Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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A-4

Appendix A-4: Stakeholder Consultation

A-4-1: Town of Caledon

A-4-2: Caledon Cycling Task Force

A-4-3: Emergency Services

A-4

Appendix A-4-1: Town of Caledon

Kate Barclay

From: Danita Taccogna <danita.taccogna@caledon.ca>
Sent: March 21, 2017 3:07 PM
To: Jessica Dorgo
Subject: RE: Region of Peel- Coleraine Drive Grade Separation EA Notice of Study Commencement

Follow Up Flag: Follow up
Flag Status: Completed

Hi Jessica,

I will circulate to appropriate staff.

Thanks
Danita

Danita Taccogna
Supervisor, FIS Administration
Finance and Infrastructure Services

Town of Caledon
6311 Old Church Road
Caledon, ON L7C 1J6
1.888.225.3366
905.584.2272 x.4226

www.caledon.ca
www.visitcaledon.ca

From: Jessica Dorgo [mailto:Jessica.Dorgo@cima.ca]
Sent: Tuesday, March 21, 2017 1:06 PM
To: Danita Taccogna
Cc: Stephen Keen; Dela Cruz, Gino
Subject: Region of Peel- Coleraine Drive Grade Separation EA Notice of Study Commencement

Good Afternoon,

Please find the attached Notice of Study Commencement for the Region of Peel Municipal Class Environmental Assessment Study for Coleraine Drive Grade Separation. If you have any comments or questions please use the enclosed response form or contact the Project Manager on the attached Notice.

Thank you,

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation

CIMA+
Partners in Excellence

March 21, 2017

Danita Taccogna
Supervisor, FIS Administration, Finance and Infrastructure Services
Town of Caledon

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

Public consultation is an integral component of the Municipal Class EA process and opportunities to comment are welcomed throughout the entire study. Consultation activities will be announced at key milestones in the project.

You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: August 20, 2019 11:43 AM
To: douglas.mcglynn@caledon.ca
Cc: Stephen Keen
Subject: Region of Peel - Coleraine Drive Grade Separation EA - Cultural Heritage Assessment Report

Good Morning Douglas,

As you may be aware, the Region of Peel is undertaking a Class EA study for the Grade Separation of Coleraine Drive with the CP Rail Line in the Town of Caledon. As part of this study, a Cultural Heritage Assessment was undertaken and a copy of the Draft Cultural Heritage Assessment report is available on the file transfer site below. Can we kindly ask that you please review this report and advise if you have any comments on our study.

Please note that this is a draft report. The plans presented in the report are preliminary and have not been shown to the public. Additionally, the report mentions a widening of Coleraine Drive to 4-lanes however we would like to note that the existing roadway is presently 4-lanes. This will be corrected in the final version of the report.

[Access the file transfer site](#)

Please feel free to contact me if you have any questions.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:42 AM
To: danita.taccogna@caledon.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Caledon Cycling Task Force Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Danita Taccogna
Supervisor, FIS Administration, Finance and Infrastructure Services
Caledon Cycling Task Force

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

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We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Bubas, Sonya <sonya.bubas@peelregion.ca>
Sent: December 12, 2019 11:12 AM
To: Ryan A. Grodecki (ryan.grodecki@caledon.ca)
Cc: Cassie.Schembri@caledon.ca; Paquette, Samantha; Stephen Keen; Jessica Dorgo
Subject: FW: Notice of Commencement - Town of Caledon, Municipal Class EA for Coleraine Drive and King Street West Stormwater Outfall Erosion Control Project
Attachments: NoticeOfCommencement-Final.pdf

Hi Ryan,

Thank you for taking the time to speak with me today and for assisting us with the following in Cassie's absence:

As discussed, we are reviewing the potential impacts between the Town's outfall erosion control project and the Region's grade separation EA. Further to our discussion, I noticed the PIC for the Town's EA was held on October 30 and we can review the materials at <https://www.caledon.ca/en/news/index.aspx?feedId=3128FF83-64FD-4E32-80D2-FC0FCOABFECD&catid=e8e226fd-74bb-4dbd-bf74-3c5b74477083&newsId=5e438caa-93de-486c-a3db-930cc428ef19>.

Could you let me know the critical dates for the Town's EA (e.g., when you would need to receive our comments)?

Could you also forward any updated information that you think we should review (the last set of information we received from Cassie was in July 2019, which included drawings and the Stormwater Management report).

Thank you for your help,

Regards,

Sonya Bubas, MCIP, RPP
Project Manager
Infrastructure Programming & Studies
Region of Peel
10 Peel Centre Dr., Suite B, 4th Floor
Brampton, ON L6T 4B9
905-791-7800 ext. 7801



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TOWN OF CALEDON PUBLIC NOTICE

Coleraine Drive and King Street West Stormwater Outfall Erosion Control Project Municipal Class Environmental Assessment

THE STUDY:

The Town of Caledon has initiated a Schedule 'B' Municipal Class Environmental Assessment Study to address erosion concerns downstream of Coleraine Drive on the south side of King Street West. The Coleraine Drive culvert outlet is perched and the channel is incised with eroded vertical banks. The current study will be undertaken as an Environmental Assessment to develop, evaluate, and recommend alternatives for reducing the risks from creek erosion in this area. To select a preferred solution, the alternatives will be evaluated based on physical, natural, social, economic, and technical criteria. The existing conditions, evaluation of alternatives, and recommended solution will be presented to the public and regulatory agencies for comment.



THE PROCESS:

The study is being carried out in accordance with the requirements of a Schedule B project under the Municipal Engineers Association (MEA) Municipal Class EA document (October 2000, as amended in 2007, 2011 and 2015), which is an approved process under the Ontario Environmental Assessment Act.

Public Participation forms an integral part of the Schedule 'B' Class EA Study to ensure that the ongoing concerns of the public and affected groups within the study area are identified, documented and addressed. One Public Information Centre (PIC) is being planned for fall of 2019 to discuss issues related to the project, including alternative solutions, evaluation criteria, environmental impacts and mitigation measures. You are encouraged to provide your comments so that they may be incorporated into the planning of this project.

Once the study is complete, the Project File containing a complete record of the activities associated with the planning of the project will be made available for public review and comment. A Notice of Completion will be issued at that stage.

Please contact either one of the following project representatives if you would like further information on the project, if you have any questions or comments, or if you would like to be added to the study mailing list.

Cassie Schembri
Senior Project Manager, Stormwater
Town of Caledon
6311 Old Church Road
Caledon, ON L7C 1J6
Tel: 905-584-2272 x4258
Fax: 905-584-4325
E-mail: Cassie.Schembri@caledon.ca

Roger Phillips, Ph.D., P.Geo.
Environmental Geoscientist,
Consultant Project Manager
Aquafor Beech Limited
#6-202-2600 Skymark Avenue
Mississauga, ON L4B 5B2
Tel: 905-629-0099 x235
E-mail: phillips.r@aquaforb企业.com

Notice First Posted: October 17, 2019

Kate Barclay

From: Stephen Keen
Sent: May 6, 2020 10:49 AM
To: Arash Olia; Jessica Dorgo
Cc: Mahmood, Tareq
Subject: RE: Region of Peel - Coleraine Drive Grade Separation Class EA

Hi Arash

On this bus-stop issue, regardless of roundabout or traffic signal, there is a challenge getting bus stops on Coleraine because of the steep banks on either side of Coleraine at this point and the need to avoid additional fill into the valley; that is why from a space point of view we would ideally recommend the stops to be on King Street.

Depending on the transit authority, some like bus bays, some don't – there is no preferred approach, just different opinions – you can let us know whether you prefer them for Caledon.

FHWA generally recommends multi-lane roundabouts have far side stops as near side tend to block sight of the pedestrian crossing (unless the bus stop is pulled somewhat back which is generally not advisable) .

So for the northbound (on Coleraine) direction of the current bus service a far side stop on King is the best option advisable – we can also include a bus bay if needed (we also have more land available at that point).

For the southbound direction – FHWA would suggest the stop on Coleraine (where property is really tight) - a near side stop with bus-bay on King would be easier to do, but think we can just squeeze one in where we have shown it (detail design would have to confirm 100% its feasibility).

So, my recommendation at this time would be to move the NB stop to King Street with bus bay – please let me know your thoughts on this matter.

Regards

STEPHEN KEEN, M.Sc., P.Eng.
Associate Partner / Director Transportation / Traffic Engineering

T 289-288-0287 ext. 6834 M 905-802-6523 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



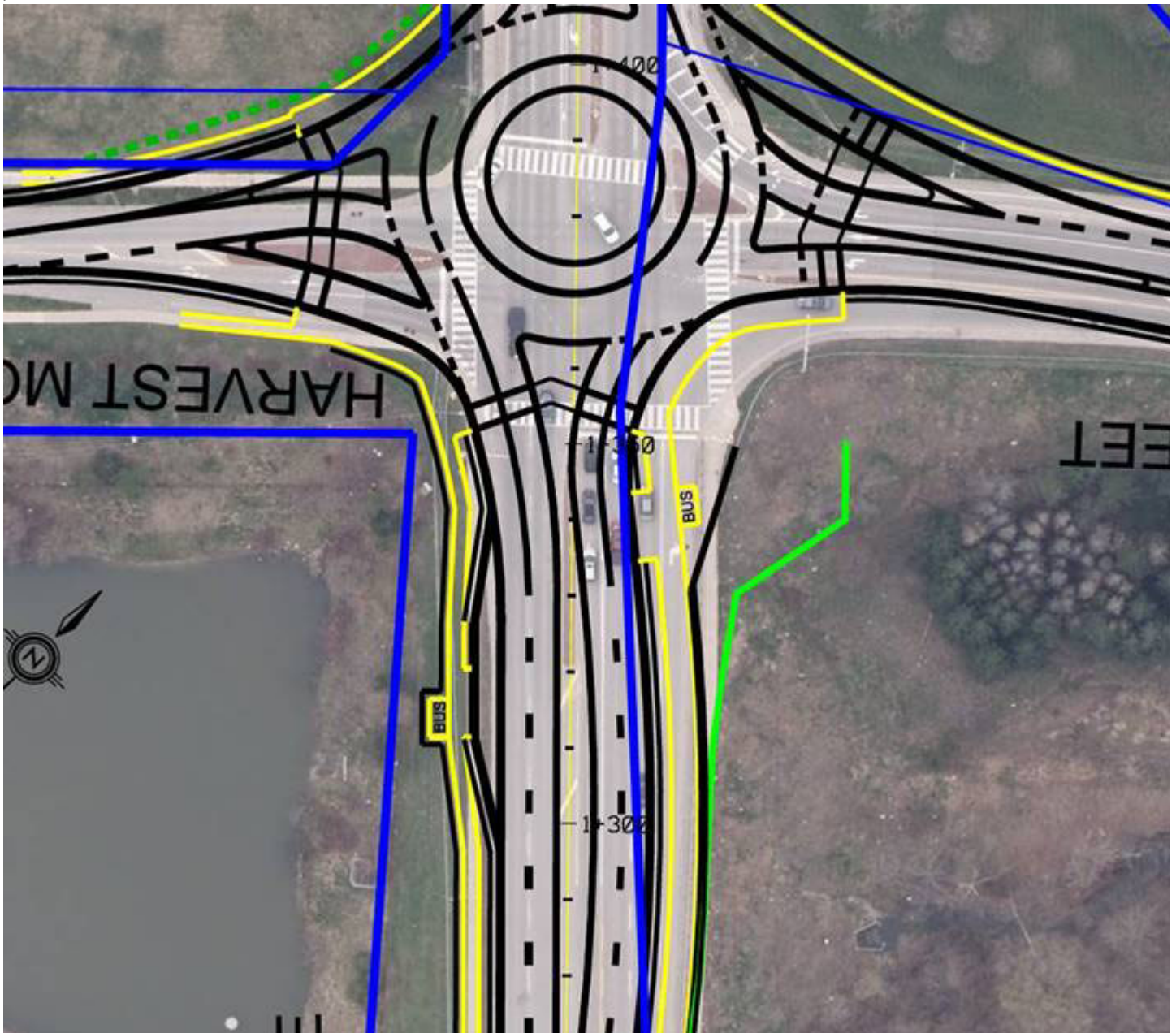
From: Arash Olia <Arash.Olia@caledon.ca>
Sent: May 5, 2020 7:52 PM
To: Jessica Dorgo <Jessica.Dorgo@cima.ca>

Cc: Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Stephen Keen <Stephen.Keen@cima.ca>
Subject: RE: Region of Peel - Coleraine Drive Grade Separation Class EA

Hi Jessica,

In the northbound direction, will the bus stop on the travel lane?

Thanks,



Arash Olia, Ph.D., P.Eng.
Manager, Transportation Engineering
Finance & Infrastructure Services

Office: 905.584.2272 x.4073
Cell: 416.452.7091
Email: arash.olia@caledon.ca

Town of Caledon | www.caledon.ca | www.visitcaledon.ca | Follow us @YourCaledon

From: Jessica Dorgo <Jessica.Dorgo@cima.ca>
Sent: Monday, May 4, 2020 1:31 PM
To: Arash Olia <Arash.Olia@caledon.ca>
Cc: Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Stephen Keen <Stephen.Keen@cima.ca>
Subject: RE: Region of Peel - Coleraine Drive Grade Separation Class EA

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Hi Arash,

We have updated one of the options (rail over road with roundabout) to reflect the addition of the relocated bus stops based on your comments. We have added one bus bay and one bus stop at the King Street/Harvest Moon Drive intersection. The drawing is available on the file transfer site below for your review.

https://cimeo365-my.sharepoint.com/:f:/g/personal/jessica_dorgo_cima_ca/Epm38sus8vFGqK63Oiqw7Y8BY9rnNvz9EDMsc-9RvWTw8Q?e=RqVcNT

Please let us know if you have any comments.

Thank you,

JESSICA DORGO, EIT
EIT / Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



From: Arash Olia <Arash.Olia@caledon.ca>
Sent: Tuesday, April 28, 2020 8:52 PM
To: Jessica Dorgo <Jessica.Dorgo@cima.ca>
Cc: Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Stephen Keen <Stephen.Keen@cima.ca>
Subject: RE: Region of Peel - Coleraine Drive Grade Separation Class EA

Hi Jessica,

We do have bus stops on both sides of Coleraine Drive at Elwood Drive as well Holland Drive at Coleraine Drive. As per your drawings, I do not think the ones of Holland Drive will be affected.

Our Bus Stop route is based on our Transit Feasibility Study, and I disagree with changing that and bring it to local streets, and I do not see any reason to do so.

One option for the Town to consider is to shift them toward King Street. That is, on both sides of Coleraine Drive at Harvest Moon Drive/King Street. You need to consider that as part of your recommendation for designing the roundabout and provide the options to the Town for review.

Thanks,

Arash Olia, Ph.D., P.Eng.

Manager, Transportation Engineering
Finance & Infrastructure Services

Office: 905.584.2272 x.4073

Cell: 416.452.7091

Email: arash.olia@caledon.ca

Town of Caledon | www.caledon.ca | www.visitcaledon.ca | Follow us @YourCaledon

From: Jessica Dorgo <Jessica.Dorgo@cima.ca>

Sent: Tuesday, April 28, 2020 10:33 AM

To: Arash Olia <Arash.Olia@caledon.ca>

Cc: Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Stephen Keen <Stephen.Keen@cima.ca>

Subject: Region of Peel - Coleraine Drive Grade Separation Class EA

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Hi Arash,

As part of the Coleraine Drive Grade Separation Class EA being undertaken by the Region of Peel, we are evaluating the bus stop locations along the corridor. Our study limits include Coleraine Drive from Harvest Moon Drive/King Street to Holland Drive. In reviewing the Caledon Public Transit routes, we noted that the Bolton line has two stops within the study area. The approximate locations are shown on the attached image however they are not visible on Google Earth. Can you please confirm if the bus stops are located on Coleraine Drive, Holland Drive, and/or Old Ellwood Drive and if they are present on both sides of the road?

As part of the grade separation, we would like to propose that the bus stops be located on the residential streets rather than Coleraine Drive. With the implementation of the grade separation, there will not be any opportunities for bus stops on the structure given the size. Old Ellwood Drive will also be closed at Coleraine Drive which will require the relocation of the bus stop at this location. Can you please advise how the Town of Caledon would like to proceed with the transit stop relocations? A copy of the draft design plans are available on the file transfer site below for the road-over-rail and road-under-rail options. Please note that we are considering a roundabout and a traffic signal at the intersection of Harvest Moon Drive/King Street and Coleraine Drive. Drawings are available for each option.

https://cimeo365-my.sharepoint.com/:f/g/personal/jessica_dorgo_cima_ca/Epl2mAb1RG1rR2lWXa3pjABArWjiB8BaeEtddbOREKUAA?e=lk1Rw2

Please let us know if you would like to arrange a teleconference to discuss further.

Thank you,

JESSICA DORGO, EIT
EIT / Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: David Hiett
Sent: November 10, 2021 12:03 PM
To: Cassie Schembri
Cc: Stephen Keen; Mahmood, Tareq; Kevin Lukawiecki
Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive
Attachments: B000738 - LID Assessment Memo_e06_reduced.pdf; B000738 - SWMP 9 Capacity Memo_e05_reduced.pdf

Hi Cassie,

Thanks again for the information previously provided. For your review and comment, we have completed our update of the Pond 9 Capacity Memo and also the LID Assessment Memo regarding the Coleraine Drive Grade Separation EA study.

As previously indicated and in summary, the proposed SWM strategy includes directing the affected drainage into the existing SWMP 9 to provide quality and quantity control. The improvements to Coleraine Drive will lead to a minor increase in impervious area and so it is proposed to implement LID options to infiltrate stormwater runoff before it reaches SWMP 9 to mitigate the impacts that the increased imperviousness has on the water balance of the study area.

Please let us know if you have any questions or require any further information,

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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From: Cassie Schembri <Cassie.Schembri@caledon.ca>
Sent: Thursday, October 28, 2021 2:40 PM
To: David Hiett <David.Hiett@cima.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive

EXTERNAL EMAIL

Hi David,

The results of the bathymetric survey are in the appendix of the report shared.

Thanks,

Cassie

Cassie Schembri

Senior Project Manager, Stormwater
Engineering Services Department

6311 Old Church Road
Caledon, ON L7C 1J6
Cell: 647.327.0060
Office: 905.584.2272 ext. 4371

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From: David Hiett <David.Hiett@cima.ca>

Sent: Thursday, October 28, 2021 2:38 PM

To: Cassie Schembri <Cassie.Schembri@caledon.ca>

Cc: Stephen Keen <Stephen.Keen@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>

Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Hi Cassie,

Thanks very much for the Report provided, can you also provide the bathymetric survey completed?

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CONFIDENTIALITY WARNING This email is confidential. If you are not the intended recipient, please notify the sender immediately and delete it in its entirety.

From: Cassie Schembri <Cassie.Schembri@caledon.ca>
Sent: Monday, October 25, 2021 3:30 PM
To: Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; David Hiett <David.Hiett@cima.ca>
Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive

EXTERNAL EMAIL

Hi Tareq,

Please find attached a memo developed by the Town's consultant reviewing the Pond's capacity.

Thanks,
Cassie

Cassie Schembri
Senior Project Manager, Stormwater
Engineering Services Department

6311 Old Church Road
Caledon, ON L7C 1J6
Cell: 647.327.0060
Office: 905.584.2272 ext. 4371

Town of Caledon | www.caledon.ca | www.visitcaledon.ca | Follow us @YourCaledon

From: Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Sent: Wednesday, October 20, 2021 3:13 PM
To: Cassie Schembri <Cassie.Schembri@caledon.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; David Hiett <David.Hiett@cima.ca>
Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive
Importance: High

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Hello Cassie,

I am following up on the request below; CIMA+ has been waiting for the information to firm up the analysis. Without the requested information, it would be challenging to review the analysis.

Best regards,

Tareq Mahmood
Project Manager, Infrastructure Programming and Studies
Transportation, Public Works, Region of Peel
Cell:905-872-6475
Email: tareq.mahmood@peelregion.ca



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which is confidential or privileged. Any unauthorized use or disclosure of this email is prohibited. If you are not the intended recipient or have received this e-mail in error, please notify the sender via return email and permanently delete all copies of the email. Thank you.

From: Mahmood, Tareq
Sent: October 15, 2021 3:53 PM
To: Cassie Schembri <Cassie.Schembri@caledon.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; David Hiett <David.Hiett@cima.ca>
Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive
Importance: High

Hello Cassie,

Would you mind provide the requested information to CIMA+ at your earliest convenience?

FYI - we plan to hold the PIC#2 within a month and working towards setting up a meeting with Councillors sometime in early November. The SWM design/report is one of the last items on our list to be addressed prior to the PIC#2.

Have a good weekend!

Thanks,

Tareq Mahmood

PM, Infrastructure Programming
and Studies, Transportation
Cell: 905-872-6475

From: David Hiett <David.Hiett@cima.ca>
Sent: October 12, 2021 9:26 AM
To: Cassie Schembri <Cassie.Schembri@caledon.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive

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Hi Cassie,

I hope you had a great thanksgiving weekend!

I just wanted to remind about the data requested: previous study that identified the increase in impervious area and bathymetric survey completed.

Please provide the information and we can firm up our analysis for your review.

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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From: David Hiett
Sent: Friday, October 1, 2021 9:28 AM
To: Cassie Schembri <Cassie.Schembri@caledon.ca>
Cc: Stephen Keen <Stephen.Keen@cima.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive

Hi Cassie,

I am just following up on the information requested below – the increase in impervious area is something we need to confirm before completing our analysis, and so if you can provide the report and survey as soon as possible, it will be greatly appreciated!

Many Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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From: David Hiett <David.Hiett@cima.ca>
Sent: Tuesday, September 28, 2021 3:52 PM
To: Cassie Schembri <Cassie.Schembri@caledon.ca>; Ryan Grodecki <Ryan.Grodecki@caledon.ca>
Cc: Andrew Pearce <andrew.pearce@caledon.ca>; Stephen Keen <Stephen.Keen@cima.ca>; Arash Olia

<arash.olia@caledon.ca>; Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Margi Sheth <Margi.Sheth@caledon.ca>; Kevin Lukawiecki <Kevin.Lukawiecki@cima.ca>

Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive

Hi Cassie and Ryan,

Thanks again for our meeting today. To document the discussion, I made a few notes below:

- CIMA+ provided recap of SWM strategy and latest updates: the TRCA have reviewed the model and analysis, and approve in principle to the proposed strategy. CIMA+ recently received further comments which will be addressed.
- CIMA+ have updated the Pond 9 Capacity Memo and will shortly provide the updated version for the Town Review
 - It is understood that the primary function of the pond was for the subdivision, and so the Town needs to confirm the capacity analysis when considering the addition of Coleraine Drive
 - If the strategy is agreed to, the Town would also like to discuss a long-term agreement with the Region (i.e. cost-sharing / maintenance etc.)
- Town advised that recent study into Pond 9 indicated an increase in impervious cover from 49% to 57%. (see below requests)
- Town noted Holland Drive is currently experiencing SWM issues. CIMA+ confirmed that the proposed strategy for both Road Over Rail and Road Under Rail options would help as they reduce flow to Holland Drive

[@Cassie Schembri](#) – As mentioned, please can you provide the copy of the previous study that identified the increase in impervious area?

Please can you also provide the bathymetric survey completed?

We would like this information prior to completing our analysis updates

Many thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400–3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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-----Original Appointment-----

From: Andrew Pearce <andrew.pearce@caledon.ca>

Sent: Friday, September 24, 2021 9:12 AM

To: Andrew Pearce; Stephen Keen; David Hiett; Ryan Grodecki; Arash Olia; Mahmood, Tareq; Margi Sheth; Cassie Schembri

Subject: FW: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive
When: Tuesday, September 28, 2021 1:00 PM-1:30 PM (UTC-05:00) Eastern Time (US & Canada).
Where: Microsoft Teams Meeting

EXTERNAL EMAIL

-----Original Appointment-----

From: Andrew Pearce <Andrew.Pearce@caledon.ca>

Sent: August 19, 2021 8:45 AM

To: Andrew Pearce; Ryan Grodecki; Arash Olia; Mahmood, Tareq; Margi Sheth; Cassie Schembri

Subject: RE: Coleraine Drive Grade Separation EA/Project - Old Ellwood Drive

When: September 28, 2021 1:00 PM-1:30 PM (UTC-05:00) Eastern Time (US & Canada).

Where: Microsoft Teams Meeting

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To review the project progress and outstanding issues

Microsoft Teams meeting

Join on your computer or mobile app

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Kate Barclay

From: Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Sent: December 16, 2021 9:22 AM
To: Rook, Sally; Van Boxmeer, Kyle; Ansari, Seema; Hamdani, Hashim <hashimali.hamdani@peelregion.ca>; Hamdani, Hashim; Caughey, Rebecca; Saiyed, Sabbir; Lee, Arthur; Mannie, Sharon; Grzesiak, Ryan; Nevland, Erik; Fernandes, Trina; Smith, Neal; Hasselbacher, John; Gallagher, Joe; Saini, Sakshi; Eberhardt, Daniel; Koethe, Wayne; Thomsen, Jeanne; Bennington, Michael; Lotecki, Nancy; Woodtke, Trevor; Detaramani, Tina; ZYG-Peel Health & Built Environment; Peter Brocks; Stephen Keen; David Hiett; Kate Barclay; Suzanne Bevan; Emma Benko; Kristen Sullivan; Kant Chawla; Margi Sheth; Arash Olia; Ryan Grodecki; Tom_Twigge@cpr.ca; jennifer_benedict@cpr.ca; Andrew Pearce; Saddi, Asha; Cassie Schembri; Thomsen, Lori-Ann; Crawford, Heather; Lavhey, James; Kovach, Steven; Toy, William; Aymar, Matthew; Ursitti, Franca; Gardiner, Len; Elysia Friedl; Evan Bearss; Sinthujan Navaratnavel; Jehan Zeb; Tatla, Manvir; Parm Chohan; Cristina Guido; Katelyn Tozer
Cc: Avsec, Joe; Ricketts, Terry
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

EXTERNAL EMAIL

Hello TAC Members,

The second PIC for the Coleraine Drive Grade Separation EA is being held virtually from **December 16, 2021 to January 21, 2022**. Attached is the PIC 2 Notice which provides additional information on how you can provide your input and how you will be able to join the virtual event.

Please contact me if you require further information.

[How you can follow Coleraine Drive Grade Separation EA PIC-2:](#)

Visit the project website:

<https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/coleraine-drive.asp>

Link to PIC-2 presentation slides:

- <https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/pdf/coleraine/pic2-presentation.pdf>
- <https://www.peelregion.ca/pw/transportation/construction/environmental-assessment/pdf/coleraine/pic2-technical-environmental-assessments.pdf>

Best regards,

Tareq Mahmood

Project Manager, Infrastructure Programming and Studies
Transportation, Public Works, Region of Peel
Cell:905-872-6475
Email: tareq.mahmood@peelregion.ca

Kate Barclay

From: Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Sent: January 31, 2022 11:33 AM
To: Cassie Schembri
Cc: Margi Sheth; David Hiatt; Stephen Keen
Subject: Coleraine Drive CP Grade Separation - SWM Report

EXTERNAL EMAIL

Hello Cassie,

Regarding the Coleraine Grade Separation SWM report, CIMA+ has completed the final draft report for the Region and Town's review Report is saved in its related folder in the Technical Reports shared folder, link below:

 [B000738 Peel-CP Grade Separation EA - Files to Region](#)

The report has been updated with the final drainage strategy, with the additional memos/analysis forming Appendix E.

Please advise if the Town has any comments by February 28th, 2022.

Thanks,

Tareq Mahmood

PM, Infrastructure Programming
and Studies, Transportation
Tel: 905-791-7800 X 7828
Cell: 905-872-6475

A-4

Appendix A-4-2: Caledon Cycling Task Force

Kate Barclay

From: Jessica Dorgo
Sent: March 21, 2017 1:06 PM
To: danita.taccogna@caledon.ca
Cc: Stephen Keen; Dela Cruz, Gino
Subject: Region of Peel- Coleraine Drive Grade Separation EA Notice of Study Commencement
Attachments: B738_Peel Coleraine GS EA_Notice of Commencement Package_DTaccogna_e01.pdf

Good Afternoon,

Please find the attached Notice of Study Commencement for the Region of Peel Municipal Class Environmental Assessment Study for Coleraine Drive Grade Separation. If you have any comments or questions please use the enclosed response form or contact the Project Manager on the attached Notice.

Thank you,

Jessica Dorgo
EIT Transportation
Traffic Engineering, Transportation

CIMA+
Partners in Excellence

3027 Harvester Road, Suite 400
Burlington Ontario L7N 3G7
CANADA
Tel: 289-288-0287 ext. 6819 / Fax: 289-288-0285



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March 21, 2017

Danita Taccogna
Supervisor, FIS Administration, Finance and Infrastructure Services
Town of Caledon

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Study Commencement**

The Regional Municipality of Peel has initiated a Schedule 'C' Municipal Class Environmental Assessment to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to inform you of the study and to invite your input. A copy of the Notice of Commencement is attached.

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The objective of this Environmental Assessment is to identify a solution that will improve the safety and operational performance of Coleraine Drive.

Public consultation is an integral component of the Municipal Class EA process and opportunities to comment are welcomed throughout the entire study. Consultation activities will be announced at key milestones in the project.

You may use the enclosed response form to request additional information or provide comments about the study.

Sincerely

P. Gino Dela Cruz, P.Eng
Project Manager
Infrastructure Programming & Studies
Transportation Division, Public Works
Region of Peel
10 Peel Centre Drive, Suite B
Brampton, ON L6T 4B9
Tel: 905-791-7800 | Fax: 905-791-1442
Email: gino.delacruz@peelregion.ca

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:42 AM
To: danita.taccogna@caledon.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Caledon Cycling Task Force Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Danita Taccogna
Supervisor, FIS Administration, Finance and Infrastructure Services
Caledon Cycling Task Force

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:43 AM
To: Steve.Mathew@caledon.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA - Notice of Public Information Centre No. 1
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Caledon Cycling Task Force Letter (2).pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Steve Mathew
Traffic Technologist, Transportation, Finance and Infrastructure Services
Caledon Cycling Task Force

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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CANADA 2021

Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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A-4

Appendix A-4-3: Emergency Service Providers

Emergency Service Providers Contact List

Organization	Title	First Name	Last Name	Email
Peel Regional Police	Inspector	Andrew	Cooper	andrew.cooper@peelregion.ca
Peel Regional Paramedic Services	Chief	Peter	Dundas	peter.dundas@peelregion.ca
Peel Regional Paramedic Services	Divisional Commander	Brian	Parkes	brian.parkes@peelregion.ca
Peel Region	Emergency Management Specialist	Cheryl	Jamieson	cheryl.jamieson@peelregion.ca
Town of Caledon Fire and Emergency Services	Deputy Chief	Dave	Pelayo	dave.pelayo@caledon.ca
Town of Caledon Fire and Emergency Services	Chief Fire Prevention Officer	Anthony	Staniscia	anthony.staniscia@caledon.ca
Ontario Provincial Police	Inspector	Mike	Garant	mike.garant@opp.ca

Kate Barclay

From: Jessica Dorgo
Sent: September 25, 2019 9:18 AM
To: peter.dundas@peelregion.ca
Subject: Region of Peel - Coleraine Drive Grade Separation Class EA
Attachments: B738_Peel Coleraine GS EA_Public Notice_e04.pdf; Region of Peel Ambulance Service Letter.pdf

Good Morning,

Please find the attached Notice of Public Information Centre No. 1 for the Region of Peel Coleraine Drive Grade Separation Class Environmental Assessment.

Thank you,

JESSICA DORGO, EIT
Transportation

T 289-288-0287 ext. 6819 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



September 25, 2019

Mr. Peter Dundas
Director-Paramedic Services
Region of Peel Ambulance Service
5299 Maingate Dr
Mississauga, ON L4W 1G6

**RE: Coleraine Drive Grade Separation
Municipal Class Environmental Assessment Study
Notice of Public Information Centre No. 1**

The Regional Municipality of Peel has initiated a Municipal Class Environmental Assessment (Class EA) to consider a range of options for long term traffic improvements and a grade separation of the CP Rail line on Coleraine Drive south of Old Ellwood Drive in the Town of Caledon. The purpose of this letter is to invite a representative of your agency to attend the first Public Information Centre (PIC).

The study is being conducted in compliance with Schedule 'C' of the Municipal Class EA, which is approved under the Ontario Environmental Assessment Act. The first of two PICs will be held to review the study including information on alternative planning solutions, existing conditions, evaluation criteria, and the preliminary preferred solution. The PIC will be an opportunity to review project information and ask questions or discuss comments with the project team. Further information and details regarding the PIC are provided in the enclosed Notice of Public Information Centre No. 1.

We look forward to seeing you at the PIC.

Sincerely,

Public Works

10 Peel Centre Dr., Suite B, Brampton, ON L6T 4B9
Tel: 905-791-7800 www.peelregion.ca

Kate Barclay

From: David Hiett
Sent: September 28, 2021 11:45 AM
To: Arash Olia; Jamieson, Cheryl; Parkes, Brian; Dave Pelayo; Garant, Mike (OPP); Anthony Staniscia; Cooper, Andrew
Cc: Mahmood, Tareq; Stephen Keen
Subject: RE: Emergency Services Access - Coleraine Grade Separation Improvements
Attachments: B738 Coleraine Drive EA - EMS Meeting Sept28-21 Minutes e01.pdf

Hi All,

Please see attached the minutes of this morning's meeting.

As mentioned at the meeting, please review and provide any comments, or confirmation of these minutes, as this input will be used as part of our EA study and in the evaluation of the final road configuration.

Also, as it is part of an EA, the input will also ultimately be public record.

Many Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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-----Original Appointment-----

From: Mahmood, Tareq <tareq.mahmood@peelregion.ca>
Sent: Friday, September 24, 2021 10:34 AM
To: Mahmood, Tareq; Arash Olia; Stephen Keen; David Hiett; Jamieson, Cheryl; Parkes, Brian; Dave Pelayo; Garant, Mike (OPP); Anthony Staniscia
Cc: Cooper, Andrew
Subject: Emergency Services Access - Coleraine Grade Separation Improvements
When: Tuesday, September 28, 2021 9:00 AM-9:30 AM (UTC-05:00) Eastern Time (US & Canada).
Where: Microsoft Teams Meeting

EXTERNAL EMAIL

Hello,

Thanks for confirming your availability. Tuesday morning, September 28th, works best for most of the invitees.

Due to the proposed Coleraine Grade Separation improvements, the existing intersections at Old EllWood Drive and Ellwood Drive will be closed. The purpose of the meeting is to seek your expert opinions and a better understanding of the requirements for proper emergency services responses, ingress and egress to this specific location.

Thanks,

Tareq Mahmood

PM, Infrastructure Programming
and Studies, Transportation
Cell: 905-872-6475

Microsoft Teams meeting

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Kate Barclay

From: Dave Pelayo <Dave.Pelayo@caledon.ca>
Sent: September 29, 2021 8:05 AM
To: David Hiatt
Cc: Anthony Staniscia
Subject: RE: Emergency Services Access - Coleraine Grade Separation Improvements

EXTERNAL EMAIL

Good Morning David,

Thank you for preparing the minutes, no additional comments from Fire & Emergency Services.

Dave Pelayo

Chief Fire Prevention Officer
Fire Prevention Division
Fire & Emergency Services

T. 905.584.2272, x.4340
Email dave.pelayo@caledon.ca

Town of Caledon | www.caledon.ca
www.visitcaledon.ca | Follow us @YourCaledon

From: David Hiatt <David.Hiatt@cima.ca>
Sent: Tuesday, September 28, 2021 11:45 AM
To: Arash Olia <Arash.Olia@caledon.ca>; Jamieson, Cheryl <cheryl.jamieson@peelregion.ca>; Parkes, Brian <brian.parkes@peelregion.ca>; Dave Pelayo <Dave.Pelayo@caledon.ca>; Mike Garant <mike.garant@opp.ca>; Anthony Staniscia <anthony.staniscia@caledon.ca>; Cooper, Andrew <andrew.cooper@peelregion.ca>
Cc: Mahmood, Tareq <tareq.mahmood@peelregion.ca>; Stephen Keen <Stephen.Keen@cima.ca>
Subject: RE: Emergency Services Access - Coleraine Grade Separation Improvements

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Hi All,

Please see attached the minutes of this morning's meeting.

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Many Thanks,

DAVID HIETT, M.Eng., P.Eng.
Project Manager / Transportation

T 289-288-0287 ext. 6841 M 905-242-2286 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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-----Original Appointment-----

From: Mahmood, Tareq <tareq.mahmood@peelregion.ca>

Sent: Friday, September 24, 2021 10:34 AM

To: Mahmood, Tareq; Arash Olia; Stephen Keen; David Hiatt; Jamieson, Cheryl; Parkes, Brian; Dave Pelayo; Garant, Mike (OPP); Anthony Staniscia

Cc: Cooper, Andrew

Subject: Emergency Services Access - Coleraine Grade Separation Improvements

When: Tuesday, September 28, 2021 9:00 AM-9:30 AM (UTC-05:00) Eastern Time (US & Canada).

Where: Microsoft Teams Meeting

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Hello,

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Thanks,

Tareq Mahmood

PM, Infrastructure Programming
and Studies, Transportation

Cell: 905-872-6475

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Meeting Minutes

- Meeting:** Emergency Services – Discussion on Ellwood Drive West and Old Ellwood Drive modifications and impacts to accessibility
- Project:** B738 Coleraine Drive CP Grade Separation EA Study
- Date and Time:** Tuesday September 28, 2021
- Location:** Virtual Meeting (Teams Meeting)
- Attendees:** Tareq Mahmood, Project Manager, Peel Region
Stephen Keen, Consultant Project Manager, CIMA+
David Hiatt, CIMA+
Arash Olia, Town of Caledon
Andrew Cooper, Peel Region
Cheryl Jaimeson, Peel Region
Mike Grant, OPP
Dave Pelayo, Caledon Fire
Brian Parkes, Paramedics
- Regrets:** Anthony Staniscia, Caledon Fire

Note: Please advise author immediately of any errors or omissions.

Discussion Topics		Action By
1. Introductions		
1.1	Introductions were made	
2. Study Overview and Purpose of Meeting		
2.1	CIMA provided an overview of the study: <ul style="list-style-type: none">Environmental Assessment Study is considering grade separation options (Road Under Rail vs Road Over Rail) for the Coleraine Drive crossing of CP Rail, to the west of downtown Bolton.	

	Discussion Topics	Action By
	<ul style="list-style-type: none"> • Regardless of option, the grade separation will prohibit a future connection between Coleraine Drive and both Old Ellwood and Ellwood Drive West - both roads currently intersect Coleraine Drive, and these intersections will be closed. • In the future, Old Ellwood Drive will be realigned to connect with Ellwood Drive West. • There are two options for Ellwood Drive West at Coleraine Drive: <ul style="list-style-type: none"> ○ A direct road connection provided to Grapevine Road, under Coleraine Drive ○ Cul-de-sac at Coleraine Drive, and only an Active Transportation (AT) connection is provided to Grapevine Road. • The purpose of this meeting was to facilitate comments on the potential local road configurations and how they may impact emergency services access and response times. In particular, the differences between a cul-de-sac on Ellwood Drive West, vs a connection through to Grapevine Road. 	
<h3>3. Discussion</h3>		
<p>3.1</p>	<p>Dave Pelayo, Caledon Fire:</p> <ul style="list-style-type: none"> • Access from Coleraine Drive (i.e., retaining existing intersections) would be the best, however as this would not be feasible, the connection of Ellwood Drive West through to Grapevine Road is preferred. • The connection will facilitate access from both the east and west and provide additional access options to the subdivision. • The best solution would be the one that provides the most options to access the area, and thus provide the better service to residents. • Nearest fire station (#302) is located at Hwy 50 / Bolton Heights Rd. The next is in Caledon East (#303). 	
<p>3.2</p>	<p>Mike Grant, OPP:</p> <ul style="list-style-type: none"> • OPP are slightly more flexible than fire and paramedics, and thus have no objections with the proposed improvements nor a strong preference between the options. 	
<p>3.3</p>	<p>Brian Parkes, Paramedics:</p> <ul style="list-style-type: none"> • Changes to the local road connections will have an impact on response times. Unlike Fire, the Paramedics services are mostly mobile and without a permanent station, and so could be 	



	Discussion Topics	Action By
	<p>approaching the area from any direction. As such, the direct connection through to Grapevine Road is preferred as it has the greatest potential to reduce response times.</p> <ul style="list-style-type: none"> • Agreed that the best solution would be the one that provides the most options to access the area. 	
<p>3.4</p>	<p>Arash Olia, Town of Caledon</p> <ul style="list-style-type: none"> • Inquired about other connection options that could provide a middle ground (see below item) • Noted that the residents of Grapevine Road will likely dislike the direct connection with Ellwood Drive. 	
<p>3.5</p>	<p>Other Options (i.e., approaches that include gates / emergency service only accesses / 'knock down' mechanisms)</p> <ul style="list-style-type: none"> • Both Fire and Paramedics advised against the use of gates. Paramedics have a fleet of 165 vehicles, and it would be arduous to outfit them all with the required mechanism to open the gate. • OPP are more adaptable to options such as gates, mostly as they would just break the lock. • 'Knock down' mechanisms or gates also have disadvantages of requiring ongoing maintenance / snow removal and can be susceptible to vandalism. • The AT connection option could also be sized to accommodate EMS vehicles for emergency situations, and signage could be used as a deterrent for public traffic, however it would be difficult to enforce. 	
<p>4. Schedule and Next Steps</p>		
<p>4.1</p>	<p>The Region will be presenting these local road connection options to the public at PIC #2, anticipated to be held later this year.</p>	
<p>4.2</p>	<p>The comments and input from the emergency services, in addition to feedback received from the public at the PIC #2, will be used in the final evaluation of the configuration options.</p>	

End of Minutes

Stephen Keen, Project Manager, CIMA+



Kate Barclay

From: Kate Barclay
Sent: December 16, 2021 9:53 AM
Cc: tareq.mahmood@peelregion.ca; Stephen Keen; David Hiatt
Subject: PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good morning,

Please find attached the notice of Public Information Centre (PIC) #2 for the Coleraine Drive and CP Rail Grade Separation EA Study, currently being undertaken by the Region of Peel.

As described in the attached notice, **PIC #2 will be live on the Region's website starting December 16, 2021 and we request all comments and feedback be provided by January 21, 2022.**

The PIC #2 website will provide opportunities to provide comments and feedback on the material presented.

If you have any questions or require further information, please contact the project team members listed on the notice.

Kind Regards,

KATE BARCLAY
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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Kate Barclay

From: Kate Barclay
Sent: January 13, 2022 4:13 PM
Cc: Mahmood, Tareq; Stephen Keen; David Hiatt
Subject: Have Your Say - PIC-2, Coleraine Drive Grade Separation Class EA (Region of Peel)
Attachments: Coleraine Drive Grade Separation EA_Notice_PIC #2.pdf

Good afternoon,

This is a reminder that Public Information Centre (PIC #2) for the Coleraine Drive and CP Rail Grade Separation Class EA study is currently live on the [Region's website](#).

The virtual PIC presents information about the evaluation of alternatives and preferred plans for Coleraine Drive and is anticipated to be the final PIC for this study. **We kindly request you visit the PIC portal on the Region's website to review the project details and submit your feedback by next Friday, January 21, 2022.** Input and comments may be provided via the PIC portal or by contacting the project team directly by phone or email.

If you have any questions or would like further information about this study, please don't hesitate to contact the project team members listed on the attached notice.

Kind Regards,

KATE BARCLAY, EIT
EIT / Transportation

T 289-288-0287 ext. 6862 F 289-288-0285
400-3027 Harvester Road, Burlington, ON L7N 3G7 CANADA



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